

# Town Center Specific Plan

City of Santa Clarita, California

July 2024



## ACKNOWLEDGEMENTS

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An aerial photograph of a university campus. In the center, a road leads towards a large, classical-style building with a prominent portico. The campus is filled with green trees and several cars are parked in a lot. On the left, a tall building has a sign that reads "QUBATULHAQIA". The background shows rolling hills under a clear sky. The entire image has a light blue and green color overlay.

# 1

## Introduction

## 1. SPECIFIC PLAN AREA

The Town Center Specific Plan (TCSP) is located in the City of Santa Clarita (City), within the community of Valencia. The Specific Plan Area (SPA) is approximately one mile east of the Interstate-5 freeway, and generally bounded by Magic Mountain Parkway to the north, Valencia Boulevard to the south and east, and McBean Parkway to the west, creating a roughly triangular project site (Figure 1-1).

### SUBAREAS

The SPA includes approximately 111 acres of largely urbanized land and is divided into four distinct subareas (Figure 1-2).

#### VALENCIA TOWN CENTER

Measuring 68 acres in size, the Valencia Town Center Subarea is the largest subarea within the SPA. The subarea is primarily characterized by the existing Valencia Town Center Mall, The Patios lifestyle center, and the eastern portion of Town Center Drive. The total complex includes approximately one-million square feet of retail and commercial space. Surrounding these uses is a large surface parking lot and a single parking structure, which is internally accessed by vehicular traffic via a two-lane ring road.

#### TOWN CENTER EAST

Separated from the Valencia Town Center Subarea by Citrus Street, a two-lane public street that provides a north-south connection between Valencia Boulevard and Magic Mountain Parkway, the Town Center East Subarea is 23 acres in size and primarily built-out with existing structures and surface parking. The subarea contains approximately 245,000 square feet of public services, office space, personal services, and retail space. Primary uses include the Valencia Branch of the Santa Clarita Public Library, two private office buildings, and a small commercial/retail center. Other uses include the former Santa Clarita Station of the Los Angeles County Sheriff's Department,

Los Angeles County Fire Department Station 126, Santa Clarita Superior Court, and offices of the Los Angeles County Planning Division, Building and Safety Division, and Fire Department.

#### TOWN CENTER DRIVE

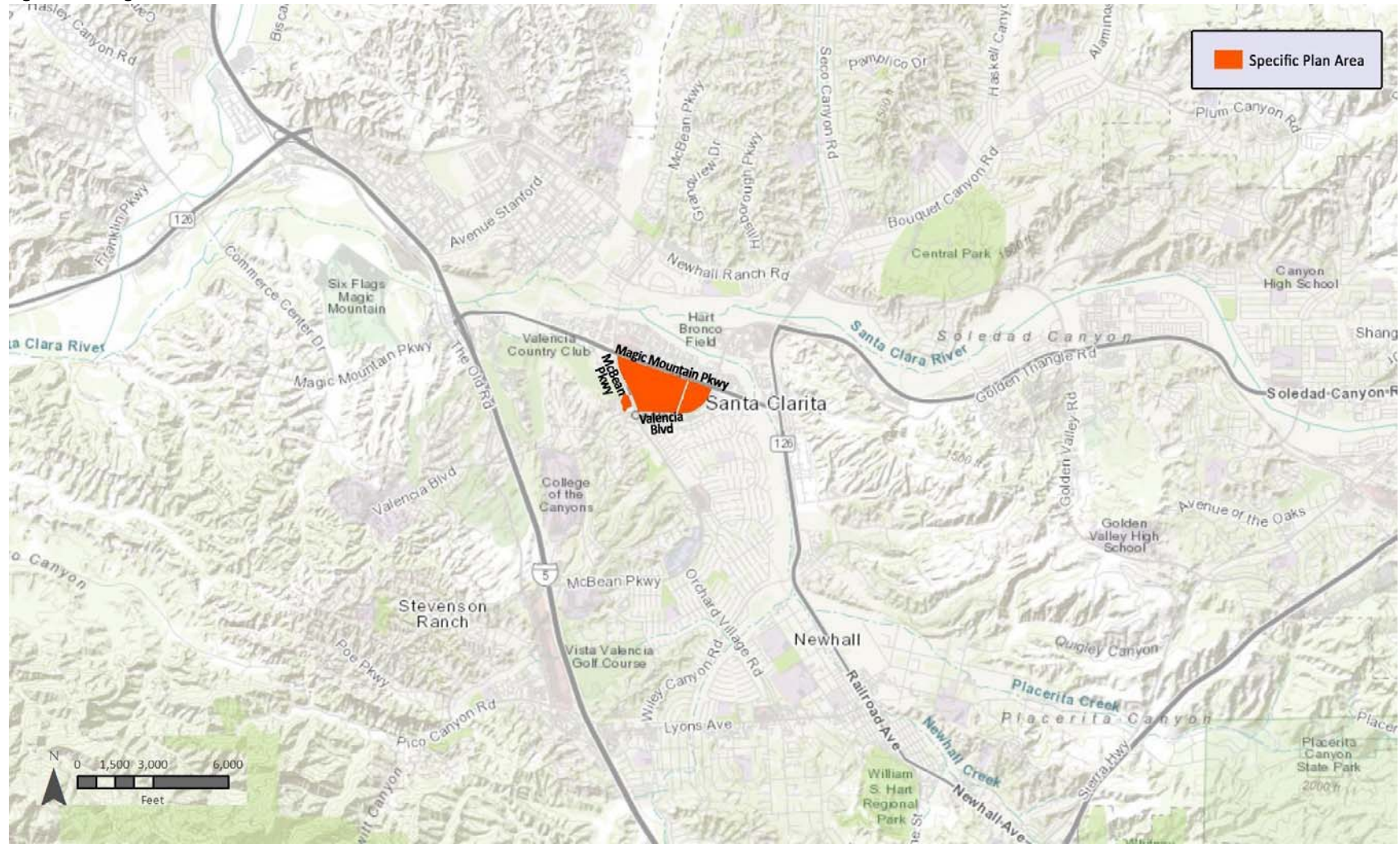
The Town Center Drive Subarea represents a transition from the retail mall and lifestyle uses associated with the Valencia Town Center Subarea into a total of 460,000 square feet of office space, entertainment, dining, personal services, and specialty retail uses that front on Town Center Drive, a two-lane interior street with angled surface parking. Prominent land uses include several office buildings measuring between four and six stories in height with ground-floor retail, restaurants, and services, a twelve-theater Regal Cinema, several one and two-story retail/office buildings, and two multi-level shared parking structures. The subarea is primarily built-out and is approximately 16 acres in size.

#### MCBEAN AND VALENCIA

At 4 acres, this is the smallest of the subareas but occupies a prominent location on the northwest corner of Valencia Boulevard and McBean Parkway; two major thoroughfares within the community of Valencia. The majority of the property is vacant, but in 2016 the property was entitled for the construction of a five-story hotel and free-standing restaurant. Initial grading has occurred, but at the time this document was published, no construction had commenced. Notably, the subarea is adjacent to the City's McBean Regional Transit Center.

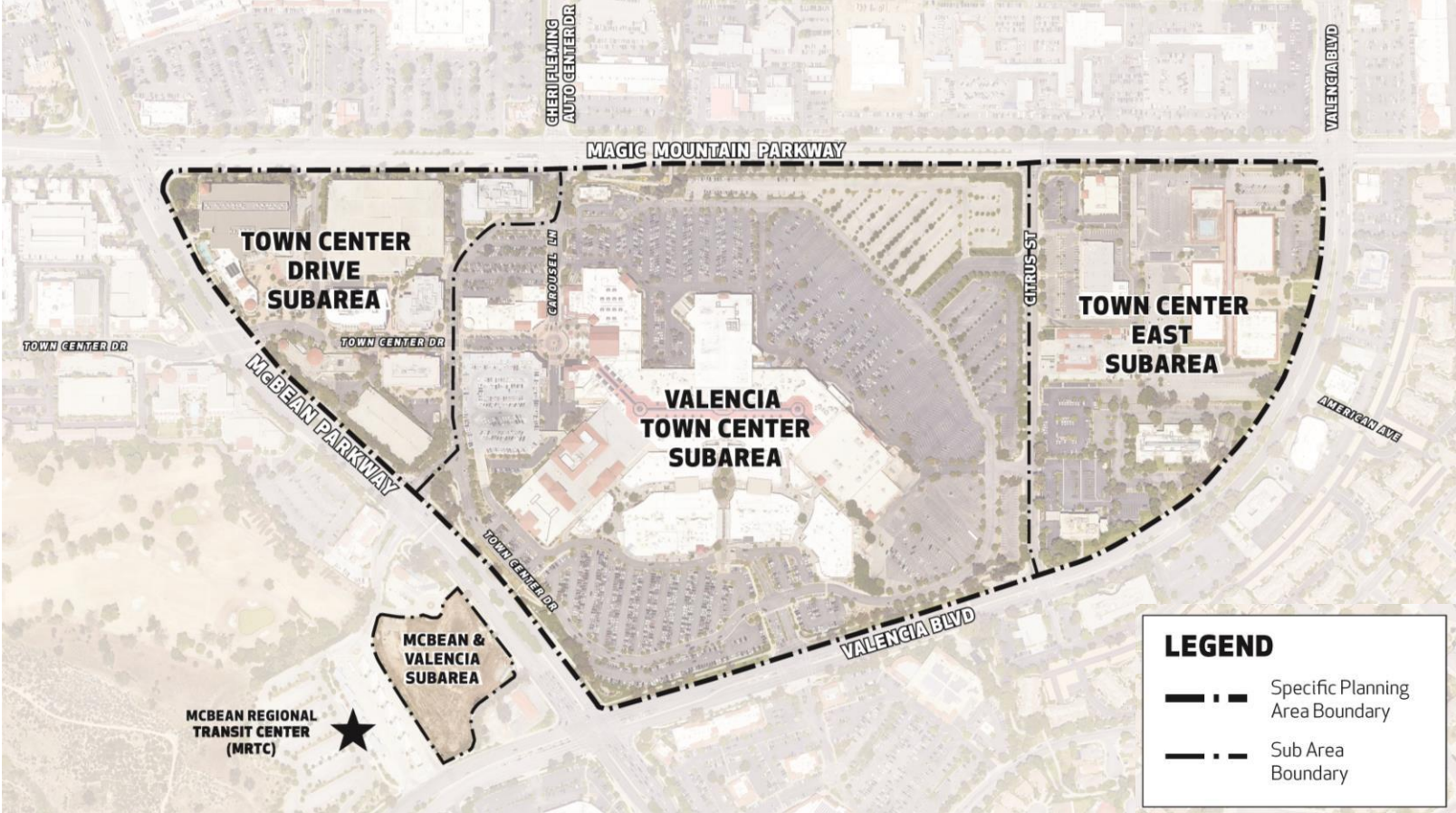


Figure 1-1. Regional Location



Source: City of Santa Clarita, Michael Baker International, ESRI

Figure 1-2. Santa Clarita Specific Plan Area and Subareas



Source: City of Santa Clarita, Michael Baker International, ESRI



## ZONING

The underlying General Plan Land Use and Zoning designation for the entirety of the SPA is Community Commercial (CR) (Figure 1-3). The CR zone contemplates the highest commercial and residential development in the City. All future development within the SPA will be consistent with specifications of the CR zone regarding unit density and Floor Area Ratio (FAR).

The SPA is also surrounded by the CR zone with land uses including major retail centers, automobile dealerships, high-density residential developments, professional and medical office buildings, and a fuel sales use. Land uses beyond the immediate boundaries of the SPA include low to moderate density housing, and a private golf course. The Santa Clara River is located approximately ¼ mile north of the SPA.

The entirety of the SPA is also located within the Jobs Creation Overlay Zone (JCOZ). The JCOZ incentivizes high-quality employment projects by providing streamlined permitting and a variety of design incentives.

Figure 1-3. Zoning and Land Use Plan



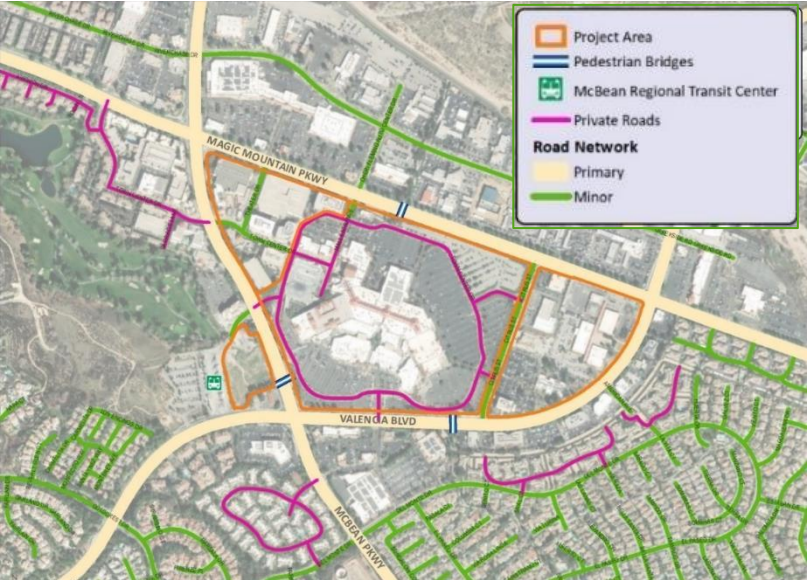
Location: Valencia Town Center



### MOBILITY

As indicated above, the SPA is bordered by three major thoroughfares, including Magic Mountain Parkway, Valencia Boulevard, and McBean Parkway, and is also bisected by Citrus Street (Figure 1-4). Access into the SPA can be taken from several points along these streets, some of which are signalized while others are not. Importantly, the McBean Regional Transit Center is located directly across from the southwest corner of the SPA. Here, 18 different bus routes provide service to points around the Santa Clarita Valley as well as service to locations outside the Santa Clarita Valley. The SPA can be also be accessed by three pedestrian bridges, and, in addition to sidewalks, a paseo system provides access to the SPA from the north and south.

Figure 1-4. Existing Mobility Network



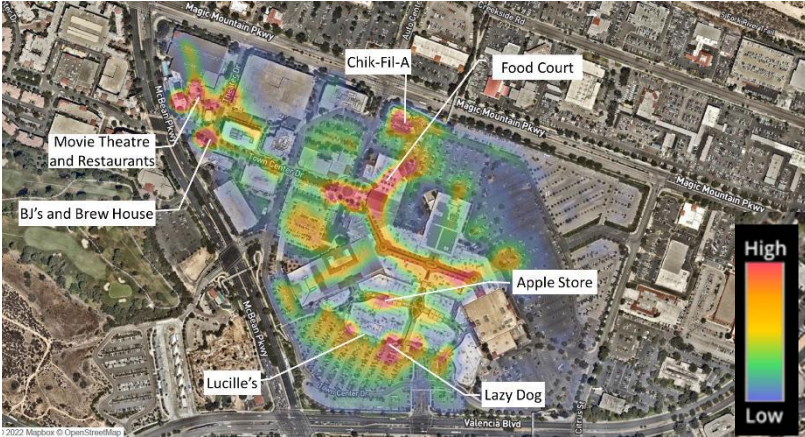
Source: City of Santa Clarita, Michael Baker International, ESRI

### DEMOGRAPHIC & ECONOMIC SNAPSHOT

An estimated 121,000 people live within a 10-minute drive from Valencia Town Center across approximately 42,000 households.

In 2023, Valencia Town Center ranked in the top 15 percent of Super Regional Malls in the United States, attracting an estimated 9.4 million visitors annually. Most visitors at Valencia Town Center stay for over an hour. Figure 1-5 demonstrates locations within the Valencia Town Center and Town Center Drive Subareas that receive the highest amount of visitor traffic.

Figure 1-5. Visitor Activity Heat Map



Source: DLR Group, Michael Baker International, Placer AI



## 2. PURPOSE

The purpose of the TCSP is to provide a long-range vision for development within the SPA by establishing a timeless and flexible framework of standards and guidelines to create a balance of land uses, incorporate great placemaking, and continue to establish the SPA as a regional destination where the community can live, work, and play.

*“The TCSP builds upon the General Plan’s vision and guiding principles.”*



Location: Valencia Town Center

## 3. VISION & GOALS

### VISION STATEMENT

“The Santa Clarita Town Center is a lively hub that embodies a spirit of community, inviting people from all walks of life to live, work, shop, play, and socialize. It features a balance of retail, office, restaurants, recreational, and residential spaces, seamlessly integrated with a pedestrian and bike-friendly setting. The Town Center features an efficient multimodal transportation system, providing easy connectivity to regional and local trail systems. The Town Center provides a community identity and is a vibrant place for people to gather, socialize, and celebrate in the City of Santa Clarita.”

### GOALS

The Goals of the TCSP are as follows:

#### **DEFINE A VISION FOR DEVELOPMENT**

Create a timeless, long-term vision for Development within the SPA that is consistent with the City’s General Plan and Housing Element.

#### **PROVIDE A BALANCE OF LAND USES**

Create a balanced mix of retail, dining, entertainment, hospitality, commercial, and residential opportunities to further establish the SPA as a premier regional destination.

#### **CREATE GREAT PLACES**

Create a distinct sense of place that is unique to the SPA via creative use of urban design, iconic architecture, signature vistas, a vibrant public realm, and gathering spaces.

#### **ESTABLISH A FLEXIBLE FRAMEWORK**

Create a flexible framework for future development that is practical, buildable, and fosters the potential for numerous development possibilities.

## 4. PLAN STRUCTURE & AUTHORITY

### PLAN ORGANIZATION

The TCSP is organized into three chapters:

#### CHAPTER 1: INTRODUCTION

Chapter 1 provides a brief introduction to the TCSP and establishes the vision and goals for the SPA.

#### CHAPTER 2: DEVELOPMENT FRAMEWORK & STANDARDS

Chapter 2 contains 16 framework elements and associated standards to guide future development within the SPA.

#### CHAPTER 3: IMPLEMENTATION

Chapter 3 provides implementation mechanisms for the specific plan in addition to the Development Framework & Standards defined in Chapter 2.

### PLAN AUTHORITY

The TCSP is established through the authority granted to the City of Santa Clarita by the California Government Code, Title 7, Division 1, Chapter 3, Article 8, Sections 65450 through 65457. The Government Code authorizes cities to adopt specific plans either by resolution or by ordinance as regulation.

All standards and procedures of the Unified Development Code (UDC) are applicable and remain in effect except where superseded by the TCSP.

### RELATIONSHIP TO OTHER DOCUMENTS

#### GENERAL PLAN

Like other specific plans within the City, the TCSP serves to implement goals, objectives, and policies of the City's General Plan.

The TCSP builds upon the General Plan's vision and guiding principles. It seeks to promote a diversity of land uses that support the needs of residents, continues to foster the development of a strong local economy, creates great public spaces, and focuses development in a manner and location that minimizes impacts on the environment. The TCSP Development Framework & Standards encourage projects that are contextually sensitive. Because the framework and development standards included in the TCSP are consistent with the specifications of both the underlying land use and zoning designations of the General Plan and UDC, the TCSP is consistent with the General Plan.

Further, the TCSP is consistent with the following General Plan goals, policies, and objectives:

- **Policy LU 1.1.5:** Increase infill development and re-use of underutilized sites within, and adjacent to, developed urban areas to achieve maximum benefit from existing infrastructure and minimize loss of open space, through re-designation of vacant sites for higher density and mixed use, where appropriate.
- **Policy LU 1.2.2:** In Valencia, promote business development, job creation, and expansion of regional commercial, civic, cultural, and entertainment uses, to create a vibrant Town Center serving as a community focal point for the entire Santa Clarita Valley.
- **Policy LU 4.1.1:** Promote expansion and enhancement of the Valencia Town Center to provide a focal point for cultural, civic, educational, and shopping activities serving the entire Santa Clarita Valley.



- **Policy LU 4.1.3:** Direct business creation and expansion for larger companies within and adjacent to existing and planned business centers and major transportation corridors.
- **Policy LU 4.2.3:** Encourage businesses to locate in all appropriate areas of the community to encourage job creation in closer proximity to workforce housing.
- **Policy C 1.2.1:** Develop coordinated plans for land use, circulation, and transit to promote transit-oriented development that concentrates higher density housing, employment, and commercial areas in proximity to transit corridors.
- **Policy CO 1.5.5:** Promote concentration of urban uses within the center of the Santa Clarita Valley through incentives for infill development and rebuilding, in order to limit impacts to open space, habitats, watersheds, hillsides, and other components of the Valley's natural ecosystems.

### 6TH CYCLE HOUSING ELEMENT

The 6<sup>th</sup> Cycle Housing Element outlines the City's housing goals for 2021 through 2029. State law requires regular updates to ensure compliance with changes in State housing laws and demonstrate the ability to meet future housing needs. A major component of this update is the 6<sup>th</sup> Cycle Regional Housing Needs Assessment (RHNA), in which the State estimates each region's housing needs for all income groups.

The City's share of the RHNA is 10,031 dwelling units for the 6<sup>th</sup> Cycle Housing Element. Of this allocation, 5,131 units are in the low- to very low-income category, 1,672 units in the moderate-income category, and 3,228 units in the above moderate-income category. The Housing Element has identified 27 sites in the City, including sites within the TCSP, that could accommodate lower- and moderate-income dwelling units.

**Figure 1-6. Housing Element Sites**



Source: City of Santa Clarita, Michael Baker International, ESRI

### COMMUNITY CHARACTER & DESIGN GUIDELINES

The City's Community Character & Design Guidelines (CC&DG) supplement the design-related goals and policies of the General Plan and UDC by offering guidance for architectural standards for new commercial and residential projects. The document identifies a variety of styles that capture the unique character of each major community within the City. Similarly, Chapter 2 of the TCSP identifies five architectural styles for use within the SPA. These styles pay homage to existing architectural styles within the SPA, continue the vision established by the CC&DG for the community of Valencia, and provide a blueprint for a unique architectural character within the SPA going forward.

### NON-MOTORIZED TRANSPORTATION PLAN

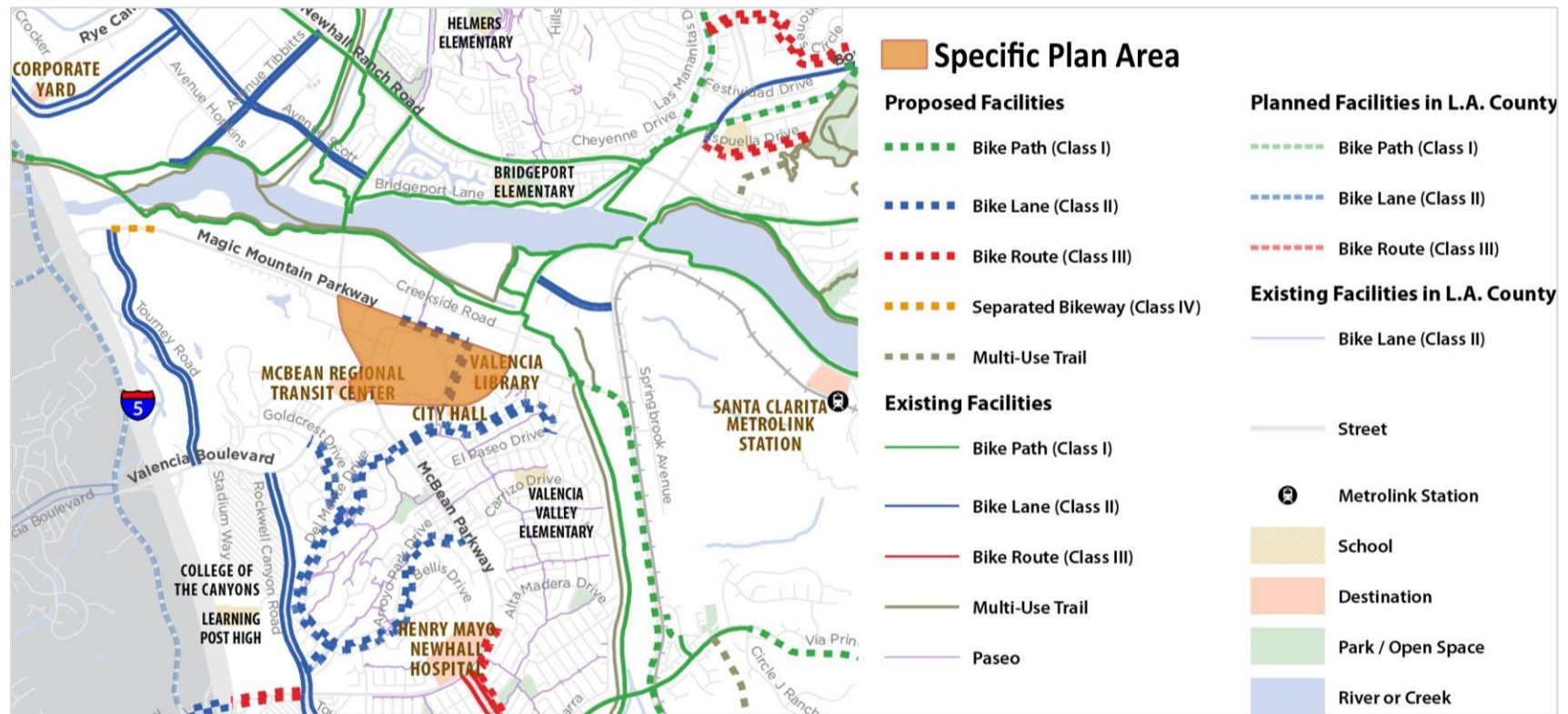
The Non-Motorized Transportation Plan (NMTP) was adopted in September 2020. It designates an ambitious 237-mile active transportation system and introduces policies, programs, projects, and other recommendations to create an environment that increases, improves, and enhances active transportation in the City and makes walking and biking a safe, healthy, and enjoyable means of transportation and recreation. Among the elements of the NMTP



are several innovations in active transportation planning for Santa Clarita, including recommendations for Bicycle Boulevards and Class IV- Cycle Tracks (Separated/Protected Bikeways). The NMTP includes an implementation strategy that details the sequencing and priorities for the selection and installation of new pedestrian and bicycle facilities. The NMTP recommends the installation of Class II bike lanes on Citrus Street through the SPA and on Magic Mountain Parkway

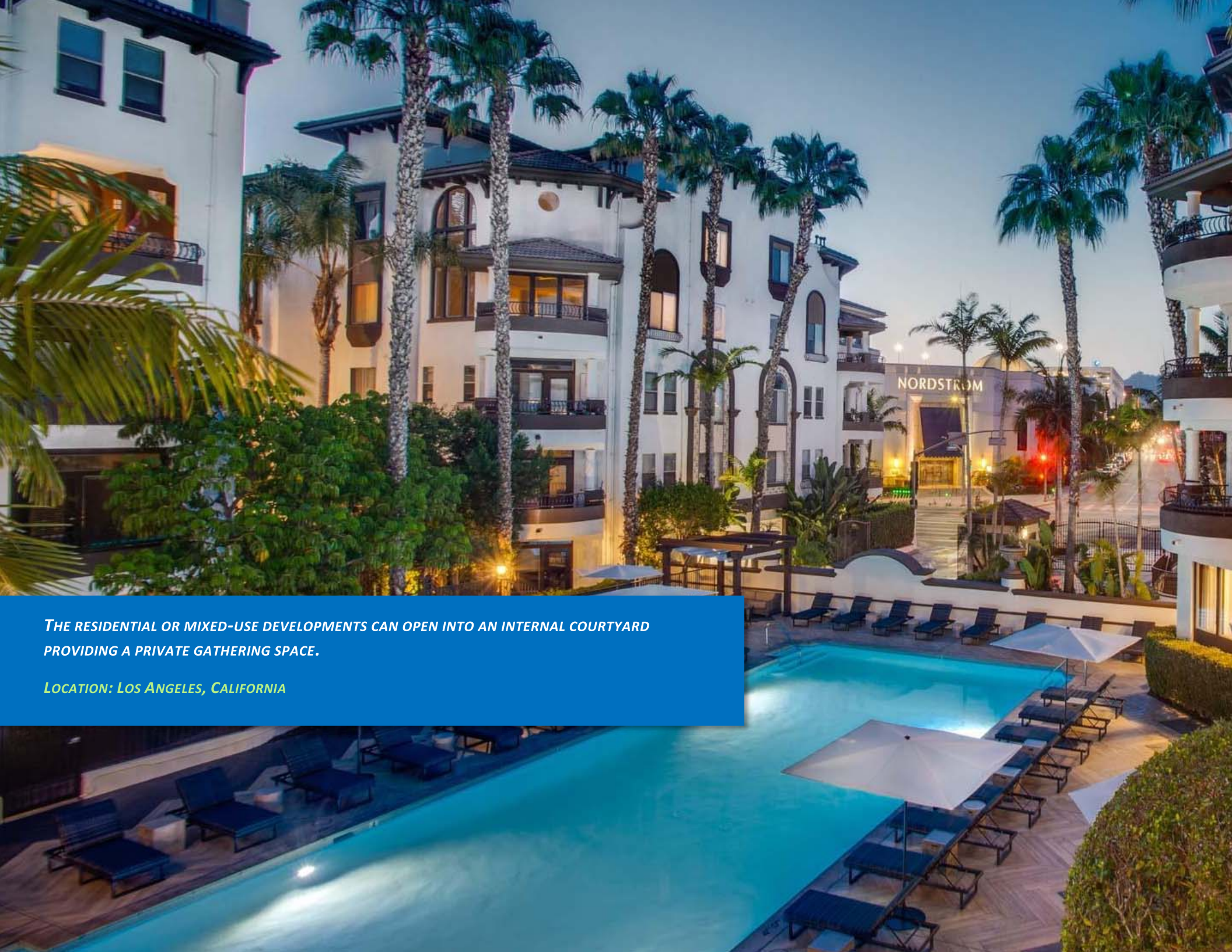
along the northern boundary of the SPA (Refer to Figure 1-7: Non-Motorized Transportation Plan). The bike facility development will provide connectivity throughout the SPA and neighboring areas south to the regional South Fork Trail via the existing paseo connection. In addition, the NMTP lays out policies for bicycle parking and supporting infrastructure.

