

VOLUME I OF II

DRAFT

**Amended Henry Mayo Newhall Hospital  
Specific Plan**

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NOVEMBER 2020



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# CHAPTER 1 INTRODUCTION

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## 1.1 EXECUTIVE SUMMARY

Upon adoption by the City Council, this document will represent the 2020 Amended Henry Mayo Newhall Hospital (HMNH) Specific Plan, which was originally adopted in 2016. The Specific Plan adopted in 2016 is referred to herein as the 2016 Specific Plan. The 2020 Amended HMNH Specific Plan is herein referred to as the 2020 Amended Specific Plan (ASP). The 2020 ASP replaces the 2016 Specific Plan in its entirety. The 2020 ASP proposes to allow for the development of up to 200,000 square feet of additional building area for a new Diagnostic and Treatment (D&T) Building, Inpatient Building 2 (including up to 92 beds), clinical services and up to 292 new parking spaces to be added to Parking Structure 4 (PS-4) through the construction of up to three above ground levels added to the existing structure.

The 2020 ASP does not change the maximum number of beds (368) permitted under the 2016 Specific Plan and 2008 Master Plan. Up to 92 beds will be relocated out of the existing Main Hospital building to Inpatient Building 2. The area within the existing Main Hospital building containing the 92 beds will be converted to office uses, other administrative uses and/or clinical/hospital support services.

More specifically, the 2020 ASP includes the following:

- Construction of a three-story, 84,300-square-foot, D&T Building within the Lot D area of the 2020 ASP (See Figure 2-1, Site Plan, in Chapter 2, Development Plan). This building will also include a new below-ground basement. Uses within this building will include various clinical related uses, a pharmacy and imaging areas.
- Construction of a five-story, 115,700-square-foot, Inpatient Building 2 within the Lot D area of the 2020 ASP and directly adjacent to the D&T Building. This building will also include a below-ground basement. Uses within this building will include inpatient beds, support services, public spaces and additional diagnostic and treatment facilities.

The 2020 ASP will also involve relocating the main entry/drop off area from its present location adjacent to the existing Main Hospital Building to in front of the new D&T Building; and adding up to 292 new parking spaces to PS-4 through the construction of up to three above ground levels added to the existing structure. Finally, the 2020 ASP includes modifications to the parking plan to permit both paid and assigned parking in the on-site parking areas within the 2020 ASP Area.

The 2020 ASP proposes revisions to the Development Plan (Chapter 2) and Development Regulations (Chapter 4) of the 2016 Specific Plan to facilitate the increase in building area and floor area ratio (FAR), modify the height zones, relocate the main entry/drop off area, and increase on-

site parking. The 2020 ASP also proposes minor revisions to the 2016 Specific Plan Introduction (Chapter 1) to reflect updates to the project background; and Infrastructure Development Plan (Chapter 3) to update changes to infrastructure development plans for drainage/water quality, water service, wastewater service, and dry utilities. A Parking Study has been completed and is included as Attachment E, which addresses the parking demand for the proposed 2020 ASP. Many Specific Plan figures have been revised as well to reflect the proposed changes to the Site Plan.

At the time of preparation of this 2020 ASP, construction of Inpatient Building 1 (including 142 hospital beds) is complete. This building was approved in 2008 as part of the HMNH Master Plan and Development Agreement and expanded as part of the 2016 Specific Plan. Construction of Inpatient Building 1 commenced in early 2017 and became operational in 2019. The Loading Dock that was previously under construction at the time of the 2016 Specific Plan is now complete and the Central Plant is also complete.

## **1.2 INTENT AND PURPOSE OF THE 2020 AMENDED SPECIFIC PLAN**

The 2020 Amended Henry Mayo Newhall Hospital (HMNH) Specific Plan (2020 ASP) codifies the development parameters of the 2008 Henry Mayo Newhall Memorial Hospital Master Plan and 2016 and 2020 Amendments to the Master Plan (herein referred to as the “2008 Master Plan and Amendments”; Appendix A) and provides a framework for future modifications to the 2008 Master Plan and Amendments. The 2008 Master Plan and Amendments continues to exist independent of the Specific Plan. A Development Agreement covering the 2008 Master Plan and Amendments area was approved by the City in December, 2008 (herein referred to as the “Development Agreement”; Appendix B). The Development Agreement will terminate in December 2023, unless extended. The Development Agreement, as amended in 2016 and again in 2020 in connection with the 2020 ASP, continues to exist independent of the 2020 ASP. The 2020 ASP contains development plans, infrastructure development plans, development regulations, design guidelines, and an implementation program for the HMNH campus. The purpose of the Specific Plan is to outline existing uses, including those approved in the 2016 Specific Plan and the 2008 Master Plan and Amendments, as well as to address new buildings and associated changes proposed as part of this 2020 ASP.

The 2016 Specific Plan was necessitated by required modifications to the 2008 Master Plan and Amendments, which resulted from 1) amendments to the California Building Standards Code that impose mandatory new requirements related to the design and construction of hospitals, and 2) the expansion of the Obstetric Services Unit through construction of two new Caesarean section rooms, in order to accommodate the growing need for obstetric services in the Santa Clarita Valley and better contract services from insurance providers, as described in detail in Section 1.3.



This 2020 ASP is necessitated by the need to bring hospital rooms up to date within the existing Main Hospital building. Upgrading the existing beds within the Main Hospital building to meet current and future Office of Statewide Health Planning and Development (OSHPD) codes and Americans with Disabilities Act (ADA) requirements within the existing space would result in excessively high costs associated with remodeling within a constrained space. Therefore, a new Inpatient Building (Inpatient Building 2) and supporting D&T Building is needed in order to maintain the permitted number of beds while complying with State and federal requirements. The addition of Inpatient Building 2 and supporting D&T Building requires an update to the 2008 Parking Study prepared in conjunction with the 2008 Master Plan and Amendments, which is included as Appendix E.

The intent of the Specific Plan is to guide the orderly growth and development of the hospital, medical office buildings and associated outpatient services to create a visually cohesive, operationally organized and successful campus that balances the needs for medical service expansion with the need to preserve the character of the neighborhoods that surround this regional institution.

The 2016 Specific Plan was prepared in conjunction with a Zone Change and General Plan Amendment request to designate the site as Specific Plan (SP). The City of Santa Clarita Municipal and Unified Development Code (SCMC), Section 17.28.110, establishes provisions for the SP zone. The SP zone is intended to achieve the following purposes:

1. To promote and protect the public health, safety and welfare;
2. To implement the objectives and policies of the General Plan;
3. To safeguard and enhance environmental amenities, such as oak trees and ridgelines, and enhance the quality of development;
4. To attain the physical, social, and economic advantage resulting from comprehensive and orderly planned use of land resources;
5. To lessen congestion and assure convenience of access: to secure safety from fire, flood and other dangers; to provide for adequate light, air, sunlight and open space; to promote and encourage conservation of scarce resources; to facilitate the creation of a convenient, attractive and harmonious community; to attain a desirable balance of residential and employment opportunities; and to expedite the provision of adequate and essential public services;
6. To facilitate development within the City in accordance with the General Plan by permitting greater flexibility and, consequently, more creative and imaginative designs for large-scale development projects than generally are possible under conventional zoning regulations;

7. To promote more economical and efficient use of the land while providing a harmonious variety of housing choices and commercial and industrial activities, a high level of urban and public amenities and preservation of natural and scenic qualities of open space; and
8. In the case of a specific plan, to provide a process for initiation, review and regulation of large-scale comprehensively planned communities that afford the maximum flexibility to the developer within the context of an overall development program and specific, phased development plans coordinated with the provision of necessary public services and facilities.

This 2020 ASP will achieve the purposes of the City’s SP zone, and will serve as the zoning code for the Specific Plan area (described more fully in Chapter 2), as amended (or modified) herein.

### **1.3 ORGANIZATION OF THE 2020 ASP**

Volume I of the 2020 ASP is divided into six chapters. The content of each chapter is described below.

**Chapter 1, Introduction**, provides an executive summary to summarize the purpose and scope of the 2020 ASP and describes the intent, purpose, structure, and organization of the 2020 ASP. The chapter also provides a background leading up to the preparation of this 2020 ASP and describes the existing setting, including location, surrounding land uses, and site characteristics. In addition, the chapter discusses the 2020 ASP authorization and addresses the relationship between the 2020 ASP and the Santa Clarita General Plan, SCMC, and compliance with the California Environmental Quality Act (CEQA).

**Chapter 2, Development Plan**, identifies the 2020 ASP objectives and development plans, including the site plan, parking plan, and landscape plan. Each plan is described in detail in this chapter.

**Chapter 3, Infrastructure Development Plan**, describes the infrastructure and utilities needed to serve the 2020 ASP area. Specifically, the chapter describes the backbone infrastructure systems (sewer, water, water quality, and storm drainage) and utilities (gas, electricity, etc.) needed to support the 2020 ASP.

**Chapter 4, Development Regulations**, sets forth the site development standards, allowed land uses and permit requirements, permitted accessory uses, parking, signage restrictions, and other regulations.

**Chapter 5, Design Guidelines**, address design themes, site planning, and architectural themes within the 2020 ASP.

**Chapter 6, Specific Plan Implementation**, describes the 2020 ASP implementation procedures.

Volume II of the 2020 ASP includes the following appendices supporting the 2020 ASP: Appendix A, 2008 Master Plan and Amendments to the Master Plan; Appendix B, Development Agreement and Amendments to the Development Agreement; Appendix C, General Plan Consistency; Appendix D, 2020 Supplemental Environmental Impact Report (EIR) to the Original Master Plan EIR and 2016 Addendum; Appendix E, Parking Demand Study; and Appendix F, Exterior Master Sign Program and Amendments.

## **1.4 BACKGROUND**

The HMNH has been operational for over 40 years. It has served as the Santa Clarita Valley’s only major regional medical center and operates the only 24-hour Emergency Department and trauma service in the Santa Clarita Valley. The hospital was originally permitted under Los Angeles County jurisdiction in 1971, prior to the City’s incorporation in December 1987. Upon adoption of the City’s first General Plan in 1991 and subsequent Unified Development Code in 1992, the HMNH campus received a General Plan and zoning designation of Residential Low (RL). At the time the City did not have an institutional land use designation; therefore, land uses such as schools, fire stations, and hospitals were designated a residential land use when located in residential areas.

Since the original approval of the hospital under Los Angeles County jurisdiction, there have been several entitlement requests for alterations and expansions to the original hospital facility, which have received subsequent approvals by both Los Angeles County and the City. In 2008, the City approved a 15-year Master Plan and Development Agreement that will guide the future expansion of the campus facilities as the medical care needs of the Santa Clarita Valley continue to grow. Prior to approval of the implementation of the 2008 Master Plan and Development Agreement, the HMNH campus contained 340,071 square feet of hospital and medical office space and 221 hospital beds in a total of 11 buildings.

### **2008 Master Plan and Development Agreement**

The 2008 Master Plan and Development Agreement ensured that future expansions will not be considered in piecemeal fashion, but rather as part of a comprehensive plan that looks ahead to the ultimate build-out of the campus. The 2008 Master Plan and Development Agreement approved an increase in the existing square footage of the HMNH campus from 340,071 square feet to 667,434 square feet, including a new inpatient hospital building, three medical buildings and a new central plant building and parking structures. The conditions and requirements of the 2008 Master Plan and Development Agreement are found in Appendix A, 2008 Master Plan and Amendments and Appendix B, 2008 Development Agreement and Amendments, to this 2020 ASP. These documents continue to be in effect, as amended.

## **2016 Specific Plan**

In 2011, the City adopted a General Plan Update, which resulted in a change in land use designation for the HMNH campus from RL to Public Institutional (PI). In 2013, the City adopted a Zoning Code Update, which resulted in the hospital campus being rezoned to PI. Subsequent to approval of the 2008 Master Plan, the California Building Standards Code (CBC) was amended to impose mandatory new requirements related to the design and construction of hospitals. Compliance with the new regulations required an increase in the square footage of the approved Inpatient Building 1 to increase the number and width of exiting corridors between departments (CBC Section 1224.4.7.1), provide two separate elevators for clean items delivered and soiled items hauled (CBC Section 1224.20.2.7), and a dedicated corridor between food storage areas and the kitchen (CBC Section 1224.20.2.1). The Hospital also sought to enlarge its Obstetric Services Unit in order to better contract services from insurance providers. This involved an increase in square footage in order to allow construction of two new Caesarean Section rooms in the area between the existing Hospital and Inpatient Building 1. In order to modify the development parameters of the 2008 Master Plan to implement the mandatory new California Building Standards Code requirements and permit the expanded obstetric services, it was determined that a Specific Plan was needed to outline existing uses, future uses approved under the 2008 Master Plan, and the proposed modifications to the development parameters of the 2008 Master Plan, and provide a cohesive set of standards and guidelines for how the proposed Specific Plan modifications will occur on the hospital campus.

The 2016 Specific Plan resulted in the increase of allowable hospital space on the site (not including parking structures) by 30,566 net square feet from what was approved in the 2008 Master Plan and Development Agreement, to an ultimate buildout of 698,000 square feet for the entire 2016 Specific Plan area, resulting from a 28,094-square-foot increase to Inpatient Building 1 and a 6,400-square-foot reduction in the Central Plant, as described in detail in Chapter 2 (Development Plan). Under the 2016 Specific Plan, no increases in programs, employees, building heights, or square footage allocated to administration uses occurred. A shifting of hospital beds occurred from what was conceptually approved in the 2008 Master Plan, as described in Chapter 2 (Development Plan); however, the total number of beds (368) did not change. The 2016 Specific Plan facilitated the implementation of the development parameters of the 2008 Master Plan and Amendments, along with the Development Agreement as amended, currently having a 15-year life expiring in 2023, unless modified.

## **2020 ASP**

This 2020 ASP is necessitated by the need to bring hospital rooms up to date within the existing Main Hospital building. Upgrading the existing beds within the Main Hospital building to meet current and future OSHPD codes and Americans with Disabilities Act (ADA) requirements within

the existing space would result in excessively high costs associated with remodeling within a constrained space. For example, many rooms within the existing Main Hospital building would not be large enough in the future to remain double occupancy rooms and therefore would have to be converted to single occupancy units, thereby reducing the number of beds. In order to maintain the existing number of beds while complying with OSHPD and ADA requirements, a new Inpatient Building (Inpatient Building 2) and supporting D&T Building is needed.

The addition of Inpatient Building 2 and supporting D&T Building requires an update to the 2008 Parking Study prepared in conjunction with the 2008 Master Plan and Amendments. This update, included as Appendix E, addresses parking demand for the proposed 2020 ASP along with other unbuilt but approved buildings and existing uses. As a result of the Parking Study, it was determined that the construction of additional parking spaces was necessary to supply the additional parking required from the new Inpatient Building 2 and D&T Buildings. Therefore, the existing parking structure PS-4 is proposed to be expanded to accommodate the required increase in parking spaces per the Update to the 2008 Parking Study.

## **1.5 EXISTING SETTING**

### **1.5.1 Project Location**

The 2020 ASP area (plan area) encompasses approximately 29.77 acres of land generally located north of the intersection of McBean Parkway and Orchard Village Road, east of Interstate 5 (I-5) in the City of Santa Clarita; see Figure 1-1, Regional Map. The plan area is coterminous with the existing HMNH campus located on McBean Parkway at the northern terminus of Orchard Village Road; see Figure 1-2, Vicinity Map.

### **1.5.2 Surrounding Uses**

The areas adjacent to the plan area include the following uses:

- North: Land uses consist of single- and multiple-family residences zoned Urban Residential (UR4) and (UR1), open space zoned Open Space (OS), medical office buildings zoned Public Institutional (PI), and the Sunrise at Sterling Canyon facility, a senior living facility that provides independent living, assisted living, and hospice care.
- South and east: Land uses consist primarily of single-family residences zoned Urban Residential (UR2), and the United Methodist Church is located on the opposite side of McBean Parkway. Residential uses immediately east and southeast of HMNH were developed in 1969.
- West: Land uses consist primarily of single-family residences zoned Urban Residential (UR1) and an open space area zoned Open Space (OS). The residential uses immediately west of HMNH were developed in 1978.

### 1.5.3 Site Characteristics

The hospital campus occupies 586,000 square feet of building area including the Central Plant and Inpatient Building 1, which started construction in early 2017 and is now complete. The hospital campus is comprised of 377,415 square feet of hospital and related uses, 24,425 square feet of support facilities, and 184,160 square feet of medical offices (see Table 1-1, Existing HMNH Facilities and Uses). This table presents the square footage of the various buildings, bed count, and building height.

As shown in Table 1-1, the 3,600-square-foot Central Plant represents a 6,400-square-foot reduction from that approved in the 2008 Master Plan. The reason for this reduction, which was done as part of the 2016 Specific Plan, was that a portion of the Central Plant services was located in the basement of Inpatient Building 1. The 8,000-square-foot former Foundation & Administrative Office Building will be removed when MOB 3 is constructed, to accommodate the development of MOB 3.

**Table 1-1  
Existing HMNH Campus Facilities and Uses**

Use	Existing Facilities (square feet)	Hospital Beds	Building Height (feet)
<i>Hospital and Related Uses</i>			
Main Hospital	146,000	136	44
Main Hospital Basement	5,286	—	—
Nursing Pavilion Building	63,800	106	28
Inpatient Building 1 (Completed per 2008 Master Plan; square footage adjusted under 2016 Specific Plan) <sup>1</sup>	153,457	118	84
Loading Dock <sup>1</sup>	8,872		16
<i>Subtotal Hospital and Related Uses</i>	<i>377,415</i>	<i>360</i>	<i>—</i>
<i>Support Facilities Uses</i>			
Hospital Bridge (covered walkway)	9,122	—	—
Mechanical Plant	8,585	—	22
Facilities Building (warehouse)	2,384	—	22
Facilities Building (office)	734	—	22
Central Plant (Completed per 2008 Master Plan)	3,600		16.3
<i>Subtotal Support Facilities Uses</i>	<i>24,425</i>	<i>—</i>	<i>—</i>
<i>Medical Office Buildings (MOB)</i>			
MOB A	5,302	—	18
MOB B	5,302	—	18
MOB C	5,302	—	18
MOB D	5,302	—	18
MOB E	31,040	—	29
MOB F	43,912	—	33

**Table 1-1**  
**Existing HMNH Campus Facilities and Uses**

Use	Existing Facilities (square feet)	Hospital Beds	Building Height (feet)
MOB 1	80,000	—	45.5
Foundation & Administrative Office Building	8,000	—	12
<i>Subtotal Medical Office Buildings</i>	<i>184,160</i>	—	—
Total	586,000	360	—
Site Acreage	29.77	—	—

## Notes:

<sup>1</sup> The 2016 Specific Plan facilitated an adjustment to Inpatient Building 1 originally approved as part of the 2008 Master Plan. Specifically, the 2016 Specific Plan increased the square footage of Inpatient Building 1 from 125,363 square feet to 153,457 square feet (an increase of 28,094 square feet from the approved 2008 Master Plan) and relocated the Loading Dock that was approved under the 2008 Master Plan. The Loading Dock was moved from Inpatient Building 1 to the Infill Area, which is the area between the Hospital and Inpatient Building 1.

Figure 1-3, Aerial Map, provides an aerial view of the existing hospital campus. The existing hospital campus is illustrated in Figure 1-4, Existing Site Plan. The plan area is oriented in a northwest/southeast configuration with the west, northwest, and north orientation along the site's back boundary and the east, southeast, and south orientation along the site's boundary along McBean Parkway.

### Site Coverage and Building Heights

As shown on Figure 1-4, the Main Hospital building is located in the center of the campus and is connected to the Nursing Pavilion Building, located in the northeastern portion of the site, by an enclosed bridge. Between the Main Hospital and the Nursing Pavilion is the Loading Dock and Inpatient Building 1. Adjacent to the Nursing Pavilion Building are several smaller buildings, including the Central Plant, Facility Office, and Disaster Supply Storage Building. MOB 1, PS 1, and PS 4 are located across the internal driveway from the Main Hospital and Nursing Pavilion buildings, and front on McBean Parkway. MOB A, MOB B, MOB C, MOB D, MOB E, MOB F, and the former Foundation and Administration Office Building are located in the western portion of the site, along with several surface parking lots.

### Parking

On-site parking currently includes both surface parking and structured parking, as shown on Figure 1-4, Existing Site Plan. The existing on-site parking supply for the HMNH campus totals 1,547 spaces, consisting of 1,099 spaces in parking structures, 413 surface parking spaces in parking lots and 35 parallel parking spaces along the internal roadways of the site.

The campus currently provides two parking structures. PS 1 and PS 4 are located along McBean Parkway. PS 1 is a six-level, 750-space parking structure (five levels aboveground with one subterranean level), located along McBean Parkway at Avenida Navarre. From the street view, the parking structure is four levels. The height of the parking structure is 47 feet to the top of the parapet, 49.5 feet to the top of the parking lot lights, and 60.5 feet to the top of the windsock, and it includes rooftop parking and a helipad. PS 4 is currently a four-level (three subterranean levels and one surface level), 349-space parking structure along McBean Parkway at its northeast intersection with Orchard Village Road.

### **Site Access and Circulation**

Access to the plan area is currently provided via three driveways located along McBean Parkway, as shown on Figure 1-5, Existing Circulation Plan. Two locations are currently controlled by traffic signals at Orchard Village Road and Avenida Navarre. A third unsignalized driveway is located just east of the westerly property line and allows both right and left turns onto and from McBean Parkway.

### **Helipads**

A new primary helipad was built atop Inpatient Building 1. There is currently a helipad located on the roof of PS 1 along McBean Parkway, which will remain as an emergency facility. No additional helipads are proposed under the 2020 ASP. HMNH does not own a helicopter, nor do any helicopters reside at HMNH. The helipads are for incoming flights for trauma and other transfers to HMNH and departing flights for specialty care, such as advanced neonatal intensive-care unit and seriously ill pediatric patients.

### **Existing General Plan and Zoning Designations**

The adoption of the 2016 Specific Plan resulted in an amendment to the City's General Plan Map and Zoning Map from Public/Institutional to Specific Plan.

## **1.6 AUTHORITY**

The 2020 ASP has been prepared pursuant to the provisions of the California Government Code, Title 7, Division 1, Chapter 3, Article 8, Sections 65450 through 65457. The California Government Code authorizes jurisdictions to adopt specific plans by resolution as policy documents or by ordinance as regulatory documents. The law allows preparation of specific plans, as may be required for the systematic execution of the General Plan and further allows for their adoption and amendment.



The 2020 ASP is regulatory in nature and will serve as the zoning code for the Specific Plan site. The 2020 ASP is prepared in accordance with SCMC, Section 17.28.110, Specific Plan Zone. Section 17.28.110 includes adoption procedures, as well as requirements for specific plans to include use regulations, development regulations, performance standards, and amendment procedures. Development plans and other entitlement requests processed in conjunction with the 2020 ASP must be consistent with both the 2020 ASP and the City’s General Plan.

## **1.7 RELATIONSHIP TO THE GENERAL PLAN**

The 2020 ASP implements the goals and policies of the City’s General Plan. Appendix C contains an analysis of the 2020 ASP’s consistency with the City’s General Plan. This analysis confirms that adoption of the 2020 ASP is consistent with the General Plan.

## **1.8 RELATIONSHIP TO THE SANTA CLARITA MUNICIPAL CODE**

The 2020 ASP, including all attachments, will be incorporated into the SCMC and provide the allowable land uses, development regulations, design guidelines, and implementation procedures for the property within the boundary of the 2020 ASP.

The regulations contained within the 2020 ASP are in addition to those set forth in the planning, zoning, and subdivision provisions of the SCMC and do not convey any rights not otherwise granted under the provisions and procedures contained in the SCMC or other ordinances, except as specifically provided for herein. Whenever the 2020 ASP contains provisions that establish regulations, including intensities, heights, parking, signage, open space, and landscaping requirements, which are different from or more restrictive or permissive than would be allowed pursuant to the provisions of the SCMC, the 2020 ASP shall prevail and supersede the applicable provisions of the SCMC.

## **1.9 COMPLIANCE WITH OTHER LAWS AND REGULATIONS**

### **1.9.1 Global Warming Solutions Act of 2006**

In 2006, the California Legislature enacted the California Global Warming Solutions Act of 2006. Also known as Assembly Bill (AB) 32, the law designates the California Air Resources Board (CARB) as the state agency responsible for monitoring and regulating sources of greenhouse gas (GHG) emissions and for devising rules and regulations that will achieve the maximum technologically feasible and cost-effective GHG emissions reductions. Specifically, the law seeks to achieve a reduction in statewide GHG emissions to 1990 levels by 2020, and sets forth a timeline for the adoption of measures to evaluate and reduce GHG emissions across all source categories. Significant among the actions included in the timeline are the requirements that CARB: (a) adopt regulations, to be enforced by January 1, 2010, to implement “early action measures” to reduce GHG emissions; and (b) adopt additional regulations, effective January 1, 2012, to achieve the GHG emissions reduction goals established by the new law.

The City adopted a Climate Action Plan (CAP) in August 2012. The purpose of the CAP is to measure the amount of greenhouse gas emissions generated within the City and to develop strategies to reduce the emissions in the future. Using the goals, objectives and policies of the General Plan as a starting point, the CAP identifies those measures that can be quantified and translated into significant reductions in the greenhouse gas emissions by the year 2020.

The 2020 ASP allows for increases in square footage beyond what was approved under the 2016 Specific Plan and 2008 Master Plan and Amendments. Appendix D of this 2020 ASP provides the 2020 Supplemental EIR to the Original 2008 Master Plan EIR and 2016 Addendum, which describes the 2020 ASP's impacts on global climate change. Implementation of the 2020 ASP will not cause the effects of on-site GHG emissions or energy emissions to become significant, nor will effects of mobile emissions be substantially more severe because there will be little or no increase in traffic generation. The 2020 ASP and 2020 Supplemental EIR also address energy, water, water quality, and landscaping components to ensure overall efficiency and conservation.

## **1.10 COMPLIANCE WITH THE CALIFORNIA ENVIRONMENTAL QUALITY ACT**

On November 19, 2008, the City of Santa Clarita certified the Master Plan EIR (State Clearinghouse no. 2004111149) for the 2008 Master Plan and Development Agreement Project. On June 28, 2016, the City certified the 2016 Addendum to the 2008 Master Plan EIR for adoption of the 2016 Specific Plan and amendments to the 2008 Master Plan and Development Agreement. The City has determined that the proposed 2020 ASP requires a supplement to the original 2008 Master Plan EIR and 2016 Addendum. The City has analyzed the potential environmental effects of the 2020 ASP and associated project approvals in accordance with CEQA and the CEQA Guidelines. The 2020 Supplemental EIR must be approved by the City Council prior to taking action on the 2020 ASP. The 2020 Supplemental EIR is included as Appendix D.



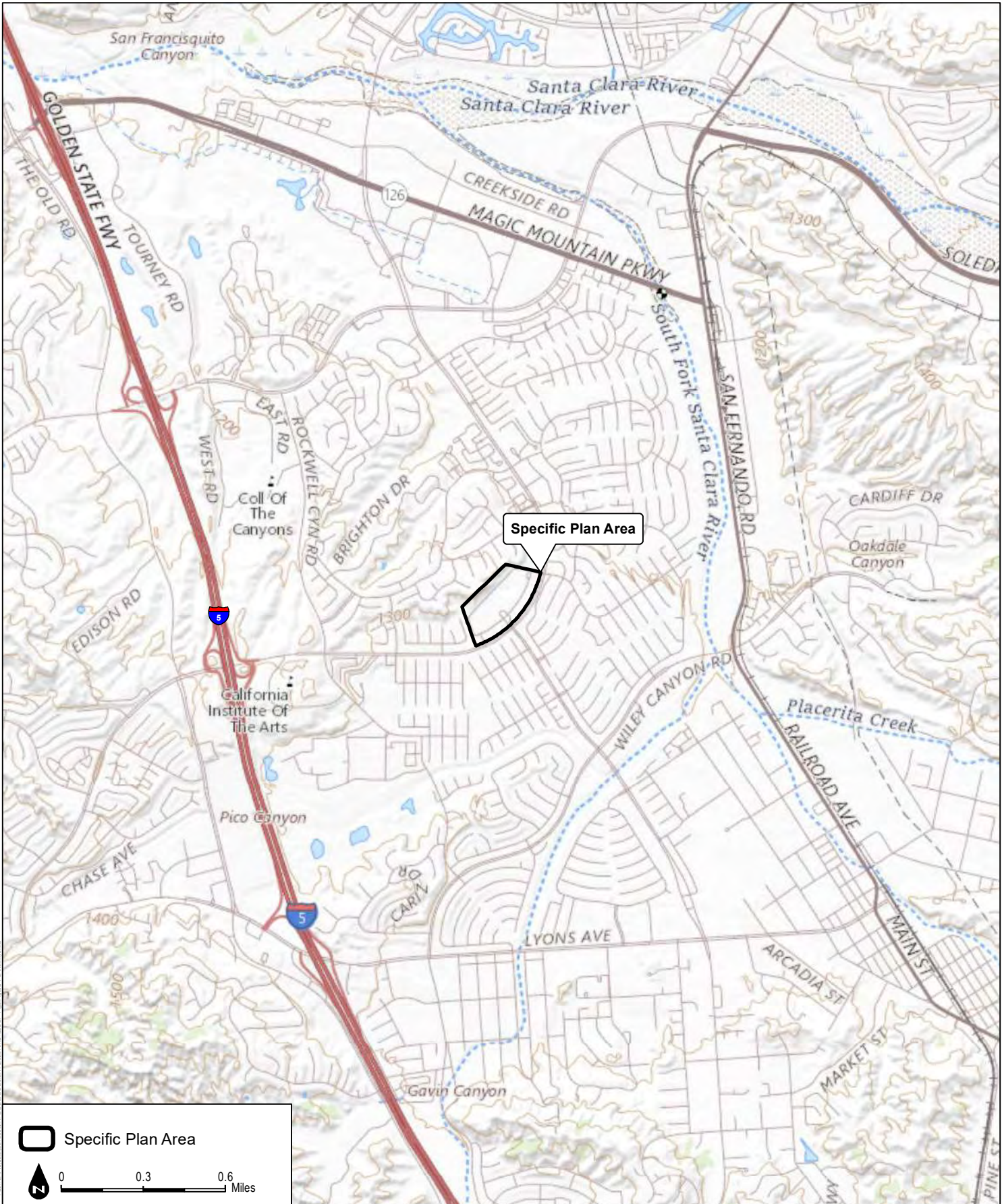
SOURCE: ESRI Shaded Relief



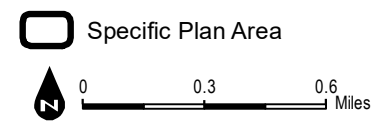
2020 Amended Henry Mayo Newhall Hospital Specific Plan

**FIGURE 1-1**  
Regional Map

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Specific Plan Area



SOURCE: USGS National Map 7.5 Minute Topo Series Newhall Quadrangle

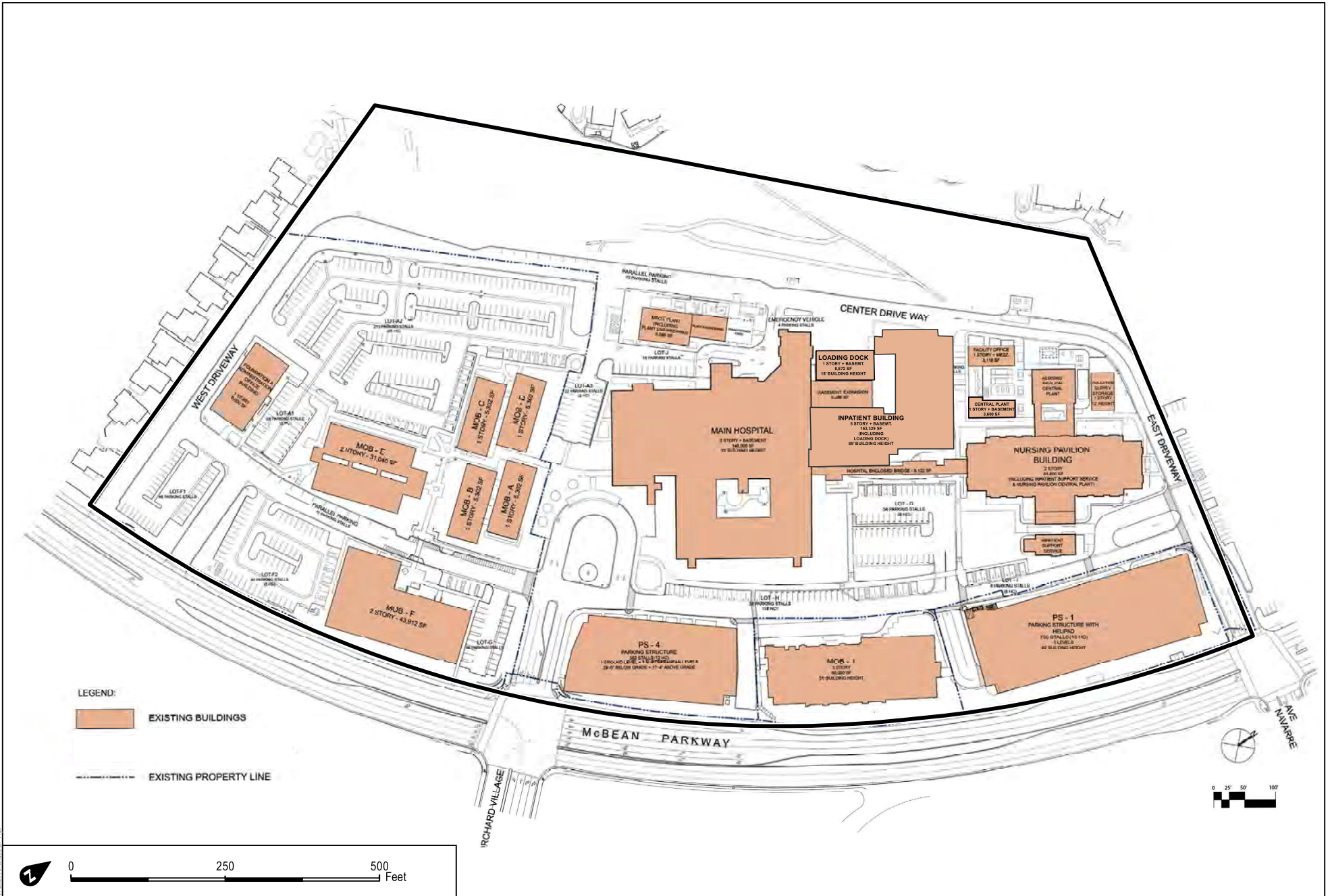
**FIGURE 1-2**  
Vicinity Map

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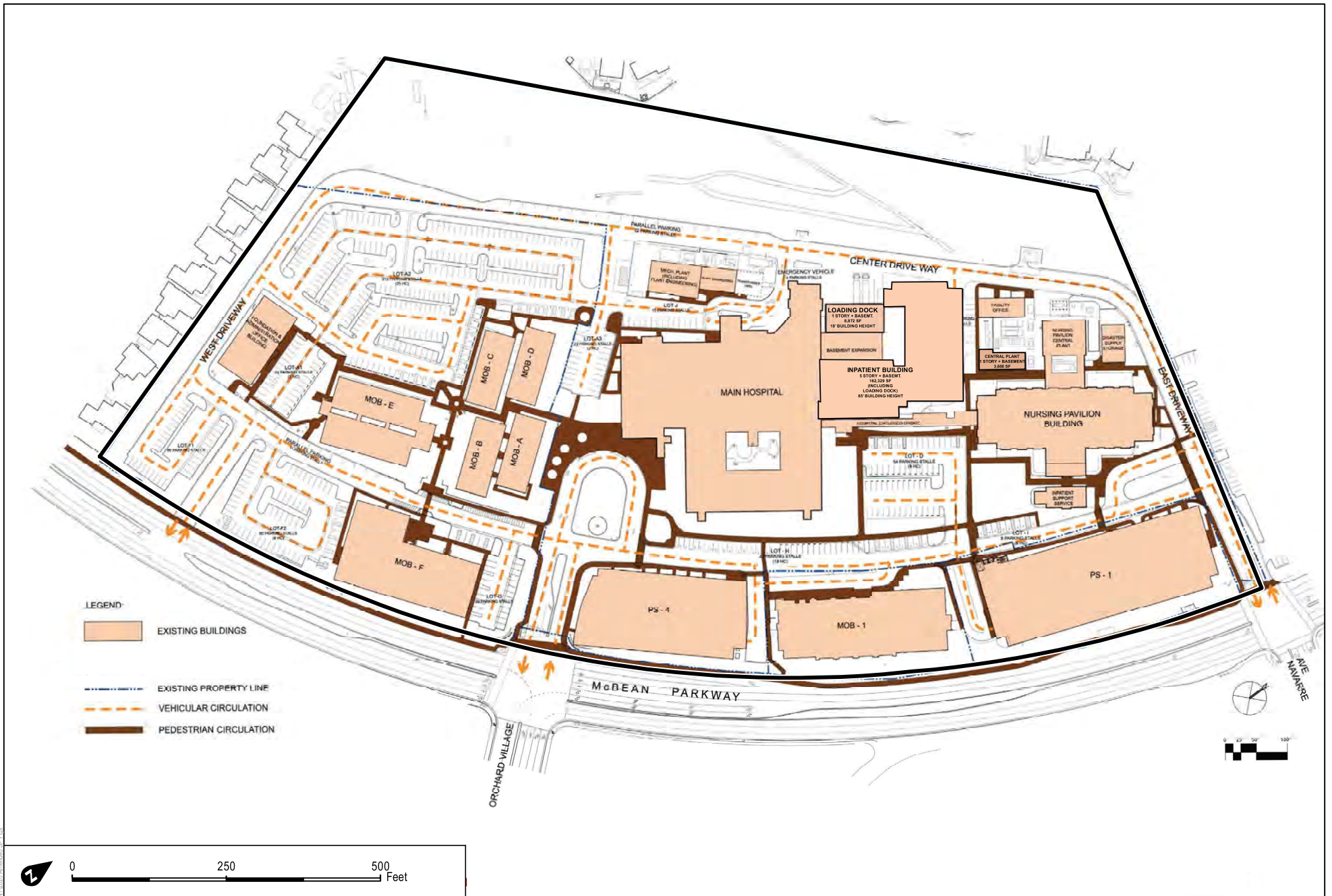
SOURCE: Project Development 2020

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2020 Amended Henry Mayo Newhall Hospital Specific Plan

**FIGURE 1-4**  
Existing Site Plan

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**FIGURE 1-5**  
Existing Circulation Plan

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## **CHAPTER 2 DEVELOPMENT PLAN**

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### **2.1 INTRODUCTION**

The 2020 ASP proposes the development of up to 200,000 square feet of additional building area for a new Diagnostic and Treatment (D&T) Building, an Inpatient Building (Inpatient Building No. 2 and initially including up to 92 beds), clinical services and 292 new parking spaces to be added to Parking Structure 4 (PS-4) through the construction of three above ground levels to the existing building.

The 2020 ASP does not change the maximum number of beds (368) permitted under the 2016 Specific Plan and 2008 Master Plan and Amendments. Up to 92 beds will be relocated out of the existing Main Hospital building to the new Inpatient Building. The area within the existing Main Hospital building containing the 92 beds will be converted to office uses, other administrative uses and/or clinical/hospital support services.

More specifically, the 2020 ASP includes the following:

- Construction of a three-story (approximately 60 feet), 84,300-square-foot, D&T Building within the Lot D area of the 2020 ASP. This building will also include a new below-ground basement. Uses within this building will include various clinical related uses, a pharmacy and imaging areas all supportive of the hospital.
- Construction of a five-story (approximately 80 feet), 115,700-square-foot, Inpatient Building within the Lot D area of the 2020 ASP and directly adjacent to the D&T Building. This building will also include a below-ground basement. Uses within this building will include inpatient beds, support services, public spaces and additional diagnostic and treatment facilities.

The 2020 ASP will also involve relocating the main entry/drop off area from its present location adjacent to the existing Main Hospital to in front of the new D&T Building; and adding up to 292 new parking spaces to PS-4 through the construction of three above ground levels added to the existing structure. Finally, the 2020 ASP will also include modifications to the parking plan to permit both paid and assigned parking in the on-site parking areas within the 2020 ASP Area as well as the implementation of various parking related measures to encourage carpooling and transit use.

This Chapter describes the proposed changes to the site plan, circulation plan, and landscaping plan; and improvements that remain to be implemented as approved under the 2016 Specific Plan and 2008 Master Plan and Amendments (Appendix A) and Development Agreement (Appendix B).

## 2.2 SPECIFIC PLAN OBJECTIVES

The objectives for this 2020 ASP are to provide up-to-date hospital beds within a new Inpatient Building (Inpatient Building No. 2) in order to comply with current and future Office of Statewide Health Planning and Development (OSHPD) codes and Americans with Disabilities Act (ADA) requirements; provide supporting services within a new D&T Building; and meet parking demands. These objectives are consistent with the 2016 Specific Plan and 2008 Master Plan objectives, provided below. Specifically, the proposed 2020 ASP will facilitate compliance with State law (Objective No. 2), provide for the present and future health care needs of Santa Clarita (Objective No. 4), and ensure adequate on-site parking facilities to accommodate patients, visitors and medical staff (Objective No. 16). New development permitted by this 2020 ASP will be required to comply with the standards and guidelines contained in Chapter 4 (Development Regulations) and Chapter 5 (Design Guidelines), in order to ensure that new development permitted by this 2020 ASP is attractive; promotes quality design consistent with the visual character of Valencia; and minimizes visual impacts through enhanced building design and landscaping (Objective No. 6). New development will be focused near the center of the site consistent with Objective No. 12. Finally, the relocation of the primary access point to in front of the D&T building will also ensure safe and convenient access for patients (Objective No. 11).

The following 2016 Specific Plan objectives and 2008 Master Plan objectives have guided the HMNH campus as it continues to meet the health care needs of the Santa Clarita Valley, and apply to this 2020 ASP.

### 2016 Specific Plan Objectives

1. **Codify Existing Plan.** To codify the existing development parameters for expansion of the HMNH campus as approved in the 2008 Master Plan.
2. **Facilitate Compliance With State Law:** Ensure that the Inpatient Building approved under the 2008 Master Plan complies with mandatory standards for the design and construction of new hospitals required by amendments to the California Building Standards Code adopted subsequent to approval of the 2008 Master Plan.
3. **Expand Obstetric Services:** Allow the design and construction of two new Caesarean section rooms to enable HMNH to meet the growing demand for obstetric services in the Santa Clarita Valley and allow HMNH to better contract services from insurance providers.

### 2008 Master Plan Objectives

4. **Present and Future Healthcare Needs.** Help meet the health care needs of Santa Clarita Valley's population growth and its increasingly aging population.
5. **Viability of Hospital.** Maintain the viability of the hospital on a site that will continue to be centrally located within the hospital's 680-square-mile service area as the community continues to grow in the future.

6. **Attractive and Quality Development.** Continue development of a well-planned hospital campus that is attractive and promotes quality design consistent with the visual character of Valencia.
7. **24-Hour Emergency Department.** Maintain the Santa Clarita Valley’s only existing 24-hour Emergency Department. This includes maintaining the hospital’s Los Angeles County designation as a Level II Trauma Center, improving its capability to treat all patients, no matter how critically ill or injured, and having the capacity to allow for patient admissions on an emergency basis.
8. **Helipad.** To improve the location of the helipad for emergency and disaster preparedness.
9. **Integration of Hospital and Other Healthcare Services.** Encourage combined inpatient and outpatient services on the HMNH campus and partnerships between project sponsors where medical practices, specialty medical centers, and other healthcare services are integrated with the operations of the hospital.
10. **Physician Specialists and Centers of Excellence.** Attract and retain physician specialists and establish Centers of Excellence, which are defined as highly specialized health care services via physician or hospital-authorized providers or hospital collaboration around a disease category.
11. **Safe Access.** Maintain a hospital campus that is designed with patients in mind by linking inpatient services and medical buildings in a single setting, providing safe access and transit opportunities.
12. **Minimize Visual Impacts.** Minimize visual impacts of the HMNH campus through the use of enhanced building design and landscaping, and focusing more intensive development near the center of the site.
13. **Land Use Buffers.** Apply land use buffering techniques between the hospital campus and adjacent residential uses through use of building setbacks and enhanced landscaping.
14. **Modernize and Upgrade Facilities.** Continue to modernize and upgrade the HMNH campus and other on-site supportive mechanical facilities to ensure the long-term viability of existing and new buildings.
15. **Efficient Circulation.** Continue to implement an efficient vehicular and pedestrian circulation system that ensures ease of movement throughout the site.
16. **Adequate Parking.** Ensure that the HMNH campus is served by adequate on-site parking facilities to accommodate patients, visitors, and medical staff.