Figure 1-7. Non-Motorized Transportation Plan



Source: City of Santa Clarita's Non-Motorized Transportation Plan





*THE RESIDENTIAL OR MIXED-USE DEVELOPMENTS CAN OPEN INTO AN INTERNAL COURTYARD PROVIDING A PRIVATE GATHERING SPACE.*

*LOCATION: LOS ANGELES, CALIFORNIA*





# 2

## **Development Framework & Standards**

# 1. PURPOSE & INTENT

The Development Framework & Standards contain the building blocks, details, and examples for the contents of the Town Center Specific Plan (TCSP). The framework and standards identified in this chapter are intended to establish the components, expectations, and general requirements for all future development plans prepared in accordance with the TCSP. The TCSP encourages a balanced mix of residential and non-residential uses throughout the Specific Plan Area (SPA) and emphasizes the importance of proper placemaking to create a sense of space for the community. Each framework element further articulates a vision for the SPA in greater detail and provides a comprehensive, flexible mechanism for its implementation. Development standards are included with the framework elements and provide specific requirements in areas where additional detail and guidance is needed. Taken in concert, the framework elements and development standards provide the blueprint to achieve the vision identified by the TCSP.

Unlike other specific plans, the TCSP does not include a static site plan. Rather, it establishes a flexible framework to guide overall development. Several potential development scenarios were analyzed by the Environmental Impact Report (EIR) and prepared in conjunction with the TCSP. These development scenarios are included in the Development Plan section of this chapter. It is important to note that these scenarios do not represent the specific or precise ultimate buildout of the SPA, but rather demonstrate concepts that reflect the framework elements and standards established by the TCSP.

Entitlement proposals submitted within the SPA that are consistent with the Framework Elements and comply with the Development Standards will enjoy the incentives provided by the TCSP relative to the underlying zone. These include streamlined permitting, enhanced project design features, and streamlined review under the California Environmental Quality Act (CEQA).



Location: Old Town, Santa Clarita, California

## 2. APPLICABILITY

Permitted uses included in **Table 2-2**, as well as the Framework & Development Standards contained in this plan, provide specific requirements and guidance for development within the SPA. The TCSP is intended to supplement, and in some cases replace, the requirements of the City's Unified Development Code (UDC), Titles 16 and 17 of the Santa Clarita Municipal Code. The provisions of this specific plan supersede regulations of the UDC on the same topic (e.g., the requirements and recommendations for numbers of parking spaces in Element PK1 of this chapter supersede the parking requirements of the UDC), but otherwise all applicable requirements of the UDC that are not covered by this specific plan apply to development within the SPA. While this specific plan supersedes certain sections of the UDC, where not expressly superseded, the reviewing provision of other sections of the UDC remain in effect, including but not limited to those sections pertaining to Variances, Adjustments, Conditional Use Permits, Minor Use Permits, Home Occupation Permits, etc. If a conflict occurs between a requirement of this specific plan and the UDC, the provisions of the TCSP shall control.

The Framework Elements and Standards apply to all new construction within the SPA. Development and land use(s) that were lawfully established, and exist within the SPA as of the effective date of this specific plan are affected by this specific plan as follows:

- Existing development and land use(s) that comply with all applicable requirements of this specific plan shall continue to

operate, and may be altered or replaced, only in compliance with the TCSP.

- Existing and previously-permitted development that does not comply with the requirements of this specific plan may continue to operate and may be sold or otherwise transferred in compliance with the City's regulations for nonconformities found in UDC 17.05 (Legal Nonconforming Uses, Lots and Structures).

For renovations of existing buildings, the portion of the building or structure that is renovated must comply with the development standards established within the TCSP. When a renovation includes a cumulative expansion of greater than 50 percent of approved building area, the entire building and project site is required to comply with the TCSP.

The City shall interpret, administer, and enforce the provisions of this chapter. The provisions of this chapter shall be interpreted in a manner that best fulfills the spirit and intent of the TCSP. The Director of Community Development may allow the submission of a Conditional Use Permit, and all other required entitlement applications, for a proposed project that meets the spirit and intent of the TCSP, but otherwise requires a modification to any requirement, framework, or development standard within the TCSP, with the exception of an increased allowance for Floor Area Ratio (FAR) and residential unit density.



### 3. SUBAREA CONTEXT

The TCSP is divided into four subareas. A summary of each subarea is provided below, along with improvements that could occur as new development and land use(s) are proposed within each subarea.

#### VALENCIA TOWN CENTER

Measuring 68 acres in size, the Valencia Town Center Subarea is the largest subarea within the SPA. The subarea is primarily characterized by the existing Valencia Town Center Mall, The Patios lifestyle center, and the eastern portion of Town Center Drive. The total complex includes approximately one-million square feet of retail and commercial space. Over half of the acreage in the subarea is covered by surface parking and, as such, the majority of new construction within the SPA is contemplated here.

#### TOWN CENTER DRIVE

The entirety of this 16-acre subarea has been built out with over 450,000 square feet of office, retail, dining, and entertainment space. Although redevelopment is possible here, the TCSP contemplates an emphasis on continuing to attract the types of tenants and land uses identified as desirable to the City Council and the community within existing commercial space.

#### TOWN CENTER EAST

Like the Town Center Drive Subarea, the majority of the 23 acres within the Town Center East Subarea are built out. However, the TCSP contemplates a mixture of redevelopment within the subarea as well as a focus on the attraction of tenants and land uses within existing commercial space on-site.

#### MCBEAN & VALENCIA

At four acres, this is the smallest of the subareas, but occupies a prominent location on the northwest corner of Valencia Boulevard and McBean Parkway; two major thoroughfares within the community of Valencia. In 2016, the City Planning Commission approved a 134-room hotel and 4,000 square-foot restaurant within this subarea. The TCSP will not impact the existing approval, but does contemplate the potential for the review of alternative entitlement proposals in the future that are consistent with the TCSP.



## 4. DEVELOPMENT PLAN

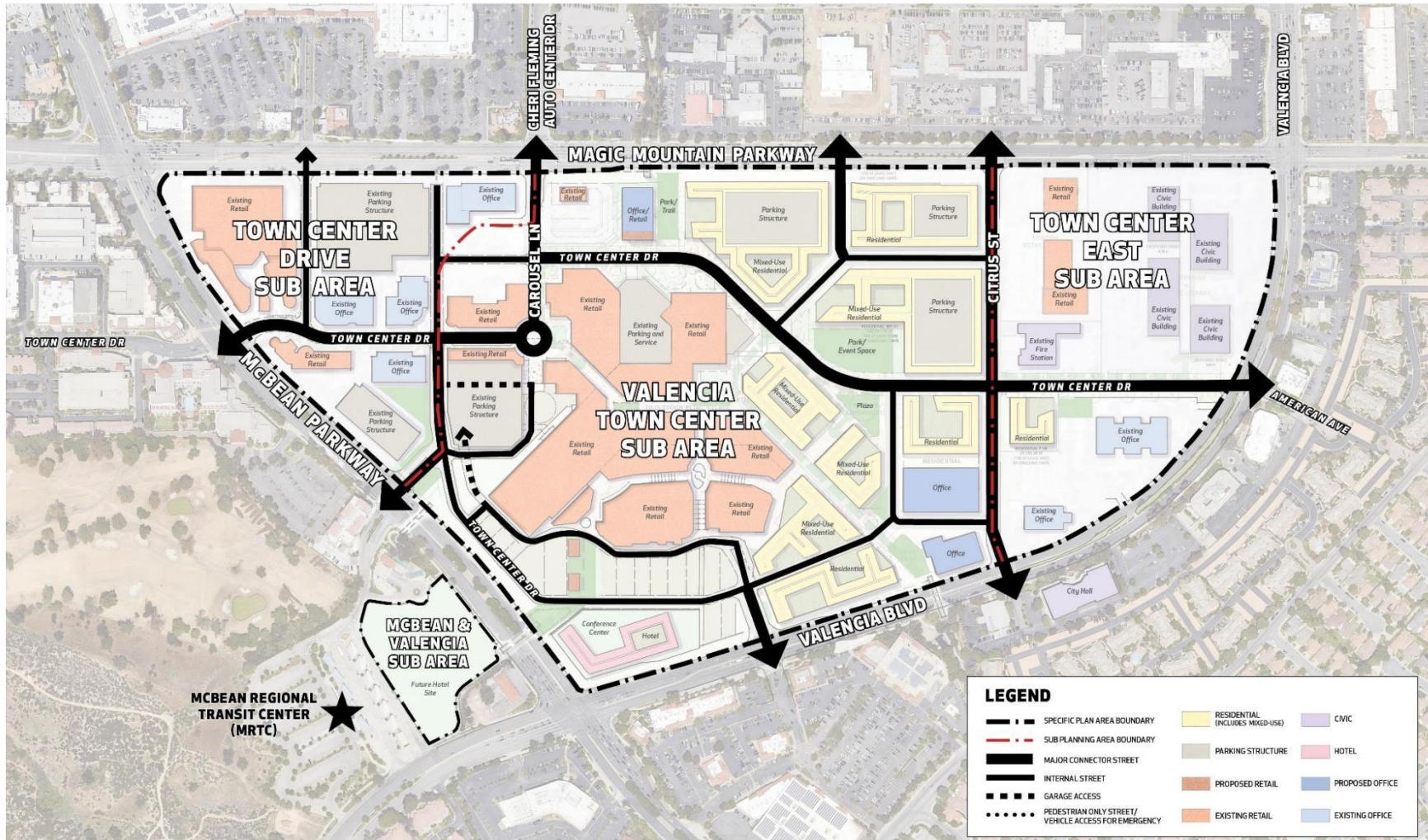
The Development Plans presented in this section depict an illustrative example of potential built-out scenarios based on the principles included in the Development Framework & Standards defined by the TCSP. Two conceptual plans were developed, a Low Buildout Scenario and a Full Buildout Scenario, each representing a different conceptual intensity. Representations of each buildout scenario are presented in Figure 2-1 and Figure 2-2.

The Low Buildout Scenario includes approximately 487,000 square feet of net-new commercial space and approximately 1,400 dwelling units. It also contemplates the demolition and replacement of a portion of the existing Valencia Town Center Mall footprint, including the former Sears space and a small portion of The Patios adjacent to the former Sears space, to create a better overall connection

throughout the site. The scenario identifies replacing these areas with mixed-used development. Town Center Drive is extended in this scenario but directs traffic around the northern portion of the Valencia Town Center Mall. Some internal streets are created, providing additional space for new development to occur.

The Full Buildout Scenario includes approximately 584,000 square feet of net-new commercial space and approximately 2,200 residential units. The scenario extends beyond the Low Buildout Scenario assumptions by including the replacement of the existing Valencia Town Center Mall food court, JCPenney space, and explores potential redevelopment scenarios for the Town Center East Subarea. The alignment of Town Center Drive is also reconfigured to provide more direct east-west access across the site.

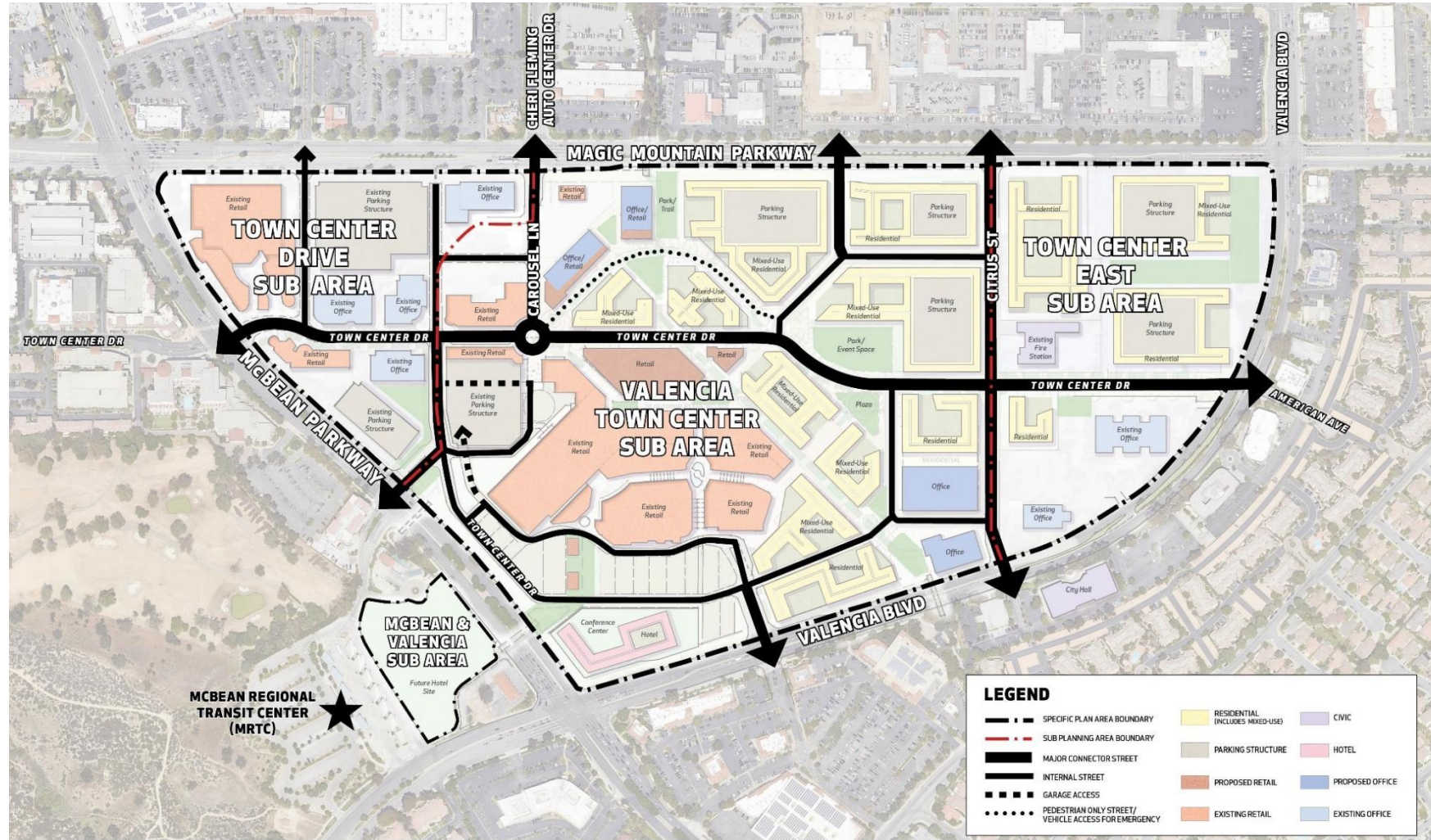
Figure 2-1. Low Buildout Scenario



Source: City of Santa Clarita, Michael Baker International, DLR Group, ESRI







Figure 2-2. Full Buildout Scenario



Source: City of Santa Clarita, Michael Baker International, DLR Group, ESRI

# DEVELOPMENT FRAMEWORK & STANDARDS

Table 2-1. List of Framework & Standards

 <b>LAND USE</b>	 <b>PLACEMAKING</b>	 <b>MOBILITY</b>	 <b>PARKING</b>
LU1: Balance of Land Uses LU2: Plan Flexibility LU3: Permitted Uses	PM1: Development Blocks PM2: Signature Vistas PM3: Architectural Design PM4: Building Specifications PM5: Gathering Spaces PM6: Gateways	MO1: Roadway Network MO2: Pedestrian Streetscapes MO3: Pedestrian & Bicycle Networks MO4: Pedestrian Bridge Connectivity MO5: Micro Mobility MO6: Transit Drop-Off Zones	PK1: Parking Strategy



## 5. DEVELOPMENT FRAMEWORK & STANDARDS



### LAND USE

The Land Use section of the TCSP provides a narrative for a flexible plan, a mix of commercial uses and housing opportunities, and further describes how each of these elements can be executed throughout the SPA in future development. A Permitted Use Chart (PUC) is also provided that establishes a comprehensive list of uses contemplated in each subarea and includes their associated required entitlement(s).

Figure 2-3. Mixed-use Development Examples



Location: Santa Clarita, California

### ELEMENT LU1 – BALANCE OF LAND USES

#### DESCRIPTION

The explicit intent of the TCSP is to ensure that a balanced mix of uses is developed within the SPA. Residential and non-residential uses must be provided in lock-step, with projects providing combinations of retail, dining, entertainment, hospitality, commercial, and

residential uses on an ongoing basis as new development and redevelopment occurs within the SPA. Both the Low and Full Buildout Scenarios provide conceptual examples of the Development Framework & Standards discussed in this chapter and act as a general blueprint for comparison of future development projects. Although the actual buildout of the SPA will occur over time and the physical location and square footage of structures, as well as the exact mix of land uses, will vary from the analyzed scenarios, all development must conform to a similar balance of commercial and residential land uses and be consistent with the urban form that is depicted in these scenarios.

For the purposes of this chapter, mixed use refers to:

1. A mixture of residential and commercial uses;
2. A mixture of a hospitality use(s) and residential or non-residential uses; or
3. A combination of office use or other types of commercial uses within the same building.

To ensure a balance of commercial and residential land uses, the entirety of the SPA must maintain a jobs-housing balance of at least 2.7 to 1, as analyzed under the Full Buildout Scenario.

#### CONTEXT

Currently, the entire built environment within the SPA includes non-residential land uses. The TCSP contemplates moving toward a balanced mix of both commercial and residential land uses. As such, mixed-use developments are incentivized within the SPA as it helps maximize land efficiency by allowing multiple land uses to coexist in the same space. By providing a variety of services and amenities in one area, mixed-use developments create convenient and accessible

neighborhoods. Residents can live, work, and play in one place, reducing commute times and enhancing overall quality of life.

Fostering a diverse mix of uses within an area cultivates an environment where employees and residents actively engage in local work and patronize nearby businesses. This relationship contributes to the success of local enterprises, creating a mutually beneficial cycle where the presence of residents and businesses both enhance the vitality of the community. With residential, commercial, and recreational spaces coexisting, mixed-use developments promote social interaction and a sense of community.

Mixed-use development is contemplated in all subareas of the SPA and can be facilitated in a variety of ways. While some subareas are more developed and may only result in new tenants within existing buildings, others provide space for new development or redevelopment.

The TCSP contemplates new residential units in conjunction with commercial development which may bring a range of potential housing types to the SPA including apartments, condominiums, and townhomes. Additional housing choices help to create new options for a more urban living environment.

The Full Buildout Scenario within the TCSP includes approximately 2,200 residential units throughout the SPA. The City's Housing Element identifies the need for 446 affordable units on specific properties within the SPA. Because of this, the EIR prepared for the TCSP contemplated 20% of units within the SPA as being affordable. As required by the state, and in order to satisfy the TCSP goals for both compliance with the City's Housing Element and to facilitate the implementation of a plan that is practical, flexible, and buildable, applicants are strongly encouraged to include affordable units to any project that proposes a residential component.

To position the TCSP for success, in conjunction with residential uses, retail, dining, entertainment, hospitality, and commercial uses should support the attractiveness of the evolving SPA and encourage

pedestrian-oriented development that enhances the sense of place that is characteristic of the Valencia Town Center. Overall, mixed-use development within the SPA should seek to grow and improve the economy by taking advantage of a larger regional attraction of visitors while also capturing the economic benefits of on-site residential uses. At no time, however, should residential growth occur in a manner that outpaces or is out of balance with commercial growth.

### **LOCATION**

A balanced mix of land uses must be provided throughout the entire SPA. This applies to both new development projects and redevelopment projects.

### **EXECUTION**

To ensure a balanced mix of commercial and residential land uses, all projects are subject to the following development standards:

#### **1. Single-Use Residential Projects**

Structures containing multi-family uses only, must provide a significant community amenity to the SPA above and beyond those already required as part of the development. The type, or combination of types, of community amenities provided must be determined by the Director of Community Development and is subject to an Administrative Permit, with notification to the Planning Commission and City Council on the date action is taken. Examples of significant community amenities include, but are not limited to:

- Providing a public space within the SPA;
- Providing a major gateway feature;
- The addition of a new or relocation of an existing pedestrian bridge;
- Providing an improvement to pedestrian bridge landing zones;
- Providing a transit hub; and/or
- Constructing the extension of Town Center Drive.



Single-use residential projects that do not provide a significant community amenity:

- Are subject to a Minor Use Permit with review and approval by the Planning Commission via the public hearing process; and/or
- Are not eligible for the design incentive to share guest parking spaces with commercial parking spaces.

**Figure 2-4. Single-Use Condominium/Apartment Building Example (Residential Only)**



*Location: One Paseo, San Diego, CA*

## 2. Development Progress

At the discretion of the Director of Community Development, the City may require an economic study to be included with an entitlement submittal that contains the 230<sup>th</sup>, 560<sup>th</sup>, 800<sup>th</sup>, 1,100<sup>th</sup>, 1,450<sup>th</sup>, 1,800<sup>th</sup>, and 2,000<sup>th</sup> residential unit proposed within the SPA and/or that includes demolition of existing non-residential space of 25,000 square feet or more. The economic study must:

- Be prepared by a qualified firm approved by the Director of Community Development;
- Include an overall economic assessment of the SPA at the time of submittal, using 2023 as the baseline for comparison;
- Include an assessment of how the proposed development will impact the economy of the SPA. Metrics to be considered must include, but are not limited to:
  - o A demonstration of how the project meets the TCSP’s goal of balanced development based on the Low and/or Full Buildout Scenarios;
  - o Job creation or loss;
  - o Sales tax creation or loss;
  - o Jobs-Housing Balance within the SPA;
  - o A demonstration of net-new commercial space.
- Include other areas of analysis at the discretion of the Director of Community Development.

If the study cannot demonstrate consistency with the overall vision of the TCSP, at their discretion, the Director of Community Development may:

- Declare that the proposed project does not meet the overall intent of the TCSP and must be modified; or
- Require the inclusion of a significant community amenity in a manner consistent with item number one, above.

## 3. Subarea Plans

At the discretion of the Director of Community Development, the City may require conceptual development plans for an entire subarea.

## ELEMENT LU2 – PLAN FLEXIBILITY

### DESCRIPTION

The TCSP is a flexible plan that establishes a pliable framework to guide overall development. There is no set site plan within this document, rather a plan that will adapt as development is proposed and new uses are introduced throughout the SPA. Developers can benefit from streamlined permitting which will save time and money, flexible design requirements, shared parking, among others.

### CONTEXT

The entirety of the SPA has a General Plan Land Use and Zoning Designation of Regional Commercial (CR). Specifications for the CR zone include a maximum residential density of 50 units per acre, a minimum residential density of 18 units per acre, and a commercial FAR of 2.0. The height thresholds for most types of buildings is 35 feet, with office buildings having a threshold of five stories under the JCOZ. Currently, new development projects and tenant improvement projects are subject to the UDC, including a Permitted Use Chart (PUC) that determines appropriate entitlements required, as well as design requirements for residential and commercial land uses. Depending on the type of entitlement required, as well as other specific characteristics, projects may be subject to review and approval by the Director of Community Development, or by the Planning Commission or the City Council via the public hearing process. Further, individual projects may be subject to analysis under the CEQA.

### LOCATION

The TCSP provides a flexible framework for development that applies to projects throughout the SPA.

### EXECUTION

The following tools have been established within the TCSP to encourage and help incentivize development within the SPA:

#### 1. Flexible Zoning and Land Use Regulations

The TCSP does not require buildings or particular uses to be established in specific locations. Instead, it allows for developers to be creative and make the best use and design of their properties. The development standards are meant to be flexible, help guide development, and encourage a mix of uses throughout the SPA.

#### 2. CEQA Streamlining

An EIR has been certified for the TCSP that contemplates the various buildout scenarios within the SPA. The EIR identifies various CEQA streamlining provisions that are available to development projects implementing the TCSP. Many projects that comply with the vision, framework elements, and development standards established within the TCSP will require no additional environmental document under CEQA.

#### 3. Expedited Permit Processing

The TCSP contains its own PUC that establishes allowable uses for each subarea within the SPA. This new PUC streamlines permit processing time and reduces permit fees for projects that propose uses which the community and City Council have identified as desirable and appropriate for the area.

#### 4. Enhanced Design Standards

Developers will benefit from increased building height when proposing mixed-use projects that meet the vision of the TCSP.

#### 5. Contemporary Parking Standards

The TCSP recommends a modernized approach to parking that reflects current industry standards, helps create great places, and enhances pedestrian mobility.

## ELEMENT LU3 – PERMITTED USES

**Table 2-2** represents the PUC for the TCSP and identifies permitted and prohibited land uses in the SPA by subarea. A parcel and building within the SPA shall only be occupied by land uses allowed in that subarea, as shown in the PUC. Any one or more land use(s) identified by the PUC as being allowable within a specific subarea may be established on any parcel within that subarea, subject to the planning permit requirement listed in the PUC, and in compliance with all applicable requirements of this specific plan. Entitlements identified in the PUC are subject to Division 3 (Applications) of the UDC. Definitions for land uses are pursuant to UDC Sections 17.11 (Definitions) and 17.42 through 17.49 (Use Classifications), unless otherwise explicitly provided within the specific plan.

Land uses not listed in the PUC are subject to Division 5 of the UDC and assume the zoning designation of CR. If a particular land use is not identified in the PUC, nor is listed within the UDC as a permissible use within the CR zone, it is not allowed within the TCSP. The Director of Community Development shall have the discretion to determine

other similar and compatible land uses in accordance with UDC 17.04 (Interpretations).

**Table 2-2** identifies permissible and prohibited land uses within the SPA as follows:

- "P" identifies land uses that are Permitted By Right.\*
- "AP" identifies land uses that require an Administrative Permit.
- "M" identifies land uses that require a Minor Use Permit.
- "C" identifies land uses that require a Conditional Use Permit.
- "X" identifies land uses that are prohibited.

\*A land use that is shown as "P" in the PUC is subject to compliance with all applicable provisions of this specific plan and in compliance with UDC 17.23 (Class II Applications – Discretionary). An Administrative Permit, Architectural Design Review Permit, and/or Development Review Permit, may be required for uses that are shown as "P" within the PUC. Multiple entitlements may apply to each project based on the development proposed.