## 2.3 SITE PLAN

The Site Plan (Figure 2-1) and the HMNH Buildout Development Program (Table 2-1) provide the framework for development of the 2020 ASP area based on the combined 2020 ASP, 2016 Specific Plan and 2008 Master Plan. Figure 2-1 illustrates the location of and identifies existing facilities, future facilities proposed under the 2020 ASP, and future facilities approved under the 2016 Specific Plan and 2008 Master Plan and any amendments. Table 2-2 provides the location and number of proposed hospital beds on campus.

The 2020 ASP proposes the construction of a three-story, 84,300-square-foot, D&T Building within the Lot D area of the 2020 ASP, as shown in Figure 2-1. This building will also include a new below-ground basement. Uses within this building will include various clinical related uses, a pharmacy and imaging areas. The 2020 ASP also proposes construction of a five-story, 115,700-square-foot, Inpatient Building within the Lot D area of the 2020 ASP and directly adjacent to the D&T Building, as shown in Figure 2-1. This building will also include a below-ground basement. Uses within this building will include inpatient beds, support services, public spaces and additional diagnostic and treatment facilities.

As shown in Figure 2-1, the proposed three-story D&T Building will be located behind the existing three-story MOB-1 Building, and will therefore not be visible from the street (i.e., McBean Parkway). Similarly, the proposed five-story Inpatient Building will be located behind the existing Parking Structure 1 (PS-1) and will therefore not be visible from McBean Parkway.

### 2016 Specific Plan

The 2016 Specific Plan facilitated an adjustment to Inpatient Building 1 originally approved as part of the 2008 Master Plan, which included the 8,872-square-foot Loading Dock. The Loading Dock was relocated pursuant to the 2008 Master Plan, and is now in the Infill Area, which is the area between the Hospital and the Inpatient Building 1, as shown in Figure 2-1, Site Plan. Specifically, the 2016 Specific Plan increased the square footage of Inpatient Building 1 from 125,363 square feet to 153,457 square feet (an increase of 28,094 square feet from the approved 2008 Master Plan) and relocated and expanded the Loading Dock that was approved under the 2008 Master Plan. Both the Loading Dock and Inpatient Building 1 have been completed.

The net increase in square footage to the Inpatient Building 1 was due to two factors 1) implementation of improvements required by amendments to the California Building Standards Code related to the design and construction of new hospitals that went into effect after adoption of the 2008 Master Plan and Development Agreement, and 2) the need for two new Caesarean Section rooms as part of the implementation of an expanded Obstetrical Services Unit. The majority of the additional square footage (35,147 square feet) is being added to the basement level, first, and second floor of the Inpatient Building. Specifically, 11,406 square feet is being



added to the basement level, and 25,560 square feet is being added to the first and second floors. These three levels are located behind the existing enclosed Hospital Bridge between the Hospital and the Nursing Pavilion, as shown in Figure 2-1. As the expansion is located behind the enclosed Hospital Bridge and is being added to the first two stories, the majority of the additional square feet will not be visible from the street.

As shown in Figure 2-1, Site Plan, the 3,600-square-foot Central Plant is complete at the time of preparation of this 2020 ASP. The 3,600-square-foot Central Plant represents a 6,400-square-foot reduction from that approved in the 2008 Master Plan. The Central Plant building provides support services for Inpatient Building 1. Some of its services will be located in the basement of the Inpatient Building, which has allowed for the reduction in the Central Plant from 10,000 square feet to 3,600 square feet.

### **Unbuilt Facilities and Campus Buildout**

The total square footage that remains to be built under this 2020 ASP, the 2016 Specific Plan and the 2008 Master Plan and amendments, as shown in Figure 2-1 and Table 2-1 (not counting buildings under construction), include the proposed Inpatient Building 2, D&T Building and PS 4 Expansion (all part of the 2020 ASP); MOB 2, MOB 3, PS 2, and PS 3 (all permitted under the 2008 Master Plan).

At buildout, the campus will include a total of 898,000 square feet of building space, including 577,415 square feet of hospital and hospital-related uses; 24,425 square feet of support facilities; and 296,160 square feet of medical office space, plus parking. The floor area ratio (FAR) of the approved 2008 Master Plan and amendments and Development Agreement, including the existing campus at that time, is 0.52, based on a lot size of 29.77 acres, or 1,296,781 square feet. The HMNH campus was a total of 30.4 acres at the time of approval of the 2008 Master Plan; however, the 2008 Master Plan required the dedication of 0.63 acre to the City for the McBean Parkway traffic improvements, thereby reducing the total area to 29.77 acres. HMNH dedicated the 0.63 acre as required. The additional FAR based on the square footage facilitated by the 2016 Specific Plan was 0.02 for a total FAR of 0.54. The additional FAR based on the square footage facilitated by this 2020 ASP is 0.15 for a total FAR of .69. See Table 2-1 below (last row) for the Site FAR under each plan.

**Table 2-1**  
**HMNH Buildout Development Program**

Use	Area (square feet)				
	Existing Facilities (Pre-2008 Master Plan)	2008 Approved Master Plan Facilities	2016 Specific Plan Development	2020 ASP (Proposed)	Total Buildout Facilities
<i>Hospital and Related Uses</i>					
Main Hospital	146,000	—	—	—	146,000
Main Hospital Basement	5,286	—	—	—	5,286
Nursing Pavilion Building	63,800	—	—	—	63,800
Inpatient Building 1)	—	125,363	28,094	—	153,457
Inpatient Building 2 (Proposed)	—	—	—	+115,700	115,700
Diagnostic and Treatment Building (Proposed)	—	—	—	+84,300	84,300
Loading Dock	—	8,872 <sup>2</sup>	—	—	8,872
<i>Subtotal Hospital and Related Uses</i>	<i>215,086</i>	<i>125,363</i>	<i>28,094</i>	<i>+200,000</i>	<i>577,415</i>
<i>Support Facilities Uses</i>					
Hospital Bridge (covered walkway)	9,122	—	—	—	9,122
Mechanical Plant	8,585	—	—	—	8,585
Facilities Building (warehouse)	2,384	—	—	—	2,384
Facilities Building (office)	734	—	—	—	734
Central Plant	—	10,000	-6,400	—	3,600
<i>Subtotal Support Facilities Uses</i>	<i>20,825</i>	<i>10,000</i>	<i>-6,400</i>	<i>—</i>	<i>24,425</i>
<i>Medical Office Buildings (MOB)</i>					
MOB A	5,302	—	—	—	5,302
MOB B	5,302	—	—	—	5,302
MOB C	5,302	—	—	—	5,302
MOB D	5,302	—	—	—	5,302
MOB E	31,040	—	—	—	31,040
MOB F Sheila R. Veloz Breast Imaging Center	43,912	—	—	—	43,912
MOB 1	—	80,000	—	—	80,000
MOB 2 (Unbuilt)	0	60,000	—	—	60,000
MOB 3 (Unbuilt)	0	60,000	—	—	60,000
Foundation & Administrative Office Building (To Be Demolished)	8,000	-8,000	—	—	0
<i>Subtotal Medical Office Buildings</i>	<i>104,160</i>	<i>192,000</i>	<i>—</i>	<i>—</i>	<i>296,160</i>

**Table 2-1**  
**HMNH Buildout Development Program**

Use	Area (square feet)				
	Existing Facilities (Pre-2008 Master Plan)	2008 Approved Master Plan Facilities	2016 Specific Plan Development	2020 ASP (Proposed)	Total Buildout Facilities
Total	340,071	327,363	30,566	200,000	898,000
Site Floor Area Ratio <sup>1</sup>	0.26	0.25	0.02	0.15	0.69
	0.52				

Note:

- <sup>1</sup> Floor area ratio (FAR) is the size of a building divided by the size of its parcel. In this instance, FAR is based on 29.77 acres, or 1,296,781 square feet.
- <sup>2</sup> The Loading Dock square footage is included in the square footage of Inpatient Building 1. It was part of Inpatient Building 1 in 2008 and later relocated to the Infill Area, which is the area between the Hospital and Inpatient Building 1.

**Table 2-2**  
**HMNH 2020 ASP Total Hospital Beds**

Use	Total Proposed Hospital Beds <sup>1</sup>
<i>Hospital and Related Uses</i>	
Main Hospital	44
Nursing Pavilion Building	106
Inpatient Building 1	118
Inpatient Building 2	92
<i>Subtotal Hospital and Related Uses</i>	<i>360</i>
<i>Permitted Maximum Number of Beds</i>	<i>368</i>

Note:

- <sup>1</sup> The total number of hospital beds in each use may be decreased and/or increased provided the total number of beds does not exceed 368.

## 2.4 CIRCULATION PLAN

### 2.4.1 Introduction

The Circulation Plan, shown in Figure 2-2, provides for a transportation/circulation system using existing routes and expanded parking facilities from what was approved in the 2016 Specific Plan. The Circulation Plan incorporates vehicular and non-vehicular modes of transportation in a system of roads, parking, and pedestrian corridors. The Circulation Plan is intended to facilitate access within and around the 2020 ASP area. The Circulation Plan as provided in this 2020 ASP does not propose any changes to the internal roadway network from what was approved in the 2016 Specific Plan. The 2020 ASP does include the addition of up to three above ground levels to the existing PS-4 to accommodate the parking needs resulting from the addition of the new Inpatient Building and D&T Building. A Parking Demand Study was prepared for the 2020 ASP and is included as Appendix E. The Parking Demand Study demonstrates that the 2020 ASP meets the project's parking demands through the implementation of various parking management solutions.

## 2.4.2 Circulation and Access

### Regional Access

Interstate 5 (I-5) provides regional access for vehicles traveling to and from the 2020 ASP area. I-5 is located west of the 2020 ASP area and can be accessed via a full interchange at McBean Parkway. Major arterial streets near the 2020 ASP area consist of McBean Parkway, Orchard Village Road, and Rockwell Canyon Road/Tournament Road.

### Site Access

Access to the 2020 ASP area is provided by three driveways along McBean Parkway. The main entrance is provided at a four-way signalized intersection with Orchard Village Road (referred to as the “Center Driveway” in the 2008 Master Plan EIR). Orchard Village Road is a four-lane roadway that terminates at the project entrance. A second four-way signalized access point is located at the McBean Parkway and Avenida Navarre (referred to as the “East Driveway” in the 2008 Master Plan EIR). Avenida Navarre is a two-lane local street serving the residential uses south of the 2020 ASP area. The third access point is an unsignalized T-intersection along McBean Parkway, west of the Orchard Village Road main entrance, which allows for unprotected right-in, right-out, left-in, and left-out movements (referred to as the “West Driveway” in the 2008 Master Plan EIR).

### Internal Circulation

Internal vehicular circulation is provided throughout the 2020 ASP area (see Figure 2-2). From the Center Driveway on McBean Parkway at Orchard Village Drive, an internal road connects vehicles to all buildings and a perimeter road that runs along the outer edge of the campus. The existing and future parking structures, surface lots, and parallel spaces are all accessible from the perimeter road and internal streets. The 2020 ASP will involve relocating the main entry/drop off area from its present location adjacent to the existing Main Hospital Building to in front of the new D&T Building. The entry/drop off area adjacent to the Main Hospital Building will remain as a secondary access and be improved with enhanced pavement for use as an outdoor gathering space.

### Pedestrian Corridors

Pedestrian circulation within the 2020 ASP area consists of a network of internal pathways, sidewalks, and crosswalks that allow patients, visitors, and employees the ability to quickly and efficiently travel on foot to any destination within the 2020 ASP area (see Figure 2-2). Pedestrian-level lighting is provided on all walkways to ensure visibility and safe movement throughout the hospital campus.

### Public Transportation

Santa Clarita Transit currently provides fixed-route transit immediately adjacent to the 2020 ASP area. Routes 5 and 6 pass through the intersection of Orchard Village Road and McBean Parkway and provide

service to the Stevenson Ranch Area, Hart High School, the Valencia Town Center, and Canyon Country. There is an existing bus stop on McBean Parkway in front of PS1 that serves the plan area.

The nearest transit centers are the McBean Regional Transfer Station, which is located on McBean Parkway approximately 1 mile north of the project site at the Valencia Town Center, and the Newhall Metrolink station located on Railroad Avenue and Market Street.

### **2.4.3 Parking Plan**

Parking supply at buildout of the 2020 ASP will total 2,441 spaces distributed throughout the hospital campus, consisting of 2,251 spaces in parking structures, 159 spaces in surface lots, and 31 parallel parking spaces along the internal circulation roadways of the site. The Parking Demand Study includes the following changes from the approved 2016 Specific Plan parking plan:

1. Removing 11 parallel parking spaces to accommodate the new main entry/drop off area in front of the proposed D&T Building;
2. Adding a total of 292 spaces in the above-ground parking levels in PS-4, for a total of 644 spaces; and
3. Implementing the various parking management solutions identified in the Parking Demand Study including the use of both paid and assigned parking in the on-site parking areas within the 2020 ASP Area.

As shown in Table 2-3, a total of 2,261 parking spaces are required according to the proposed buildout of the 2020 ASP. The parking supply of 2,441 spaces exceeds the City's parking requirement by 180 spaces. The required number of parking spaces is based on the total number of licensed hospital beds and the modified gross building floor area.

In addition to evaluating compliance with the 2020 ASP under the City's parking code, the Parking Demand Study evaluated peak parking utilization for the campus. The peak parking utilization for the campus was 91% occurring at 11 a.m. Typically, utilization above 85-90% causes drivers to spend additional circling around parking areas scanning for available spaces. The Parking Demand Study indicates that the proposed 2,441 parking spaces is adequate to meet future demand at full buildout, though at 95% utilization its likely that campus users will face difficulty finding available spaces during periods of peak demand. A variety of parking management solutions can effectively reduce peak utilization and are identified in the Parking Demand Study. These strategies include transportation demand strategies, parking access and revenue (paid parking), and tandem and valet service. Combined, these measures can reduce parking demand by up to 24%. Upon adoption of the 2020 ASP, an annual monitoring program along with the other requirements of the Parking Demand Study would go into effect requiring annual monitoring (including an updated parking inventory) and implementation of one or more of the parking management solutions to reduce peak utilization to 85% or better.

Figure 2-1 illustrates the location of existing and proposed parking facilities on campus. All facilities were approved under the 2008 Master Plan, with the exception of the proposed PS 4 addition. The PS-4 parking structure approved in the 2008 Master Plan was a 17’-4” high, 352-space ground level structure with three subterranean levels. The 2020 ASP proposes to add up to three additional above grade levels, for a total height of four stories, 40 feet to the top of the parapet and 50 feet to the top of the parking lot lights. The PS-4 façades shall provide decorative metal tubing elements on all facades visible from McBean Parkway to allow vine growth up the sides of the parking structure, pursuant to Chapter 5, Design Guidelines.

Two parking structures (PS 2 and PS 3) remain to be built and will provide a total of 857 parking spaces. PS 2 and PS 3 will be constructed along the west/northwest perimeter of the 2020 ASP area within an existing parking lot. PS 2 will be a six-level (five levels aboveground with one subterranean level), 579-space parking structure in the westerly portion of the 2020 ASP area. The parking structure will be 45.5 feet to the top of the parapet and 49.5 feet to the top of the parking lot lights. A temporary solid wall along the west-facing façade of the parking structure will be included before the construction of PS 3, which will then be reconstructed to interconnect with PS 2.

PS 3 will be a four-level (three levels aboveground and one subterranean level), 278-space parking structure in the westerly portion of the 2020 ASP area. The parking structure will be 27 feet to the top of the parapet and 30 feet to the top of the parking lot lights. This structure will have a minimum setback from the westerly property line of 75 feet. An architecturally enhanced solid wall will be provided along the western façade of the parking structure.

**Table 2-3  
Existing, Approved and Proposed Parking Code Requirement**

		Existing <sup>1</sup>		Approved Specific Plan		Proposed Specific Plan Amendment	
<i>Land Use</i>	<i>Parking Ratio</i>	<i>Units</i>	<i>Spaces</i>	<i>Units</i>	<i>Spaces</i>	<i>Units</i>	<i>Spaces</i>
Hospital – Inpatient	2 Spaces/Bed	236 Beds	472	368 Beds	736	NC <sup>2</sup>	736
Hospital – Outpatient	2.5 Spaces/KSF	11.375 KSF	29	15.375 KSF	39	45.375 KSF	114
Medical Services	5 Spaces/KSF	164.916 KSF	825	280.916 KSF	1,405	NC	1,405
Professional Office	4 Spaces/KSF	8.734 KSF	35	0.734 KSF	3	NC	3
Storage	1 Space/KSF	2.384 KSF	3	2.384 KSF	3	NC	3
Parking Code Requirement		1,364		2,186		2,261	
Parking Supply		1,547		2,227		2,441	
Code Surplus/Shortfall		+183		+41		+180	

Source: Santa Clarita Municipal Code, Chapters 17.43, 17.44, and 17.45, 2020  
KSF = 1,000 Square Feet

<sup>1</sup> For the purposes of our analysis, the “Existing” parking code requirement refers to March 2018 – the period which LLG performed its inventory and utilization survey. Therefore, this existing requirement does not consider the new inpatient building which opened in 2019, however the approved bed count is not changing under the 2020 Amended Specific Plan.

<sup>2</sup> NC = No Change from Approved Specific Plan.

## **Bicycle Parking**

Bicycle racks will be provided throughout the 2020 ASP area at a ratio of 1 bike stall per 25 parking stalls. The location of these facilities will be shown on a site plan as part of the development review for each building. This requirement is a condition of the 2008 Master Plan.

### **2.4.4 Helipads**

The 2016 Specific Plan included the construction of a new above-grade primary helipad on the roof of Inpatient Building 1, as previously approved in the 2008 Master Plan. The 2016 Specific Plan relocated the helipad from the front of the roof of the Inpatient Building to the back of the roof in order to accommodate direct elevator access between the helipad and the emergency room in order to comply with mandatory airport obstruction clearance criteria. This helipad has since been completed in compliance with the 2016 Specific Plan.

The other operational helipad located on the roof of PS 1 along McBean Parkway is allowed to remain, as approved in the 2008 Master Plan. This is for two reasons: (1) to keep a secondary helipad for use during a major disaster/emergency, and (2) for use during any construction activities on the hospital campus that may temporarily preclude use of the Inpatient Building helipad due to aeronautical safety concerns. Outside of these two situations, both helipads will not be operational at the same time. The locations of the existing and proposed helipads are illustrated in Figure 2-1. No additional helipads are proposed under the 2020 ASP.

## **2.5 LANDSCAPE PLAN**

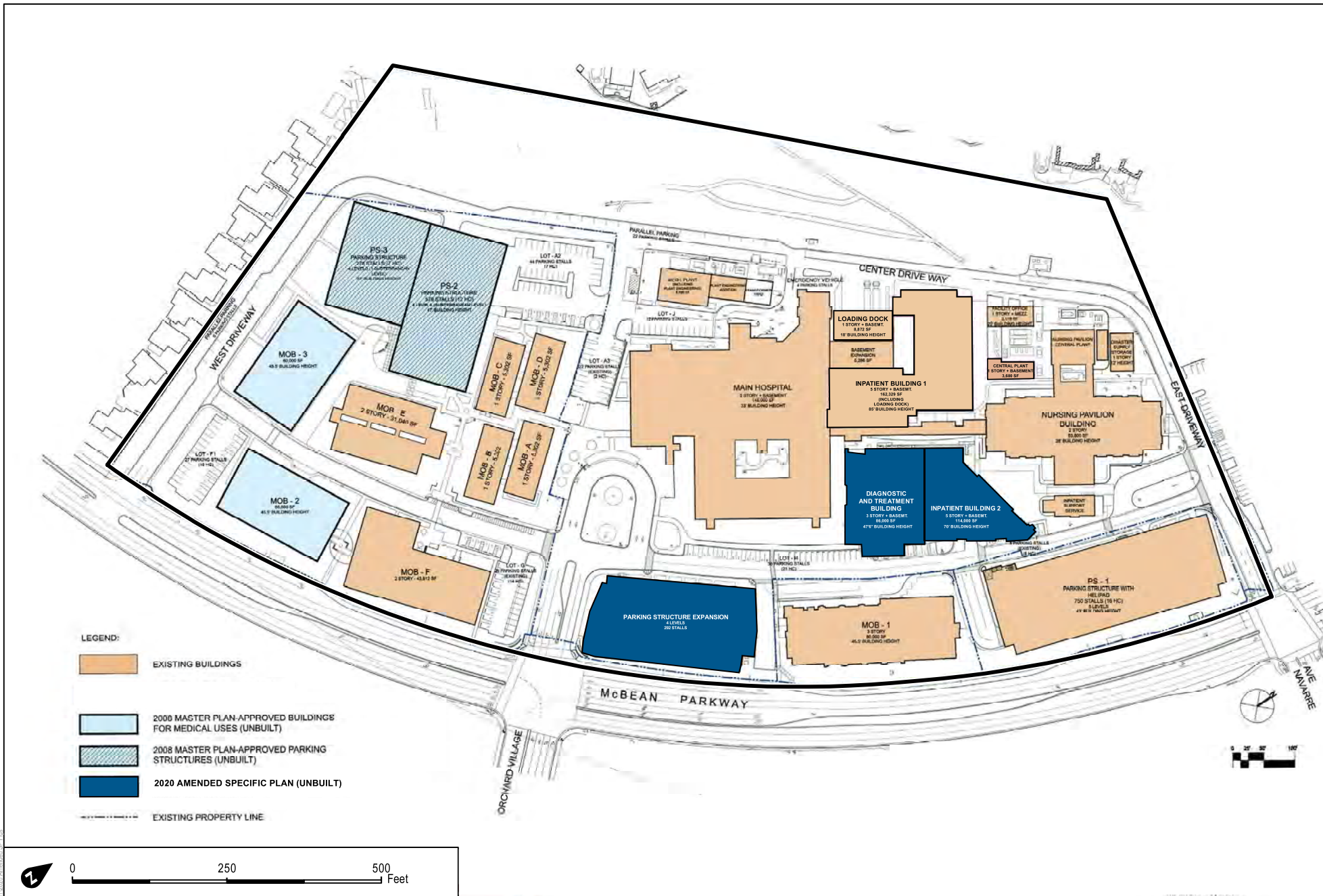
The approved Conceptual Landscape Plan approved in accordance with the 2008 Master Plan, shown in Figure 2-3, builds upon the existing landscaping within the ASP 2020 area and focuses primarily on the use of native and drought-tolerant tree and plant species. All plant species listed, both native and non-native, have been chosen due to their ability to thrive in the Santa Clarita climate and their ability to complement the open space/landscape areas within the 2020 ASP area. Plants listed that are non-native or not drought-tolerant will be used sparingly and only in areas that require their unique properties. The use of turf will be limited. New irrigation systems will be designed, installed, operated, and maintained in conformance with the State Water Efficient Landscape Ordinance.