Table 2-2. Town Center Specific Plan Permitted Use Chart

Land Use	Valencia Town Center Subarea	Town Center East Subarea	Town Center Drive Subarea	McBean and Valencia Subarea
<b>Residential</b>				
Caretaker’s Residence	X	X	X	X
Community Care Facility	X	X	X	X
Joint Living and Working Quarters (Live/Work Units)	M	M	M	M
Multi-Family Residential	P	M	M	M
Mixed Use	P	P	P	P
<b>Commercial</b>				
Airport	X	X	X	X
Animal Day Care	M	M	M	M
Animal Grooming and Pet Stores	P	P	P	P
Banks and Financial Services	P	P	P	P
Business Support Services	P	P	P	P
Call Centers	X	M	M <sup>1</sup>	M
Convention Center	M	M	M	M
Day Care for Children or Adults	AP <sup>1</sup>	AP	AP <sup>1</sup>	AP
Drive-Through Automated Teller Machine (ATM)	AP	AP	AP	AP
<b>Eating and Drinking Establishments</b>				
<i>Banquet Facilities without Alcohol</i>	P	P	P	P
<i>Banquet Facilities with Alcohol</i>	AP	AP	AP	AP
<i>Bars and Alcohol Drinking Establishments</i>	AP	M	AP	AP
<i>Beer Garden/Micro-Brewer/Wine Bar</i>	AP	AP	AP	AP
<i>Catering Establishments<sup>1</sup></i>	P	P	P	P
<i>Fast Food Restaurants with Drive-Through</i>	M	M	M	M

NOTE: Any uses not shown are subject to the UDC

<sup>1</sup> This use is allowed only on an upper floor or behind a primary ground floor use.

<sup>2</sup> No two such establishments shall be located within 300 feet of one another.

<sup>3</sup> Subject to the Temporary Use Permit requirements established in UDC 17.23.200 (Temporary Use Permit) and UDC 17.67 (Temporary Uses).

Table 2-2. Town Center Specific Plan Permitted Use Chart

Land Use	Valencia Town Center Subarea	Town Center East Subarea	Town Center Drive Subarea	McBean and Valencia Subarea
<b>Commercial [Cont.]</b>				
<i>Full Service Restaurants without Alcohol</i>	P	P	P	P
<i>Full Service Restaurants with Alcohol</i>	AP	AP	AP	AP
<i>Ghost Kitchens<sup>1</sup></i>	P	P	P	P
<i>Hookah Bar/Cigar Club<sup>2</sup></i>	M	M	M	M
<i>Limited Service Restaurants without Alcohol</i>	P	P	P	P
<i>Limited Service Restaurants with Alcohol</i>	AP	AP	AP	AP
<i>Take Out/Delivery</i>	P	P	P	P
Equipment Repair- Consumer Electronics <sup>1</sup>	P	P	P	P
Equipment Repair- Small Engine/Equipment	X	X	X	X
Farmer’s Market, Permanent	M	M	M	M
Heliport	X	X	X	X
Lodging				
<i>Bed and Breakfasts</i>	X	X	X	X
<i>Hotels - with or without Conference Space</i>	P	P	P	P
<i>Motels</i>	X	X	X	X
Medical Services and Urgent Care (Non-Surgical)	AP	P	AP	AP
Nightclubs <sup>2</sup>	M	M	M	M
Personal Services	P	P	P	P
Professional Offices	P	P	P	P

NOTE: Any uses not shown are subject to the UDC

<sup>1</sup> This use is allowed only on an upper floor or behind a primary ground floor use.

<sup>2</sup> No two such establishments shall be located within 300 feet of one another.

<sup>3</sup> Subject to the Temporary Use Permit requirements established in UDC 17.23.200 (Temporary Use Permit) and UDC 17.67 (Temporary Uses).

Table 2-2. Town Center Specific Plan Permitted Use Chart

Land Use	Valencia Town Center Subarea	Town Center East Subarea	Town Center Drive Subarea	McBean and Valencia Subarea
<b>Commercial [Cont.]</b>				
<b>Recreation, Commercial</b>				
<i>Amusement Center- Up to Three (3) Electronic Devices</i>	P	P	P	P
<i>Amusement Center- Four (4) and more Electronic Devices</i>	AP	AP	AP	AP
<i>Amusement Park</i>	C	C	C	C
<i>Indoor Entertainment</i>	AP	AP	AP	AP
<i>Indoor Sports and Recreation</i>	AP	AP	AP	AP
<i>Outdoor Entertainment &lt;2500 SF</i>	AP	AP	AP	AP
<i>Outdoor Entertainment &gt;2500 SF</i>	M	M	M	M
<i>Outdoor Sports and Recreation</i>	M	M	M	M
<b>Retail, General</b>	P	P	P	P
<b>Retail, Specific</b>				
<i>Carpet and Flooring Stores</i>	P	P	P	P
<i>Convenience Store &lt;3,500 SF</i>	P	P	P	P
<i>Department Stores</i>	P	P	P	P
<i>Drugstores</i>	P	P	P	P
<i>Feed and Tack Stores</i>	X	X	X	X
<i>Firearm Sales Stores</i>	X	X	X	X
<i>Garden Supply Stores</i>	X	X	X	X
<i>Liquor Stores<sup>2</sup></i>	C	C	C	C
<i>Nurseries- Retail Sale</i>	X	X	X	X
<i>Secondhand Stores</i>	M	M	M	M
<i>Supermarket/Grocery &gt;3,500 SF</i>	P	P	P	P
<i>Swap Meets and Flea Markets</i>	X	X	X	X

NOTE: Any uses not shown are subject to the UDC

<sup>1</sup> This use is allowed only on an upper floor or behind a primary ground floor use.

<sup>2</sup> No two such establishments shall be located within 300 feet of one another.

<sup>3</sup> Subject to the Temporary Use Permit requirements established in UDC 17.23.200 (Temporary Use Permit) and UDC 17.67 (Temporary Uses).



Table 2-2. Town Center Specific Plan Permitted Use Chart

Land Use	Valencia Town Center Subarea	Town Center East Subarea	Town Center Drive Subarea	McBean and Valencia Subarea
<b>Commercial [Cont.]</b>				
<i>Thrift Stores</i>	C	C	C	C
<i>Tobacco Paraphernalia Stores<sup>2</sup></i>	C	C	C	C
<i>Vendors, Long-Term</i>	X	X	X	X
Schools, Instructional	P	M	P	M
Schools, Vocational	AP <sup>1</sup>	AP	AP <sup>1</sup>	AP
Tattoo Parlors <sup>2</sup>	AP	M	AP	M
Vehicle Sales and Services				
<i>Boat and Camper/Recreational Vehicle Repair</i>	X	X	X	X
<i>Boat and Camper/Recreational Vehicle Sales and Rental</i>	X	X	X	X
<i>Car Wash, Automated Self-Service (No Attendants)</i>	X	X	X	X
<i>Car Wash, Full-Service (Provides Attendants)</i>	X	X	X	X
<i>Fuel Sales</i>	P	P	P	P
<i>Motorcycle Sales and Services</i>	X	X	X	X
<i>Truck, Large Vehicle Repair Sales and Rental</i>	X	X	X	X
<i>Truck, Large Vehicle Repair Services</i>	X	X	X	X
<i>Vehicle Rentals</i>	X	X	X	X
<i>Vehicle Repair and/or Maintenance</i>	X	X	X	X
Veterinary Clinics	M	M	M	M

NOTE: Any uses not shown are subject to the UDC

<sup>1</sup> This use is allowed only on an upper floor or behind a primary ground floor use.

<sup>2</sup> No two such establishments shall be located within 300 feet of one another.

<sup>3</sup> Subject to the Temporary Use Permit requirements established in UDC 17.23.200 (Temporary Use Permit) and UDC 17.67 (Temporary Uses).

Table 2-2. Town Center Specific Plan Permitted Use Chart

Land Use	Valencia Town Center Subarea	Town Center East Subarea	Town Center Drive Subarea	McBean and Valencia Subarea
<b>Industrial</b>				
Recycling				
<i>Collection of Trucks and/or Bins as an Accessory Use</i>	X	X	X	X
<i>Recycling Center, Small</i>	X	X	X	X
<i>Recycling Center, Large</i>	X	X	X	X
<i>Vending Machines as an Accessory Use</i>	X	X	X	X
Studios, Recording- Music	M <sup>1</sup>	M <sup>1</sup>	X	X
Studios, Recording- Radio	AP	AP	AP	AP
<b>Public and Semi-Public</b>				
Ambulance or Paramedic Dispatch	X	M	X	X
Corporate Yards	X	X	X	X
Museums (Private and Public)	P	P	P	P
Parks, Public and Private	P	P	P	P
Places of Community Assembly	C	C	C	C
Private Primary and Secondary Schools	C <sup>1</sup>	C	C <sup>1</sup>	C
Public Primary and Secondary Schools	C <sup>1</sup>	C	C <sup>1</sup>	C
Rehabilitation Facility	C <sup>1</sup>	C	C <sup>1</sup>	C <sup>1</sup>
Utility Substation	X	X	X	X
<b>Agricultural Uses</b>				
Horticulture- For Commercial Sale	X	X	X	X

NOTE: Any uses not shown are subject to the UDC

<sup>1</sup> This use is allowed only on an upper floor or behind a primary ground floor use.

<sup>2</sup> No two such establishments shall be located within 300 feet of one another.

<sup>3</sup> Subject to the Temporary Use Permit requirements established in UDC 17.23.200 (Temporary Use Permit) and UDC 17.67 (Temporary Uses).

Table 2-2. Town Center Specific Plan Permitted Use Chart

Land Use	Valencia Town Center Subarea	Town Center East Subarea	Town Center Drive Subarea	McBean and Valencia Subarea
<b>Accessory Use</b>				
Alcohol Sales	AP	AP	AP	AP
Accessory Structure	P	P	P	P
Firearm Sales	AP	AP	AP	AP
Gambling	M	M	M	M
Helipad	C	C	C	C
Live Entertainment, Indoor	P	P	P	P
Live Entertainment, Outdoor	AP	M	AP	M
Other Accessory Use	P	P	P	P
Structured Parking	P	P	P	P
<b>Temporary Use</b>				
Temporary Uses <sup>3</sup>	P	P	P	P
Farmer’s Market <sup>3</sup>	P	P	P	P

NOTE: Any uses not shown are subject to the UDC

<sup>1</sup> This use is allowed only on an upper floor or behind a primary ground floor use.

<sup>2</sup> No two such establishments shall be located within 300 feet of one another.

<sup>3</sup> Subject to the Temporary Use Permit requirements established in UDC 17.23.200 (Temporary Use Permit) and UDC 17.67 (Temporary Uses).





## PLACEMAKING

As important as the TCSP’s goal for creating a balanced mix of land uses, is the goal to simultaneously make great, unique places that further establishes the SPA as a regional destination and creates a true Town Center. Done in lock-step, these two goals will not only attract the kinds of land uses desired by the City Council and the community, but create a vibrant, exciting, and visually interesting environment where visitors can shop, dine, be entertained, live, work, or simply spend time. Components of successful placemaking include:

- Using unique, iconic architecture that honors the history of the community, integrates into existing design, and looks to the future for inspiration.
- Creating definitive visual interest by combining architecture, building massing, a thoughtfully designed public realm, and pathways to create signature vistas throughout the SPA.
- Creating larger public spaces that can be programed for events, community celebrations, markets, exhibits, and more.
- Creating a series of smaller, more intimate common spaces for gatherings, live music, or relaxing.
- Encouraging outdoor dining.
- Creating gateway features at key entry points that define the space and establish an expectation of aesthetics once inside.

## ELEMENT PM1 – DEVELOPMENT BLOCKS

### DESCRIPTION

The basis for realizing the TCSP’s vision starts with appropriately-sized development blocks that utilize orientation of structures, architecture, and an internal road network centered around a modified grid pattern. Combined, these four factors provide the foundation for creating great places, compelling site lines, and enhanced visual interest throughout the SPA.

### CONTEXT

Currently, no meaningful grid pattern of internal roads exists within the SPA other than within the Town Center Drive Subarea. The Valencia Town Center Subarea is characterized by a large surface parking lot with a ring road for vehicular traffic within the site. The Town Center East Subarea is comprised of various independently-owned properties with little or no connectivity. Finally, the McBean and Valencia Subarea is currently vacant. The inclusion of appropriate development block sizes and a modified grid pattern of internal roads will serve to guide development in an organized manner that will organically create great places.

### LOCATION

Appropriate block sizes, the creative use of building orientation, architectural massing, and a modified grid network of internal roadways is required throughout the SPA for all proposed development or redevelopment projects.

### EXECUTION

#### 1. Extension of Town Center Drive

To serve as an anchor for the creation of future development blocks in the Valencia Town Center, Town Center Drive must be extended between its current terminus at the western entry to the Valencia Town Center Mall, east to Citrus Street. A further extension is encouraged between Citrus Street and Valencia Boulevard to

establish an anchor for development blocks in the Town Center East Subarea.

## **2. Block Size and Shape**

Development blocks should be appropriately sized for the subarea in which they are located and follow the shape defined by a modified grid pattern of internal roads. Block shapes should vary, and the repetitive use of rectangular and/or square blocks is discouraged. Block edges may be linear or curved.

## **3. Building Size and Orientation**

Within blocks, buildings should be appropriately sized and oriented to create public and common spaces. Additionally, space should be left between buildings to facilitate cross-block pedestrian mobility and prevent the creation of monolithic structures.

## **4. Modified Grid Pattern**

Internal roads must be designed in a modified grid pattern. To create visual interest, repetitive use of long, straight roadways and 90-degree intersections are discouraged.

## **5. Building Mass and Architecture**

Building mass and architecture should be utilized to direct and terminate sightlines and to create signature vistas.

## **ELEMENT PM2 – SIGNATURE VISTAS**

### **DESCRIPTION**

A signature vista is a deliberate placemaking strategy where a prominent building, monument, fountain, public art piece, public space, or common space is positioned at a strategic point along a visual axis or at the end of a visual axis. Visual axes could include internal roads, intersections, or pathways. The goal is to create an impactful amenity that enhances the unique character of the SPA and helps create an identifiable sense of space.

### **CONTEXT**

Signature vistas can help in organizing and structuring urban spaces, guiding people's movement and creating a sense of order. These can aid in wayfinding by providing a recognizable landmark at the end of a street or pathway, helping people navigate the urban environment.

There are currently three examples of signature vistas within the SPA: 1) the length of Town Center Drive that begins at McBean Parkway and ends at the western entrance to the Valencia Town Center Mall; 2) the interior pedestrian pathways within The Patios as visitors approach a central plaza and water feature from the west, east, north, and south; and 3) the colonnade within the Los Angeles County Government Center in the Town Center East Subarea.

### **LOCATION**

Signature vistas should be created at locations including, but not limited to, key intersections, prominent junctions of paths and/or trails, the terminus of or along established site lines, gateway locations, public and common spaces, and the center of roundabouts.

### **EXECUTION**

The following are guidelines for the creation of signature vistas:

1. The required extension of Town Center Drive should be aligned to create signature vistas along its length. The entire

alignment of the extension should not be straight, but rather include one or more changes in direction to create more visual interest and more than a single sightline.

2. Incorporate prominent architectural features including, but not limited to, increased massing, tower elements, and prominent rooflines.
3. Where appropriate, utilize monumental architecture, large fountains, or other significant focal points.
4. Use architectural elements such as arches, colonnades, or landscape elements such as tree-lined avenues to draw the viewer's eye towards the focal point.
5. Utilize building massing along pedestrian paths that guide sightlines and terminate in common spaces and/or with significant architectural features.
6. Create transitional spaces that gradually reveal signature vistas.
7. Incorporate landscaping and vegetation to frame the signature vistas and create a sense of depth and perspective.
8. Use lighting effects to frame the signature vistas and create a dramatic visual impact.

**Figure 2-5. Examples of Signature Vistas**



*Location: Old Town Newhall, Santa Clarita, CA; Signature vista using prominent architecture*

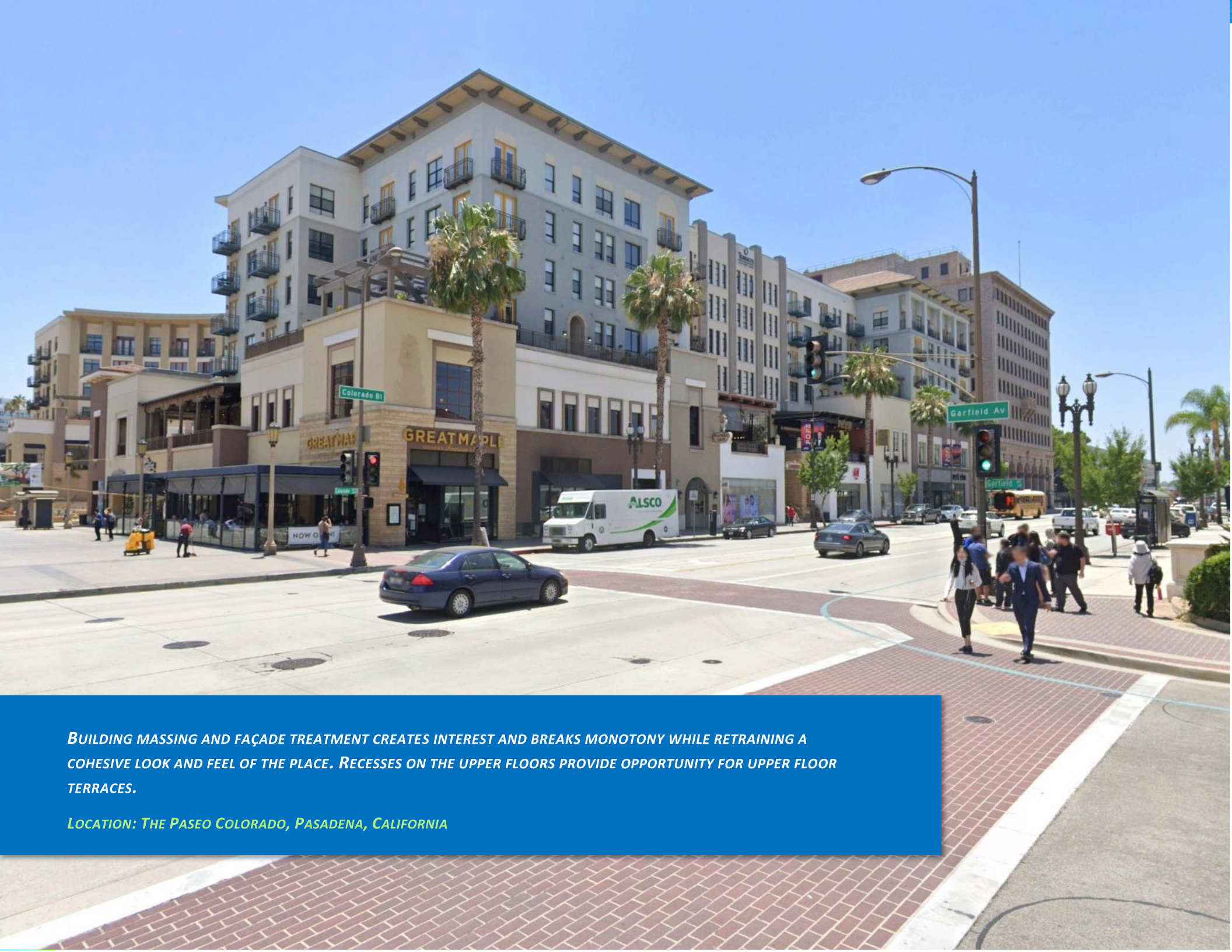


*Location: Pasadena, CA; Signature vista using architecture and corner treatment*



*Location: Glendale, CA; Signature vista using landmark and public gathering space*





*BUILDING MASSING AND FAÇADE TREATMENT CREATES INTEREST AND BREAKS MONOTONY WHILE RETRAINING A COHESIVE LOOK AND FEEL OF THE PLACE. RECESSES ON THE UPPER FLOORS PROVIDE OPPORTUNITY FOR UPPER FLOOR TERRACES.*

*LOCATION: THE PASEO COLORADO, PASADENA, CALIFORNIA*

## ELEMENT PM3 – ARCHITECTURAL DESIGN

### DESCRIPTION

Defining architectural character will provide coherence and a unified visual identity to the SPA. By drawing from the past, embracing the present, and looking towards the future, the TCSP will contribute to a well-integrated and forward-looking community design that respects its roots while embracing contemporary style. This approach aims to create a balanced and cohesive urban environment by respecting the context of on-site architecture, meeting current standards for functionality and sustainability, and fostering creativity to anticipate future architectural trends.

Architectural character refers to the distinct qualities and features that define the overall appearance, style, and identity of a space. It encompasses various elements, including form, materials, details, and overall design language. The objective of defining the architectural character is to encourage functionality, sustainability, and integration of buildings with the surroundings.

### CONTEXT

The built environment within the SPA is reflective of numerous architectural styles that evolved over time without an overall vision for long-term development. As such, there is little architectural cohesion between subareas within the SPA.

The Valencia Town Center Subarea consists of no fewer than five architectural styles between the original mall structure itself, The

Patios, and the various larger retail locations including JCPenney, Macy's, and the former Sears. Subsequent façade improvements to the western and northern entrances to the Valencia Town Center Mall reflect a more contemporary style similar to The Patios. However, there is very little architectural cohesion between styles within this subarea.

Most of the structures within the Town Center East Subarea were constructed from the 1970s to the present, and reflect a contemporary architectural style mixed with natural materials, new materials, and attached accents. Common building materials consist of stucco, native stone, glass, and wood. Examples include the Bank of America building, medical office plaza, and the Los Angeles County Government Center, which includes the Valencia Branch of the Santa Clarita Public Library.

The Town Center Drive Subarea contains a set of integrated architectural styles that define its character. Constructed between the late 1990s and early 2000s, the subarea is dominated by several office buildings in the art deco style, while smaller retail buildings include Spanish and contemporary styles.

Although vacant, the McBean and Valencia Subarea includes an entitled hotel and restaurant which has yet to be constructed. The approved architecture for the project is consistent with the City's Community Character and Design Guidelines (CC&DG) standards for the community of Valencia and reflects a more contemporary style.



Figure 2-6. Existing Architecture in the SPA





Figure 2-7. Architectural Styles in the SPA



## LOCATION

Architectural standards are applicable to all new development and redevelopment projects within the SPA. Architectural standards must be applied to unwrapped parking structures (or unwrapped portions of wrapped parking structures) to the satisfaction of the Director of Community Development.

## EXECUTION

Projects must choose from a palette of five architectural styles, each of which reflects a different design philosophy. These various standards either demonstrate the tradition of the City's current architectural requirements for the community of Valencia as defined by the CC&DG, honor the history of architectural styles that are currently within the SPA, or provide a vision for a new architecture style moving forward. The thoughtful application of these styles will create a unique character that defines and identifies the SPA within the community and the region. The palette of possible architectural styles includes:

### 1. Valencia Community Architectural Style

Reflecting the tradition of development for the community, applicants may propose projects that are consistent with the City's current CC&DG for Valencia. This style is reflected by a mixture of natural elements and contemporary style. Structures are highly articulated and generally characterized by materials including native stone, textured or smooth stucco, tile, wood, and timber. Roofing materials include clay or cement tiles, shingles, and/or brushed metal. Finishes that are reflective, highly polished, mirrored, or corrugated are not consistent with this style.

Examples throughout the community of Valencia include Bridgeport Marketplace, Valencia Crossroads, the Madison at Town Center Apartments, and Monticello.

## 2. On-Site Architectural Styles

Honoring the history of architectural styles throughout the SPA, the following three architectural styles are currently reflected in the built environment on-site:

### *Contemporary Architectural Style*

Contemporary architectural style refers to the design philosophy and approach to buildings that reflects the current trends, cultural context, and technological advancements of the present era. The style emphasizes innovation, creativity, and adaptability while incorporating elements of minimalism, sustainability, and functionality. Contemporary Architecture is characterized by clean lines, geometric shapes, open spaces, and the use of modern materials such as glass, steel, and concrete. Unlike traditional architectural styles tied to specific historical periods, Contemporary Architecture is constantly evolving and is shaped by the ever-changing societal tastes.

Contemporary Architecture is contemplated as an appropriate style within the CC&DG for the community of Valencia. Examples can be found within The Patios portion of the Valencia Town Center Mall, and Town Center Drive.

### Overall Building Design

- Sleek, minimalist design with clean lines and geometric forms.
- Asymmetrical shapes and open floor plans that prioritize functionality and flexibility.
- Focus on maximizing natural light and ventilation.

### Walls

- Variety of wall materials including concrete, glass, brushed metal, and wood.
- May feature exposed concrete or brushed metal beams.
- Glazing to create a seamless connection between indoor and outdoor spaces.



**Roofs**

- Although articulation of rooflines is preferred, green roofs, solar panels, and other sustainable features are often integrated into roof designs.

**Articulation and Decorative Elements**

- Favor simplicity and restraint in articulation and decorative elements.
- Clean lines, minimalist detailing, and geometric shapes.
- Decorative elements serve a functional purpose.

**Windows and Doors**

- Large and expansive, allowing for maximum natural light and views.
- Floor-to-ceiling windows, sliding glass doors, and pivot doors blurring the boundaries between indoor and outdoor spaces.
- Frames may be made of aluminum or steel for a sleek, modern look.

**Materials and Colors**

- Traditional options like concrete and steel to innovative alternatives such as glass-reinforced concrete and engineered timber.
- Focus on the form and texture of the materials.
- Neutral color palettes, including shades of white, gray, black, and earth tones.
- Accents of bold colors or natural materials may be used selectively to add visual interest and contrast.

Figure 2-8. Examples of Contemporary Architectural Style





### ***Spanish/Mediterranean Revival***

These similar architectural styles combine elements such as stucco exteriors, tile roofing, arched openings, wrought iron details, and courtyards to create buildings that exude warmth, elegance, and charm. Drawing from both Spanish and Mediterranean architectural influences, this style features a harmonious blend of textures, colors, and forms, often characterized by whitewashed walls, terracotta roofs, and ornate detailing. With a focus on indoor-outdoor living and a sense of timeless sophistication, Spanish/Mediterranean Revival Architecture evokes a tranquil and inviting ambiance, particularly suited to regions with warm climates and rich cultural heritage.

Elements of Spanish and Mediterranean Architecture can be found at the Valencia Town Center Mall in the Valencia Town Center Subarea, and low-rise commercial buildings within the Town Center Drive Subarea.

#### **Overall Building Design**

- Asymmetrical, articulated buildings with a focus on indoor-outdoor living.
- Emphasis on tower or spire elements.
- Courtyards and central patios serving as focal points for social gatherings and providing natural ventilation and light to interior spaces.

#### **Walls**

- Stucco or smooth exterior surfaces.
- May feature textured finishes.
- Typically, whitewashed appearance.

#### **Roofs**

- Clay tile roofing, either flat or low-pitched.
- Typically, terracotta or red in color and help to deflect the intense heat of the sun, maintaining comfortable interior temperatures.
- Focus on tower or spire elements.

#### **Articulation and Decorative Elements**

- A variety of articulated surfaces, both vertical and horizontal.
- Arched openings, including doorways, windows, and arcades.
- Decorative wrought ironwork, such as window grilles, balconies, and gates.
- Wooden beams and corbels may be exposed.

#### **Windows and Doors**

- Arched or rounded shapes.
- Wooden shutters and doors.

#### **Materials and Colors**

- Stucco, tile roofs, wooden doors, shutters, exposed beams, and wrought iron.
- Earth tones such as white, cream, beige, brown, and terracotta with darker accent colors where appropriate.