Landscaping is focused around each of the existing buildings, surface parking lots, and the hillside in the northwest portion of the campus. The majority of the 2020 ASP area is currently landscaped; however, as future buildings are constructed, additional landscaping is required to be provided.

Since the 2008 Master Plan, a number of trees were removed during development of MOB 1, PS 1, and PS 4, and new trees have been planted, as approved in the 2008 Master Plan. Additional trees

will be removed with future development as shown on Figure 2-4, and as approved in the 2008 Master Plan; and new trees will be planted to provide shade and create an aesthetically pleasing environment around new buildings and parking areas. As shown on Figure 2-3, new trees may include sycamore, purple leaf plum, crape myrtle, creeping fig, camphor tree, African sumac, and Chinese flame tree.





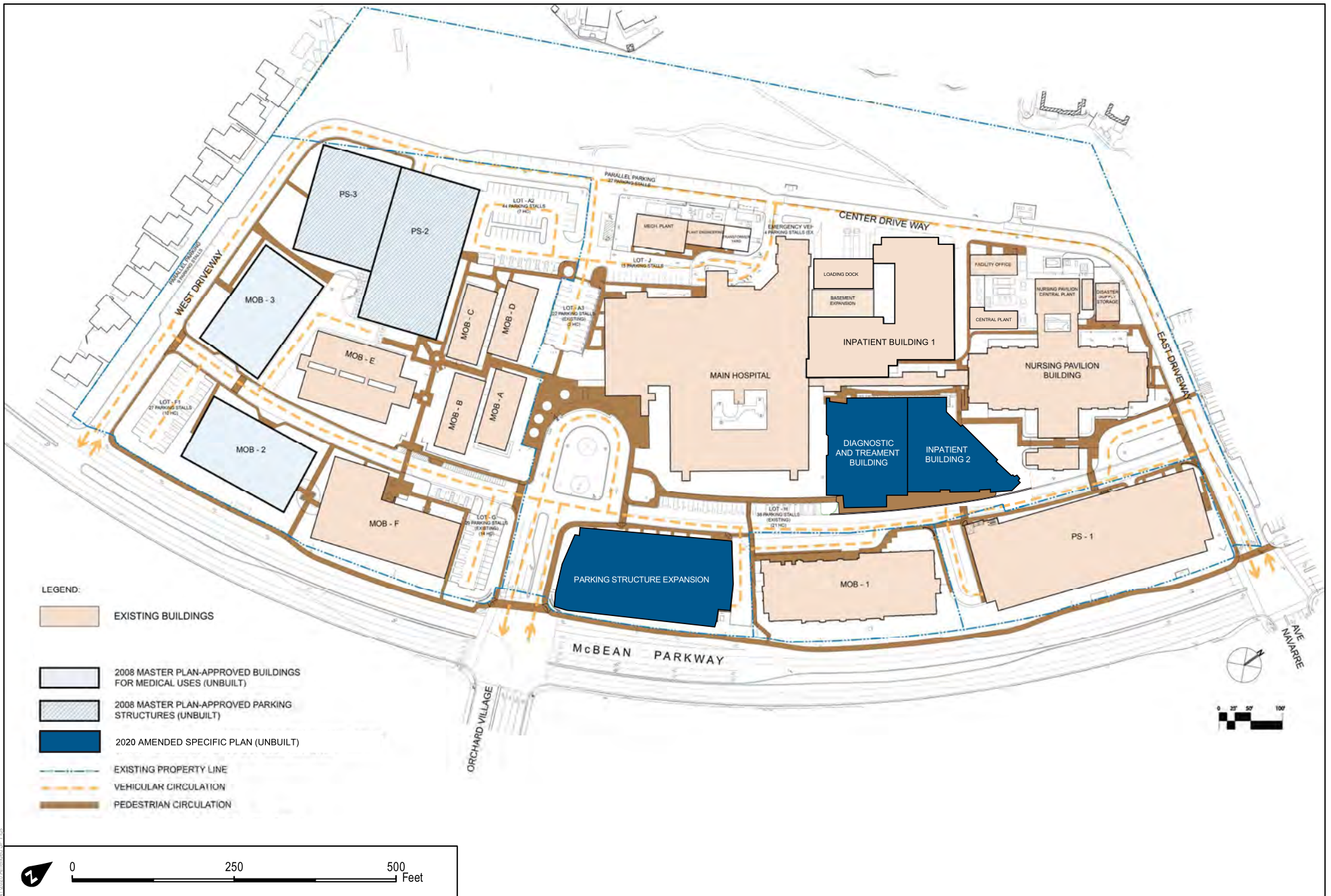
SOURCE: Project Development 2020

**DUDEK**

2020 Amended Henry Mayo Newhall Hospital Specific Plan

**FIGURE 2-1**  
Site Plan

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SOURCE: Project Development 2020

**DUDEK**

2020 Amended Henry Mayo Newhall Hospital Specific Plan

**FIGURE 2-2**  
Circulation Plan

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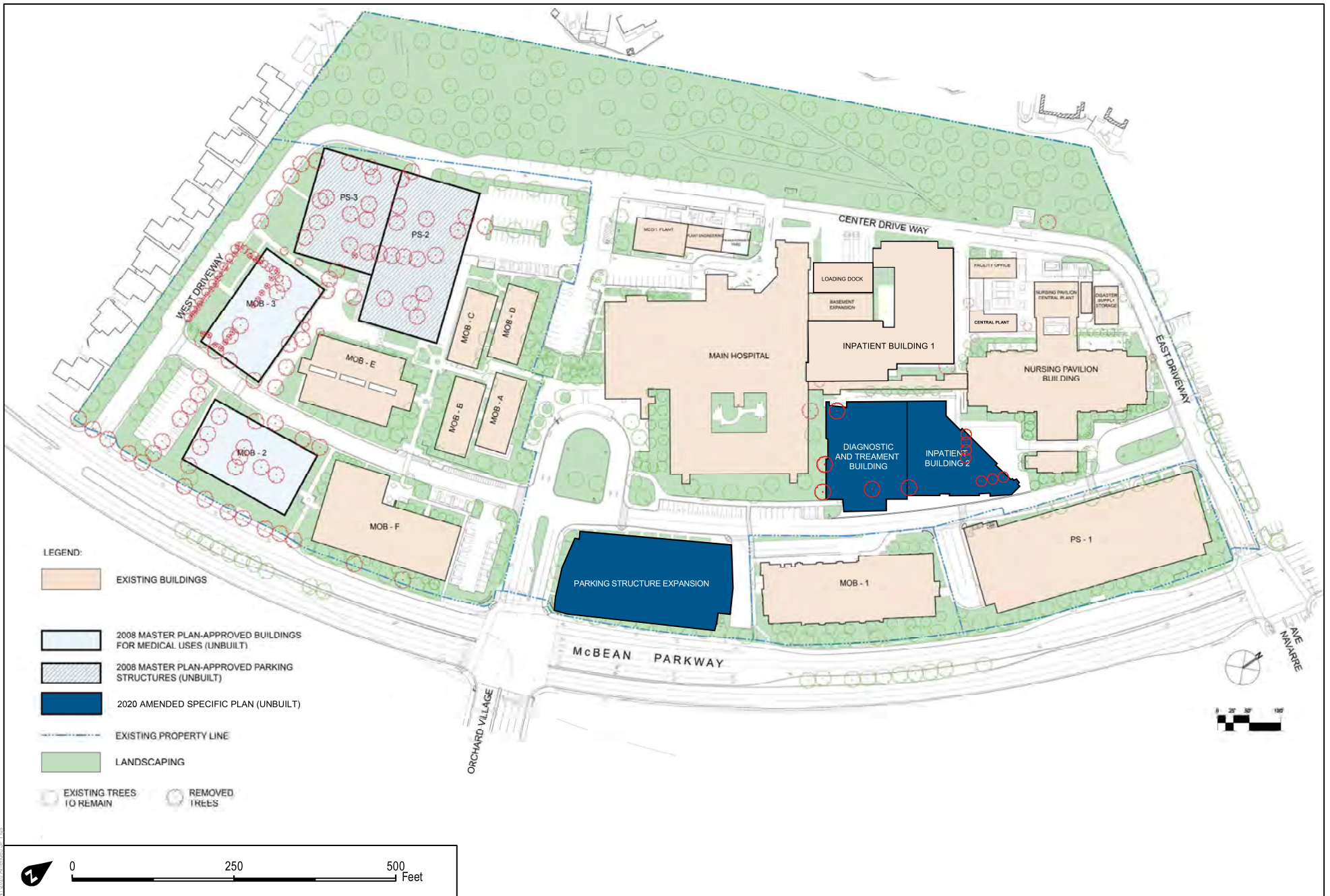
SOURCE: Project Development 2020

**DUDEK**

2020 Amended Henry Mayo Newhall Hospital Specific Plan

**FIGURE 2-3**  
Landscape Plan

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SOURCE: Project Development 2020



2020 Amended Henry Mayo Newhall Hospital Specific Plan

**FIGURE 2-4**  
Tree Removal Plan

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## **CHAPTER 3 INFRASTRUCTURE DEVELOPMENT PLAN**

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### **3.1 INTRODUCTION**

This chapter addresses the 2020 ASP's infrastructure development plans for drainage/water quality, water service, wastewater service, and dry utilities. The information included in this chapter is based on the Final Environmental Impact Report (EIR) for the 2008 Master Plan, prepared in November 2008, with updated information provided by the 2016 EIR Addendum and 2020 Supplemental EIR. The 2008 Master Plan (including amendments) and Development Agreement require the actions and activities referenced with respect to each of the infrastructure development plans described in this chapter. This 2020 ASP incorporates all of those existing requirements, and the existing requirements fully cover the modifications approved under this 2020 ASP.

### **3.2 DRAINAGE/WATER QUALITY**

There are currently several drainage improvements throughout the 2020 ASP area, including curbside catch basins, storm-drain pipes, gutters, and in-street channels, mostly within parking areas, which convey runoff to the south and southeast to larger stormwater conveyance facilities. This runoff exits the 2020 ASP area via overland sheet flow (in parking/paved areas) and within storm drains in McBean Parkway. Numerous storm-drain pipes then convey flow from southeast portions of the 2020 ASP area eastward to the Santa Clara River.

The majority of pervious surface areas on site include landscaping and slope areas along the north and west perimeter, with small pervious areas distributed throughout the site in required landscape planters. The remainder of the 2020 ASP area is made up of impervious surfaces, including buildings and other structures, sidewalks, and parking lots. The impervious portions of the 2020 ASP area contribute the majority of surface runoff to on- and off-site stormwater conveyance infrastructure.

Buildout of the 2020 ASP will alter drainage in such a way that could result in increased local erosion and runoff. However, future development within the 2020 ASP area is required to comply with the stormwater pollution control requirements of the Los Angeles Regional Water Quality Control Board Municipal Separate Storm Sewer System (MS4) Permit and the City of Santa Clarita Stormwater Ordinance.

In addition, an Urban Stormwater Management Plan (USMP) has been prepared for the entire 2020 ASP area as part of implementation of the 2008 Master Plan, and has been updated to include the additional square footage at buildout under the 2008 Master Plan and this 2020 ASP. The USMP complies with post-construction and other applicable best management practices, as detailed in the

Los Angeles County Standard USMP Manual. Best management practices identified in the USMP include infiltration devices, bioretention basins, bioswales, catch basin filters, regular street and parking lot sweeping, porous pavement, roof runoff controls, efficient irrigation, alternative building materials, storm-drain signage, trash enclosures, preservation of existing vegetation, hydraulic mulch, hydroseeding, soil binders, straw mulch, geotextiles and mats, wood mulching, earth dikes and drainage swales, velocity dissipation devices, slope drains, polyacrylamide, and stockpile management. Water quality impacts associated with implementation of the 2020 ASP will be addressed through infiltration devices.

### **3.3 WATER SERVICE**

Water service to the 2020 ASP area is provided by the Santa Clarita Valley Water Agency (SCVWA). SCVWA's service area includes most of the Santa Clarita Valley including the City of Santa Clarita and unincorporated portions of Los Angeles County. SCVWA supplies water from local groundwater, imported water, and recycled water. Recycled water is not currently available for the 2020 ASP area.

The 2008 Master Plan EIR found a less than significant impact on Water Supply. Implementation of the proposed 2020 ASP will cause an increase in the use of water beyond that analyzed in the Master Plan EIR. However, the 2020 Supplemental EIR confirms that there is sufficient capacity to accommodate this increase.

### **3.4 WASTEWATER SERVICE**

The 2020 ASP area currently uses an on-site wastewater collection system to convey wastewater flow from the site. Local conveyance facilities on site consist of 8-, 10-, and 12-inch pipelines. An 8-inch sewer main extends from the northwest boundary of the 2020 ASP and connects to a 10-inch private sewer main located at Ramillo Way and Dalgo Drive. The on-site 10-inch sewer main is connected at the upstream end, which begins approximately 9 feet northerly of the southwesterly corner of the 2020 ASP area and drains to the residential tracts to the west. The wastewater flows northeast along the 2020 ASP boundary until it connects to an off-site 12-inch sewer main, which is maintained by the Los Angeles County Department of Public Works Consolidated Sewer Maintenance District.

Wastewater flows from the 12-inch sewer main to a manhole located 70 feet northwesterly of the intersection of McBean Parkway and Avenida Navarre. The manhole is the junction where the 12-inch sewer main connects to the Sanitation Districts of Los Angeles County's (LACSD's) 24-inch trunk sewer located in the southbound side of McBean Parkway and flows from Orchard Village Road northerly along McBean Parkway to the manhole near Avenida Navarre. The manhole outlets and flows northerly into LACSD's 18-inch Valencia trunk sewer.

Development under the 2020 ASP will be required to connect to the LACSD's existing sanitary wastewater system. The City's Department of Public Works is the agency responsible for local wastewater in the City of Santa Clarita, and any developer constructing a new wastewater line must coordinate the construction and dedication of any such wastewater line with the City's Department of Public Works for future operation and maintenance. It will then be the responsibility of the LACSD to upgrade the wastewater collection and treatment systems by providing relief for existing trunk lines nearing capacity and expanding treatment plants to provide sanitation service to outlying areas. The 2020 ASP will not result in any changes to the wastewater system.

### **3.5 DRY UTILITIES**

#### **3.5.1 Electricity**

Southern California Edison currently provides electricity service to the 2020 ASP area. Southern California Edison maintains and operates transmission and distribution infrastructure to provide purchased power to end users throughout its service area.

The hospital campus is currently served by 16-kilovolt lines that extend underground along McBean Parkway. These existing transmission lines are considered adequate to serve the increased electricity demands at the buildout contemplated by the 2020 ASP, and the electrical loads are within the parameters of projected load growth, which Southern California Edison is planning to meet in the area. All on-site electricity lines will be installed to serve future uses including the proposed 2020 ASP, and no other improvements related to electricity are necessary.

#### **3.5.2 Natural Gas**

The Southern California Gas Company provides natural gas service to the 2020 ASP area. In the project vicinity, Southern California Gas Company upsized the existing, on-campus 2" diameter natural gas line with a new 4" diameter line, and upsized their meter adjacent to the new Central Plant accordingly to serve the new Inpatient Building 1. No additional demands for natural gas consumption are required by the proposed 2020 ASP since the number of total licensed beds on campus, as approved in the 2008 Master Plan, does not increase.

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## **CHAPTER 4 DEVELOPMENT REGULATIONS**

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### **4.1 INTRODUCTION**

This chapter provides regulations for the development and land uses within the area of the 2020 ASP, and describes how these development regulations will be used as part of the City of Santa Clarita's (City's) development review process. Proposed development and land uses within the 2020 ASP area must comply with all applicable requirements of this 2020 ASP. The purpose of this chapter is to codify land uses approved under the 2008 Master Plan and Amendments, as well as guide the review of new development allowed under this 2020 ASP.

### **4.2 CONSISTENCY WITH ADOPTED PLANS**

As required by California Government Code Sections 65454 and 65455, upon adoption, the 2020 ASP will be consistent with the City's adopted General Plan. The entirety of the 2020 ASP area is designated Specific Plan (SP).

Appendix C of this 2020 ASP discusses this consistency in detail. All subsequent land use entitlements and permits implementing the 2020 ASP must also be consistent with the General Plan and the 2008 Development Agreement, and its amendments, as applicable.

### **4.3 RELATIONSHIP TO THE CITY'S UNIFIED DEVELOPMENT CODE**

The 2020 ASP intends to provide a comprehensive set of regulations governing the use and development of the land within the 2020 ASP area. These development regulations replace the requirements of the City's Unified Development Code, Titles 16 and 17 of the Santa Clarita Municipal Code (SCMC). Any matter or issue that is not specifically and directly covered by this 2020 ASP shall be subject to the nonconflicting regulations and procedures of the SCMC. If a conflict arises between the 2020 ASP and the City's Unified Development Code, the provisions of this 2020 ASP shall prevail. Procedures for amending the 2020 ASP are provided in Chapter 6, Implementation.

### **4.4 PERMITTED USES**

The entirety of the 2020 ASP area is zoned SP. The primary land uses for the 2020 ASP area are hospital, medical office and outpatient services. The uses listed in Table 4-1 have been approved pursuant to the 2008 Master Plan and Amendments, and/or are consistent with the definition of hospital uses in the SCMC and are, therefore, expressly permitted within the 2020 ASP area:

**Table 4-1  
Permitted Uses<sup>1</sup>**

Use	Permitted (P)
Acute care services	P
Administrative services	P
Central utility plant	P
Community centers	P
Disaster supply storage	P
Education centers	P
Helipad	P
Imaging centers	P
Laboratories	P
Medical office buildings and clinics	P
Medical retail (medical supplies)	P
Mechanical plant	P
Nursing buildings	P
Offices	P
Off-site street parking, parking structures, or parking lots	P
Optometry services	P
Outpatient service buildings	P
Parking structures (associated with a permitted use)	P
Pharmacies	P
Physical therapy or rehabilitation centers	P
Wellness centers	P
Warehouse	P
Other uses not listed herein, which are determined by the City's Community Development Director to be similar to those listed.	P (See Section 17.37.020 [Public Institutional] of the SCMC for a list of uses appropriate for the 2020 ASP area).

**Note:**

<sup>1</sup> Land uses not listed in Table 4-1 or not determined by the Community Development Director to be similar in use are not permitted.

## 4.5 GENERAL SITE DEVELOPMENT REGULATIONS

The following site development regulations listed in Table 4-2 shall apply throughout the 2020 ASP area. In order to create development regulations that are sensitive to adjacent uses, the 2020 ASP contains variable setbacks and building height zones. As indicated in the City's Zoning Code, a setback is defined as the distance from a defined point or line governing the placement of buildings, structures, parking or uses on a lot. The site development regulations are consistent with the approved 2008 Master Plan and Amendments.

**Table 4-2  
Site Development Regulations**

<b>Category</b>	<b>Regulation</b>
Maximum Square Footage	898,000 square feet
Minimum Setbacks	Front Yard Setback <sup>1</sup> : 20 feet West Side Yard Setback <sup>2</sup> : 75 feet East Side Yard Setback <sup>2</sup> : variable; match East Driveway <sup>3</sup> Rear Yard Setback <sup>2</sup> : variable; match Center Driveway <sup>3</sup>
Maximum Building Height <sup>4</sup>	Refer to Figure 4-1, Building Height Zones
Parking	Parking pursuant to the 2020 Parking Demand Study as a part of the 2020 ASP and the Development Agreement (including any amendments)

**Notes:**

- <sup>1</sup> As defined in the SCMC, the front yard setback shall be measured from the ultimate street right-of-way or the line established by the General Plan, whichever is greater, and be removed therefrom by the perpendicular distance prescribed for the front yard setback. Therefore, the front yard setback is measured from the McBean Parkway right of way; refer to Figure 4-1, Building Height Zones.
- <sup>2</sup> As defined in the SCMC, the rear or side yard setback shall be measured from the property line, removed therefrom by the perpendicular distance prescribed for the yard setback; refer to Figure 4-1, Building Height Zones.
- <sup>3</sup> The area along the northern and western edge of the property line measured by a perpendicular distance from the property line to the internal ring road is a no build area. No habitable structures or buildings are allowed within the rear and side yard setbacks; refer to Figure 4-1, Building Height Zones.
- <sup>4</sup> Building height is measured to the top of parapet. Exceptions such as mechanical equipment penthouse, antenna, elevators, and override equipment rooms may exceed the maximum height as dictated in Figure 4-1, provided they do not exceed 20% of the building roof area.