Figure 2-9. Examples of Spanish/Mediterranean Revival Architectural Style



### **Art Deco**

The Art Deco architectural style emerged in the early twentieth century and is characterized by its bold geometric shapes, streamlined forms, and lavish ornamentation. Influenced by various artistic movements, the art deco style often features symmetrical façades, stepped setbacks, and decorative motifs such as sunbursts, symmetric lines, and chevrons. Common materials used in Art Deco Architecture include concrete, steel, glass, and polished stone, which are employed to create sleek and glamorous structures. The style is known for its opulence and luxury, with buildings incorporating intricate detailing, stylized sculptures, and vibrant color schemes to evoke a sense of modernity and sophistication.

Elements of the Art Deco style can be found in multi-story office buildings in the Town Center Drive Subarea.

### **Overall Building Design**

- Bold geometric shapes, symmetrical compositions, and sleek lines.
- Streamlined forms with a vertical emphasis, sometimes incorporating setbacks to create tiered articulation.
- Creates a sense of classic urban design, sophistication, and luxury.

### **Walls**

- Smooth and often adorned with decorative elements such as geometric patterns, stylized motifs, and/or brushed metal.
- Stucco, plaster, or polished stone may be used to create a sleek and elegant finish.
- Emphasis on vertical elements and lines.

### **Roofs**

- Flat or low-pitched, with articulated decorative elements.
- Parapets or stepped setbacks.

### **Articulation and Decorative Elements**

- Lavish ornamentation and decorative elements.
- Stylized motifs such as sunbursts, chevrons, and geometric patterns.
- Reliefs, intricate metalwork, and decorative grilles.

### **Windows and Doors**

- Arranged in symmetrical patterns and may feature geometrically-shaped openings.
- Large windows that may be set within metal frames, often with decorative mullions or grilles.
- Doors may also incorporate geometric motifs and decorative details, such as elaborate handles or carved panels.

### **Materials and Colors**

- Variety of materials used in contrasting combinations.
- Smooth or polished stone, concrete, stucco, metal, and glass.
- Bold and vibrant colors with rich jewel tones such as emerald green, sapphire blue, ruby red, and gold are frequently used to accentuate the decorative elements.
- Emphasize the sense of opulence and luxury.



Figure 2-10. Examples of Art Deco Architectural Style



### 3. New Architectural Style

The following reflects a new style that is not currently contemplated by the CC&DG within the community of Valencia, and no examples exist within the SPA.

#### *Neoclassical*

The Neoclassical architectural style, prevalent from the late-18<sup>th</sup> to the early-19<sup>th</sup> century, is characterized by a revival of classical Greek and Roman architectural principles and motifs. Buildings in this style often feature symmetrical façades, columns, pediments, and other elements inspired by ancient Greek and Roman architecture. Neoclassical structures exude a sense of grandeur, order, and balance, with an emphasis on proportion, symmetry, and harmony. Common materials used in Neoclassical Architecture include stone, marble, and stucco, which are employed to create imposing and dignified edifices. This style became particularly popular for civic buildings, government institutions, and monuments, reflecting the ideals of democracy, reason, and enlightenment.

#### **Overall Building Design**

- Grand and impressive designs that emulate symmetry.
- Symmetrically proportioned with a sense of balance and harmony.
- Prominent features such as columns, pediments, and porticos.

#### **Walls**

- Stone or brick that gives the appearance of solidity and permanence.
- Smooth and may be adorned with decorative elements such as pilasters or columns.

#### **Roofs**

- Flat or low-pitched with simple profiles.
- Parapet or balustrade.

#### **Articulation and Decorative Elements**

- Classical ornamentation and decorative elements.
- Columns—Ionic, Doric, and Corinthian orders supporting entablatures.
- Pediments, friezes, cornices, and balustrades.

#### **Windows and Doors**

- Arranged in symmetrical patterns.
- Windows may be framed by pilasters or decorative moldings.
- Doors often feature pediments or entablatures supported by columns or pilasters.

#### **Materials and Colors**

- Materials that give the appearance of solidity and permanence including stone, marble, or brick.
- Materials left in their natural state or painted in muted earth tones such as white, beige, or gray.
- Occasionally, colorful accents or decorative elements may be added.



Figure 2-11. Examples of Neoclassical Architectural Style





#### 4. Selection and Demonstration of Style

Architectural styles should be implemented throughout the SPA in a manner that promotes cohesion and identity. However, to provide balance and interest, no single style should be implemented repetitively within a single subarea.

All development projects within the SPA must include a narrative that details how proposed structures comply with the guidelines of the selected architectural styles.

#### 5. Iconic Buildings

Identified by the General Plan as a major community center, the SPA represents one of the most significant and recognizable destinations in the Santa Clarita Valley and northern Los Angeles County. Home to some of the largest and tallest structures in the City, the scale of existing development within the SPA lends itself to the inclusion of buildings that make an architectural statement. These types of structures should exhibit significant articulation, ornamentation, features, height, and mass within the chosen architectural style that serve as primary anchor points for signature vistas. Iconic buildings define the spaces around them, acting as major landmarks and providing a regional signature for the SPA.

As examples, the Old Town Newhall Library and Newhall Crossings complex in Old Town Newhall are iconic buildings within the context

of the Old Town Newhall Specific Plan. For comparison, iconic buildings within the SPA would reflect the context and character reflective of requirements outlined in the TCSP.

#### 6. Additional Standards

- Three hundred sixty (360) degree architectural treatments must be included.
- Façades fronting peripheral streets and major connectors must contain additional architectural treatments.
- In addition to colored elevations, three-dimensional architectural renderings must be provided for projects exceeding two stories or 35 feet in height.
- All three-dimensional renderings must be provided in color.
- Renderings must demonstrate the structure(s) from the northern, southern, eastern, and western elevations, plus other elevations deemed necessary by the Director of Community Development.
- When requested by the City, renderings must demonstrate projects with associated landscaping at time of completion, and at a maturity 10 years after the time of building completion.

**7. Other Architectural Elements – Lighting**

- General lighting standards throughout the SPA shall be consistent with those identified in UDC Section 17.51.050 (Outdoor Lighting Standards).
- With individual projects, lighting fixtures shall comply with the architectural style of the project they are associated with.
- Lighting must comply with the standards included in Table 2.3.
- All light standards along the extension of Town Center Drive, within the Valencia Town Center Subarea, and Town Center East Subarea must be of consistent design and height with light standards along the existing alignment of Town Center Drive between McBean Parkway and the western entrance of the Valencia Town Center Mall.

**Table 2-3. Lighting Standards**

Lighting Type	Maximum Height	Appropriate Locations	Other Requirements
Pedestrian Scale Lighting	14 feet	Furnishing Zone, pedestrian bridge landing areas, plazas, paseos, and other public gathering areas	
Streetlights (double mast arms)	25 feet	Median	<ul style="list-style-type: none"> <li>• To be consolidated with traffic signal(s) where possible</li> <li>• Provide for banners</li> </ul>
Streetlights (single mast arms)	25 feet	Bulb-out areas	<ul style="list-style-type: none"> <li>• Maximum one per block at the intersection</li> <li>• To be consolidated with traffic signal(s) where possible</li> </ul>
Bollards	4 feet	Furnishing Zone, pedestrian bridge landing areas, pedestrian/multiuse paths, plazas, paseos, and other public gathering areas	
Sculptural Lighting	N/A	Roundabouts, plazas, bulb-outs, entrances, gateway elements	<ul style="list-style-type: none"> <li>• As approved by the Director of Community Development</li> </ul>
String Lights	N/A	Paseos, plaza areas	<ul style="list-style-type: none"> <li>• Minimum 10-foot clearance from ground</li> </ul>
Landscape Lighting	1 foot	Any landscaped area	

**8. Other Architectural Elements – Signage**

- General signage standards throughout the SPA shall be consistent with those identified in UCD Section 17.51.080 (Sign Regulations), unless otherwise identified by this Specific Plan.
  - Sign programs for individual subareas and/or projects are encouraged. Sign programs for the Valencia Town Center and Town Center Drive must be complementary or consistent.
  - Informational and wayfinding signs must be consistent in size and design within the Valencia Town Center Drive and Town Center Drive Subareas.
  - External informational and wayfinding signs may have a maximum dimension of 6 feet in height by 4 feet in width and be two-sided. Digital content is permissible, but only for signage that is not visible from the public right-of-way and not located within the parkway zone within the public realm.
  - One monument sign may be located at the main entrance to each primary entrance of individual developments within the SPA.
- One monument sign may be located at gateways and must not exceed the following dimensions:
    - Major Gateway: 8 feet in height by 15 feet in width.
    - Minor Gateway: 6 feet in height by 8 feet in width.
    - Monument signs are encouraged to be incorporated into decorative architectural features. Architectural features are not considered as part of the sign area.
  - For any type of structure (including parking structures) of three stories or more, and in addition to a primary wall sign, a single projecting sign may be approved subject to a Sign Review Permit and the following development standards:
    - For buildings of three or four stories, the projecting sign may have a maximum dimension of 12 feet in height by 4 feet in width, and a minimum ground clearance of 15 feet.
    - For buildings of five to seven stories, the projecting sign may have a maximum dimension of 25 feet in height by 4 feet in width, and a minimum ground clearance of 20 feet.



## ELEMENT PM4 – BUILDING SPECIFICATIONS

### DESCRIPTION

Appropriate, practical, and purpose-driven development standards for the built environment will incentivize a balanced mix of commercial and residential uses and, in concert with other design elements, create great and unique places throughout the SPA. Development standards for building specifications include:

1. Setbacks to create comfortable pedestrian environments, and establish distances between buildings and other features including property lines, roads, and other buildings;
2. Density requirements for commercial square footage and the quantity of residential units to ensure the intensity of development within the SPA will be consistent with the General Plan;
3. Building height requirements to help define the SPA aesthetically and architecturally, and focus on creating mixed use development; and
4. A palette of building types and building orientation standards to provide guidance for functional spaces and maximize aesthetics throughout the SPA.

### CONTEXT

The underlying General Plan land use designation as well as the zoning designation of the entirety of the SPA is CR. The CR zone is defined as being *“...applied to central and regional commercial districts in the planning area, generally located around the Valencia Town Center and other major community centers. This designation is intended to promote the development of regional focal points for commercial, entertainment, cultural, and business uses serving the public and drawing from a market area encompassing the entire Santa Clarita Valley. Multiple-family dwellings...may be allowed in this zone.”*

The CR zone represents the most intensive commercial and residential zone within the City, and contemplates twice the commercial density of other zones, while also contemplating the highest residential density of up to 50 units per acre. Commercial and residential density specifications for the TCSP are consistent with those of the CR zone.

The entirety of the SPA is also included within the JCOZ. Various design incentives are included in the JCOZ to promote the creation of projects that provide high-quality employment opportunities.

Note that for the purposes of the TCSP, mixed use includes:

1. A mixture of residential and commercial uses;
2. A mixture of a hospitality uses and residential or other commercial uses; or
3. A combination of office use and other types of commercial uses within the same building.

### LOCATION

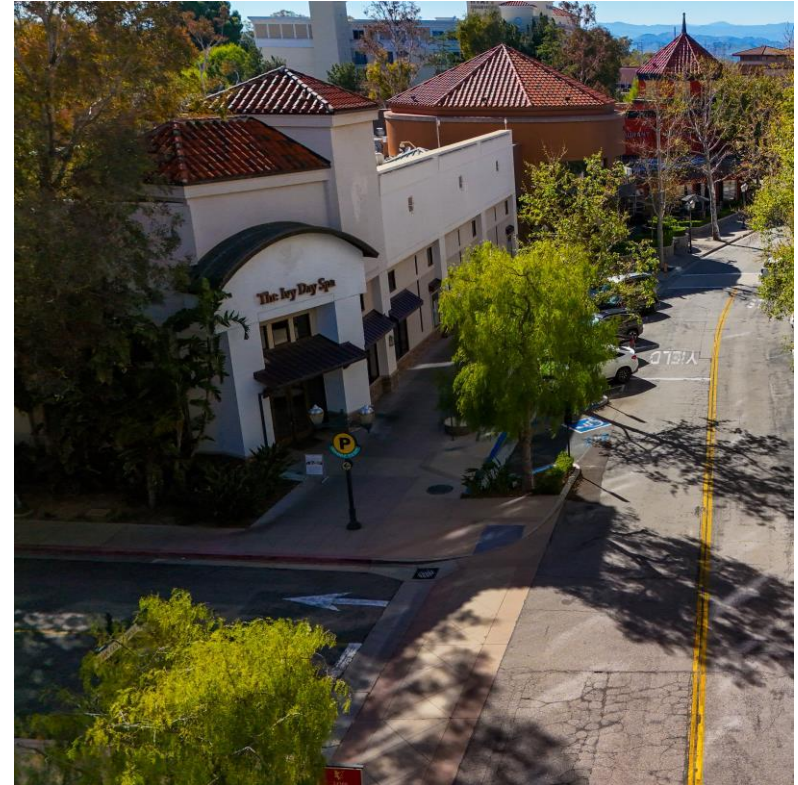
Specifications for buildings apply to all new development and redevelopment projects within the SPA.

## EXECUTION

Projects within the SPA are subject to the following requirements:

### 1. Setbacks

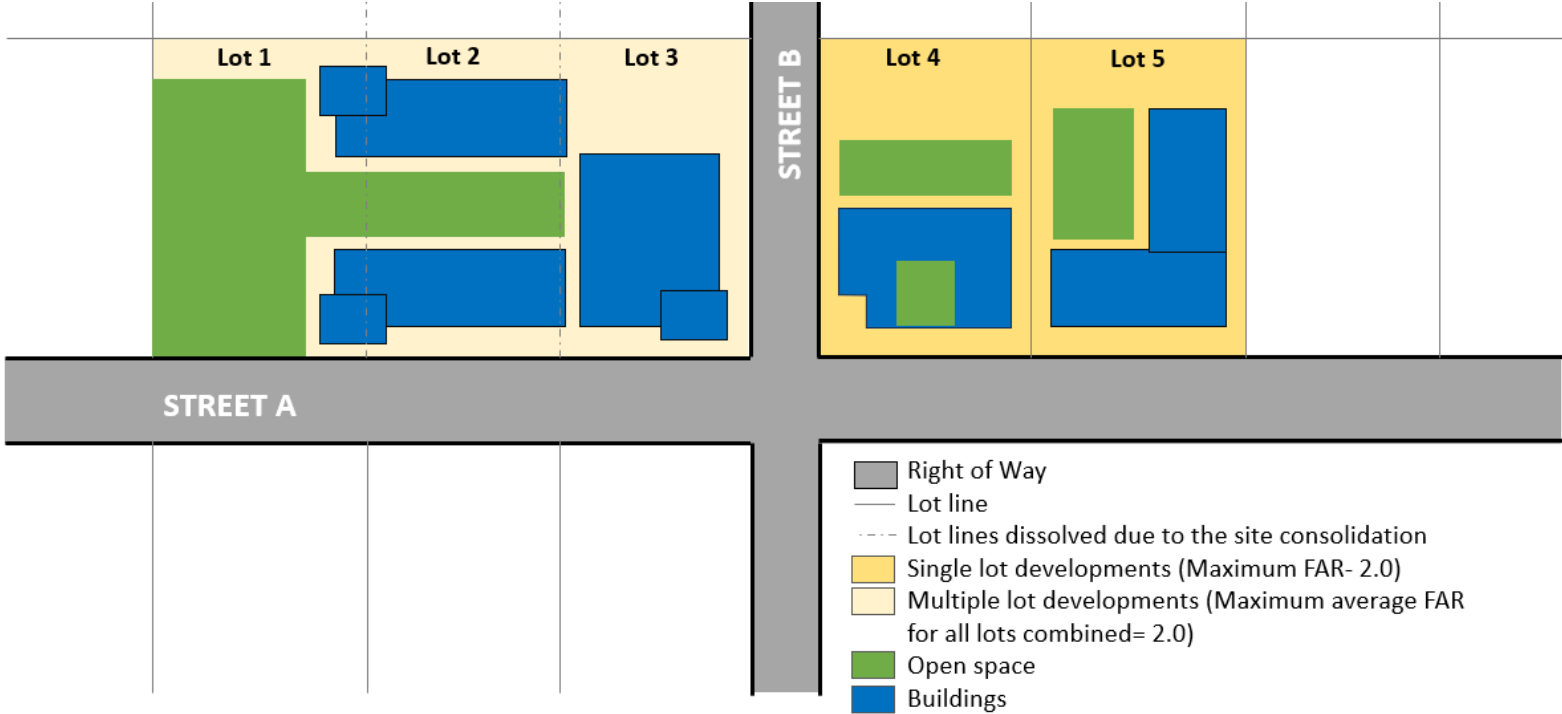
- General setback requirements throughout the SPA are consistent with Chapter UDC 17.53 (Property Development Standards – Commercial and Industrial), 17.55 (Property Development Standards – Mixed Use), and 17.57 (Property Development Standards – Residential) unless otherwise identified by this Specific Plan.
- Setbacks from major connectors and internal roads must comply with the Element MO2 Pedestrian Streetscape which defines the widths of the Sidewalk Zones (Parkway, Pedestrian and Activity zones). Where there are inconsistencies with UDC, the Specific Plan will prevail.



2. Floor Area Ratio (FAR)

- The maximum FAR for the overall SPA must not exceed 2.0.
- The maximum FAR for developments within the SPA must not exceed 2.0 as indicated by Figure 2-12.

Figure 2-12. FAR



Source: City of Santa Clarita, Michael Baker International



### 3. Residential Density

- The maximum residential density for the overall SPA must not exceed 50 units per acre.
- Residential, or mixed-use projects with a residential component, must not exceed a density of 50 residential units per acre and not include less than 18 residential units per acre.

### 4. Building Height

The following building height thresholds are permitted by right for the following structures:

- Structures containing professional and/or medical office uses only – six stories.
- Structures containing a mix of commercial uses – seven stories.
- Structures containing multi-family residential uses only – three stories.
  - o An additional floor of residential may be added with the inclusion of a level of ground floor commercial use(s), for a total of five stories.
  - o Two additional levels of residential may be added with the inclusion of two lower levels of commercial land uses, for a total of seven stories.
  - o In lieu of the inclusion of a commercial floor, multi-family projects or residential mixed-use projects may add an additional floor of residential by providing a significant community amenity to the SPA above and beyond those already required as part of the development. The type or combination of types of community amenities provided must be determined by the Director of Community Development and is subject to an Administrative Permit, with notification to the Planning Commission and City Council on the date action is taken. Examples of significant community amenities include, but are not limited to: 1) providing a public space within the SPA; 2) providing a major gateway feature; 3) the addition of a new or relocation of an existing pedestrian bridge; 4) providing an improvement to pedestrian bridge landing zones; 5) providing a transit hub; and/or 6) constructing the extension of Town Center Drive.
- Structures containing hospitality uses only – six stories.
  - o An additional floor of hospitality may be added with the inclusion of meeting space plus a single ballroom measuring at least 15,000 square feet.
- Expansions to Valencia Town Center Mall – 55 feet.
- Stand-alone retail structures – 40 feet.
- Maximum height for monumental architectural elements – 40 feet.
  - o Monumental architecture includes statement elements that are used as focal points within public spaces and signature vistas, and serve as visual anchors within the SPA. Examples include, but are not limited to, large fountains, arches, spires, and public monuments.
- Wrapped parking structures – Equivalent to the number of floors included in the wrapped portion of the structure. Note that unwrapped portions of parking structures must not constitute the primary frontage of the building.
- Unwrapped parking structures developed in conjunction with other buildings – five stories or the maximum height of the building(s) the parking structure is associated with, whichever is less.
- Unwrapped parking structures not developed in conjunction with other buildings – three stories.
- At the discretion of the Director of Community Development, treatments that exceed the maximum height thresholds

defined by this section may be approved without a Conditional Use Permit provided that: 1) the addition does not exceed 10 feet in height; 2) the allowance would be compatible with the architectural design of the associated project; and 3) the allowance would provide additional architectural articulation and/or screening of rooftop equipment that could not otherwise be achieved within the height thresholds defined by this section.

- Total height of the structure must be measured from the lowest adjacent grade to the highest point of the structure. Loading docks that extend below the grade of the finished floor must not be counted as the lowest grade.
- To avoid structural monotony and increase visual interest, projects must include the use of non-repetitive building heights and unique rooflines.

## 5. Building Types

The following types of structures are encouraged within the SPA. Where appropriate, several of these building types may be incorporated into a single structure.

### *Podium/Mixed-Use Buildings*

Podium/mixed-use buildings are structures that incorporate a vertical mix of multiple functions within a single structure. These buildings typically include apartments and condominiums on the upper floors and commercial spaces including retail stores, offices, restaurants, and cafés on the lower floors. Other potential configurations may include a mix of commercial uses with office or hospitality space occupying the upper floors and retail uses on the lower floors.

**Figure 2-13. Podium/Mixed-Use Building Example (mixed-use)**



*Location: Bethany, OR*

**Wrap Buildings**

Wrap buildings refers to a residential, commercial, or mixed-use building constructed to surround or partially enclose a parking structure. This design provides more convenient parking for tenants of the wrapped portion of the structure and is a more aesthetically pleasing alternative to stand-alone parking structures.

**Figure 2-14. Wrap Building Example**



Location: San Marcos, CA

**Courtyard Buildings**

A courtyard building refers to a structure where one or more buildings are arranged around a central open space or courtyard. The courtyard is typically an enclosed or semi-enclosed area that serves as a focal point within the building complex. It provides an area for social interaction, relaxation, or recreational activities.

**Figure 2-15. Courtyard Building Example**



Location: Arlington, VA



### ***Freestanding Structured Parking with Commercial***

Freestanding structured parking refers to a type of parking facility that is not directly attached to another structure. It is a stand-alone parking structure that is designed to accommodate vehicles. This type of parking facility is often multilevel and can be constructed independently, providing parking spaces for cars in a designated area. The ground floor of the structure can be partly or fully dedicated to commercial use, thus activating the pedestrian realm.

**Figure 2-16. Example of Freestanding Parking Garage with Commercial on the Ground Floor**



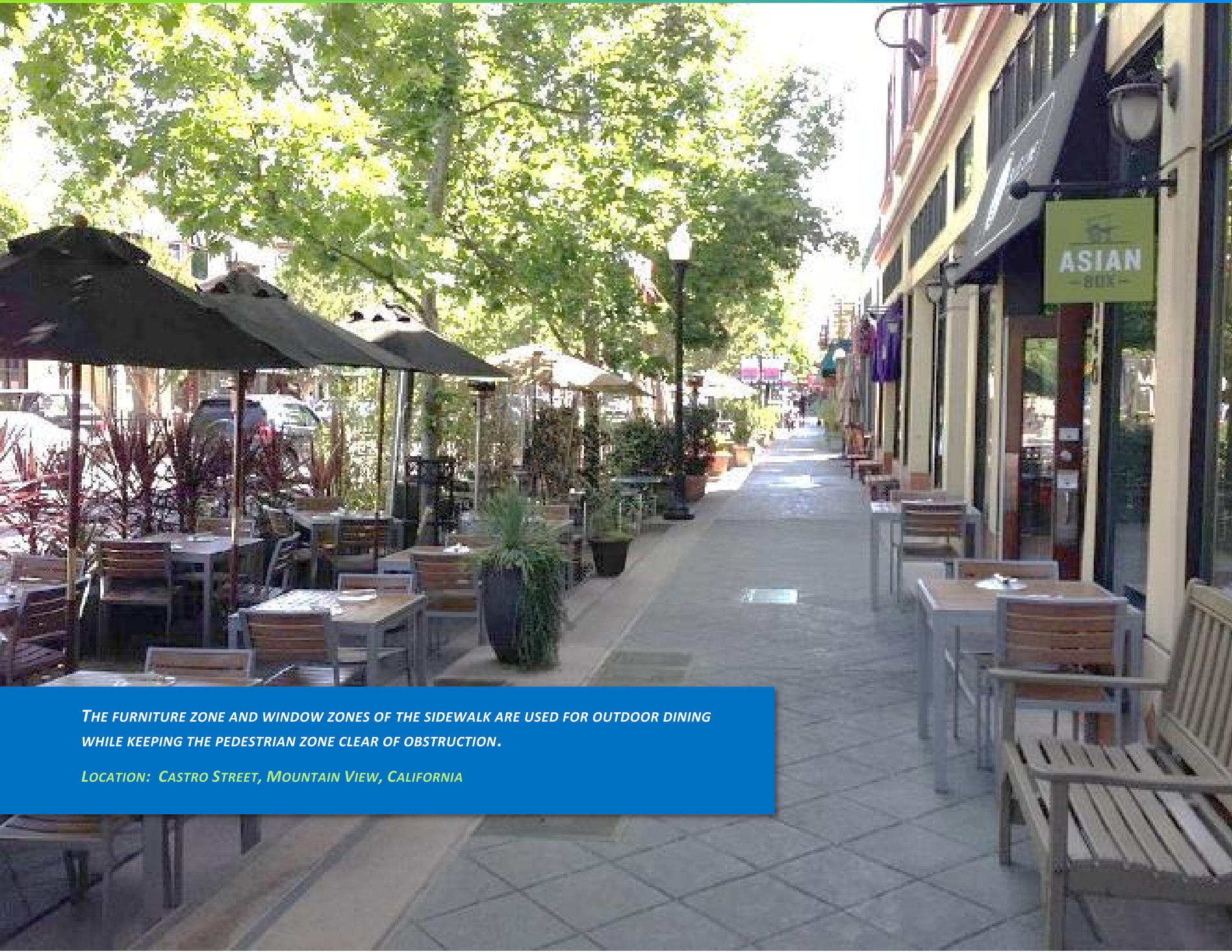
*Location: Athens, GA*

### **6. Building Orientation and Access**

- All buildings along streets, whether a major connector or internal road, must have their main façades oriented toward the highest order of street level in accordance with the street hierarchy.
- Entry to garages, parking lots, and loading areas are encouraged to be from internal roads, and not from a major connector.

### **7. Landscaping**

- Landscaping for projects must be consistent with the requirements in UDC 17.51.030 (Landscaping and Irrigation Standards).



*THE FURNITURE ZONE AND WINDOW ZONES OF THE SIDEWALK ARE USED FOR OUTDOOR DINING WHILE KEEPING THE PEDESTRIAN ZONE CLEAR OF OBSTRUCTION.*

*LOCATION: CASTRO STREET, MOUNTAIN VIEW, CALIFORNIA*

## ELEMENT PM5 – GATHERING SPACES

### DESCRIPTION

Gathering spaces that are conveniently located throughout the SPA, that utilize architecture and building mass to define space, and that act as focal points along significant vistas, all contribute to creating great places that foster community involvement, encourage pedestrianism, create vibrant outdoor settings for retail and dining, establish community character, and create recognizable landmarks. The TCSP establishes a hierarchy of gathering areas including large public serving spaces, more intimate neighborhood-focused spaces, and smaller private spaces that will serve both the entire community as well as residents and employees within the SPA.

### CONTEXT

Although community events are held regularly within the Valencia Town Center and Town Center Drive Subareas, there are no large, dedicated public spaces within the entirety of the SPA. Event examples, including viewing 4<sup>th</sup> of July Fireworks, a seasonal circus, and a seasonal ice-skating rink, are generally held within the large surface parking lot adjacent to the Valencia Town Center Mall or on a portion of Town Center Drive.

### LOCATION

At least one large public space must be developed within the Valencia Town Center Subarea, but others may be located in any area of the SPA. Common spaces must be located throughout the SPA, and each should be located within a convenient walking distance of other common spaces. Residential tenant and private spaces are required for each project that include a multi-family residential component.