## **4.6 SIGNAGE REGULATIONS**

### **4.6.1 Purpose**

The signage regulations shall govern the design and maintenance of all signage within the 2020 ASP area. The regulations are intended to result in functional, attractive signage that incorporates a uniformly high level of design, graphics, continuity, consistency, and maintenance throughout the 2020 ASP area. All signage is intended to be consistent with the design intent of the 2020 ASP as a whole.

The purposes of the signage regulations are to:

- a. Present a professional, non-commercial appearance complementing the high quality of care provided by the hospital and on-campus medical practices.
- b. Provide a way-finding program for pedestrians and drivers to get them to the service or building they are seeking.
- c. Specify location for tenant signs, as well as their size, square footage, type of sign and method of illumination.
- d. Present a low-key stonewall look with night illumination through indirect (halo) light sources along McBean Parkway.

## **4.6.2 Signage Regulations**

The 2020 ASP shall comply with the City-approved Exterior Master Sign Program, dated March 17, 2011 attached as Appendix E plus amendments. An amendment to the Exterior Master Sign Program may be allowed pursuant to Section 17.51.080 Sign Regulations (Private Property).

## **4.7 FENCES AND WALLS**

### **4.7.1 Purpose**

All fencing and walls shall be consistent with the design intent of the 2020 ASP as a whole. Consistency shall be determined in terms of size, character, quality, and materials in accordance with the architecture/design of the adjacent buildings.

### **4.7.2 Fence and Wall Regulations**

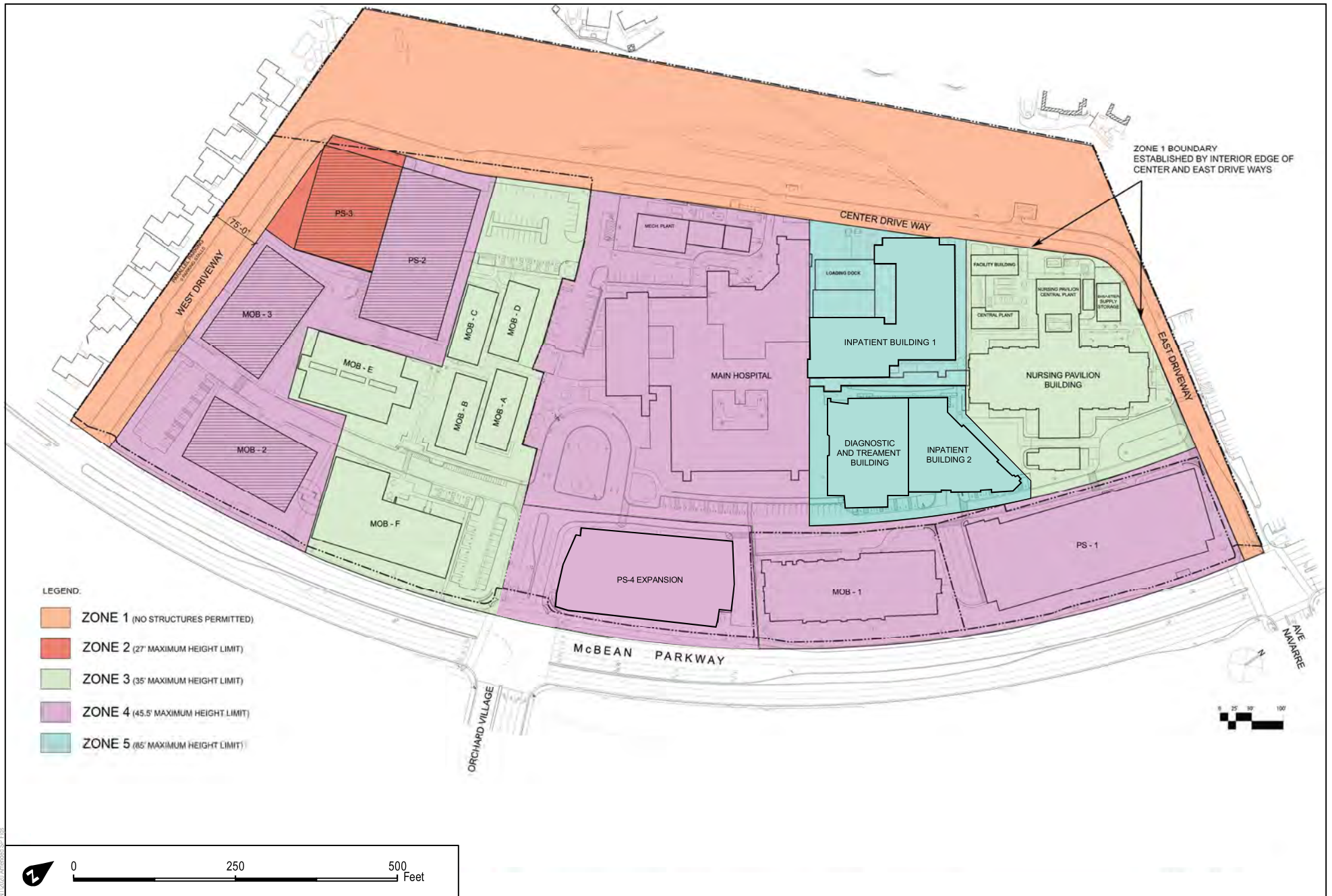
All fences and walls shall comply with the following fencing and wall regulations:

- a. A wall or fence shall not be constructed in such a manner so as to block or restrict vehicular access to a dedicated or implied dedicated alley, access, or way.
- b. Equipment enclosures, landscaping and/or fencing should be used to screen views of ground-mounted utility boxes and mechanical equipment. To the maximum extent feasible, utility and mechanical equipment should be located to the rear of buildings rather than along public sidewalks.

## **4.8 GRADING**

Grading shall meet the terms of the approved 2020 ASP and 2008 Master Plan and Amendments.





SOURCE: Project Development 2020



2020 Amended Henry Mayo Newhall Hospital Specific Plan

**FIGURE 4-1**  
Building Height Zones

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## **CHAPTER 5 DESIGN GUIDELINES**

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### **5.1 PURPOSE AND INTENT**

This chapter is intended to define the overall design vision of the 2020 ASP area. The design guidelines supplement the development standards in Chapter 4 by providing direction on the more qualitative aspects of any new development.

The language in this chapter follows these principles:

- “Shall” refers to a mandatory design guideline that all projects must follow.
- “Should” or “may” refers to a design guideline that is strongly recommended, in order to meet the guideline’s intent. If a project does not follow one or more guideline(s), it must use other design strategies to achieve the guidelines’ intent.

This chapter includes photos and graphics that illustrate the design guidelines, and are not meant to show the only possible design solution for any particular guideline.

### **5.2 RELATIONSHIP TO CITYWIDE DESIGN GUIDELINES**

The 2020 ASP area is within the established master-planned community of Valencia, designed by urban planner Victor Gruen. The City’s *Community Character and Design Guidelines* describe Valencia as having “a mixture of bright colors, natural materials, and contemporary design.” The architectural design intent of the 2020 ASP area is to be compatible with the Valencia community, while also establishing a distinctive identity.

The requirements of this chapter take precedence over the *Santa Clarita Community Character and Design Guidelines*. If a conflict arises between the 2020 ASP or the *Santa Clarita Community Character and Design Guidelines*, the provisions of this 2020 ASP and the 2008 Master Plan and Amendments shall prevail.

### **5.3 SITE PLANNING**

The following site planning guidelines are based on the 2008 Master Plan and Amendments.

- a. The following pedestrian amenities shall be provided:
  1. A direct, pedestrian walkway with pavers from Parking Structure 1 to Medical Office Building 1;
  2. A pedestrian plaza area to provide a clear path of travel between Medical Office Building 3 with the adjacent Parking Structure 2 and Parking Structure 3 to include landscape and hardscape features and other pedestrian amenities to aesthetically enhance the area and soften the massing of PS 2 and PS 3; and

3. A covered pedestrian trellis feature between Medical Office Building 2 and the existing Medical Office Building F. This shall include landscape and hardscape features and other pedestrian amenities that will provide an aesthetic enhancement along the McBean Parkway corridor and provide a clear pedestrian connection between the two buildings.
- b. All roof-mounted equipment shall be adequately screened from public view with a parapet/screen wall or other approved architectural treatment.
- c. Adequate trash enclosures shall be provided to serve the project site. The enclosures shall be architecturally compatible with the 2008 Master Plan and Amendments, have solid roofs, and be convenient for both tenants and for the collection vehicles.
- d. Loading spaces and drop-off/pick-up areas shall be provided as part of each building in accordance with the City’s Unified Development Code (UDC).
- e. A solid wall shall be placed along the western façade of parking structure 2.
- f. As part of the 2020 ASP, enhancements to the hospital drop off/pick up area shall be made upon relocation of the main entry to the proposed Diagnostic and Treatment Building. These enhancements may include seating areas, decorative pavement, etc. to accommodate outdoor events.

## **5.4 ARCHITECTURE**

### **5.4.1 Architectural Style**

The following architectural guidelines are based on the 2008 Master Plan and Amendments.

- a. All structures, including those used for parking, shall include an enhanced level of architectural detail consistent with MOB 1 and PS 1 approved with the 2008 Master Plan and Amendments and to the satisfaction of the Director of Community Development.
- b. All windows shall utilize minimally reflective glass and all other materials shall minimize reflective glare. This shall include decorative metal tubing elements on all parking structure facades visible from adjacent parcels or McBean Parkway to allow vine growth up the sides of the parking structure. This shall also include an enhanced roofline cornice detail.
- c. All buildings require 360-degree architecture and must conform to all of the City’s development standards including the Commercial/Industrial Standards of the UDC, Section 17.50, along with the City’s Community Character and Design Guidelines.

### **5.4.2 Height, Mass, and Scale**

- a. The massing and scale of the buildings shall respect the visual and physical relationship of adjacent buildings. Refer to Figure 4-1, Building Height Zone.

### 5.4.3 Signage

- a. The design of signage within the 2020 ASP area shall conform to the City-approved Exterior Master Sign Program and amendments, dated March 17, 2011 attached as Appendix E.
- b. The goal of the City-approved Exterior Master Sign Program is to present a professional, non-commercial appearance complementing the high quality of care provided by the hospital and on-campus medical practices.

### 5.4.4 Fences and Walls

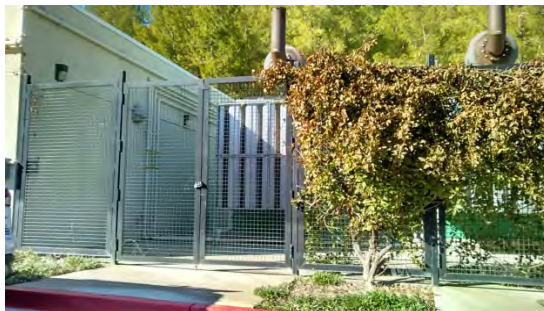
- a. Refer to Section 4.7.2 of this 2020 ASP for fence and wall regulations.
- b. Fencing and walls shall be architecturally compatible with other on-site development in color, style and material. Acceptable materials include concrete masonry units, concrete planters, painted stucco, painted concrete and vine coverage. Acceptable fencing for utility and mechanical equipment, such as the central plant, includes black metal frame fencing with vine coverage and chain link fencing. Some examples are pictured below.



*Concrete Planter  
(Nursing Pavilion Building)*



*Concrete Masonry Unit Wall with Vine Coverage  
(Nursing Pavilion Building)*



*Black Wire Fence with Vine Coverag  
(Transformer Yard near Central Plant)*

## 5.5 LANDSCAPE GUIDELINES

The following landscape design guidelines are based on the 2008 Master Plan and Amendments.

- a. The Landscape Plans shall conform to all current Municipal Code/Unified Development Code requirements.
- b. The applicant shall comply with the tree preservation plan submitted as part of the 2008 Master Plan and Amendments.
- c. The applicant shall provide trees and other landscape plantings along the project's frontage in a manner to screen the hospital campus buildings and parking structures from McBean Parkway and to maintain the landscape characteristics of the McBean Parkway corridor consistent with the conceptual landscape plan shown as part of the 2008 Master Plan and Amendments exhibits. The applicant shall provide mature trees and other landscape plantings along McBean Parkway near the main entrance and the main entry drive into the 2020 ASP area from McBean Parkway on the north side fronting the PS 4 parking garage, to the satisfaction of the Director of Community Development. The trees shall be a minimum of 36" box and shall include three 48" box.
- d. Significant plantings shall be installed along the western property line to screen all future site development from the adjacent residences.
- e. Planting on the northern slope shall be augmented to further screen the buildings and parking structures in the 2020 ASP area from the Summit residential community to the north.
- f. Each parking structure shall include a façade trellis planting system such as a green screen modular trellis, or its equivalent, on the sides that are visible from the public streets and adjacent residences. The facade trellis system shall include permanent irrigation and be maintained in perpetuity. In addition, each parking structure shall include landscape buffer areas directly adjacent and shall include plant material that will adequately provide additional screening of the parking structures.
- g. Conventional surface parking lots shall include planting areas that total at least five percent of the gross lot area. Parking lots shall include canopy trees capable of attaining at least 30' in height and at least 30' canopy diameter and arranged such that 50 percent of the paved lot area is shaded. Surface lots shall, at a minimum, contain one tree per four parking stalls.
- h. The community holiday tree, currently located in the center roundabout median at the main hospital building, provides a focal point when entering the campus at the main driveway and shall be accessible for community events.
- i. Prior to issuance of a certificate of occupancy for the Inpatient Building 1, the applicant shall provide a "healing garden" for the campus to the satisfaction of the Director of

Community Development. The "healing garden" shall be a space designed for passive use for patients, visitors, and staff. It shall be suitably separated and screened from other uses, shall include elements to engage interest-such as a water feature and plantings-and shall be adequately accessible for the intended users.

- j. The planting plan for any improvement on-site shall not include Eucalyptus trees. All improvements to the landscape design are to be complementary to the characteristics of the surrounding area and suitable for the environmental conditions of the Santa Clarita Valley. Commonly over-planted species such as *Agapanthus*, *Rhaphiolepis*, *Dietes*, *Pittosporum tobira*, and *Phormium tenax* are not permitted.
- k. Landscape plans shall be prepared by a California-registered Landscape Architect who is familiar with the plant palette suitable for Santa Clarita (Sunset Western Garden Book Zone 18).
- l. Final Landscape Plans for each building and parking structure are subject to review and shall conform to the following:
  1. Trees visible from the property's public street frontage shall be a minimum 24" box size with 25% of the trees 36" box, 10% of the trees 48" box, and 5% of the trees 60" box size specimens (Santa Clarita Architectural Design Guidelines adopted December 2002).
  2. When applicable, Site Plans or Landscape Plans shall include a calculation showing the square footage of surface parking and percentage of planting area in parking lot(s). Surface parking lots shall include planting areas that total at least five percent of the gross lot. (§ 17.51.030).
  3. Surface parking lots shall have at least one 24" box tree per four parking stalls and 36" box trees in planters at the ends of parking aisles. The plans shall show tree species selection, distribution and spacing to provide 50 percent canopy coverage of all surface parking areas within five years of planting. Parking spaces within parking structures are exempt from this requirement. (§ 17.51.030).
  4. Landscape plans shall show screen hedges and/or planted earthen berms, not less than thirty (30) inches or more than forty-two (42) inches in height, at specified locations on parking lot perimeters. Individual hedgy plants shall be 36" tall and spaced so that they touch leaf-to-leaf at time of final inspection. (§ 17.51.030).
  5. Where parking and/or drive aisles abut walls, fences, property lines, walkways or structures, landscape and site plans shall show planter beds delineated by continuous concrete curbing at least six (6) inches high and six (6) inches wide, at least (3) feet from such walls, fences, etc. These planter beds shall be landscaped except as permitted by the Director of Community Development. (§ 17.51.030 (9)).

6. The plant palette shall not include any plants listed as invasive exotic pest plants by the California Invasive Plant Council (lists available on-line), or other plants determined to be invasive by a competent botanist or biologist.
7. Landscape plans shall show plant material to screen, at maturity, all trash enclosures, transformer boxes, vault boxes, backflow devices, and other exterior mechanical equipment. Screening material may include trees, shrubs (15-gallon minimum size), clinging vines, etc. Masonry block (concrete masonry unit) trash enclosures shall be screened with both shrubs and clinging vines. (§ 17.51.030).
8. Landscape plans shall show locations of all lighting standards, where applicable.
9. The applicant shall apply jute netting to all graded or disturbed slopes five feet (5') and higher in vertical elevation and elsewhere where needed for erosion control, and shall landscape graded slopes. (§ 17.51.030).
10. The applicant shall design all irrigation systems for water conservation.
11. The applicant shall place water-conserving mulching material on all exposed soil in planting areas not covered by turf grass or significant ground cover. Mulching material may include, and is not limited to, shredded bark, river rock, crushed rock, pea gravel, etc., and must be at least three inches deep.
12. Trees planted within fourteen feet of the paved road section along McBean Parkway shall conform to Municipal Code § 13.76.110 et seq (Parkway Tree Influence Area) and the property owner shall irrigate and maintain these trees according to City standards.
13. Trees planted within City right-of-way shall conform to Municipal Code § 13. 76 et seq. (Parkway Trees).



## **CHAPTER 6 IMPLEMENTATION**

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### **6.1 PURPOSE AND INTENT**

This section sets forth the 2020 ASP's implementation procedures. The section also contains criteria under which the City's Community Development Director may make a determination of whether a proposal is in substantial conformance with the 2020 ASP; or whether an amendment to the 2020 ASP is required. In addition, the section provides a discussion of the California Government Code provisions governing the amendment of specific plans.

### **6.2 IMPLEMENTATION PROCEDURES**

The City's Community Development Director is responsible for ensuring that construction activities and future uses are substantially consistent with the 2020 ASP, which incorporates the 2008 Master Plan and Amendments and, while in effect, the Development Agreement as amended.

Following the expiration of the Development Agreement as amended in 2033, implementation of the 2020 ASP shall be in concert with the 2008 Master Plan and Amendments, the parameters of the 2020 ASP, and the City's Unified Development Code as applicable. All new development applications shall be submitted, reviewed and approved in accordance with this 2020 ASP. Any matter or issue that is not specifically and directly covered by this 2020 ASP shall be subject to the nonconflicting regulations and procedures of the Santa Clarita Municipal Code. If a conflict arises between the 2020 ASP and the City's Unified Development Code, the provisions of this 2020 ASP shall prevail.

### **6.3 AMENDMENT PROCEDURES**

Development proposals that do not meet the above implementation procedures shall require a Specific Plan Amendment in compliance with Chapter 17.28.110 of the Santa Clarita Municipal Code (SCMC), which may warrant an amendment to any of the incorporated attachments of the 202 ASP. Pursuant to Chapter 17.28.110 of the SCMC, an amendment to a Specific Plan may be initiated by the Director, Commission, Council, or the applicant for the Specific Plan provided such applicant or successor has, at the time of application for an amendment, a continuing controlling interest in development or management of uses within the Specific Plan.

### **6.4 FINANCING**

Improvements identified within this 2020 ASP are private in nature and will be financed privately within the boundaries of the 2020 ASP Area. No public financing is assumed.

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APPENDIX A  
*2020 Amended Master Plan and  
2008 Master Plan*



HENRY MAYO NEWHALL HOSPITAL  
**2020 Amendment to the Master Plan**

NOVEMBER 2020

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## PROJECT DESCRIPTION

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### 2020 AMENDMENT TO THE MASTER PLAN

#### New Campus Facilities

##### Hospital Beds

The 2020 Amendment to the Master Plan (AMP) does not change the maximum number of beds (368) permitted under the 2008 Master Plan. Up to 92 beds will be relocated out of the existing Main Hospital building to Inpatient Building 2. The area within the existing Main Hospital building containing the 92 beds will be converted to office uses, other administrative uses and/or clinical/hospital support services.

##### New Hospital Buildings

1. Construction of a three-story, 84,300-square-foot, Diagnostic and Treatment (D&T) Building within the Lot D area. This building will also include a new below-ground basement. Uses within this building will include various clinical related uses, a pharmacy and imaging areas.
2. Construction of a five-story, 115,700-square-foot, Inpatient Building 2 within the Lot D area and directly adjacent to the D&T Building. This building will also include a below-ground basement. Uses within this building will include inpatient beds, support services, public spaces and additional diagnostic and treatment facilities.

##### Circulation and Parking

The 2020 AMP will involve relocating the main entry/drop off area from its present location adjacent to the existing Main Hospital Building to in front of the new D&T Building; and adding up to 292 new parking spaces to PS-4 through the construction of up to three above ground levels added to the existing structure. The 2020 AMP also includes modifications to the parking plan to permit both paid and assigned parking in the on-site parking areas within the 2020 AMP Area. An updated Parking Study has been completed, which addresses the parking demand for the 2020 AMP

This table replaces the 2008 Master Plan Parking Summary:

**Existing, Approved and Proposed Parking Code Requirement**

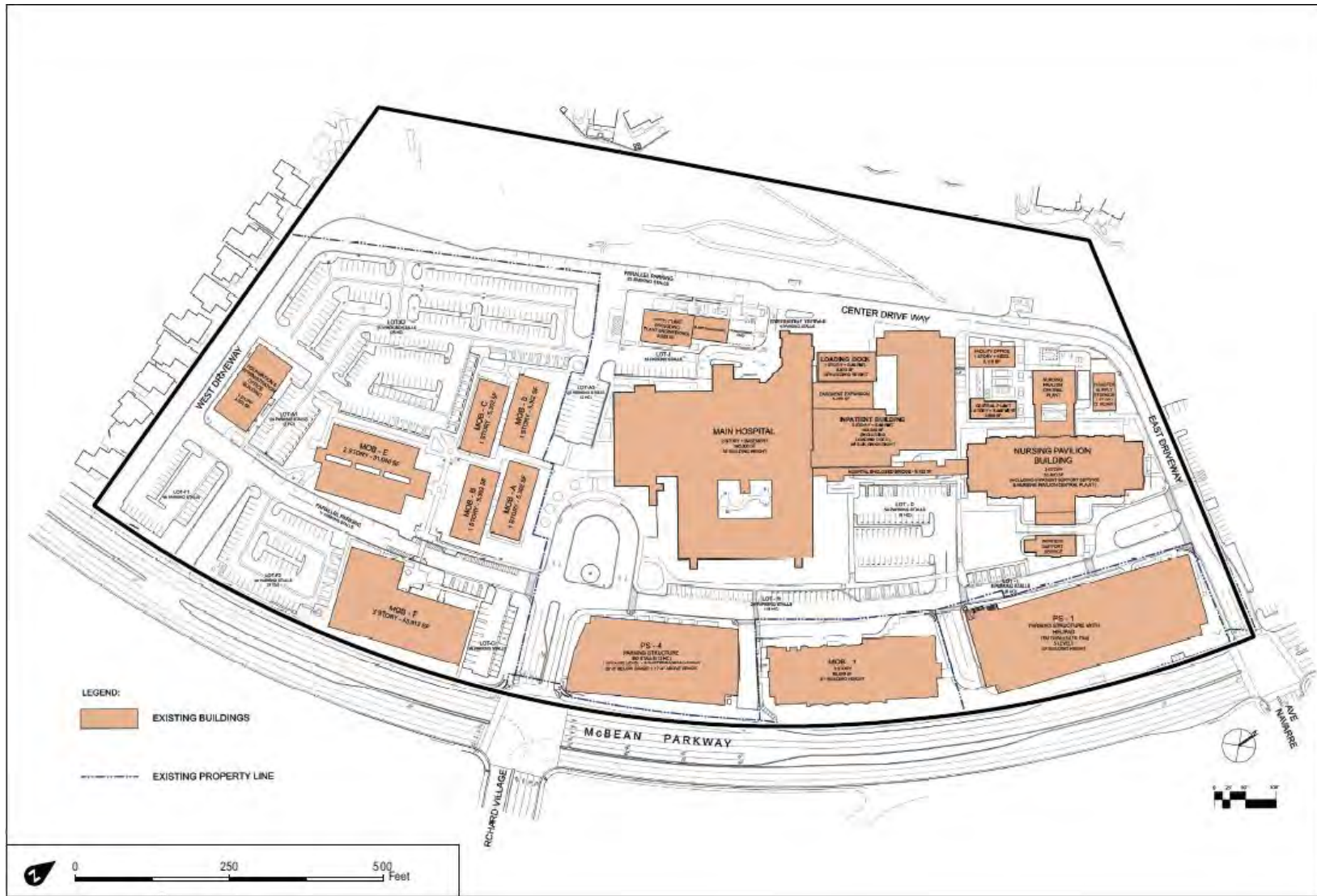
		Existing <sup>1</sup>		Approved Specific Plan		Proposed Specific Plan Amendment	
<i>Land Use</i>	<i>Parking Ratio</i>	<i>Units</i>	<i>Spaces</i>	<i>Units</i>	<i>Spaces</i>	<i>Units</i>	<i>Spaces</i>
Hospital – Inpatient	2 Spaces/Bed	236 Beds	472	368 Beds	736	NC <sup>2</sup>	736
Hospital – Outpatient	2.5 Spaces/KSF	11.375 KSF	29	15.375 KSF	39	45.375 KSF	114
Medical Services	5 Spaces/KSF	164.916 KSF	825	280.916 KSF	1,405	NC	1,405
Professional Office	4 Spaces/KSF	8.734 KSF	35	0.734 KSF	3	NC	3
Storage	1 Space/KSF	2.384 KSF	3	2.384 KSF	3	NC	3
Parking Code Requirement		1,364		2,186		2,261	
Parking Supply		1,547		2,227		2,441	
Code Surplus/Shortfall		+183		+41		+180	

Source: Santa Clarita Municipal Code, Chapters 17.43, 17.44, and 17.45, 2020  
 KSF = 1,000 Square Feet

- <sup>1</sup> For the purposes of our analysis, the “Existing” parking code requirement refers to March 2018 – the period which LLG performed its inventory and utilization survey. Therefore, this existing requirement does not consider the new inpatient building which opened in 2019, however the approved bed count is not changing under the 2020 Amended Specific Plan.
- <sup>2</sup> NC = No Change from Approved Specific Plan.

# Existing Site Plan

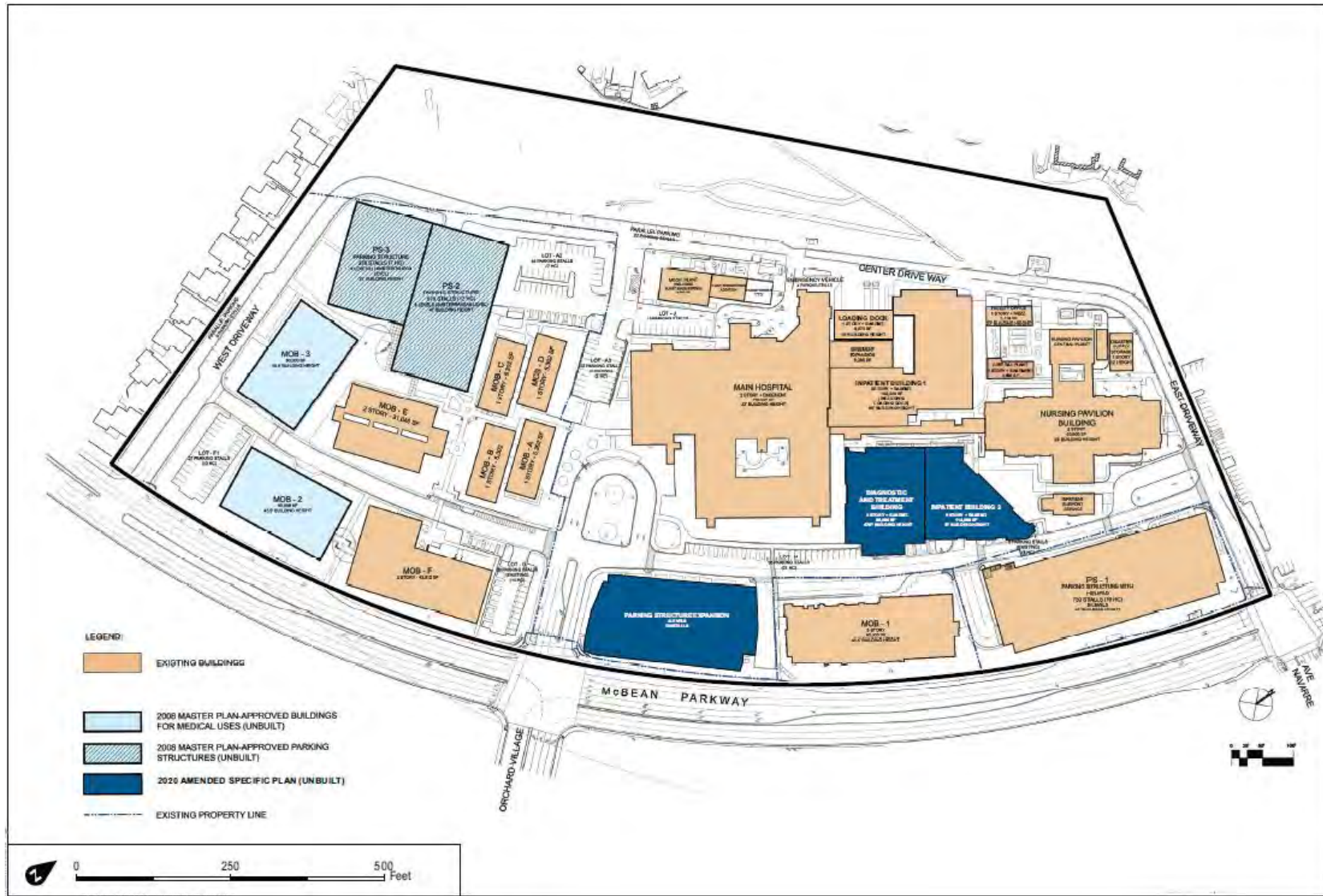
This figure replaces the 2008 Master Plan Existing Site Plan and Construction Projects figure.



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# Site Plan

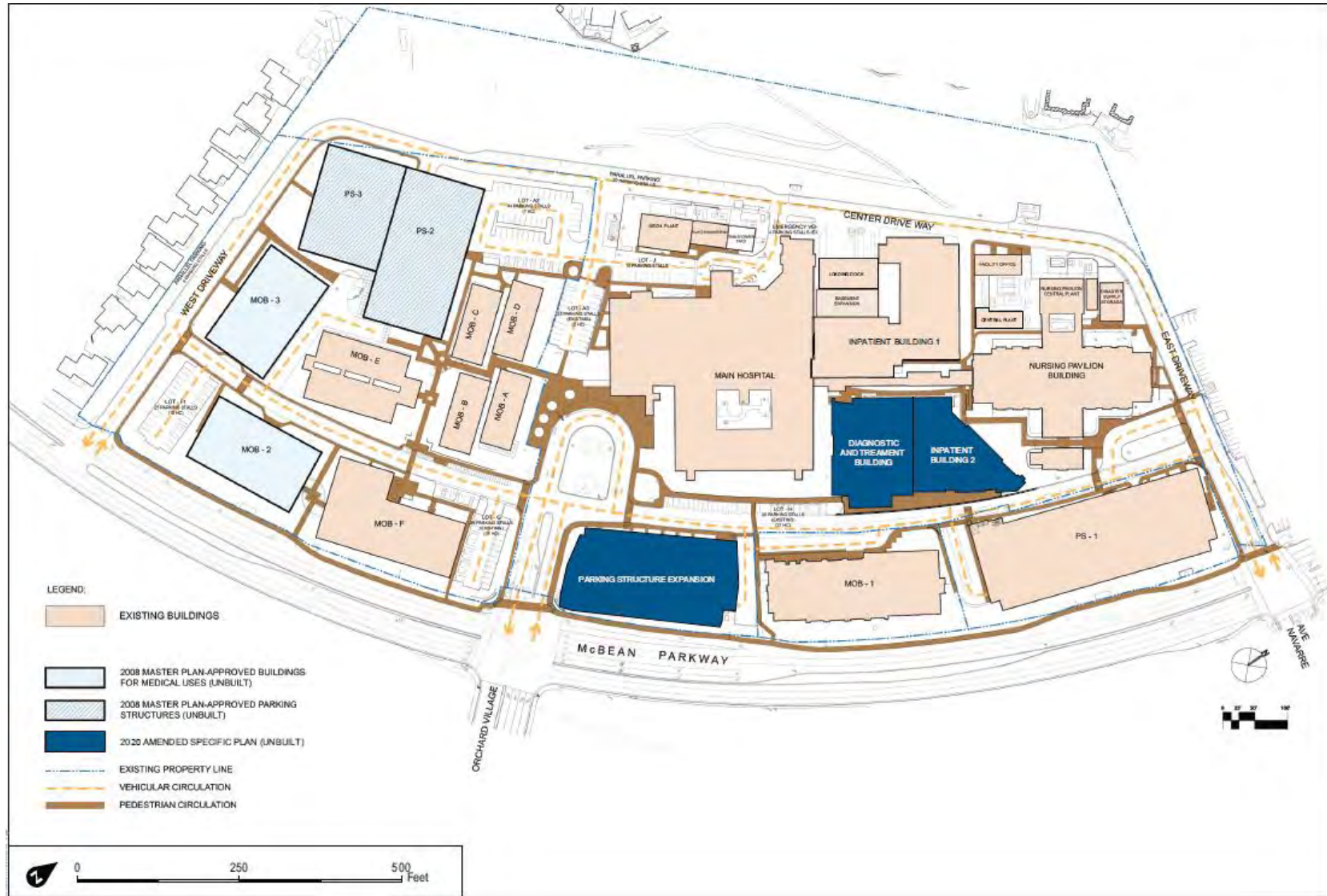
This figure replaces the 2008 Master Plan exhibit.



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# Circulation Plan

This figure replaces the 2008 Master Plan – Pedestrian Linkages figure.

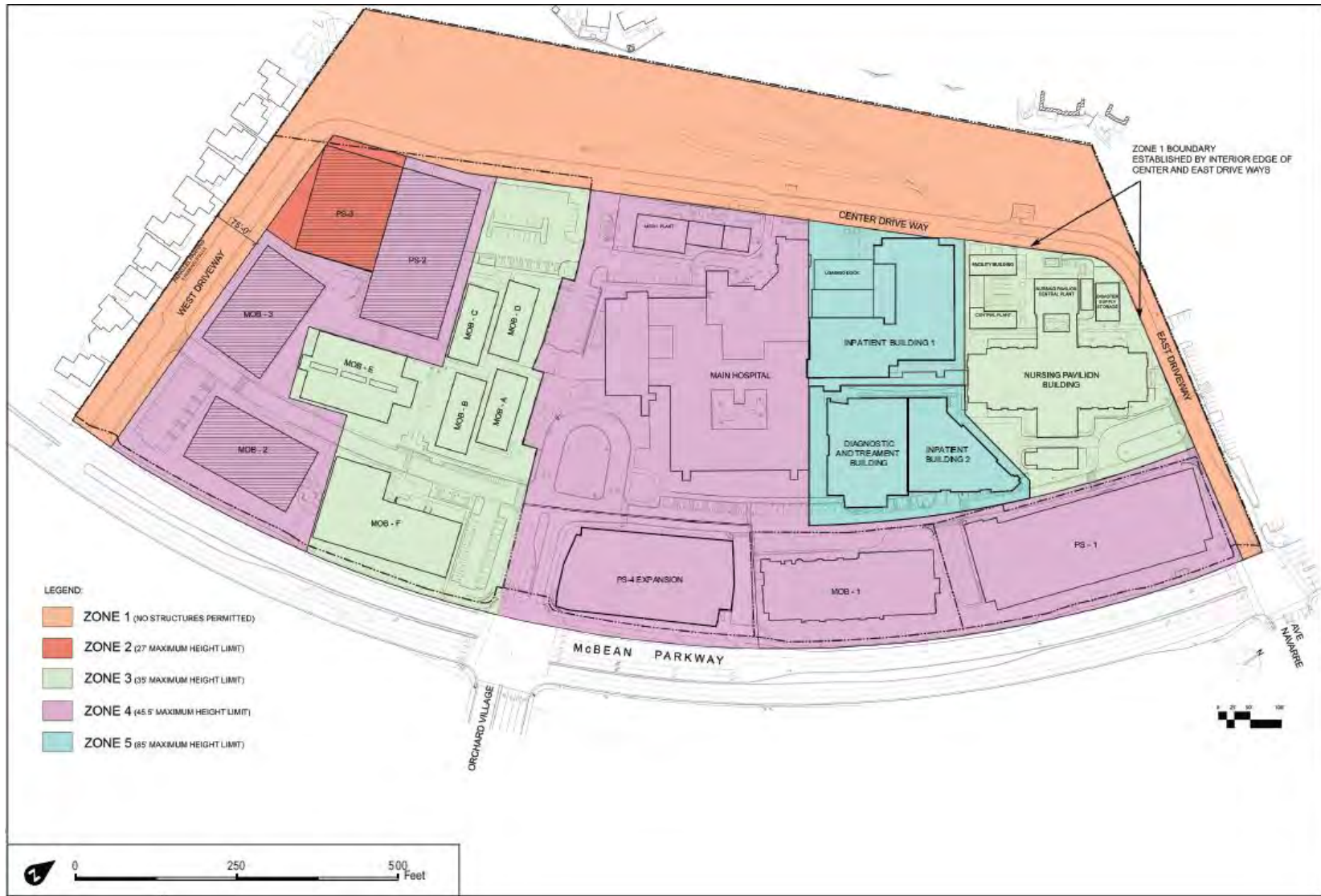


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# Building Height Zones

This figure replaces the 2008 Master Plan Proposed Building Height Limit Zones figure.



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## Bird's Eye

In addition to the Aerial View exhibits in the 2008 Master Plan, the following exhibit shows the new Inpatient Building 2 and D&T buildings.



Henry Mayo Newhall Hospital  
Strategic Plan Amendment Study

Bird's Eye





## Elevations and Visual Simulations for 2020 Amended Master Plan

The following exhibits show the elevations for the parking structure expansion, D&T and Inpatient Buildings.



Henry Mayo Newhall Hospital  
Strategic Plan Amendment Study

Existing Campus - East View







Henry Mayo Newhall Hospital  
Strategic Plan Amendment Study

**Parking Structure Expansion - East View**









Henry Mayo Newhall Hospital  
Strategic Plan Amendment Study

**Parking Structure Expansion - West View**







Henry Mayo Newhall Hospital  
Strategic Plan Amendment Study

**Parking Structure Expansion - North View From Orchard and Mc Bean Intersection**







Henry Mayo Newhall Hospital  
Strategic Plan Amendment Study

**D&T Building North - East View**







Henry Mayo Newhall Hospital  
Strategic Plan Amendment Study

**D&T and Inpatient Buildings North - East View**









Henry Mayo Newhall Hospital  
Strategic Plan Amendment Study

**D&T and Inpatient Buildings - West View**





## Section L-M - Site Cross Sections

The following exhibit supplements the cross sections in the 2008 Master Plan.



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# Tree Removal Plan

This figure is an augment to the 2008 Master Plan Tree Removal Plan.



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# **APPENDIX A**

*2008 Master Plan and Amendments  
to the Master Plan*





HENRY MAYO NEWHALL MEMORIAL HOSPITAL  
MASTER PLAN



EXHIBIT A  
NOVEMBER 19, 2008



48 east holly street  
pasadena, ca 91103  
tel: 626.793.9805 • fax: 626.793.9807

COVER SHEET

INDEX SHEET

PROJECT DESCRIPTION

01\_MASTER PLAN PARKING SUMMARY  
02\_EXISTING SITE PLAN & CONSTRUCTION PROJECTS  
03\_MASTER PLAN  
04\_MASTER PLAN - PEDESTRIAN LINKAGES  
05\_MASTER PLAN - PROPOSED BUILDING HEIGHT LIMIT ZONES  
06\_AERIAL VIEW - PROPOSED BUILDING AND PARKING STRUCTURE LOCATIONS  
07\_MASTER PLAN - CROSS SECTION LOCATION MAP  
08\_PS 1 / SECTION - A & B  
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11\_MOB 3 / SECTION - F  
12\_PS 2 & PS 3 / SECTION - G  
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16\_MASTER PLAN TREE SUMMARY  
17\_TREE REMOVAL PLAN (ALONG McBEAN PARKWAY)  
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23\_VISUAL SIMULATION - KEY PLAN  
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31\_VISUAL SIMULATION - VIEW 3 (LANDSCAPE: 15 YEAR GROWTH)  
32\_VISUAL SIMULATION - VIEW 3 (LANDSCAPE: 25 YEAR GROWTH)  
33\_LANDSCAPE TRELLIS (AT MOB 2 AND BUILDING F): PERSPECTIVE  
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## INDEX

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway, Santa Clarita, CA 91355

SWA #: 060307 NOVEMBER 19, 2008



48 east holly street  
pasadena, ca 91103  
tel: 626.793.9905 • fax: 626.793.9807

# HENRY MAYO NEWHALL MEMORIAL HOSPITAL MASTER PLAN PROJECT

## NOVEMBER 19, 2008

### ENTITLEMENTS REQUESTED

The entitlement requests before the City Council comprise the following:

1. Master Plan
2. Development Agreement

### HMMNH MASTER PLAN PROJECT

#### *Proposed Campus Facilities*

The project request will increase the existing square footage of the hospital campus from 340,071 square feet to 667,434 square feet, a 327,363 net square-foot increase. The 15-year Master Plan includes the following components:

#### New Hospital & Medical Office Buildings

1. INPATIENT BUILDING: A 125,363 square-foot, 120-bed Inpatient Building located in the central portion of the campus. This building will be 85 feet in height to the top of the parapet and 100 feet in height to the top of the wind sock and elevator shaft. This building will also include a rooftop helipad;
2. MEDICAL OFFICE BUILDING 1: An 80,000 square-foot medical office building (MOB 1) measuring 45.5 feet in height to the top of the parapet and 51.5 feet to the top of the screen and roof;
3. MEDICAL OFFICE BUILDING 2: A 60,000 square-foot medical office building (MOB 2) measuring 45.5 feet to the top of the parapet and 51.5 feet to the top of the screen and roof access;
4. MEDICAL OFFICE BUILDING 3: A 60,000 square-foot medical office building (MOB 3) measuring 45.5 feet to the top of the parapet and 51.5 feet to the top of the screen and roof access; and
5. CENTRAL PLANT: A 10,000 square-foot Central Plant building measuring 26 feet in height.

#### Parking Structures

1. PARKING STRUCTURE 1: A six-level (five levels above ground with one subterranean level), 750-space parking structure (PS 1) along the McBean Parkway frontage at Avenida Navarre. The parking structure height will be 47 feet to the top of the parapet, 49.5 feet to the top of the parking lot lights, and 60.5 feet to the top of the wind sock. This parking structure will also include a rooftop helipad;
2. PARKING STRUCTURE 2: A six-level (five levels above ground with one subterranean level), 579-space parking structure (PS 2) in the northwestern portion of the campus. The parking structure height will be 47 feet to the top of the parapet and 49.5 feet to the top of the parking lot lights. A solid wall along the western façade of the parking structure will be provided prior to construction of PS 3. PS 3 will connect directly to PS 2;
3. PARKING STRUCTURE 3: A four-level (three levels above ground with one subterranean level), 278-space parking structure (PS 3) in the northwestern corner of the campus. The parking structure height will be 27 feet to the top of the parapet, and 30 feet to the top of the parking lot lights. A solid wall along the western façade of the parking structure will be provided; and
4. PARKING STRUCTURE 4: A three-level (two subterranean levels and one surface level), 316-space parking structure (PS 4) to be located along McBean Parkway at the intersection of McBean Parkway and Orchard Village Road. This area will initially be improved with a 71-space surface parking lot to be improved with the construction of MOB1.