### EXECUTION

The following development standards apply to the creation of gathering spaces:

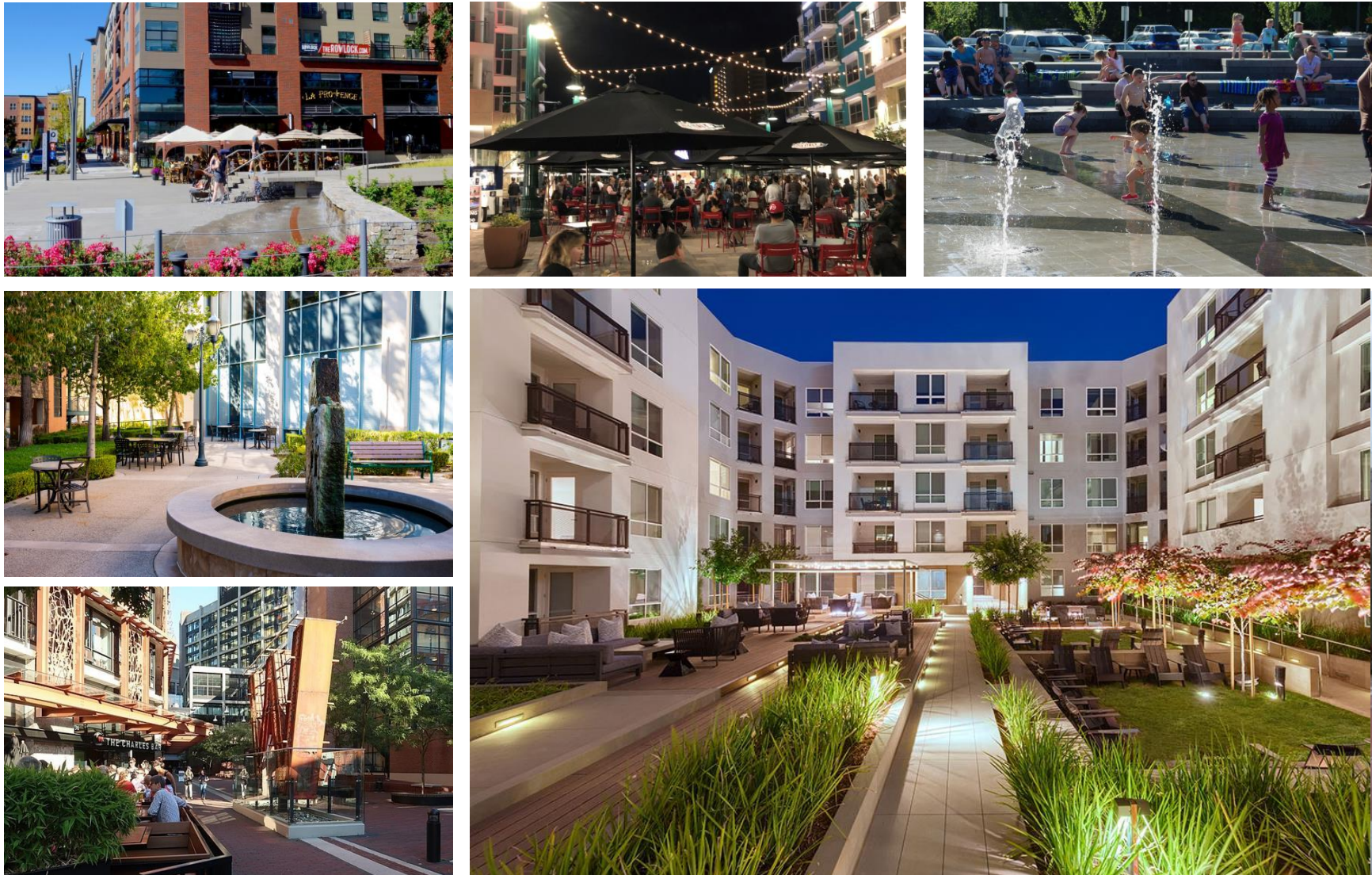
#### 1. Public Gathering Spaces

Public gathering spaces are large areas intended for the use and enjoyment of the general public. Public gathering spaces:

- Should be large enough to be programmed for community events including concerts, farmers and specialty markets, viewing 4<sup>th</sup> of July fireworks, plays, lectures, art displays, and celebrations. At least one public gathering space must be provided within the Valencia Town Center Subarea.
- May use hardscape, turf, synthetic surfaces, or a combination of these as appropriate.
- Must be at least one acre in size.
- Are encouraged to be located along or near a major connector.
- Are encouraged to be located adjacent to or near an iconic building.
- Are encouraged to be lined by ground floor dining and retail uses to create a vibrant outdoor environment.
- Must have significant pedestrian and bicycle connectivity to other portions of the SPA.
- Must have convenient access to parking and transit services.
- Must be the focal point of a signature vista.
- Must be bordered by buildings with significant massing in order to create a defined space.
- Are encouraged to be located in conjunction with other venues or event spaces such as a hospitality use with conference/convention space.
- Are encouraged to contain community amenities including monumental architecture and public art.



Figure 2-17. Examples of Small and Large Public Gathering Spaces



**2. Common Spaces**

Common spaces are required areas intended for the use and enjoyment of both the general public and residents of multi-family and mixed-use buildings in their vicinity. Common spaces:

- Are smaller and more intimate than public spaces and may include village lawns, well-shaded gathering areas, playgrounds, areas for small musical performances,

appropriately-sized water features or fountains, gardens, and sitting areas.

- Must be at least 2,500 square feet in size and can be counted toward the overall requirement for residential tenant spaces (below).
- May be provided as many individual spaces, or fewer larger spaces.
- Are encouraged to use architecture of surrounding buildings to help define the space.
- Are encouraged to be either the focal point of a significant vista or located along a visual axis.
- May use hardscape, turf, synthetic surfaces, or a combination of these as appropriate.
- Are encouraged to be lined with or near neighborhood-serving retail, dining, or services to help engage the space.
- Must have excellent pedestrian access and be located within a convenient walking distance from other common spaces.

**3. Residential Tenant Spaces**

Residential tenant spaces are shared areas accessible only to the residents of multi-family or mixed-use developments and may include fitness centers, clubhouses, multipurpose rooms, swimming pools, rooftop decks, lounge areas, common courtyards, barbecue areas, among others. The total square footage for tenant spaces must be calculated via the following ratios:

- 50 square feet / Studio unit
- 75 square feet / One-bedroom unit
- 100 square feet / Two-or-more bedroom unit
- Square footage for residential tenant space may, instead, be partially or totally committed toward common space.

**4. Private Spaces**

Private spaces are areas accessible to individual tenants of multi-family or mixed-use developments and generally included as balconies. The amount of private space provided per unit type is defined by the following:

- 25 square feet / Studio unit
- 38 square feet / One-bedroom unit
- 50 square feet / Two-or-more bedroom unit.
- Square footage for private space may, instead, be partially or totally committed toward common space or residential tenant space.



## ELEMENT PM6 – GATEWAYS

### DESCRIPTION

Gateways are specific locations or areas that serve as entry points to the SPA. Gateways are distinct, welcoming features that signal the transition from the surrounding area and establish a tone that reflects what those entering the SPA can expect.

### CONTEXT

There are currently five gateways leading into the SPA, and three potential gateway locations pending future development. Major gateways represent primary entrances into a subarea at signalized intersections of major public streets, while minor gateways represent secondary entrances without signalized intersections.

Existing major gateways include: (Figure 2-18)

- Town Center Drive and McBean Parkway entering the Town Center Drive Subarea.
- Mall Entrance and Valencia Boulevard entering the Valencia Town Center Subarea.
- Cheri Fleming Auto Center Drive and Magic Mountain Parkway entering the Valencia Town Center Subarea.

Existing minor gateways include: (Figure 2-18)

- Mall Entrance and McBean Parkway entering the Town Center Drive Subarea.
- Theater Drive and Magic Mountain Parkway entering the Town Center Drive Subarea.

Future major gateways are anticipated at the following locations: (Figure 2-18)

- **Citrus Street at the intersections of Magic Mountain Parkway and Valencia Boulevard.** These gateways would mark the entrance to the primary north-south vehicular corridor bisecting the SPA.
- **Town Center Drive at Valencia Boulevard.** This gateway would be created with the potential future extension of Town Center Drive east of Citrus Street. This gateway would enter the Town Center East Subarea.

### LOCATION

Specifications apply to all new and refreshed gateways throughout the SPA.

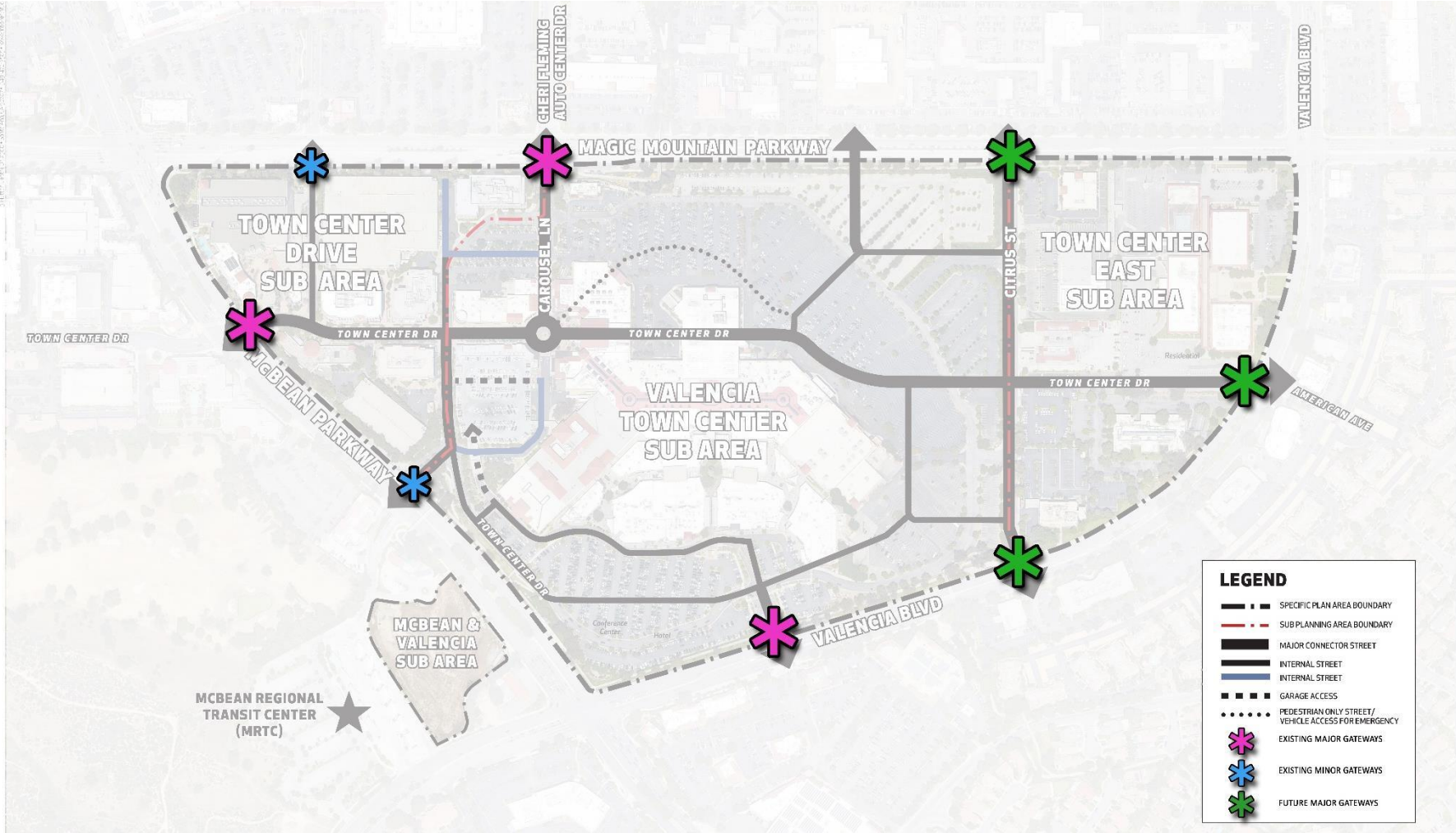
### EXECUTION

All existing gateways are encouraged to be refreshed and new gateways must be developed according to the following standards:

1. At major gateways, design should be grand, include the use of monumental architecture, capturing and project a sense of welcoming to the SPA. An appropriate aesthetic and thematic approach should be used.
2. Major gateways should feature both sides of the entrance, either by spanning the roadway or using vertical elements on both sides of the roadway.
3. Emphasis should be on vegetation around gateway elements.
4. Gateway elements should create an attractive and interesting view that leads visually into the SPA.
5. Gateway elements should encourage pedestrian traffic around and into the SPA.



Figure 2-18. Existing and Future Gateways



Source: City of Santa Clarita, Michael Baker International, DLR Group, ESRI

Figure 2-19. Examples of Gateway Features



Location: Carmel Arts and design District, Carmel, IN



Location: Fulton Market District, Chicago, IL



## MOBILITY

An overall mobility network that provides safe, efficient, convenient, and attractive facilities for vehicle, bicycle, and pedestrian traffic is critical to the daily operation of development within the SPA. A successful mobility network will:

- Create great places by establishing visual axes for significant vistas.
- Define great spaces by creating vibrant, pedestrian-scale street scenes.
- Create a robust east-west connection through the SPA with the required extension of Town Center Drive to Citrus Street and, potentially, to Valencia Boulevard.
- Provide key linkages for multiple modes of transportation throughout the SPA.
- Integrate into existing City trails, pedestrian bridges, and sidewalks surrounding the SPA.
- Enhance opportunities for businesses by providing efficient multi-modal access to and throughout the SPA.
- Contribute to a “park once” strategy, by creating better pedestrian and micro-mobility connectivity between subareas within the SPA.
- Enhance on-site safety while reducing traffic congestion and air pollution.

## ELEMENT MO1 – ROADWAY NETWORK

### DESCRIPTION

The basis for realizing the TCSP’s vision starts with appropriately-sized development blocks that utilize orientation of structures, architecture, and an internal road network centered around a modified grid pattern. Combined, these factors provide the foundation for creating great places, compelling site lines, and enhanced visual interest throughout the SPA. The foundation for this is based upon the establishment of a hierarchy of roads with appropriate design features that not only establish the overall shape of development, but also provide for the safe and efficient movement of vehicle, bicycle, and pedestrian traffic. Further, appropriate roadway design helps define the larger pedestrian space within the public realm and create signature vistas throughout the SPA.

### CONTEXT

Currently, no meaningful grid pattern of internal roads exists within the SPA other than within the Town Center Drive Subarea. The Valencia Town Center Subarea is characterized by a large surface parking lot with a ring road for vehicular traffic within the site. The Town Center East Subarea is comprised of various independently-owned properties with little or no connectivity. Finally, the McBean and Valencia Subarea is currently vacant. The inclusion of appropriate development block sizes and a modified grid pattern of internal roads will serve to guide development in an organized manner that will organically create great places.

### LOCATION

The established roadway hierarchy and additional development standards for roadway design apply to all subareas within the SPA.



**EXECUTION**

All roadway improvements within the SPA are subject to the following development standards:

**1. Street Hierarchy**

The hierarchy of streets and roads surrounding and within the SPA include:

**Peripheral Streets**

Peripheral streets include existing public rights of way that border or bisect the SPA including Magic Mountain Parkway, McBean Parkway, Valencia Boulevard, and Citrus Street. These streets are subject to improvement commiserate with impacts to traffic service levels as development within the SPA occurs, and at the expense of the developer. Note that all other road types within the SPA, other than peripheral streets, will be privately-owned and maintained.

**Major Connectors**

Major connectors provide primary access ways through the SPA. The extension of Town Center Drive will serve as the major connector for east-west vehicle, bicycle, and pedestrian traffic. Major connectors are characterized as two vehicle traffic lanes (one in each direction), with Class II bike lanes, on-street parking, and curb extensions/bulb-outs. Major connectors are also bordered by a more significant public realm space than other types of roads within the SPA.

**Internal Roads**

Internal roads include the majority of the on-site network for vehicular travel and also form a critical linkage for bicycle travel throughout the SPA. Internal roads are divided into two types:

- **Type A:** Type A internal roads are characterized by two vehicle traffic lanes (one in each direction) and must include either a Class II bike lane or parallel on-street parking. They also include parkway landscaping within the public realm, in addition to sidewalks. Preference should be given to Type A internal roads that provide a Class II bike lane to establish a robust network of bicycle paths throughout the SPA, connect to bicycle lanes

on major connectors, and ultimately integrate with the City’s surrounding trail network. Type A internal roads are the preferred type of internal roads, and should be located in more visible areas throughout the SPA.

- **Type B:** Type B internal roads are characterized by two vehicle traffic lanes (one in each direction) which are adjacent to the curb and sidewalk. Type B internal roads are intended to provide simple vehicle travel in less prominent areas within the SPA.

**2. Street Specifications**

Major connectors and internal roads must comply with the following specifications:

Table 2-4. Street Specifications

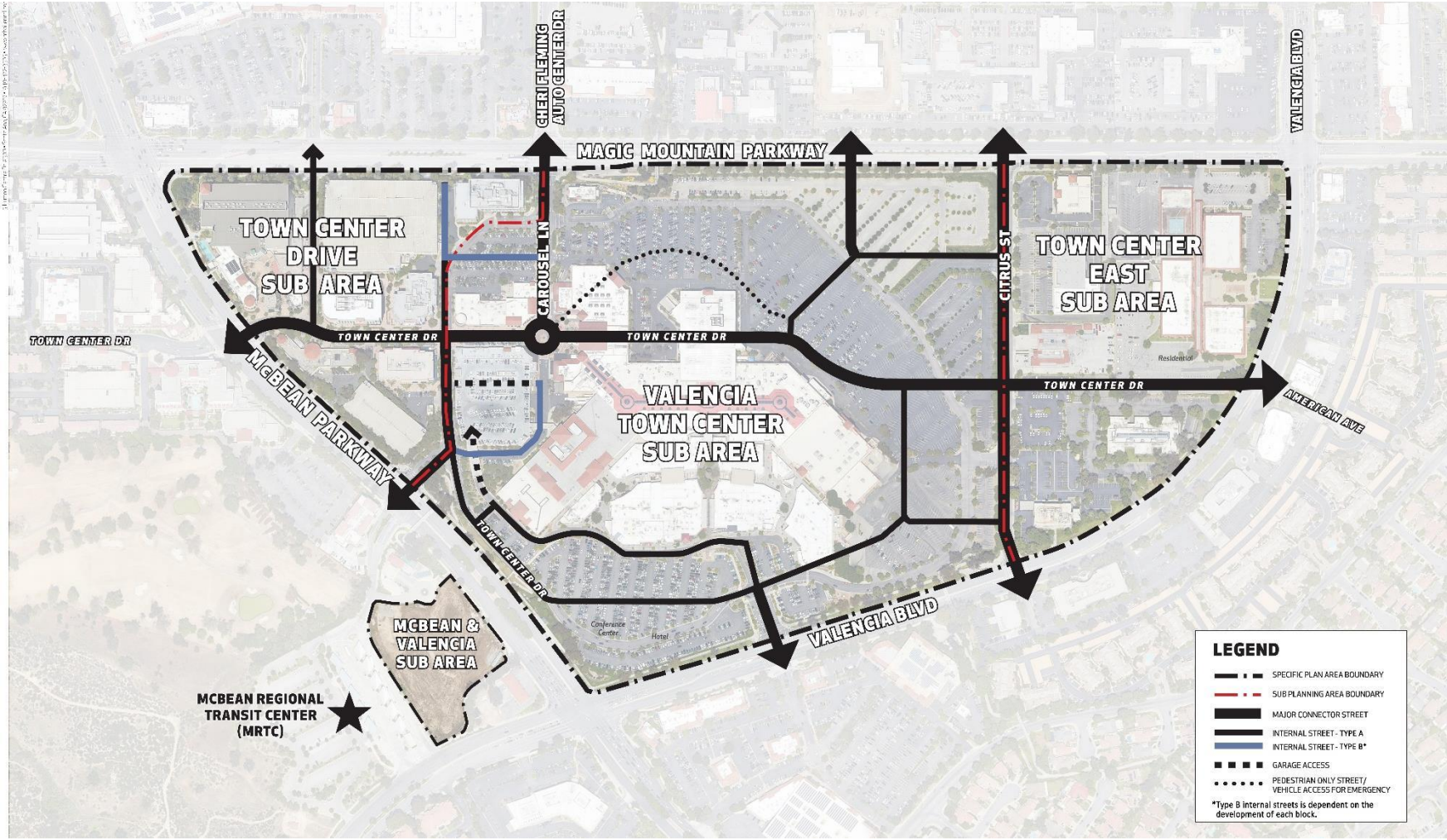
Specification	Major Connectors	Type A Internal Road	Type B Internal Road
Number of Lanes	2	2	2
Curb-to-Curb Width	60 to 72 feet	38 feet	26 feet <sup>1</sup>
Minimum Lane Width	12 feet	11 feet	11 feet
Class II Bike Lane Width	6 feet	6 feet <sup>2</sup>	--
Class II Bike Lane Buffer Width	Minimum 2 feet	Minimum 2 feet	--
On-Street Parking Width	16 feet – Angled <sup>3</sup> 10 feet - Parallel	10 feet – Parallel <sup>2</sup>	--

<sup>1</sup> Also subject to Los Angeles County Fire Department requirements.

<sup>2</sup> Type A internal roads must include either Class II bike lanes or parallel parking, with Class II lanes being preferred.

<sup>3</sup> Major connectors must include on-street parking.

Figure 2-20. Map of Suggested Roadway Network



Source: City of Santa Clarita, Michael Baker International, DLR Group, ESRI



Figure 2-21. Cross-Section of Major Connector with Angled Parking



Source: City of Santa Clarita, Michael Baker International, Streetmix

Figure 2-22. Example of Major Connector



Location: Cambridge, MA

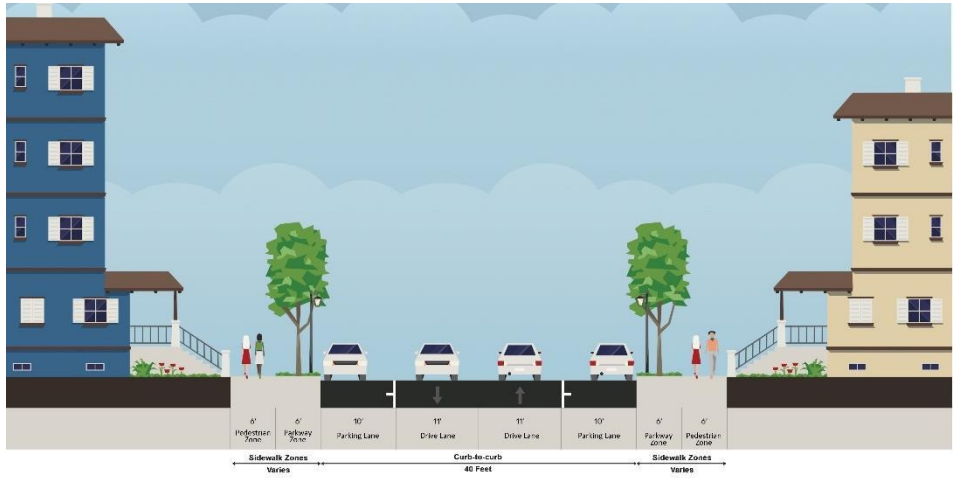


Figure 2-23. Cross-Section of Major Connector with Parallel Parking



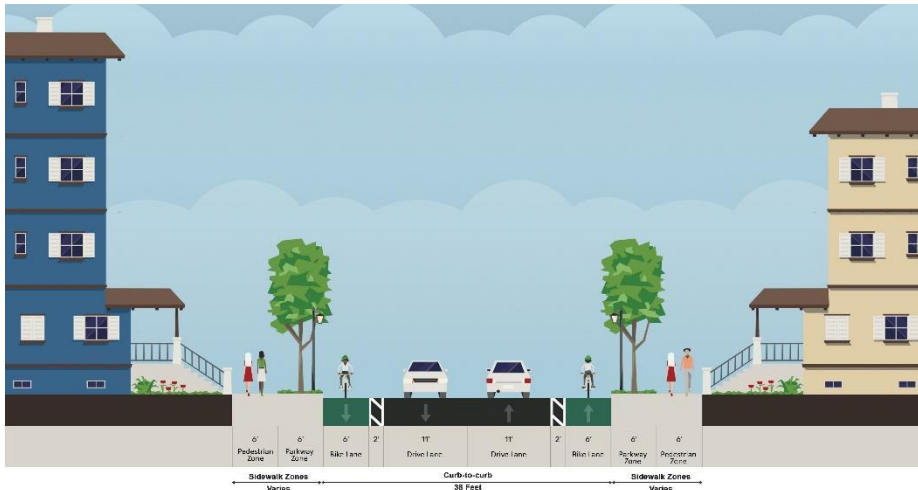
Source: City of Santa Clarita, Michael Baker International, Streetmix

Figure 2-24. Cross-Section of Type A Internal Road with Parallel Parking



Source: City of Santa Clarita, Michael Baker International, Streetmix

Figure 2-25. Cross-Section of Type A Internal Road with Bike Lanes



Source: City of Santa Clarita, Michael Baker International, Streetmix

Figure 2-26. Cross-Section of Type B Internal Road



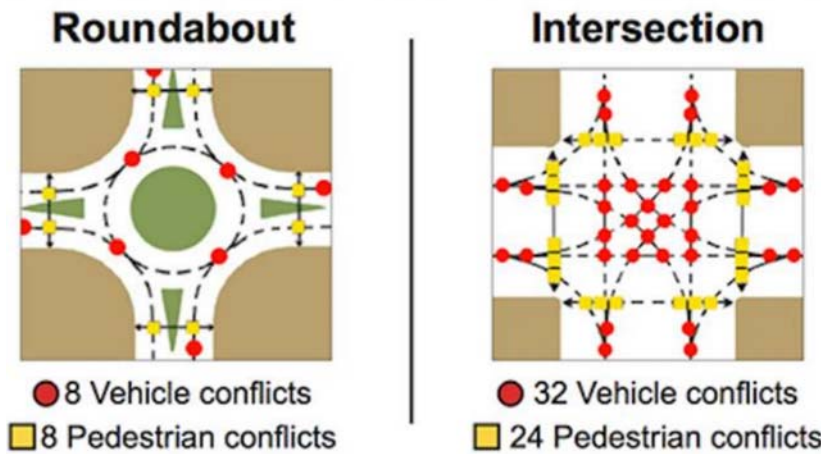
Source: City of Santa Clarita, Michael Baker International, Streetmix

**3. Roundabouts**

Roundabouts are encouraged at intersections throughout the SPA because they serve to reduce vehicle speed, reduce traffic congestion, increase vehicle safety, reduce the number of conflict points with pedestrians and bicyclists, and reduce vehicle emissions when compared to traditional signalized intersections and/or intersections with four-way stops. Roundabouts are subject to the following standards:

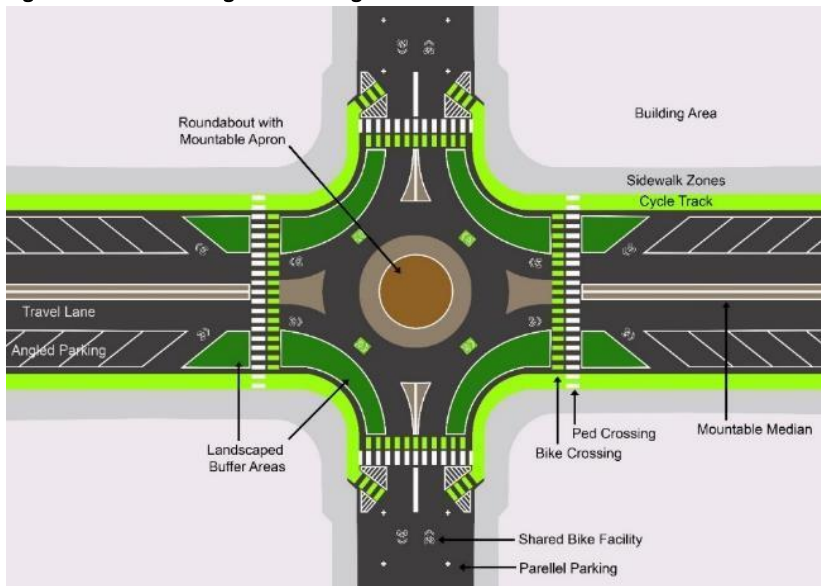
- Roundabouts must be of an appropriate size to fit the context in which they are located and consider issues including, but not limited to, traffic volumes, desired vehicle speeds, and line-of-site in their design.
- Roundabouts must include safe pedestrian crossings and are encouraged to use enhanced concrete/paving designs in lieu of traditional painted crosswalks as noted below.
- Roundabouts are encouraged to be located as focal points or along visual axes within signature vistas.
- Roundabouts must have landscaping, monumental architecture, fountains, public art and/or other architectural features located in their center. However, central features should be designed to not block line-of-sight for vehicle operators using the roundabout.