### Other HMMNH Campus Modifications

In addition to construction of the above facilities, the HMMNH Master Plan proposes to:

1. Add nine new beds in the Nursing Pavilion Building;
2. Demolish the 8,000 square-foot Foundation building to accommodate Medical Office Building 3;
3. Reconfigure surface parking to provide a total of 308 on-site spaces;
4. Provide a helipad on the rooftop of both Parking Structure 1 and the Inpatient Building;
5. Provide right turn pockets and modify traffic signals along the McBean Parkway project frontage;
6. Reconfigure 9,770 square feet of current administration space in the existing hospital building to accommodate 18 additional new ICU beds. The current hospital administrative functions would move to space within Medical Office Building 1;
7. Export up to 93,293 cubic yards of dirt associated with subsurface excavation for the Inpatient Building and Parking Structures 1, 2, 3, and 4; and
8. Dedicate a minimum of 58 feet of public right-of-way from the centerline along the project frontage plus additional right-of-way dedication to accommodate a new right-turn lane from eastbound McBean Parkway to southbound Orchard Village Road to address future traffic conditions.

#### *On-Site Parking*

A total of 2,231 on-site parking spaces will be provided, to include 110 handicapped parking spaces. This will be accomplished with some surface parking (308 spaces) and the construction of four parking structures to provide a total of 1,923 spaces, as listed above. Required on-site parking is 2,190 spaces, per the City's Unified Development Code.

## PROJECT DESCRIPTION

**MASTER PLAN PARKING SUMMARY**

**SWA ARCHITECTS**

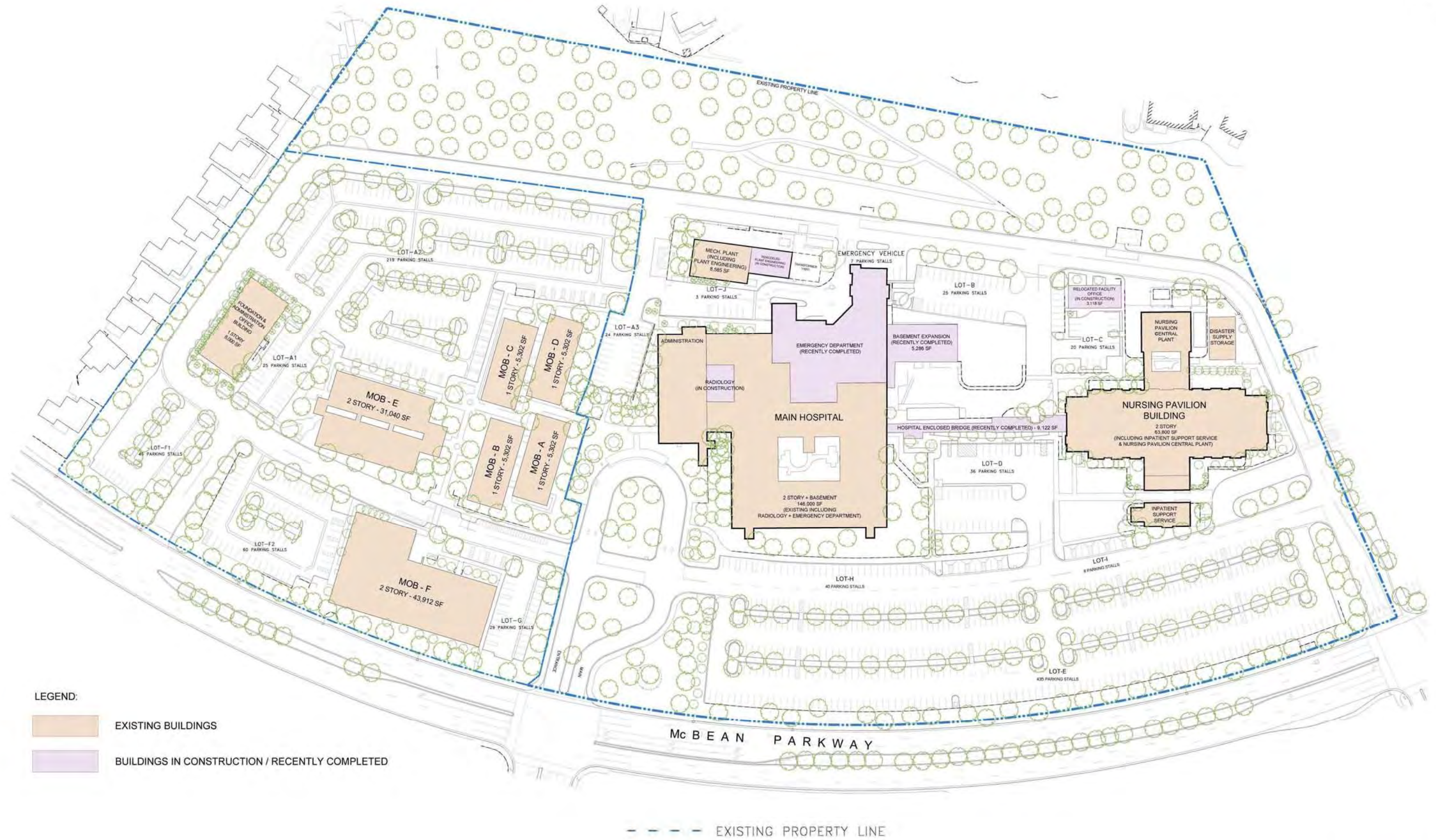
HENRY MAYO NEWHALL MEMORIAL HOSPITAL MASTER PLAN  
DATE: NOVEMBER 19, 2008

48 E. Holly Street  
Pasadena, CA 91103

REQUIRED PARKING				PARKING PER DESIGN	
BUILDING NAME	BUILDING AREA	BED COUNT	REQ'D PARKING STALLS	PARKING NAME/TYPE	PROVIDED PARKING STALLS
<b>MEDICAL BUILDINGS</b>					
MOB A (EXISTING)	5,302 / <b>4,567</b> SF	N/A	23	PS1 with 1 SUBTERRANEAN LEVEL	750
MOB B (EXISTING)	5,302 / <b>4,560</b> SF	N/A	23	PS2 with 1 SUBTERRANEAN LEVEL	579
MOB C (EXISTING)	5,302 / <b>4,561</b> SF	N/A	23	PS3 with 1 SUBTERRANEAN LEVEL	278
MOB D (EXISTING)	5,302 / <b>4,560</b> SF	N/A	23	PS4 with 2 SUBTERRANEAN LEVELS	316
MOB E (EXISTING)	31,040 / <b>25,508</b> SF	N/A	128	SURFACE (GENERAL)	253
MOB F (EXISTING)	43,912 / <b>43,912</b> SF	N/A	220	SURFACE (FOR PHYSICIANS-CURRENTLY GATED)	48
FOUNDATION BLDG. (EXISTING)	8,000 SF	N/A	32	SURFACE (FOR EMERGENCY)	7
MOB 1 (NEW)	80,000 / <b>78,400</b> SF	N/A	392	<b>TOTAL PARKING PROVIDED</b>	<b>2,231</b>
MOB 2 (NEW)	60,000 / <b>58,800</b> SF	N/A	294	<b>HANDICAP PARKING</b>	
MOB 3 (NEW)	60,000 / <b>58,800</b> SF	N/A	294	PARKING NAME/TYPE	PROVIDED HANDICAP PARKING
FOUNDATION BLDG. (REMOVED)	(8,000 SF)	N/A	-32		
<b>SUBTOTAL</b>			<b>1,420</b>		
<b>HOSPITAL BUILDINGS</b>					
MAIN HOSPITAL (EXISTING)	N/A	EXISTING: 121 FUTURE ICU: 18	242 36	PS1	15
EMERGENCY DEPT. URGENT CARE (EXISTING)	5,518 SF	N/A	18	PS2	12
RADIOLOGY OUTPATIENT(EXISTING)	2,952 SF	N/A	8	PS3	6
RADIOLOGY(OUTPATIENT) (IN CONSTRUCTION)	2,905 SF	N/A	7	PS4	7
				SURFACE	70
				<b>TOTAL HANDICAP PARKING PROVIDED</b>	<b>110</b>
NURSING PAVILION (EXISTING)	N/A	EXISTING: 100 FUTURE SUB ACUTE UPGRADE: 9	200 18	Note: SF in bold reflects gross square footage per City of Santa Clarita Municipal Code, Chapter 17.07.010. "Floor area (gross)" shall mean the sum of the gross horizontal areas of the several floors of the building measured from the exterior faces of the exterior walls excluding exterior balconies and porches. Floor area shall not include interior parking spaces, loading spaces for motor vehicles, any space where the floor-to-ceiling height is less than six (6) feet, vertical shafts and attics and mechanical penthouses provided there are no usable rooms, no windows and the mechanical penthouse area is used exclusively for mechanical equipment. Floor area shall include any area used for storage and, for auto repair uses, any work area, or related facility, where vehicles are serviced and repaired.	
HOSPITAL ENCLOSED BRIDGE (RECENTLY COMPLETED)	9,122 SF	N/A	N/A		
FACILITY BUILDING (IN CONSTRUCTION)	OFFICE AREA 734 SF WAREHOUSING 2,384 SF	N/A	5		
INPATIENT BLDG (NEW)	N/A		120		
	OUTPATIENT SERVICES 4,000 SF	N/A	10		
<b>SUBTOTAL</b>			<b>784</b>		
<b>TOTAL PARKING REQUIRED</b>			<b>2,204</b>		

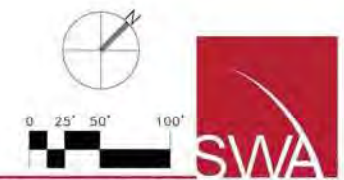
**MASTER PLAN PARKING SUMMARY**





### EXISTING SITE PLAN & CONSTRUCTION PROJECTS

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway, Santa Clarita, CA 91355



SWA # 060307 NOVEMBER 19, 2008 ARCHITECTS

48 east holly street  
pasadena, ca 91103  
HP FOR 781.6828 - for 781.9877





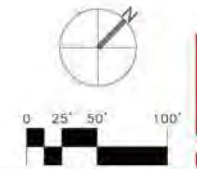
LEGEND:

- EXISTING BUILDINGS
- BUILDINGS IN CONSTRUCTION / RECENTLY COMPLETED
- PROPOSED BUILDINGS FOR HOSPITAL AND MEDICAL USES
- PROPOSED PARKING STRUCTURES
- PROPOSED SUBTERRANEAN PARKING STRUCTURE (PS-4)

- RIGHT-OF-WAY DEDICATION
- EXISTING PROPERTY LINE

LEGEND:

- EXISTING TREES TO REMAIN
- PROPOSED TREES



MASTER PLAN

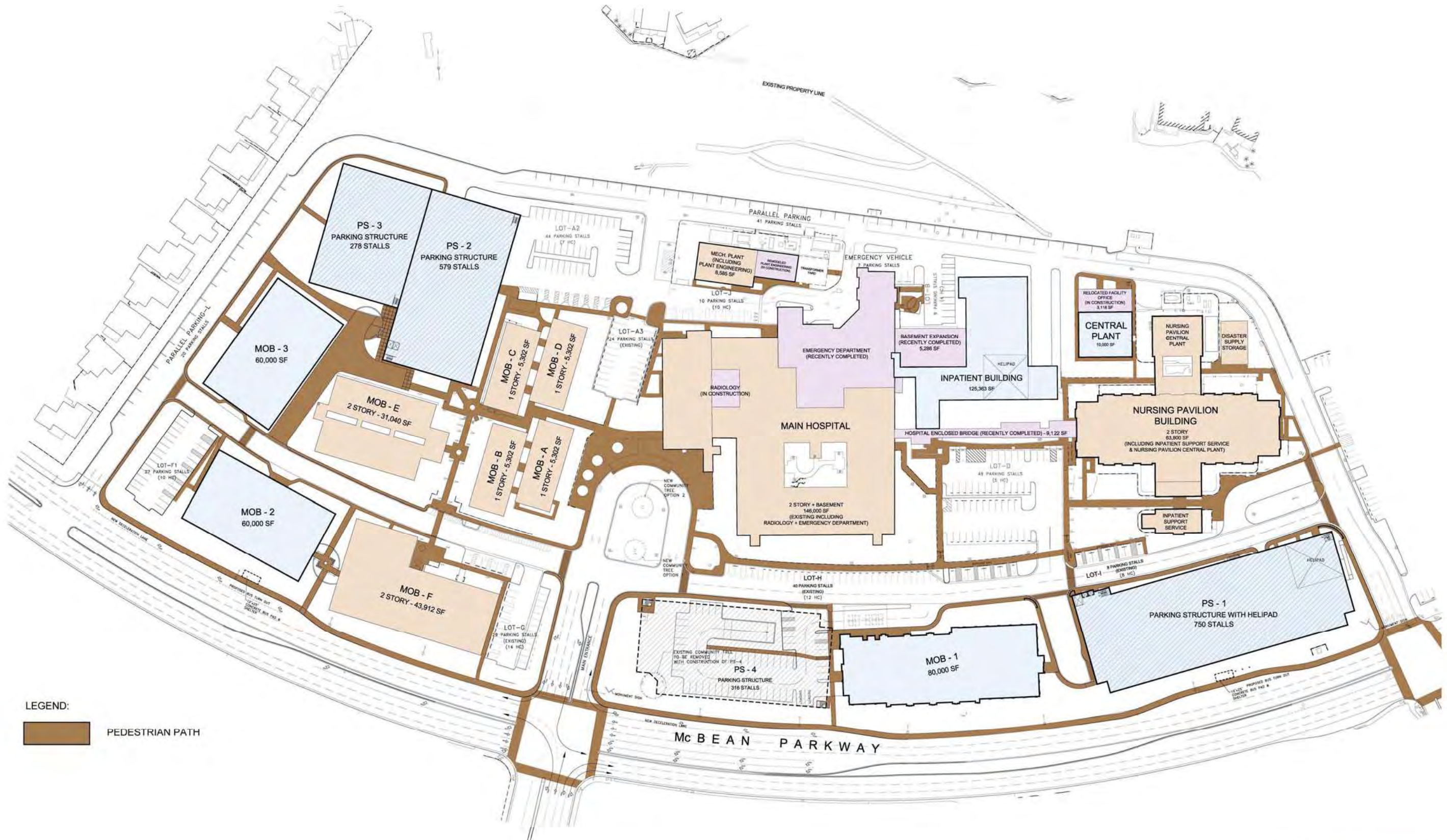
HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway, Santa Clarita, CA 91355

SWA # 060307 NOVEMBER 19, 2008 ARCHITECTS

48 east holly street  
pasadena, ca 91103  
tel 626 791 9977 fax 626 791 9917



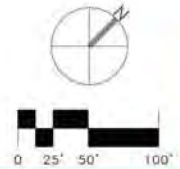




LEGEND:  
 PEDESTRIAN PATH

**MASTER PLAN - PEDESTRIAN LINKAGES**

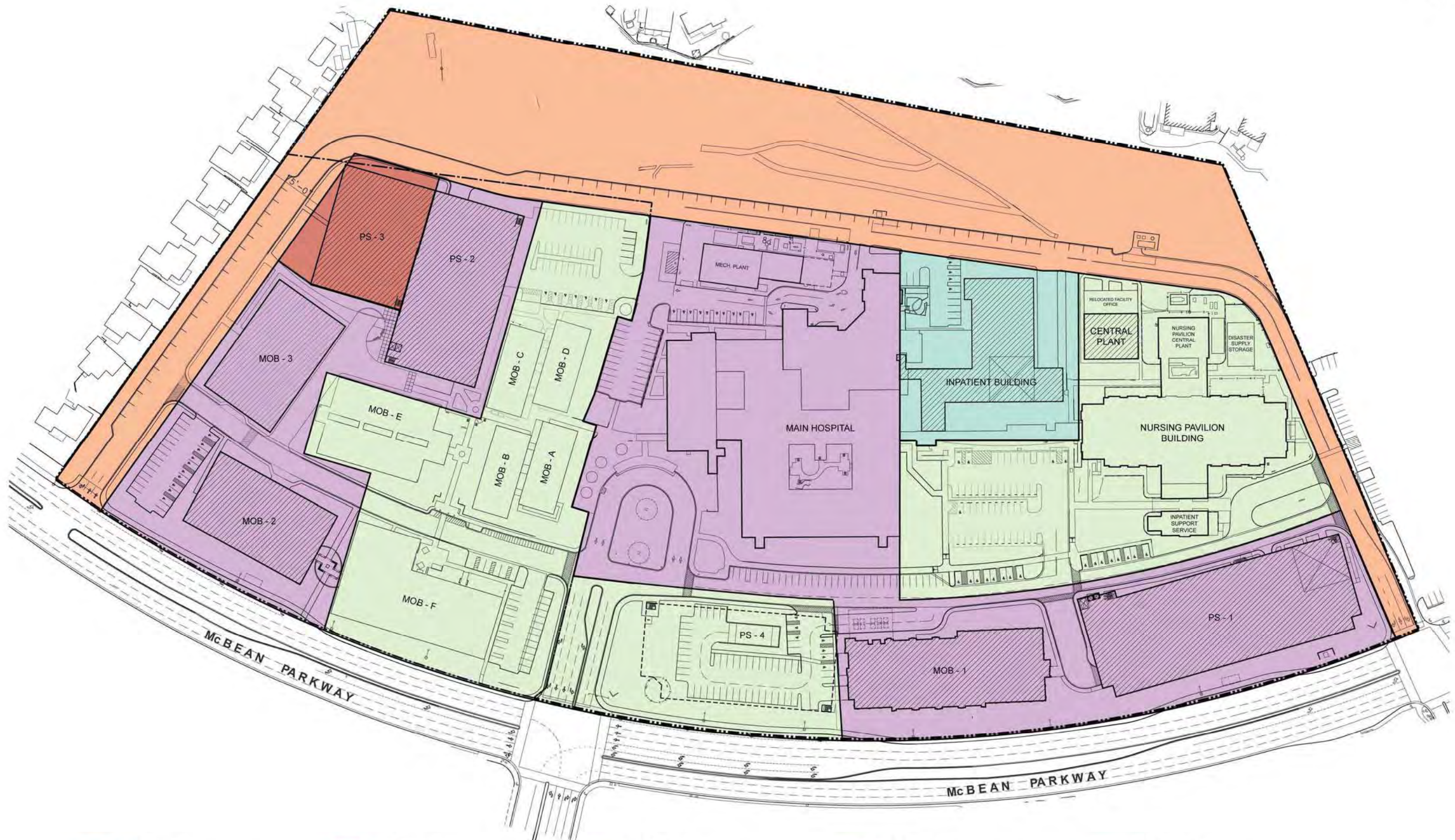
HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
 23845 McBean Parkway, Santa Clarita, CA 91355



SWA #: 060307 NOVEMBER 19, 2008 ARCHITECTS

48 east holly street  
 pasadena, ca 91103  
 tel: 626.793.9505 fax: 626.793.9507





**ZONE 1**  
(NO STRUCTURES PERMITTED)

**ZONE 2**  
(27' MAXIMUM HEIGHT LIMIT)

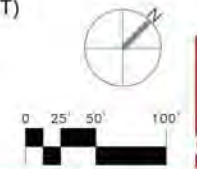
**ZONE 3**  
(35' MAXIMUM HEIGHT LIMIT)

**ZONE 4**  
(47' MAXIMUM HEIGHT LIMIT)

**ZONE 5**  
(85' MAXIMUM HEIGHT LIMIT)


**MASTER PLAN - PROPOSED BUILDING HEIGHT LIMIT ZONES\***

\* MAXIMUM HEIGHT LIMIT IS TO TOP OF PARAPET  
(EXCLUDES LIGHT POLES, MECH. EQUIPMENT, EQUIPMENT SCREENS & WINDSOCKS, ETC.)