**Figure 2-27. Roundabout vs Intersection Conflict Points**



Source: [github.com/CitiesSkylinesMods/TMPE/issues/73](https://github.com/CitiesSkylinesMods/TMPE/issues/73), Accessed April 3, 2022

**Figure 2-28. Bike Navigation Through the Roundabout**



Source: Michael Baker International



*ROUNDBABOUTS ARE ENCOURAGED TO BE LOCATED AS FOCAL POINTS OR ALONG VISUAL AXES WITHIN SIGNATURE VISTAS.*

*LOCATION: OLD TOWN NEWHALL, SANTA CLARITA, CALIFORNIA*

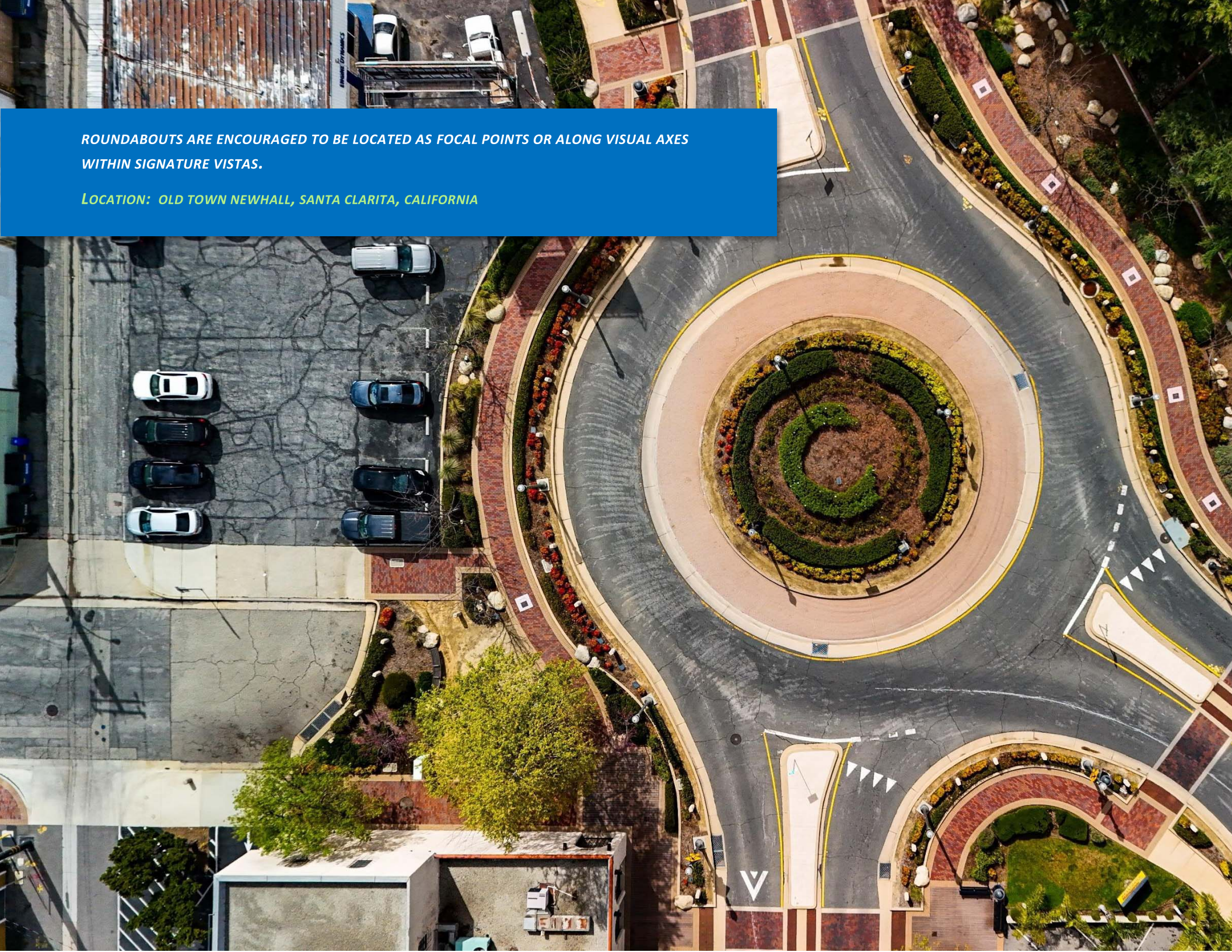




Figure 2-29. Example of Roundabouts Along East-West and North-South Connectors



Location: Santa Clarita, CA

Figure 2-30. Examples of Mini Roundabouts on Internal Streets



Location: Left - Edmonds WA; Right - Maryland

**4. Additional Requirements**

The following are additional requirements related to the roadway network throughout the SPA:

**Bulb-outs**

Bulb-outs enhance pedestrian safety, contribute to traffic calming, and help create a sense of place by adding visual interest to intersections. Bulb-outs throughout the SPA:

- Must be provided at intersections with a major connector.
- Are strongly encouraged to be provided at intersections with Type A internal roads.
- Must provide architectural treatments including landscaping, lighting fixtures, street furniture, and/or non-view obscuring architectural elements such as pony walls.
- Bulb-outs must be provided at all mid-block crosswalks on major connectors.

Figure 2-31. Bulb-out Examples



Location: Ephrata, PA



Location: Bridgman, MI

### Crosswalks

Crosswalks must be provided at all intersections throughout the SPA. Crosswalks are subject to the following design requirements:

- Must be clearly marked and highly visible.
- Must include traffic control measures including, but not limited to, stop signs.
- Must include decorative paving at intersections with major connectors.
- Mid-block crosswalks are encouraged, when appropriate and with traffic control measures, in areas along major connectors where long distances exist between intersections.

Figure 2-32. Highly Visible Crosswalk Examples



Location: Charlotte, NC

### 5. Signature Vistas

Major connectors, and both types of internal roads must be used as visual axes to create signature vistas throughout the SPA.



## ELEMENT MO2 – PEDESTRIAN STREETSAPES

### DESCRIPTION

Creating pedestrian-friendly streetscapes fosters vibrant, livable, and sustainable communities. Well-designed pedestrian infrastructure, such as sidewalks, plazas, and walkways, can encourage social interaction, support local businesses, and create inviting public spaces. This contributes to a sense of community, enhances the quality of life, and improves the overall attractiveness of an area.

Figure 2-33. Green Space Examples



Location: Bethesda, MD

### CONTEXT

The portion of Town Center Drive within the Town Center Drive Subarea and western portion of the Valencia Town Center Subarea represents the only effective street scene in the SPA. It will serve as the foundation for the required extension of Town Center Drive from its current terminus at the western entrance to the Valencia Town Center Mall, through the Valencia Town Center Subarea to Citrus Street as well as a potential extension to Valencia Boulevard.

### LOCATION

Streetscape requirements apply to all major connectors and internal roads within the SPA.

### EXECUTION

Improvements within the public realm are subject to the following development standards.

#### 1. Sidewalk Zones

All roads must contain one or more of the following sidewalk zones, depending on the type of road. The maximum combined width of all sidewalk zones from building façade to curb is 30 feet.

##### *Activity Zone*

When required, the activity zone is located between the façade of a building and the Pedestrian Zone. The activity zone is intended to establish a lively street scene by providing space for outdoor dining, displays of merchandise, and seating areas. Activity Zones have a maximum width of 18 feet, but are encouraged to vary in width along the length of the major connector. Activity Zones may create recesses in the building façade beyond the back of sidewalk, provided that the distance between the façade and the Pedestrian zone is no wider than 18 feet.

##### *Pedestrian Zone*

The pedestrian zone provides a clear, unobstructed sidewalk for pedestrians measuring a minimum of 6 feet in width.

##### *Parkway Zone*

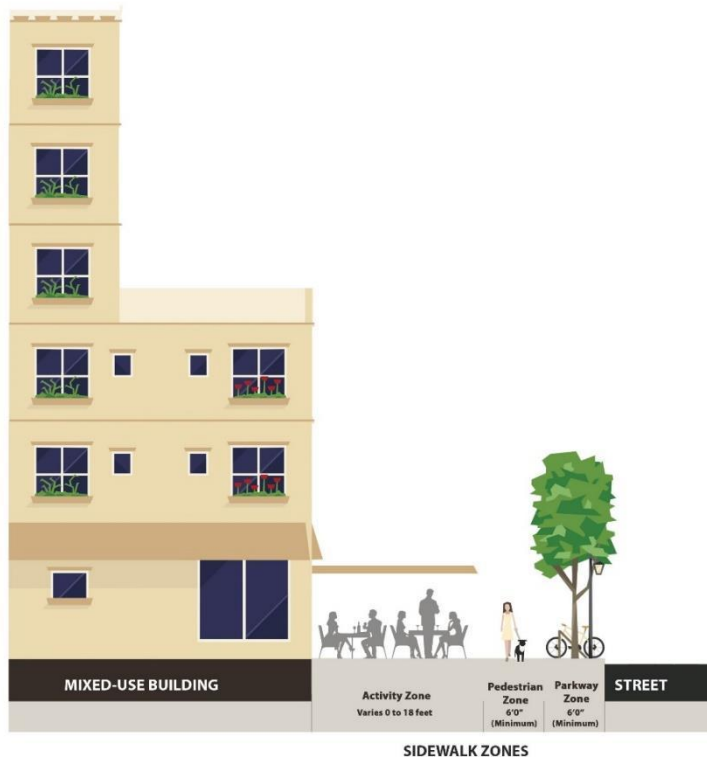
When required, the parkways zone is located between the pedestrian zone and the road. This zone contemplates parkway landscaping, street trees, street furniture, light fixtures, and wayfinding signage. The minimum width for the parkway zone is 6 feet.

Sidewalk zones are required as follows:

Table 2-5. Sidewalk Zones

Type	Activity Zone 18' Maximum	Pedestrian Zone 6' Minimum	Parkway Zone 6' Minimum
Major Connector	Required	Required	Required
Type A Internal Road	Optional	Required	Required
Type B Internal Road	Optional	Required	Optional

Figure 2-34. Sidewalk Zones



Source: City of Santa Clarita, Michael Baker International, Streetmix

## 2. Public Realm Amenities

Public realm amenities include elements that create a more pleasant, comfortable pedestrian environment within the public realm. These include, but are not limited to, outdoor dining and display of merchandise, pedestrian-scale architectural features, street furniture, street trees, and landscaping. Public realm amenities are generally contemplated for the activity and parkway zones within the public realm. Together, these elements function in concert to create vibrant street scenes and to define smaller, intimate spaces within a larger urban context. For major connectors, the theme and style of public realm amenities must be consistent along the entire alignment.

### Outdoor Dining

Individual commercial tenants such as restaurants, cafés, ice cream parlors, and bakeries may place street furniture for outdoor dining in the activity zone subject to the following design standards:

- Outdoor dining is subject to review and approval of the Director of Community Development.
- Outdoor dining may not encroach into the pedestrian zone, obstruct any accessible walkway, other business, parking spaces, bicycle parking, or other space required by local, state, or federal building or safety codes.
- The width of the outdoor dining area must not exceed the frontage width of the individual tenant space.
- Furniture must be of an appropriate scale to fit the space and must not be affixed to the ground.
- Furniture must be made of durable material and well maintained without stains, rust, tears, splinters, or discoloration.

- Furniture in the Activity Zone is strongly encouraged to be brought indoors during hours when the business is not in operation.
- Umbrellas must be constructed of fade resistant, durable materials and have a minimum clearance of 7 feet between the ground and the lowest edge of the umbrella, and not be taller than 9 feet overall.
- Striped or heavily patterned materials are discouraged.
- Above-grade planters or pots used as part of the outdoor dining space must be a minimum height of 2 feet and a maximum height of 4 feet. All landscaped material must be kept in healthy condition.
- Each outdoor dining use may have one menu board measuring no more than 9 square feet. Menu boards may not be affixed to building frontages or the ground.
- No off-site signage is permitted, and any proposed signage to be located upon umbrellas, barriers or similar are subject to the approval of the Director of Community Development.

Figure 2-35. Sidewalk Dining Examples



Location: Mountain View, CA



Location: Miami Beach, FL



**Outdoor Display of Merchandise**

Individual retail tenants may display merchandise in the activity zone subject to the following development standards:

- Outdoor display of merchandise is subject to review and approval of the Director of Community Development.
- Outdoor display of merchandise may not encroach into the pedestrian zone, obstruct any accessible walkway, other business, parking spaces, bicycle parking, or other space required by local, state, or federal building or safety codes.
- The width of the outdoor display of merchandise area must not exceed the frontage width of the individual tenant space.
- Merchandise is limited to one display per business and must be displayed during regular business hours only.
- Merchandise displayed in the Activity Zone is strongly encouraged to be brought indoors during hours when the business is not in operation.
- Displays may not be affixed to the ground or building façade.

**Pedestrian-Scale Architecture:**

Pedestrian-scale architecture refers to treatments on the first floor of street facing building façades that enhance the pedestrian experience and help define the public realm as a space. Pedestrian-scale elements must be included for façades along major connectors and is encouraged for façades along Type A internal roads. Examples include, but are not limited to:

- Enhanced architectural details and finishes.
- Additional building articulation.
- Varied building setbacks.
- Awnings.
- Large windows.

- Decorative lighting.

**Figure 2-36. Pedestrian Scale Design**



Location: San Francisco, CA

**Figure 2-37. Example of Light Pole**



Location: Long Beach, CA

**Landscaping**

For frontages along major connectors and Type A internal roads, landscaping must be provided in the parkway zone. Landscaped parkways are not required to occupy the entire width of the parkways zone, but should be designed to provide a comfortable setback between the pedestrian zone and street. Street trees must be included and be of a specimen size and distribution frequency that creates a consistent vertical landscaping element along the entire alignment of the adjacent roadway.

**Figure 2-38. Street Trees**



*Location: Irvine, CA*

**Street Furniture**

Street furniture provides the opportunity to relax and gather within the public realm. Street furniture may be located within the activity zone or within the parkway zone. Furniture within the parkway zone should be of a consistent style and material along the entire alignment of major connectors.



Figure 2-39. Street Furniture Examples





**Bicycle Parking**

Bicycle parking, including bicycle racks and/or lockers, must be provided in the public realm along major connectors and Type A internal roads and may be placed in the activity and parkway zones. Providing bicycle parking at a rate that exceeds the requirements of the California Building Code is strongly encouraged. Bicycle parking facilities are also encouraged to be decorative whenever feasible.

**Wayfinding Signage**

Wayfinding signage may be placed in the parkway zone, subject to the following development standards:

- Wayfinding signage must have a maximum dimension of 6 feet in height by 4 feet in width.
- Wayfinding signage must not contain digital content within the public realm.
- Wayfinding signage must not be located in a manner that obstructs line-of-site for vehicle or bicycle operators.
- Wayfinding signage must not encroach into the pedestrian zone.

Figure 2-40. Wayfinding Signage Examples



Location: Madrid, Spain



Location: Seattle, WA

***A-Frame Signage***

A-frame signage may be placed in the activity zone or parkway zone, subject to the following standards:

- A single A-frame sign may be placed in front of each individual commercial tenant space during operating hours of the associated business.
- A-frame signs must be no taller than 36 inches in height with a maximum sign area of 7 square feet per side.
- A-frame signage must not be affixed to the ground.

- When located in the parkway zone, a-frame signs may not be placed within a landscaped area or planter.

***Temporary On-Street Events***

Incorporating design features in the extension of Town Center Drive that allow for a portion, or portions of, its alignment to be closed to vehicle and bicycle traffic for temporary events is strongly encouraged. Events including, but not limited to, festivals, seasonal celebrations, street markets, art shows, and live music would occur both within the public realm as well as the street alignment itself. Events are subject to the review and approval of the Director of Community Development via the Temporary Use Permit process.



Figure 2-41. Temporary Use Examples



Location: Left - New York City, NY; Right - New York City, NY



Location: Left - Brea, CA; Right - Los Angeles, CA



## ELEMENT MO3 – PEDESTRIAN & BICYCLE NETWORKS

### DESCRIPTION

A vital component of the TCSP is the creation of an extensive and functional network of pedestrian and bicycle paths throughout the SPA. The networks must provide robust linkages across the full extent of the SPA, both to the east and west as well as to the north and south. To do so, development must focus on creating pedestrian and bicycle facilities that are safe, accessible, convenient, practical, and attractive that integrate into the City’s existing sidewalks, paseos, paseo bridges and bicycle paths, and bus stops surrounding the SPA. Special emphasis should be placed on improving pedestrian bridge connectivity between the SPA and the McBean Regional Transit Center.

Improving pedestrian mobility ensures that all individuals have equal access to essential services, amenities, and public spaces. Improving pedestrian mobility and creating a pedestrian-friendly environment also fosters vibrant, livable, and sustainable communities. Well-designed pedestrian infrastructure, such as sidewalks, paseos, plazas, and walkways, can encourage social interaction, support local businesses, and create inviting public spaces. This contributes to a sense of community, enhances the quality of life, and improves the overall attractiveness of an area.

Establishing a comprehensive network of bike infrastructure and supporting facilities in the SPA and connecting it to surrounding trails will improve bike connectivity and encourage more people to choose cycling as a sustainable and efficient mode of transportation. This not only benefits individual cyclists but also contributes to reducing traffic congestion, improving air quality, and promoting healthier and more livable communities.

### CONTEXT

No integrated pedestrian or bicycle networks exist within the SPA and, as such:

- Walking or riding between subareas is impractical, time consuming, and emphasizes reliance on transportation by car. This, in turn, diminishes the ability for visitors, employees, and residents within the SPA to “park once” while on-site.
- There are no convenient pedestrian or bicycle paths across the SPA that form linkages between the peripheral streets that define its boundary. This forces pedestrians and bicycle riders using the existing City trail network to take longer routes around the perimeter of the SPA to reach their destinations.

### LOCATION

Pedestrian and bicycle networks must be developed throughout the SPA.

### EXECUTION

The following development standards apply to the creation of robust pedestrian and bicycle networks that provide connectivity throughout the SPA and are integrated into the City’s sidewalk and trail network along peripheral streets.

#### 1. Pedestrian Path Standards

Pedestrian paths, including sidewalks and paseos, provide for the safe and efficient flow of foot traffic in the SPA. Each type of pedestrian path is subject to the following development standards:

**Sidewalks**

Sidewalks are the primary form of pedestrian travel in the SPA, have a minimum width of 6 feet, and are generally part of the public realm along major connectors and internal roads.

**Paseos**

Whereas sidewalks form the primary pedestrian network along streets and the perimeters of development blocks, paseos are pedestrian pathways that provide mid-block connections and traverse inward across development blocks. Paseos are typically designed to enhance opportunities for pedestrian travel away from major connectors and public roads, and provide access to inward-facing commercial land uses within a development block including shops, cafés, and restaurants as well as interior features like public spaces and common spaces. Paseos are subject to the following development standards:

- Paseos must be a minimum of 8 feet in width, with 10 feet being preferred.
- Paseos may accommodate pedestrian and bicycle traffic.
- Commercial uses are strongly encouraged to front along paseos.
- Paseos are encouraged to be used as visual axes for the creation of signature vistas in conjunction with water features, monuments, architectural elements, and public art.
- Paseos must incorporate seating areas, landscaping, and appropriate lighting.

Figure 2-42. Examples of Paseos (Commercial and Residential Areas)



Location: La Jolla, CA



Location: Pasadena, CA

Figure 2-43. Examples of Paseos



Location: New York City, NY

## 2. Network Connectivity

### *Connectivity of Development Blocks*

- Developments within individual blocks must provide direct access to all other adjacent development blocks via sidewalks and crosswalks and are encouraged to provide access via paseos.
- Developments within individual blocks must provide bicycle access via Class II bicycle lanes on major collectors and/or Type A internal roads to other adjacent development blocks.
- Both pedestrian and bicycle networks must contain wayfinding signage.

### *Connectivity to Peripheral Streets*

Developments that are adjacent to peripheral streets must provide direct pedestrian access to facilities in the public right-of-way including sidewalks, trails, paseos, pedestrian bridges, and bus stops. Project design must coordinate with the City's Non-Motorized Master Plan regarding integration with future projects planned within the public right-of-way.



## ELEMENT MO4 – PEDESTRIAN BRIDGE CONNECTIVITY

### DESCRIPTION

Three existing pedestrian bridges provide a safe method of pedestrian travel over peripheral streets and provide key linkages between the surrounding community and the SPA. Enhancing the appeal of these facilities will provide connections to a comprehensive walking path and paseo network within the SPA, encourage pedestrianism to and from the McBean Regional Transit Facility, and incent pedestrianism to and throughout the site.

### CONTEXT

There are currently three pedestrian bridges that connect the surrounding trail network in the community with the SPA. However, each of the landing spots within the SPA should be enhanced to not only provide better access to the interior of the SPA, but to also make the pedestrian experience more enjoyable. Each bridge spans one of the major thoroughfares bordering the SPA, and each land within the Valencia Town Center Subarea.

- **McBean Parkway, directly south of the intersection with Mall Entry:** This bridge provides the primary trail linkage between the McBean Regional Transit Center to the west and the SPA to the east. Within the SPA, the bridge lands at a sidewalk on the west side of a two-lane ring road that provides vehicle access to the surface parking lot near the western edge of The Patios. The nearest pedestrian access point into the interior of the SPA is approximately 275 feet south of the bridge's landing spot.
- **Magic Mountain Parkway, directly east of the intersection of Cheri Fleming Auto Center Drive:** This bridge provides the primary linkage between the existing trail system to the north and the SPA to the south. Within the SPA, the bridge lands near the southeastern corner of the intersection behind an existing fast food restaurant and then turns south, crossing a two-lane

ring road that provides vehicle access to the surface parking lot near the food court of the Valencia Town Center Mall.

- **Valencia Boulevard, west of the intersection with Citrus Street:** This bridge provides the primary linkage between the existing trail system to the south and the SPA to the north. Within the SPA, the bridge lands at the sidewalk west of the intersection. The nearest pedestrian access point into the SPA is nearly 600 feet west at the primary southern entrance into the Valencia Town Center Mall.

### LOCATION

Development standards apply to all existing and future pedestrian bridges that connect the SPA with the surrounding community.

### EXECUTION

#### 1. Landing Area Design

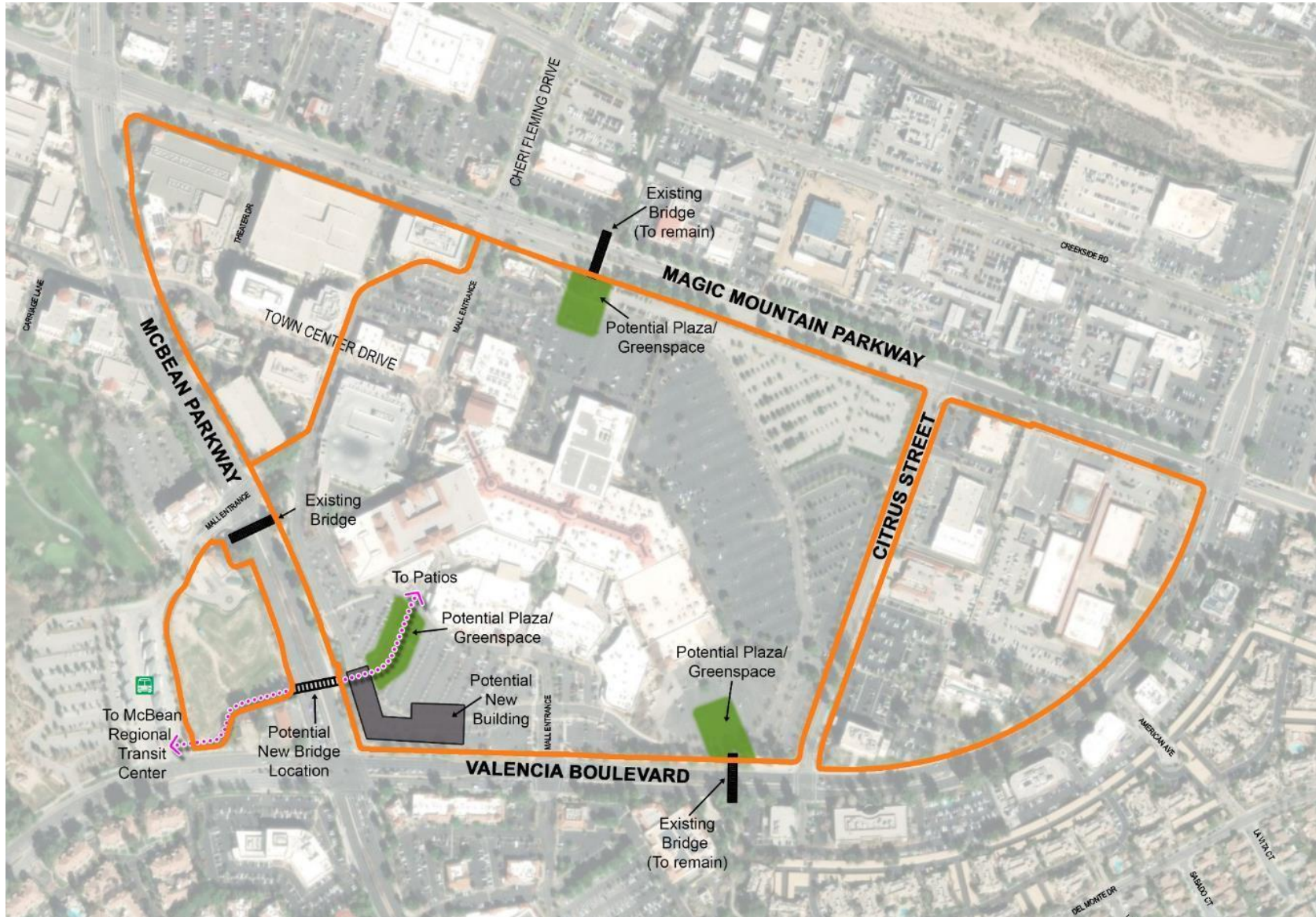
Pedestrian bridge landing areas are subject to the following requirements:

- Proposed projects that are adjacent to existing or new pedestrian bridges must provide a pedestrian bridge landing area.
- Pedestrian bridge landing areas must:
  - o Be of an appropriate size to provide a shaded seating area and have direct pedestrian connectivity into adjacent developments within the SPA.
  - o Be equipped with wayfinding signage.

#### 2. Bridge Relocation

The relocation of the McBean pedestrian bridge to a location south of the existing bridge, and north of the intersection of McBean Parkway and Valencia Boulevard is strongly encouraged to enhance pedestrian access between the SPA and the McBean Regional Transit Center. Future bridge design should accommodate comfortable use by pedestrian and bicycle traffic.

Figure 2-44. Pedestrian Bridge Location



## ELEMENT MO5 – MICRO MOBILITY

### DESCRIPTION

Micro-mobility refers to the use of small, lightweight, modes of transportation for short-distance travel within urban areas. These modes of transport typically include electric scooters, electric bicycles, and other compact vehicles. Micro-mobility is well-suited for "last-mile" transportation, bridging the gap between public transit stations or bus stops and a final destination, and can also provide quick and efficient transportation between subareas within the SPA.

### CONTEXT

There are currently no micro-mobility stations within the SPA.

### LOCATION

Design standards for micro-mobility apply to the entire SPA.

### EXECUTION

Micro-mobility must comply with the following development standards:

1. Micro-mobility stations should be located in high activity areas including, but not limited to, the Regal Cinema, The Patios, public and common spaces, pedestrian bridge landing areas, major employment areas, and transportation drop-off zones.
2. Stations should be located in areas that present minimal conflict with vehicle traffic.
3. All maintenance of micro-mobility stations and the removal of abandoned vehicles is the responsibility of the property owner or station operator. Timelines for the prompt removal of abandoned vehicles may be conditioned at the discretion of the Director of Community Development.



## ELEMENT MO6 – TRANSIT DROP-OFF ZONES

### DESCRIPTION

A drop-off zone refers to designated areas where taxis and services such as Santa Clarita Transit, and transportation network companies (for example Uber and Lyft), pick-up or drop-off passengers. The purpose of the zone is to provide a convenient and safe area for quick pick-ups and drop-offs without obstructing traffic flow.

### CONTEXT

Eight locations for Santa Clarita Transit services are currently located within the SPA, but there are no designated drop-off areas for transportation network companies.

### LOCATION

A minimum of two drop-off zones must be created within the Valencia Town Center Subarea, and one drop-off zone must be created within the remaining subareas.

### EXECUTION

Drop-off zones are subject to the following requirements:

#### 1. Location

Drop-off zones must:

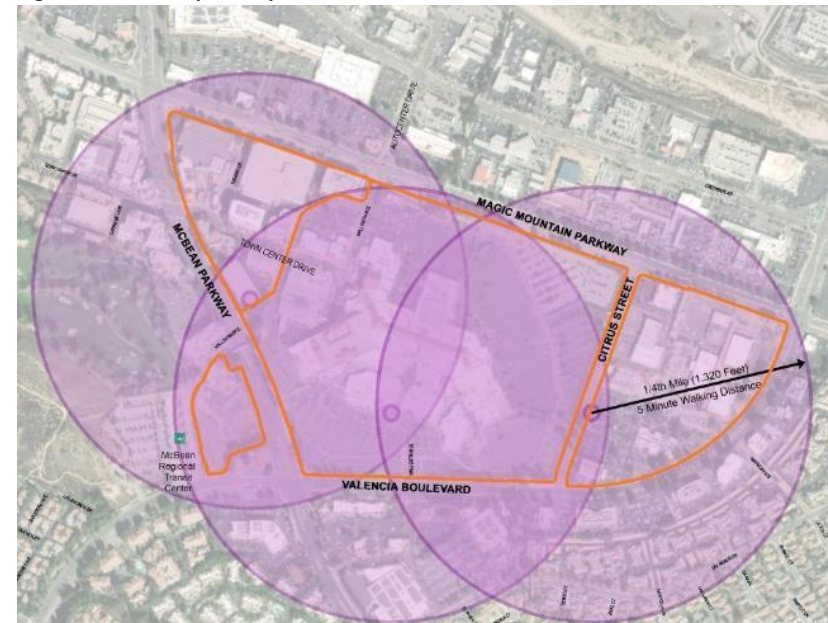
- Be within a convenient walking distance from significant amenities and/or destinations within the SPA.
- Be able to accommodate both Santa Clarita Transit services as well as transportation network companies and taxicabs.
- Be located in areas that avoid the potential for vehicle and pedestrian conflict to the greatest extent possible.

#### 2. Design

Drop-off zones must include the following:

- Adequate space to allow for vehicle storage and turn arounds.
- A shaded seating area.
- Wayfinding signage.

Figure 2-45. Example Drop off Areas 5-minute Pedestrian Shed



Source: City of Santa Clarita, Michael Baker International, ESRI



## PARKING

A successful, comprehensive vehicle and bicycle parking strategy that provides safe, convenient, and attractive facilities is a vital component for the efficient daily operation of development within the SPA. Benefits include:

- Enhanced, convenient access to shopping, dining, entertainment, employment, and residential opportunities.
- Integration of wrapped parking structures that maximize available space and integrate into signature vistas within the SPA to create a more interesting visual environment in place of large surface parking lots.
- Integration into an interconnecting pedestrian network and vibrant street scene that fosters a “park once” philosophy.

Figure 2-46. Examples of Pedestrian Movement in Parking Area



## ELEMENT PK1 – PARKING STRATEGY

### DESCRIPTION

Parking within the SPA must provide direct, convenient access to shopping, dining, entertainment, employment, and residential opportunities in a manner that maximizes the efficient use of available space. As such, the TCSP contemplates the utilization of wrapped, structured parking over surface parking because it serves to reduce the time and distance required for visitors, employees, and residents to travel between their vehicle and their ultimate destination, or destinations. Further, wrapped parking structures in alignment with signature vistas, public and common spaces, a robust pedestrian street scene, and an interconnected, on-site pedestrian network will create a more comfortable and visually interesting environment than travelling across open surface parking lots on foot. This encourages pedestrianism throughout the SPA and fosters a “park once” philosophy. Finally, a balanced on-site mix of uses will take advantage of shared parking opportunities between residential units and commercial space, both within the project and throughout the SPA.

### CONTEXT

From a parking perspective, the SPA is dominated by the over 4,300 parking spaces distributed throughout 14 surface parking lots and one parking structure that surround the Valencia Town Center Mall within the Valencia Town Center Subarea. This has created a significant mass of surface parking along the southern, eastern, and northern mall frontages. In some cases, visitors and patrons must walk almost 900 feet from lots located on the northeast corner of the subarea to the nearest entrance of the mall. Further, many of the outlying parking lots go largely unused throughout the year, with peak demand only occurring once per year during the holiday season. This configuration is inefficient, inconvenient, and underutilizes the space within the subarea.

The majority of parking within the Town Center Drive Subarea is provided by two large parking structures, each five decks in height. Additional surface parking is provided along the length of Town Center Drive. Parking in this subarea is efficient and convenient, providing direct access to shops, dining, entertainment, and employment options.

The Town Center East Subarea consists of several properties and several individually-owned properties. Each property provides parking for the use or uses on-site, but with little to no connectivity to other properties or surface lots. There is no pooled or shared parking within the subarea and access to the Valencia Town Center Subarea, directly to the west, is limited.

Although no surface parking has been constructed within the McBean and Valencia Subarea, code required parking has been included as part of the approved entitlement for hospitality and restaurant space.

Under Assembly Bill (AB) 2097, the City has a restricted ability to define parking minimums for projects because the SPA is within ½ mile of the McBean Regional Transit Center. However, AB 2097 does not remove the obligation for the provision of parking.

### LOCATION

Parking requirements apply throughout the entire SPA for all development and redevelopment projects.

### EXECUTION

#### 1. Parking Rates

##### *Commercial Parking*

- Commercial uses must provide 1 space per 285 square feet of commercial space (3.5 spaces / 1,000 square feet).
- Parking for outdoor dining or outdoor display of merchandise within the activity zone of the public realm is exempt from parking rate requirements.



***Residential Parking***

Residential projects must provide parking according to the following schedule:

- Units with two or more bedrooms: Two enclosed or structured parking spaces.
- Studio units or units with one bedroom: One enclosed or structured parking space.
- Guest parking: ½ space per unit. This may be shared with commercial parking spaces, subject to a parking analysis and approval of the Director of Community Development.
- Enclosed parking spaces should include storage space for tenants. If spaces are not enclosed, storage space must be provided within the tenant’s living space.

***Hospitality Parking***

- One parking space must be provided per room, plus parking for additional commercial or residential uses.

A parking demand study that includes a parking analysis and survey data conducted by a licensed traffic engineer, or other traffic professional acceptable to the Director of Community Development, must be submitted for any proposed project that does not meet the minimum parking requirements identified above, per the Director’s Policy dated March 6, 2024.

**2. Other Parking Standards**

Unless otherwise identified within the TCSP, parking lot design and landscape standards must be consistent with UDC Chapters 17.51.030 (Landscaping and Irrigation Standards) and 17.51.060 (Parking Standards)

- Wrapped, structured parking is strongly encouraged over surface parking.
- Angled and parallel on-street parking provided on major connectors and Type A internal roads may be counted toward on-site parking requirements for developments they are associated with.
- Loading areas must be provided at a rate consistent with UDC Chapter 17.51.060.L (Loading Areas), unless otherwise determined by the Director of Community Development.
- All projects must comply with Tier 2 standards within the California Green Building Code for electric vehicle charging stations. Except in cases where good cause is shown, and subject to the Discretion of the Director of Community Development, 50% of the required number of stations must be fully installed prior to occupancy, or comply with current Tier 2 California Green Building requirements for the number of fully installed stations, whichever is higher.
- At the discretion of and to the satisfaction of the Director of Community Development, a parking management plan describing the operation of on-site, project-related parking enforcement must be prepared.
- Pedestrian paths shall connect parking lots to main building entrances and prominent entry features within the SPA with minimal vehicular conflicts. Enhanced colors and textured materials and/or landscaping must be incorporated into pedestrian paths within parking lots.

- Shared parking agreements are encouraged between various compatible uses. Shared parking agreements are subject to UDC Chapter 17.51.060.N (Modification of Off-Street Parking Requirements).
- Bicycle parking is subject to the requirements of the California Green Building Code. However, exceeding identified requirements is encouraged to provide greater bicycle access throughout the SPA.

Figure 2-47. Example of Parking with Bulb-outs



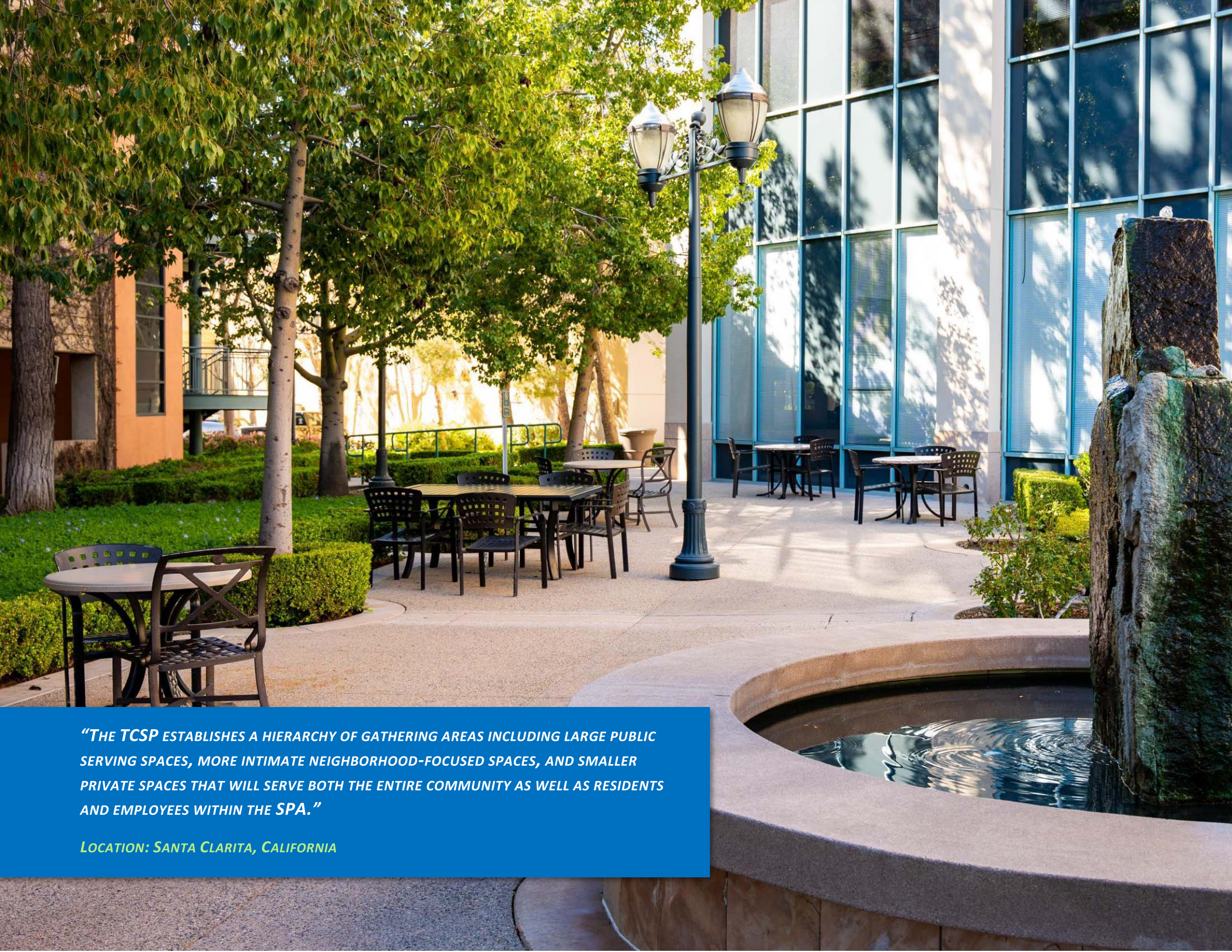
Location: Long Beach, CA

Figure 2-48. Example of Protected Bike Lane with Parking



Location: Kansas City, MO





*“THE TCSP ESTABLISHES A HIERARCHY OF GATHERING AREAS INCLUDING LARGE PUBLIC SERVING SPACES, MORE INTIMATE NEIGHBORHOOD-FOCUSED SPACES, AND SMALLER PRIVATE SPACES THAT WILL SERVE BOTH THE ENTIRE COMMUNITY AS WELL AS RESIDENTS AND EMPLOYEES WITHIN THE SPA.”*

*LOCATION: SANTA CLARITA, CALIFORNIA*



3) Chapter 3

# 3

## Implementation & Administration

# 1. IMPLEMENTING DOCUMENTS & ACTIONS

This chapter includes the primary steps and components necessary to implement and administer the Town Center Specific Plan (TCSP).

## GENERAL PLAN AMENDMENT

To ensure internal document consistency, the City of Santa Clarita General Plan shall be amended concurrently with adoption of the TCSP. The following amendments shall occur as part of General Plan Amendment (GPA) 24-003:

- Amend the General Plan Land Use Map to change the land use designation of parcels located within the Specific Plan Area (SPA) from Regional Commercial (CR) to Specific Plan (SP); and
- Amend all other narrative and element maps in the General Plan to reflect and incorporate the new SP designation.

## ZONING CODE & ZONING MAP AMENDMENT

Section 17.28.110 Specific and Corridor Plans of the Santa Clarita Zoning Code shall be amended to list the TCSP. The Zoning Map shall be amended to rezone all lands within the TCSP area from CR to SP. The Zoning Map Amendment shall occur concurrently with City Council approval of the TCSP and GPA.

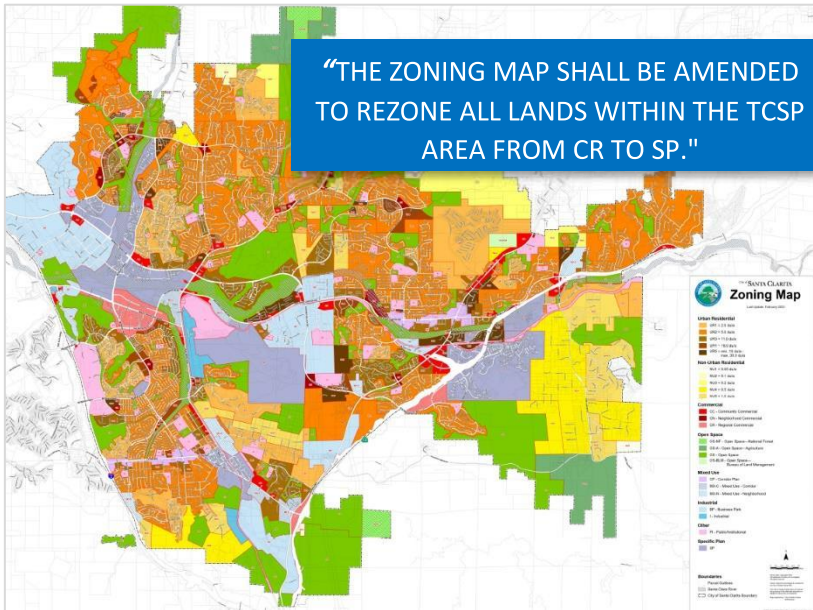
## UNIFIED DEVELOPMENT CODE COMPLIANCE

The TCSP provides new development standards and streamlined procedures for the properties within the SPA. All properties within the TCSP area remain subject to the Santa Clarita Municipal Code except where superseded by the TCSP. More specifically, all future

development within the SPA shall comply with and follow all of the standards and procedures of Titles 16 and 17 of the Santa Clarita Municipal Code, collectively referred to as the Unified Development Code (UDC). Where the regulations of the TCSP differ from the provisions of the UDC, such regulations shall supersede the provisions of the UDC and the TCSP shall govern.

All uses and all development within the SPA must comply with the Development Framework & Standards in Chapter 2, and all other provisions set forth in this Specific Plan in addition to all other provisions of law. Any failure to comply with the Development Standards, Design Guidelines, or any other provision of this Specific Plan shall constitute a violation of the Municipal Code. Any such violations shall be subject to the enforcement provisions of the Municipal Code.

Figure 3-1. Santa Clarita Zoning Map



## 2. PROGRAM ENVIRONMENTAL IMPACT REPORT

A Program Environmental Impact Report (Program EIR) has been prepared for this Specific Plan in accordance with the California Environmental Quality Act (CEQA) and CEQA Guidelines Section 15168—Program EIR. CEQA Guidelines Section 15165 requires the preparation of a Program EIR where a phased project is to be undertaken and where the total undertaking comprises a project with significant environmental effects.

The TCSP is anticipated to be implemented through a series of multiple development projects over many years, many of which will require discretionary development permit approvals and will therefore require consideration of CEQA. However, many such future actions will not require additional CEQA documentation, as they will be either be exempt from CEQA or will be within the scope of the TCSP Program EIR. Section 1.6 of the Program EIR describes various methods for utilizing the Program EIR for future activities, which include but are not limited to CEQA Guidelines Section 15168(c) for use of a Program EIR with later activities; CEQA Guidelines Section 15152 for tiering; CEQA Guidelines Sections 15162-15164 for subsequent or supplemental CEQA documentation and addendums; CEQA Guidelines Section 15182 for projects pursuant to a specific plan; and CEQA Guidelines Section 15183 for projects consistent with a community plan or zoning.

### MITIGATION MONITORING & REPORTING PROGRAM

Public Resources Code Section 21081.6 requires that a reporting or monitoring program be adopted to ensure compliance with the mitigation measures included in the Program EIR during

implementation of the TCSP project. The City has prepared a Mitigation Monitoring and Reporting Program (MMRP) in conjunction with the preparation of the Final Program EIR for the TCSP. The MMRP summarizes the required mitigation for impacts attributable to the project; identifies the party responsible for monitoring project compliance with the mitigation measures; and identifies the time period or project phase in which the mitigation measures are to be completed.

### TRANSPORTATION DEMAND MANAGEMENT (TDM)

The UDC currently requires a TDM plan for projects that include a commercial or mixed-use development which generates 50 or more full-time employees. As set forth in the Program EIR, Air Quality Mitigation Measure-1 (MM-AQ-1), the TCSP also requires a TDM for mixed residential developments with 100 or more units.

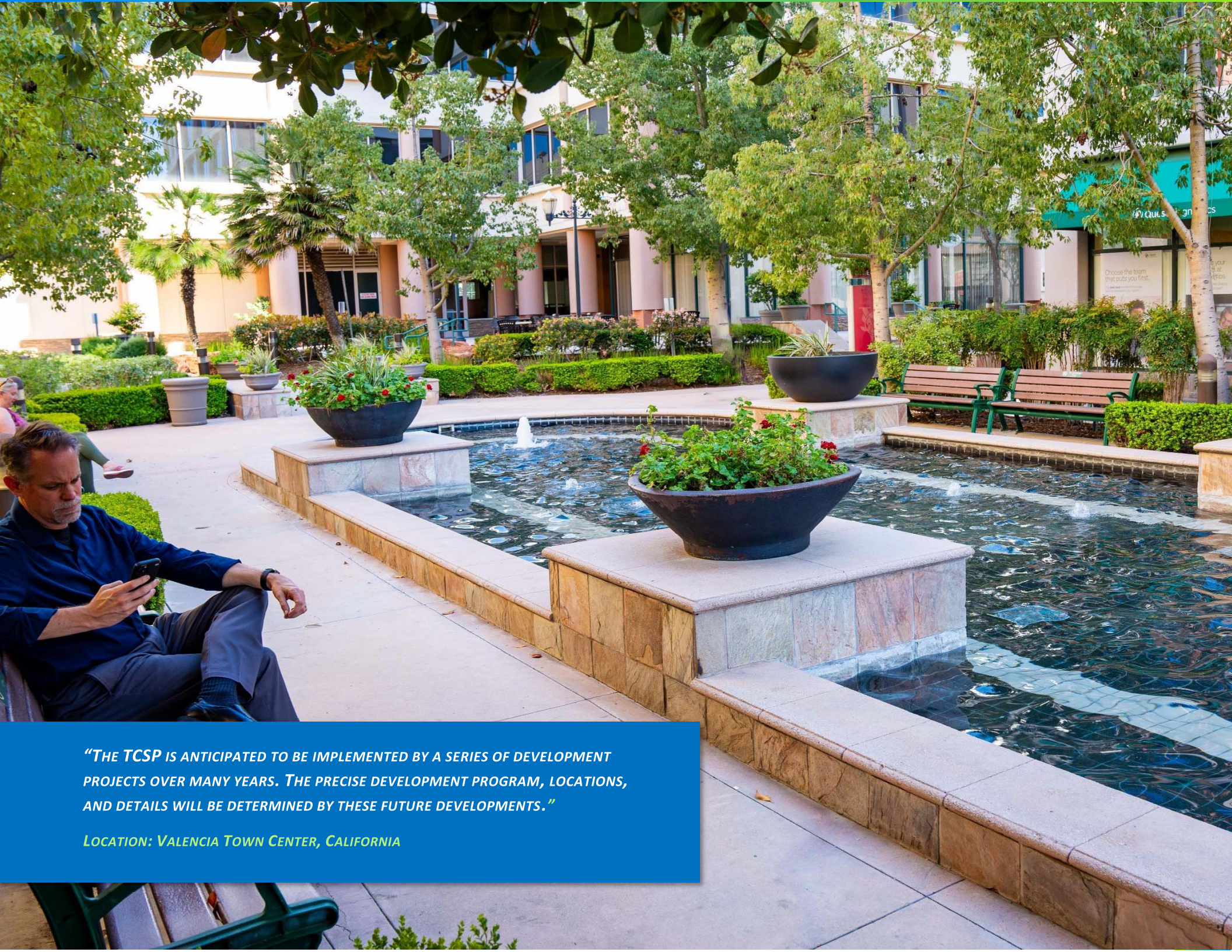
### ELECTRIC VEHICLE CHARGING STATION

Chapter 2: Development Framework & Standards includes a design standard to include electric vehicle charging stations at a rate, and in locations, that are consistent with Tier 2 requirements of the CalGreen Code.

### ELECTRIC LANDSCAPING EQUIPMENT

All future projects that require a final landscape approval will include a condition that strongly encourages the use of electric (not gas-powered) equipment for landscape maintenance.





*“The TCSP is anticipated to be implemented by a series of development projects over many years. The precise development program, locations, and details will be determined by these future developments.”*

*LOCATION: VALENCIA TOWN CENTER, CALIFORNIA*



### 3. ILLUSTRATIVE FULL BUILDOUT PLAN

The TCSP is anticipated to be implemented by a series of development projects over many years. The precise development program, locations, and details will be determined by these future developments. Chapter 2: Development Framework & Standards provides detailed articulation of the vision, goals, policies, and expectations for future development within the SPA. It also establishes specific standards that set the key parameters of development, yet allows room for creativity and innovation. Chapter 2 of the TCSP:

- Requires individual and collective development projects to meet the ongoing objectives of the City for the SPA; and
- Allows future development the ability to exercise creativity, reflect and respond to market conditions and realities, and evolve over time.

The TCSP also serves as an overall tool and method to incentivize the desired development. This is accomplished with a streamlined development application and environmental review process. The vast majority of projects that are consistent and compliant with and fully implement the TCSP may be approved administratively by the City's Community Development Department and with a streamlined environmental review, saving the City and applicants valuable time and resources.

Each development application will be reviewed for compliance with the TCSP, specifically Chapter 2: Development Framework & Standards.

Chapter 2 articulates the building blocks, expectations, and general requirements of the TCSP. It contains the Illustrative Full-Build Scenario (Figure 3-2), which illustrates the vision, goals, expectations, and essential components of the Specific Plan.

There are many components and layers of planning that comprise the Illustrative Full-Build Scenario. The following figures highlight several of the key components of the Illustrative Full Build Scenario:

- Land Use (refer to Figure 3-3)
- Suggested Major Connectors and Internal Roads (refer to Figure 3-4)
- Gathering Spaces & Pedestrian Mobility (refer to Figure 3-5)

The purpose of the figures above, is to better identify these essential elements of the specific plan. These figures will be used to help evaluate future development proposals for consistency with, and implementation of, the TCSP.

It is important to note that the Buildout Scenario does not serve as a rigid blueprint for development, but rather as a source of guidance and inspiration for future endeavors, considering the community's goals, market trends, and long-term needs. The Full Buildout Scenario and figures do, however, articulate the essential components of this Specific Plan and guide the necessary coordination of future development projects in the SPA as they implement the TCSP.

The figures identify and enumerate the key components and relevant amounts of each of the components that make up the Illustrative Plan. These figures will provide guidance for review of compliance with the overall development program and ensure coordination of the essential elements for each, and between each, development project for the entire SPA and subareas.

For example, Figure 3-3 Land Use, is a generalized land use map that reflects the proposed building types and primary land uses in the Illustrative Plan – Full Buildout Scenario. Figure 3-2 provides an overview of the spatial relationships between and the relative sizes and proportions of the different uses and building types envisioned for the SPA. This and the other component plans are instructive, not

mandatory. The exact boundaries and precise mix of future uses and building types will be determined and implemented with approval of proposed developments. The plan reflects land area presumed to be necessary to accommodate the full buildout. Deviations are allowed, provided future developments do not materially prevent or limit the full buildout.



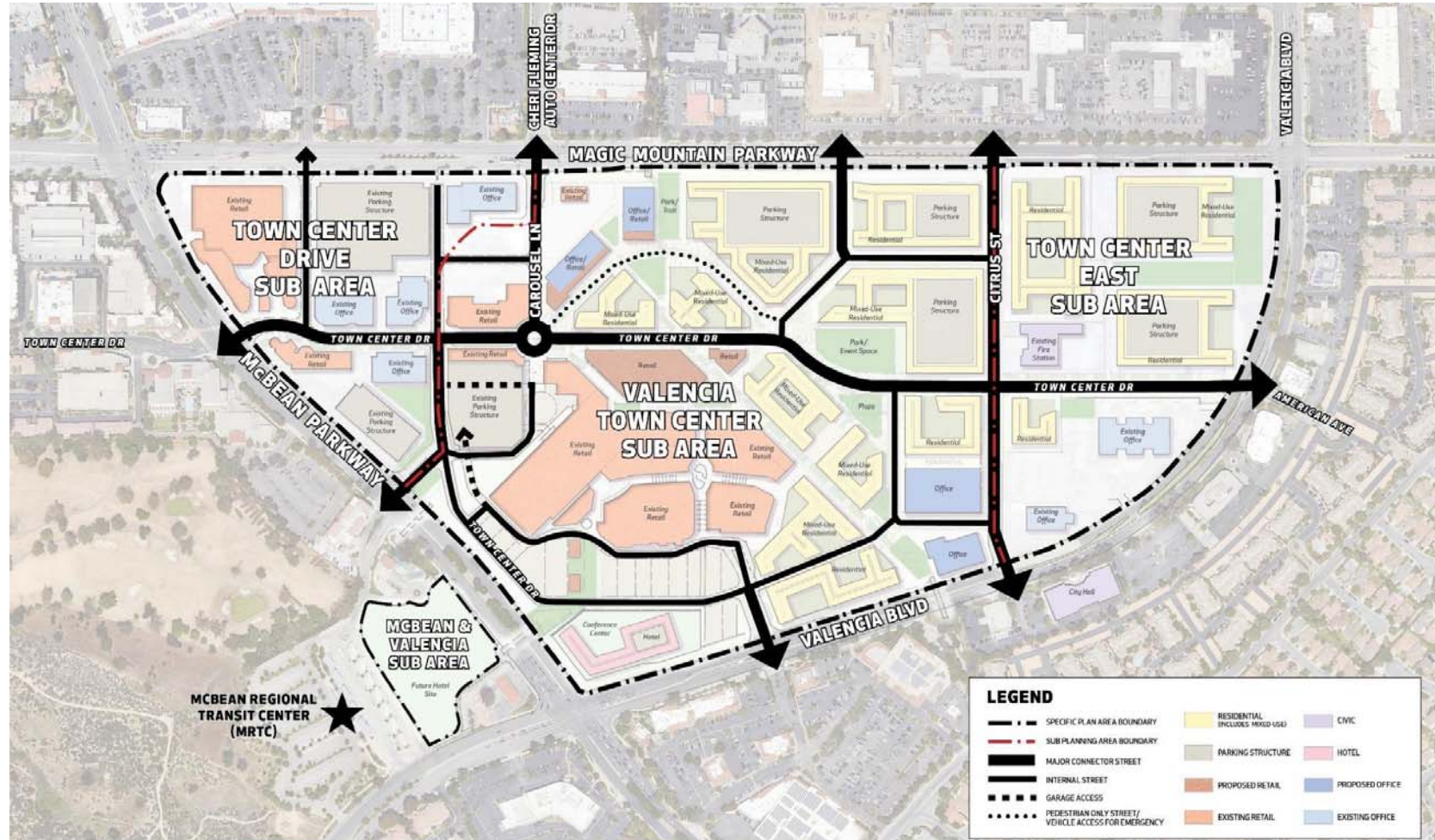
Location: Valencia Town Center



Location: Valencia Town Center

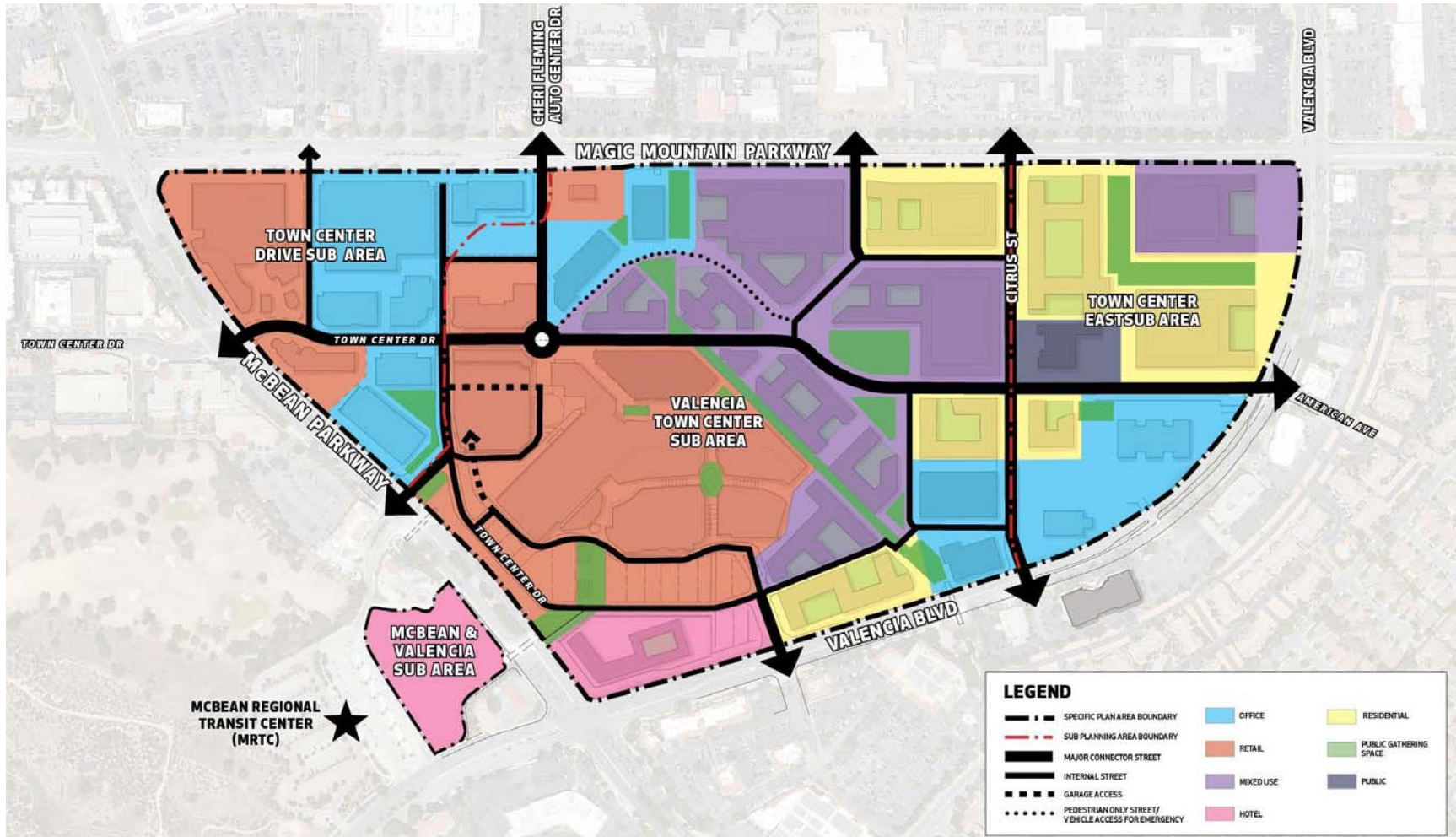


Figure 3-2. Full-Build Scenario



Source: City of Santa Clarita, Michael Baker International, DLR Group, ESRI

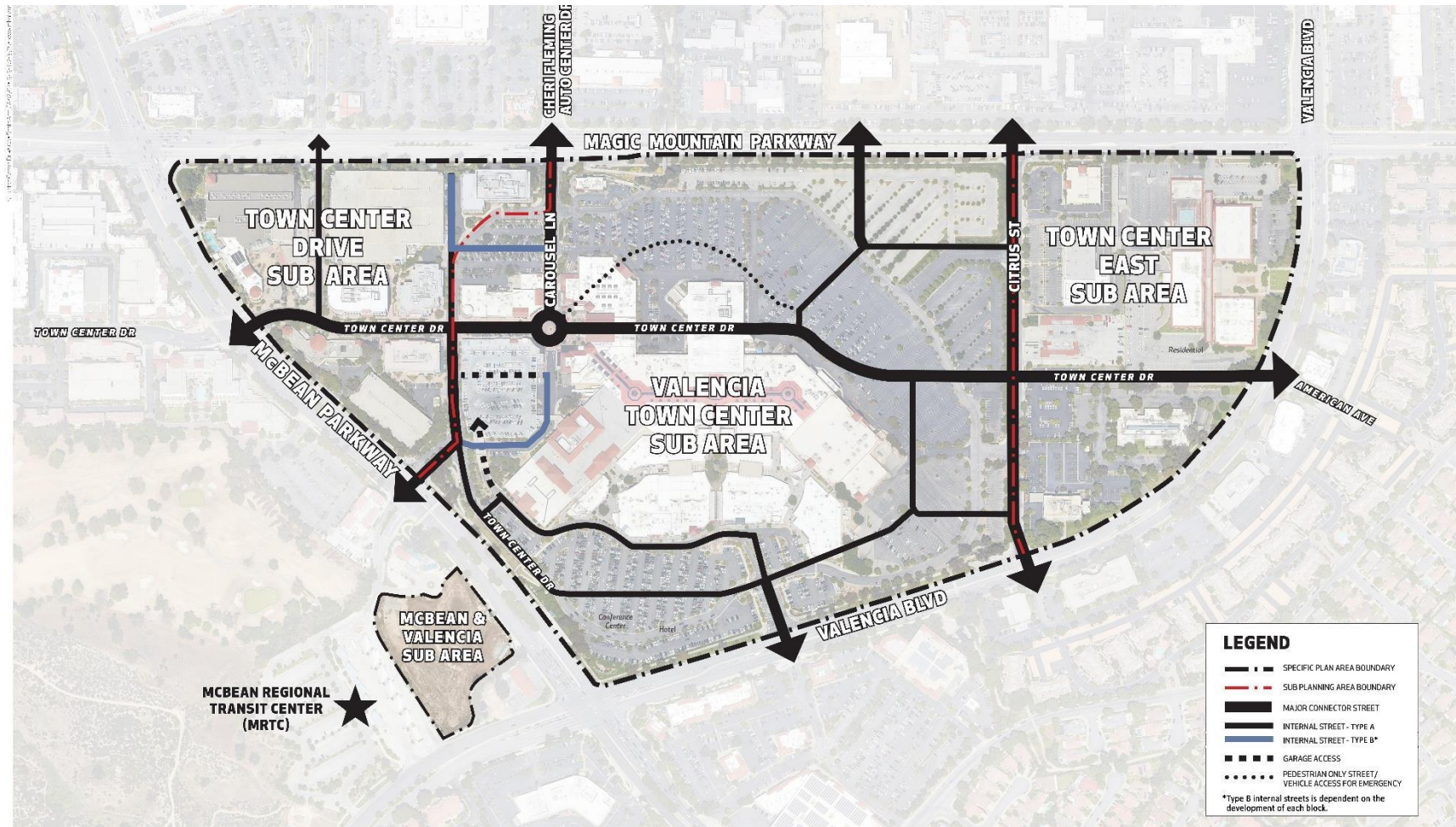
Figure 3-3. Land Use



Source: City of Santa Clarita, Michael Baker International, DLR Group, ESRI



Figure 3-4. Suggested Major Connectors and Internal Roads



Source: City of Santa Clarita, Michael Baker International, DLR Group, ESRI



Figure 3-5. Gathering Spaces and Pedestrian Mobility



Source: City of Santa Clarita, Michael Baker International, DLR Group, ESRI

## SPECIFIC PLAN COMPLIANCE

Each development application will be reviewed for compliance with the TCSP, specifically Chapter 2: Development Framework & Standards.

Fully compliant plans will generally be approved by the Director of Community Development. The Director of Community Development shall have the authority to require conditions of approval as necessary to comply with the framework elements, development standards and design guidelines in Chapter 2 of the TCSP. Applicants may then proceed with the satisfaction of any conditions of approval, and preparation of construction plans and building permit applications.

Plans that the City does not deem to be fully compliant must demonstrate to the satisfaction of the Director of Community Development that:

1. They are consistent with the vision, goals, and framework elements of the TCSP; and
2. Will not preclude or prevent future compliance and implementation of the specific goals and design elements of the TCSP.

Plans that fail these standards may be denied by the City or modified by the applicant.

## BALANCE OF LAND USES

The Full Buildout Scenario within the TCSP includes approximately 2,200 residential units throughout the SPA. The City's Housing Element identifies the need for 446 affordable units on specific properties within the SPA. Because of this, the EIR prepared for the TCSP contemplated 20% of units within the SPA as being affordable. As required by the State, and in order to satisfy the TCSP goals for both compliance with the City's Housing Element, and to facilitate the implementation of a plan that is practical, flexible, and buildable,

applicants are strongly encouraged to include affordable units to any project that proposes a residential component.

As important as the TCSP's goal of creating a balanced mix of land uses, is the goal to simultaneously make great, unique places that further establish the SPA as a regional destination and creates a true town center. As described more fully in Chapter 2, this mix and balance of land uses and the placemaking amenities and design elements must be done in lock-step to:

- Attract the kinds of land uses desired by the City Council and the community; and also
- Create a vibrant, exciting, and visually interesting environment where visitors can shop, dine, be entertained, live, work, or simply spend time.

At no time should residential growth occur in a manner that outpaces, or is out of balance with commercial growth and the placemaking framework elements. Chapter 2 further establishes the following four compliance tests and requirements to ensure that all development projects implement and achieve the desired balance of uses and amenities:

### DEMONSTRATE COMPLIANCE WITH ILLUSTRATIVE PLAN

All development applications shall demonstrate compliance with the TCSP, including the Development Framework & Standards established in Chapter 2.

### RESIDENTIAL PROJECTS

Each development project with a residential component:

1. Shall include a significant commercial component consistent with the Illustrative Plan; or
2. Shall provide a significant community amenity to the SPA above-and-beyond those already required as part of the development; or

- 3. Single-use residential projects that do not provide a significant community amenity:
  - a. Are subject to a Minor Use Permit with review and approval by the Planning Commission via the public hearing process; and
  - b. Are not eligible for the design incentive to share guest parking spaces with commercial parking spaces.

**DEVELOPMENT PROGRESS/ECONOMIC STUDY**

At the discretion of the Director of Community Development, the City may require an economic study to be included with an entitlement submittal that contains the: 230th, 560th, 800th, 1,100th, 1,450th, 1,800th, and 2,000th residential unit proposed within the SPA and/or that includes demolition of existing non-residential space of 25,000 square feet or more. The economic study must:

- 1. Include an assessment of how the proposed development will impact the economy of the SPA and the City;
- 2. Demonstrate how the project meets the TCSP’s goal of balanced development based on the Low and/or Full Buildout Scenarios; and
- 3. Achieves the desired Jobs-Housing Balance within the SPA.

If the study cannot demonstrate consistency with the overall vision of the TCSP, at their discretion, the Director of Community Development may:

- 1. Declare that the proposed project does not meet the overall intent of the TCSP and require the proposed project be modified; or
- 2. Require the inclusion of a significant community amenity in a manner consistent with item number two, above.

**SUBAREA PLANS**

At the discretion of the Director of Community Development, the City may require conceptual development plans for an entire subarea as set forth in Section 3.1.14 Subarea Conceptual Development Plans, below.

**LOT CONSOLIDATION**

It is the intent of the TCSP to actively encourage the voluntary consolidation and coordination of small and strategically located properties by owners working in concert with the City. Comprehensively planned development results in greater land use efficiencies, improved design, and reduced incremental costs typically associated with larger scale projects. Existing legal land uses and development that do not meet the requirements of this Specific Plan shall be permitted to continue indefinitely under legal nonconforming status, subject to the nonconforming regulations of UDC 17.05 (Legal Nonconforming Uses, Lots and Structures). This market-driven approach is intended to ensure that Specific Plan implementation benefits current businesses and landowners as well as future landowners, developers, and the larger Santa Clarita community.

**LAND DEVELOPMENT & COMMUNITY BENEFIT INCENTIVES**

Chapter 2: Development Framework & Standards of the TCSP includes a package of incentives that are available to promote and facilitate a variety of desirable project and community benefits, including consolidation and coordination of property and projects for appropriate plan implementation and compliance. The incentives will be requested and granted as part of the review and approval of development applications.



## FINANCING STRATEGIES

Section 65451 of the California Government Code requires that a specific plan include financing measures necessary to implement a proposed project. Typically, a variety of financing measures can be used to finance project construction; they include but are not limited

to special assessment districts, general obligation bonds, exactions, and other private financing methods. All of the required and proposed improvements, including the provision of significant community amenities, within the SPA are the financial responsibility of the property owner(s) and future builder(s).



*Location: Old Town Newhall, Santa Clarita, California*

## 4. DEVELOPMENT REVIEW PROCESS

### DEVELOPMENT PLAN SUBMITTAL REQUIREMENTS

All development applications shall follow the processes and submittal requirements of the UDC except as otherwise required in and superseded by the TCSP.

### SUBAREA CONCEPTUAL DEVELOPMENT PLANS

At the discretion of the Director of Community Development, the City may require conceptual development plans for an entire subarea.

### VOLUNTARY AND INCENTIVIZED SUBAREA PLANS

It is the intent of the TCSP to provide for coordinated and integrated development at the earliest possible point in the development review process and to discourage uncoordinated, piecemeal, inconsistent, or contradictory development. The cohesive, coordinated development will occur most effectively through the voluntary efforts of individual and/or multiple property owners through a subarea conceptual development plans process. Chapter 2: Development Framework & Standards includes a package of available incentives for the preparation of a subarea conceptual development plan.

### REQUIRED SUBAREA PLANS

The Director of Community Development may require preparation of a subarea conceptual development plan for all existing properties in, and all projects proposed within, the SPA or subarea if the Director of Community Development finds that a project-specific application(s) would result in piecemeal development that ultimately prevents or precludes future development of adjacent properties in a manner consistent with the TCSP.

### CONSTRUCTION TRAFFIC & ACCESS MANAGEMENT PLANS

Construction Traffic and Access Management Plans are used to avoid construction-related safety hazards and to reduce disruptions of traffic operations. Construction Traffic and Access Management Plans will be required of development project applicants, to address temporary traffic impacts that could occur during the construction of the TCSP. The plans will be subject to approval by the City of Santa Clarita Department of Public Works. They will address construction traffic routing (e.g., detours and/or lane closures) and traffic control (e.g., with signage and construction flaggers), as well as vehicle, bicycle, and pedestrian safety. The plans will also be required to identify designated haul routes and construction staging areas, construction crew parking, emergency access provisions, traffic control procedures, and avoidance of traffic safety impacts during construction. The construction management plan will coordinate and ensure business, resident, and customer parking, traffic flow, and business operations are maintained throughout the construction period. Construction management plans must be prepared in coordination when two or more projects are to be in construction simultaneously within the SPA.

### LOCAL TRANSPORTATION ASSESSMENT REQUIRED

All development projects will be reviewed in accordance with the Santa Clarita Local Transportation Assessment Guidelines for studying a project’s effects on the transportation system.

The purpose of the Local Transportation Assessment is to provide an in-depth project review to determine if operational improvements are needed to accommodate a project. The Local Transportation





Assessment Report includes vehicle operations analysis (intersection and segment level of service (LOS) and identifies any local transportation improvements required to accommodate the development while maintaining an acceptable LOS. This report will be reviewed by the City’s Public Works Department as part of the decision-making process. The cost of any required improvements will be the responsibility of the developer.

The Local Transportation Assessment is required to be completed to the satisfaction of the City’s Public Works Department prior to the issuance of a land use entitlement and is required for projects generating 50 or more net new trips during either the AM or PM peak hours. Alternatively, a Local Transportation Assessment can be prepared to cover the projected buildout of a TCSP subarea or the entire SPA. Projects consistent with the development buildout analyzed in Local Transportation Assessment covering the applicable subarea or entire SPA shall be required to contribute their fair share of any required improvements identified within such Assessment, to the satisfaction of the City’s Public Works Department.



## 5. ADEQUATE INFRASTRUCTURE AND UTILITIES

The TCSP area has adequate Infrastructure and utilities for the existing and anticipated future development. This includes water supply, sewer, stormwater, power supply, gas supply, solid waste disposal, and telecom systems serving the SPA. It also includes public realm elements such as roadway network, access to the SPA, and parking which have been discussed in previous sections.

The General Plan and the existing CR zone currently plan for and allow a floor area ratio (FAR) of 2.0 and residential densities up to 50 units per acre. The CEQA documentation for the General Plan Update accounted for the capacity and supply of basic utilities such as gas, electricity, water, sewer, and stormwater to support the FAR of 2.0 and density of 18 to 50 units per acre. The TCSP is consistent with and does not change or increase the land use or density allowed by the General Plan or existing CR zone, and therefore does not impact or affect the adequacy of the infrastructure for the area.

Any development in the SPA will be required to submit detailed plans for utilities provided to the structure. These will be in accordance with the building permit package requirements set forth by the City of Santa Clarita.

Costs to provide gas, electricity, sewer, water, and telecommunication to each development area will be borne by the developers to the extent that off-site infrastructure is required.

Below is a discussion on status of primary utilities serving the SPA.

### WATER

Water is provided to SPA by Santa Clarita Valley (SCV) Water. SCV Water has evaluated the long-term water needs (water demand) within its service area and has compared these needs against existing and potential water supplies. Demand projections are based on applicable population projections and county and City land use plans, and account for conservation as well as climate change impacts and other relevant factors. Results indicate that the total projected water supplies available to the SCV Water service area over the 30-year projection during normal, single-dry, and multiple-dry year (five-year drought) periods are sufficient to meet the total projected water demands throughout the Valley provided that SCV Water continues to utilize available SWP Table A Supply Reliability (See Table 1), and will continue to incorporate conjunctive use (coordinated use of surface water and groundwater), water conservation, water transfers, recycled water, and water banking as part of the total water supply portfolio and management approach to long-term water supply planning and strategy<sup>1</sup>.

<sup>1</sup> 2020 Urban Water Management Plan for Santa Clarita Valley Water June 2021 Agency, /[https://yourscvwater.com/sites/default/files/SCVWA/SCVWA-2020-UWMP-Volume-I\\_FINAL.pdf](https://yourscvwater.com/sites/default/files/SCVWA/SCVWA-2020-UWMP-Volume-I_FINAL.pdf), accessed March 19, 2024

**Table 3-1. SWP Table A Supply Reliability**

Wholesaler (Supply Source)	2020	2025	2030	2035	2040-2050
<b>Average Water Year<sup>(c)</sup></b>					
SWP Table A Supply	55,216	53,312	51,408	50,456	49,504
% of Table A Amount <sup>(d)</sup>	58%	56%	54%	53%	52%
<b>Single-Dry Year</b>					
SWP Table A Supply <sup>(e)</sup>	6,664	7,616	8,568	9,520	10,472
% of Table A Amount <sup>(e)</sup>	7%	8%	9%	10%	11%
SWP Table A Supply <sup>(f)</sup>	4,760	4,760	4,760	4,760	4,760
% of Table A Amount <sup>(f)</sup>	5%	5%	5%	5%	5%
<b>Multiple-Dry Year<sup>(g)</sup></b>					
SWP Table A Supply <sup>(g)</sup>	23,800	23,800	23,800	23,800	23,800
% of Table A Amount <sup>(d)</sup>	25%	25%	25%	25%	25%

Notes:

- (a) Supplies to SCV Water are based on DWR analyses presented in its 2019 DCR, assuming existing SWP facilities and current regulatory and operational constraints (except as otherwise indicated in Note f).
- (b) Table A supplies include supplies allocated in one year that are carried over for delivery the following year.
- (c) Based on average deliveries over a repeat of the study's historic hydrologic period of 1922 through 2003.
- (d) Supply as a percentage of SCV Water's Table A Amount of 95,200 AF.
- (e) Based on a repeat of the worst case historic single dry year of 1977 (from 2019 DCR)
- (f) Based on the worst-case actual allocation of 2014.
- (g) Supplies shown are annual averages over five consecutive dry years, based on a repeat of the historic five-year dry period of 1988-1992.

## SEWER

The City of Santa Clarita owns the local-serving sewer lines and the Los Angeles County Sanitation Districts owns trunk sewers in the project vicinity, with maintenance provided by Los Angeles County Sewer Maintenance Division (SMD). The City of Santa Clarita's Public Works Department ensures that the public sewer infrastructure is correctly designed and adequately sized prior to transfer of the sanitary sewer system to the County of Los Angeles SMD for maintenance. A sewer area study is required to be submitted by the developer to assess adequate sizing of the any new portion or

<sup>2</sup>City of Santa Clarita SSMP, [https://santaclarita.gov/wp-content/uploads/sites/42/migration/City SSMP\\_2020.pdf](https://santaclarita.gov/wp-content/uploads/sites/42/migration/City_SSMP_2020.pdf), accessed March 19, 2024.

connection to the sanitary sewer. The SPA has trunk lines on Magic Mountain Parkway and McBean Parkway to serve the project. Quick Structural Rating Report Priority List from the July 2018 City of Santa Clarita Sewer System Management Plan does not indicate any repairs needed to the sewer lines surrounding SPA<sup>2</sup>.

## STORMWATER

The City of Santa Clarita has a network of stormwater infrastructure, including curbs, gutters, and storm drains, that prevent flooding on local streets by redirecting water into the Santa Clara River.

The City is required to comply with the requirements of the National Pollutant Discharge Elimination System (NPDES) Municipal Separate Storm Sewer System (MS4) Permit for stormwater and non-stormwater discharges from the MS4 within the coastal watersheds of Los Angeles County. In order to meet the standards created by the MS4 permit, there are a variety of stormwater quality control measures that can be implemented such as Green Street Low Impact Development (LID) practices within transportation corridors, such as City streets, roadways and parking areas. The developers are highly encouraged to use best management practices as discussed in City of Santa Clarita Green Streets Selection Guidance Manual<sup>3</sup> for handling stormwater runoff. The use of stormwater treatment techniques such as bioswales, Silva Cells, and permeable paving is encouraged in landscape and parking areas.

County of Los Angeles SSMP, [https://www.lacsd.org/home/showpublisheddocument/970/63763756595463000\\_0](https://www.lacsd.org/home/showpublisheddocument/970/63763756595463000_0), accessed March 19, 2024.

<sup>3</sup> City of Santa Clarita Green Streets Selection Guidance Manual, [https://filecenter.santa-clarita.com/GreenSC/2021-01-15\\_B351\\_Santa%20Clarita%20GS%20LID%20Guidance%20Manual\\_web.pdf](https://filecenter.santa-clarita.com/GreenSC/2021-01-15_B351_Santa%20Clarita%20GS%20LID%20Guidance%20Manual_web.pdf), accessed March 19, 2024.

Figure 3-6. Existing SMD Sanitary Sewer Facilities





## **GAS**

Southern California Gas Company (SoCalGas) provides gas supply to the SPA.

## **ELECTRICITY**

Southern California Edison provides electricity to the SPA.

The use of solar panels to provide shade in parking areas is encouraged. Creative use of solar panels for building facades and roofs is also encouraged. Additionally, the provision of shade through the use of trees and temporary/permanent structures is encouraged.

## **TELECOMMUNICATION**

AT&T and Comcast provide telecommunication services to the SPA.

## **SOLID WASTE DISPOSAL**

The City's current franchised waste hauler is Burrtec Waste Industries, Inc. (Burrtec), which provides trash and recycling services for multi-family and commercial properties in the City of Santa Clarita. The developer will work with the City's franchised waste hauler for solid waste management services including bulk item pickup, cardboard recycling, and compactor services. The developer will provide detailed solid waste management plans per the City's requirements when submitting any entitlement package.

## 6. ADMINISTRATION

### OWNERSHIP & MAINTENANCE

#### PRIVATE OWNERSHIP & MAINTENANCE

Except as explicitly proposed and approved as part of the project entitlement, or as already owned and maintained by the City, all proposed improvements will be owned, maintained, and managed by the project developer.

#### PARKWAYS & PUBLIC AREAS

The TCSP includes the development of public gathering spaces, and landscape parkways that propose pedestrian activities and provide an aesthetic backdrop to development. The maintenance of these common areas shall be the responsibility of the developer(s), a Property Owners Association (POA), or a maintenance assessment district established for all projects within the TCSP. For public areas with enhanced improvements, the developer shall enter into a bonded maintenance agreement ensuring maintenance of all landscaping and appurtenances within the right-of-way until such time a maintenance assessment district, homeowners association (HOA), POA, or other such mechanism is established for the maintenance of all landscaping and appurtenances.

#### PRIVATE DEVELOPMENT LANDSCAPED AREAS

Landscaping, private recreational amenities, and open areas will also be developed in conjunction with private development proposals. The maintenance of these areas will be the responsibility of individual property owner(s) or a POA.

### NONCONFORMING USES

Any uses, lots, or structures that become legal nonconforming as a result of the adoption of the TCSP shall be subject to the provisions and requirements of UDC 17.05 (Legal Nonconforming Uses, Lots and Structures). See also Chapter 2, Section 2. Applicability, regarding legal nonconforming uses.

### AMENDMENTS

All proposed or required amendments to the TCSP shall follow the requirements and procedures of UDC 17.28.110 (Specific and Corridor Plans, subsection H, Adoption and Amendments).

### SEVERABILITY

In the event that any regulation, condition, program, policy, or provision of this Specific Plan, or the application thereof to any person or circumstance, is held to be invalid or unconstitutional by any court of competent jurisdiction, such regulation, condition, program, policy, or provision shall be deemed separate, distinct, and independent, and shall not affect the validity of the remaining provisions of this Specific Plan, or applications thereof that can be implemented without the invalid provision or application, unless the deletion of such regulation, condition, program, policy, or provision would result in a material change so as to cause compliance or enforcement of the Specific Plan to be unreasonable.