 PROPOSED BUILDINGS & PARKING STRUCTURES

**AERIAL VIEW - PROPOSED BUILDING AND PARKING STRUCTURE LOCATIONS**

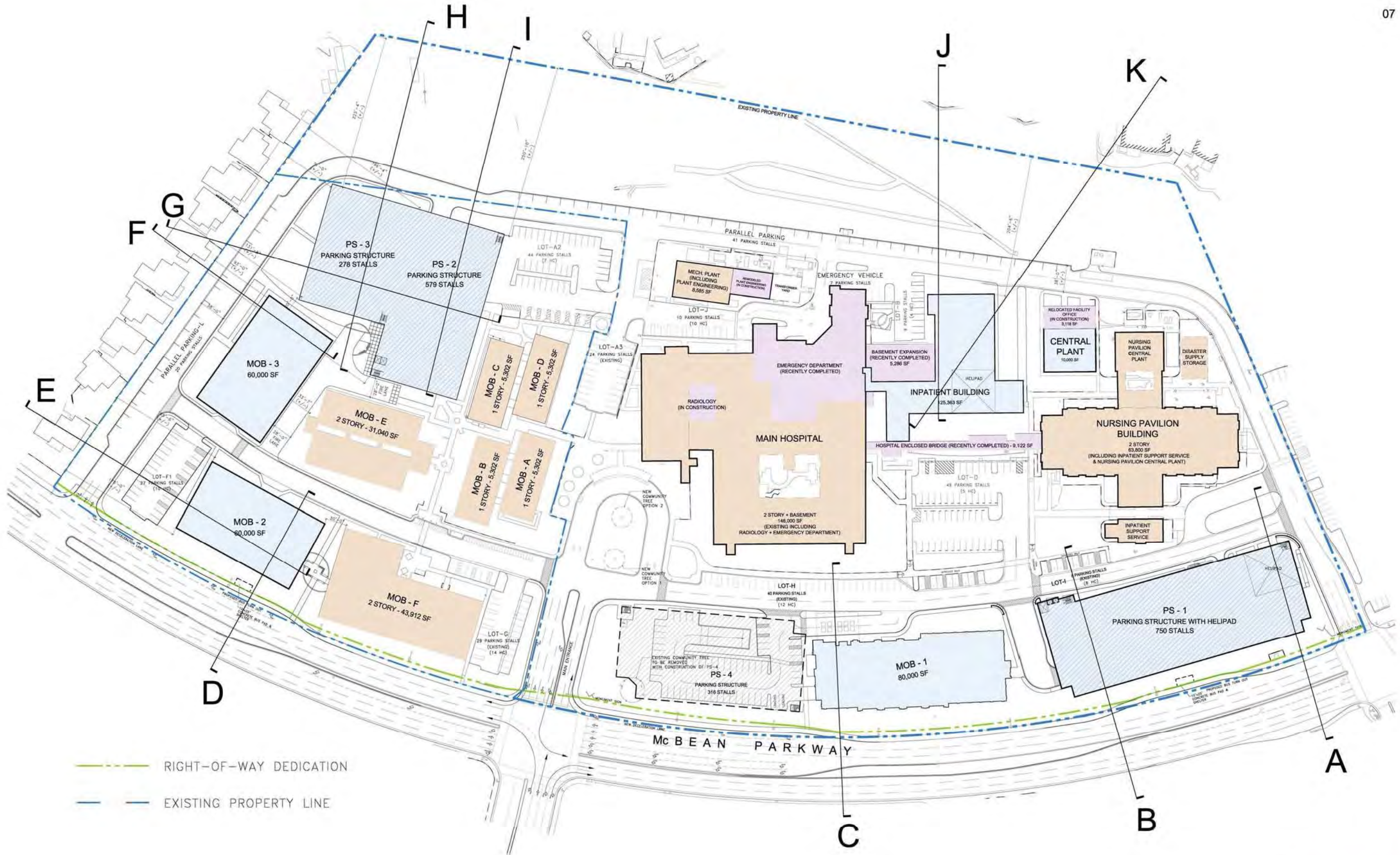
HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
 23845 McBean Parkway, Santa Clarita, CA 91355



SCALE: NTS  
 SWA # 060307 NOVEMBER 19, 2008



48 east holly street  
 pasadena, ca 91103  
 tel. 626.793.9805 - fax 626.793.9807





 RIGHT-OF-WAY DEDICATION  
 EXISTING PROPERTY LINE

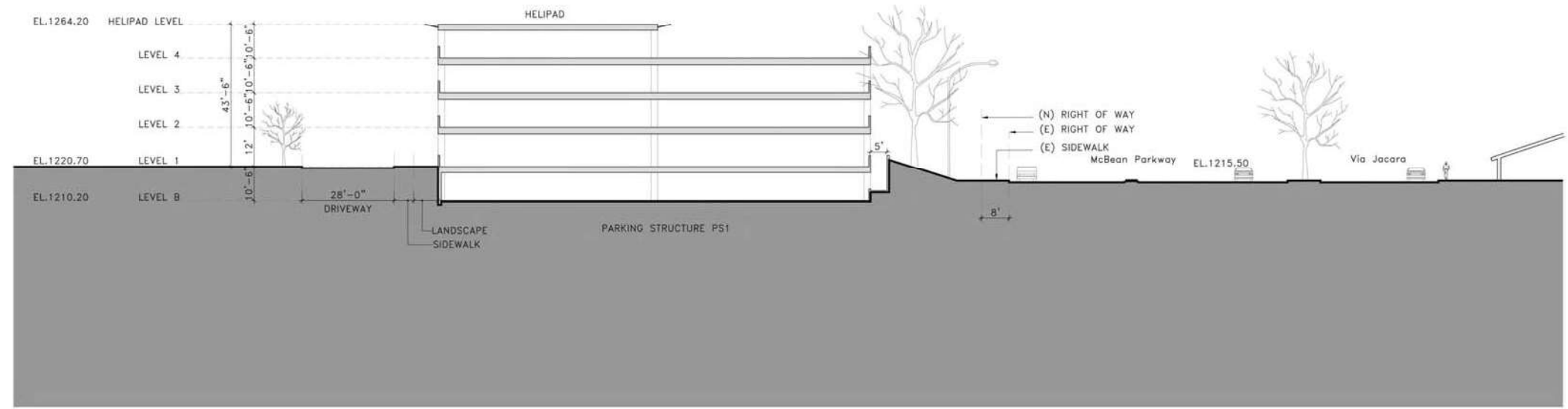
**MASTER PLAN - CROSS SECTION LOCATION MAP\***

\* CROSS SECTIONS SHOW MAXIMUM HEIGHT LIMIT IS TO TOP OF PARAPET.  
(EXCLUDES LIGHT POLES, MECH. EQUIPMENT, EQUIPMENT SCREENS & WINDSOCKS, ETC.)

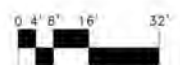
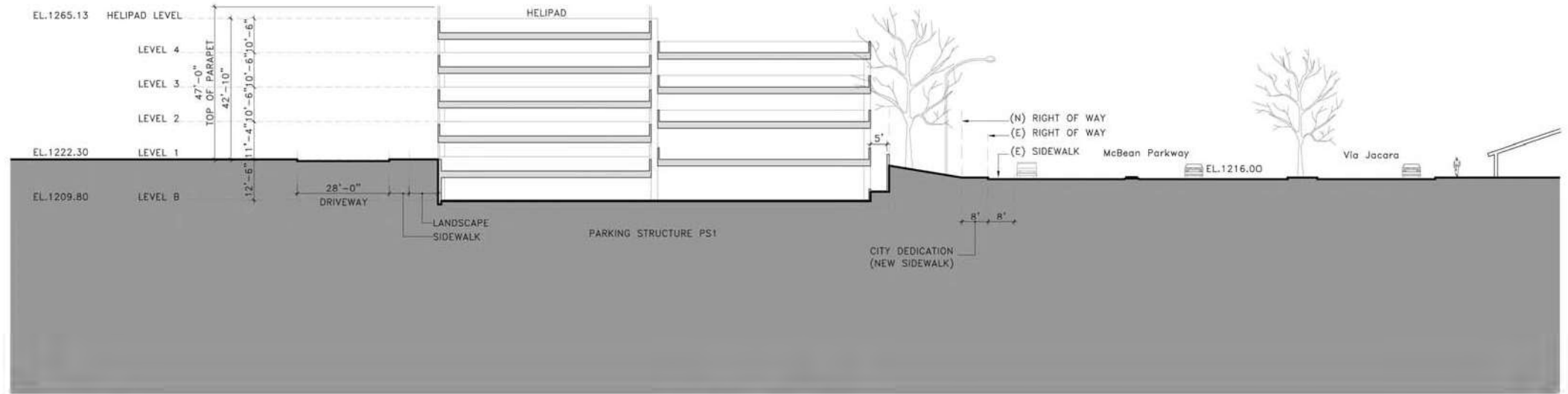




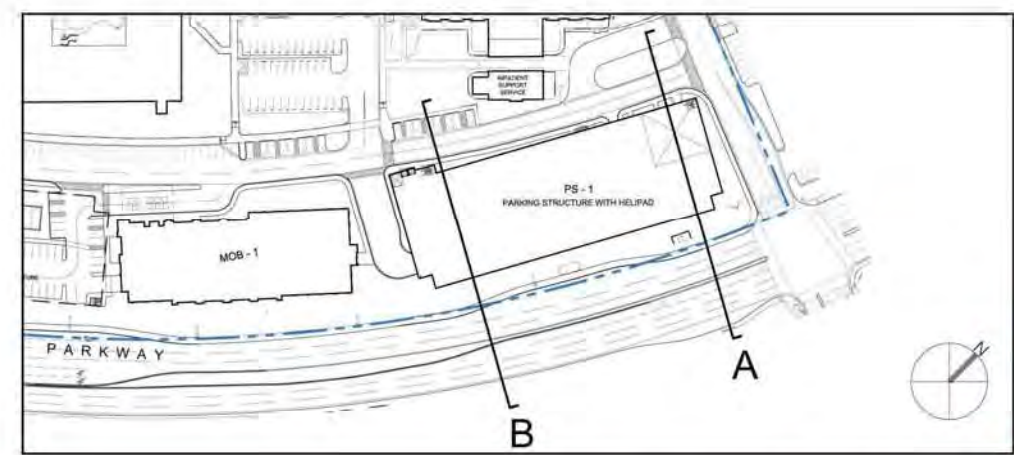




SECTION - A



SECTION - B



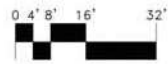
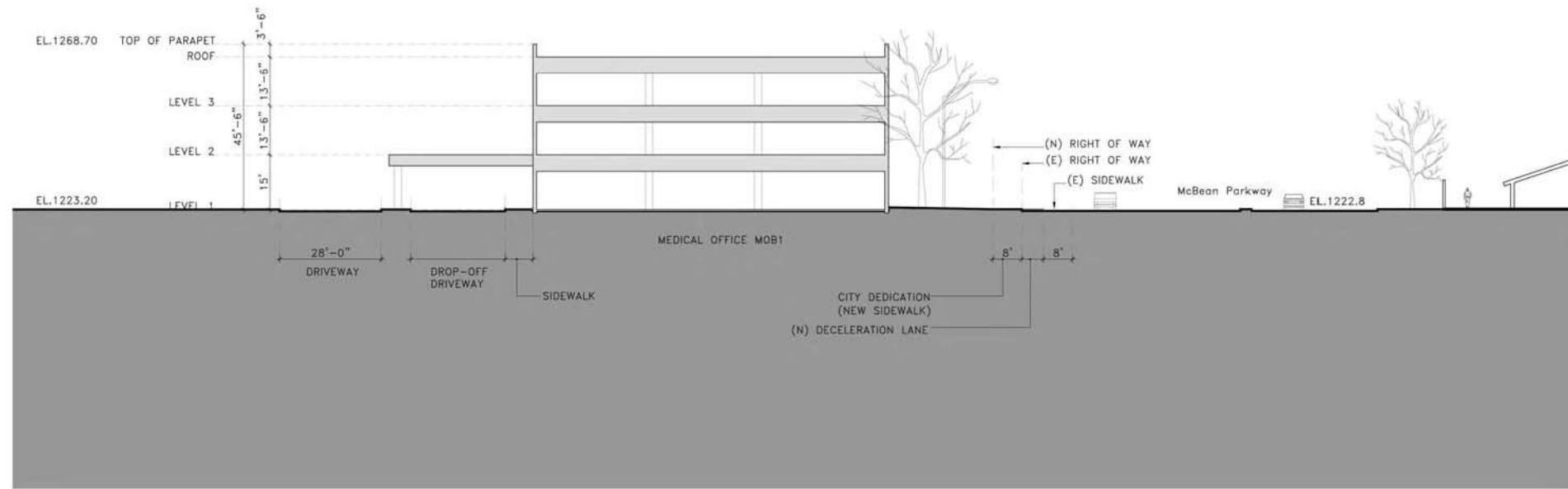
PS1 / SECTION - A & B

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway, Santa Clarita, CA 91355

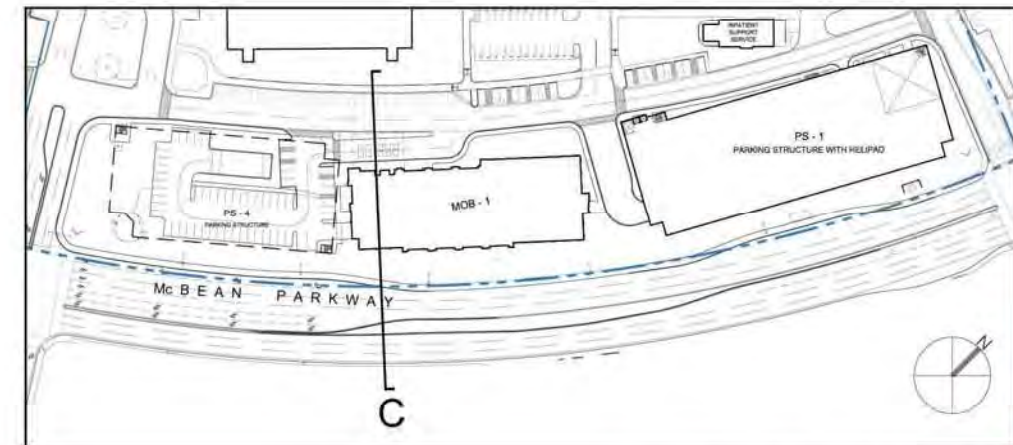
SWA # 060307 NOVEMBER 19, 2008 ARCHITECTS



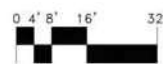
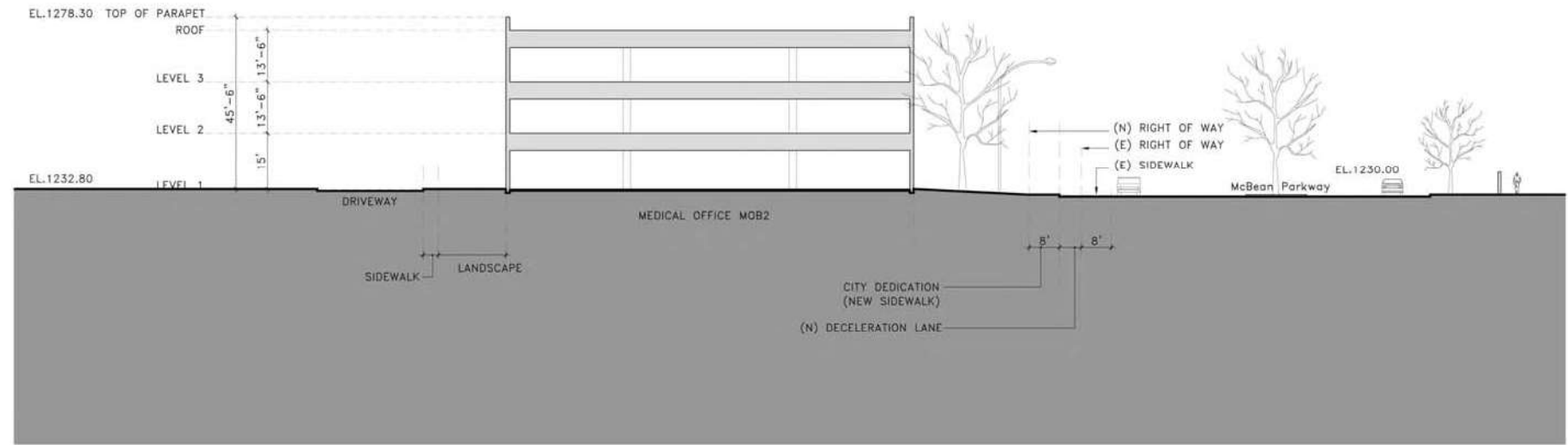
48 east holly street  
pasadena, ca 91103  
tel 626 791 9875 - fax 626 791 9711



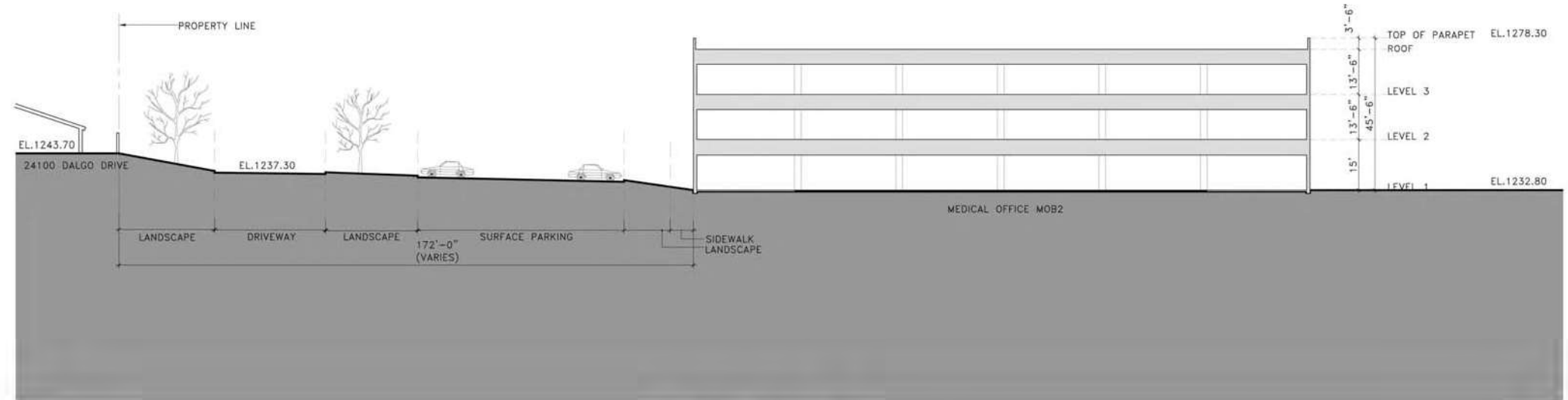
SECTION - C



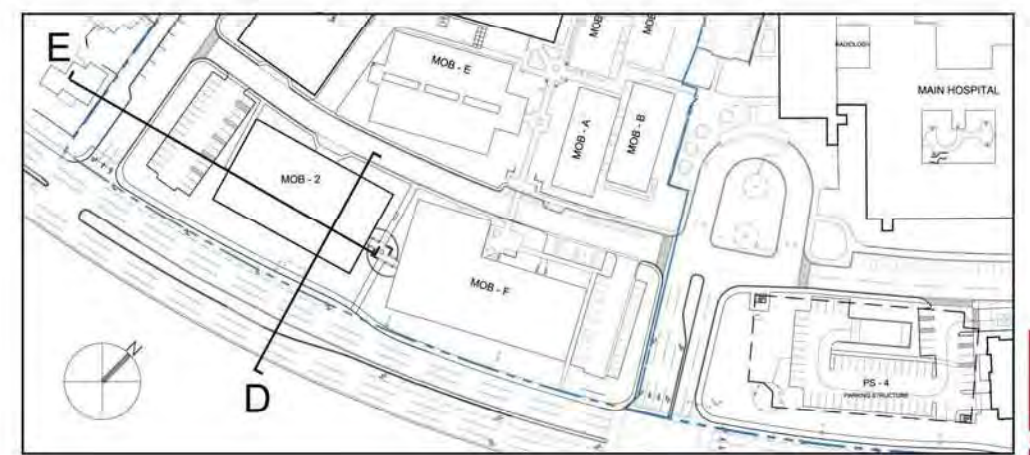
MOB1 / SECTION - C



SECTION - D



SECTION - E



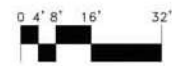
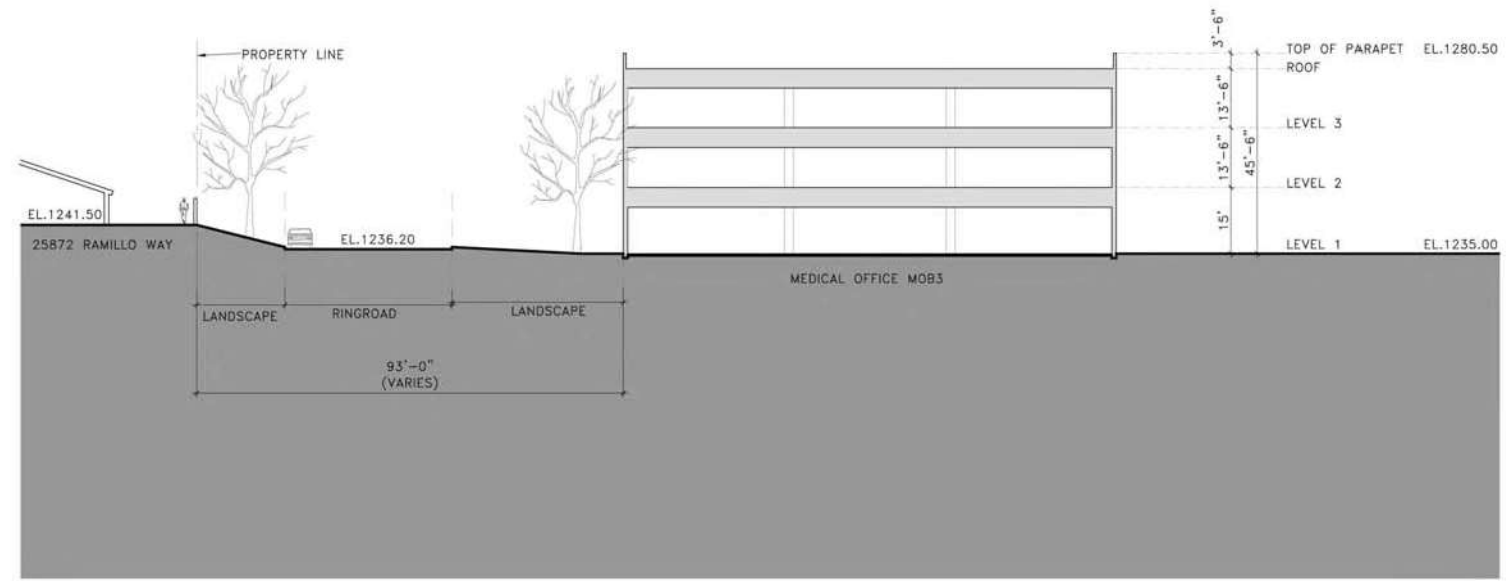
MOB2 / SECTION - D & E

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway, Santa Clarita, CA 91355

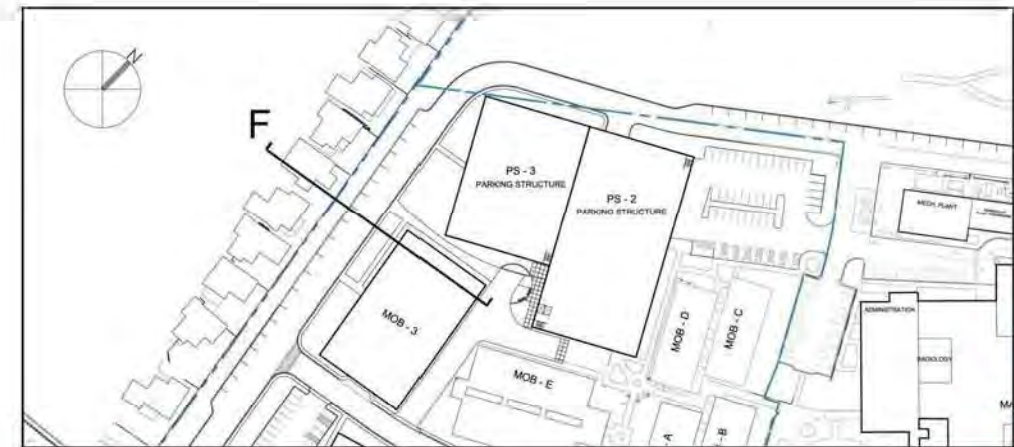
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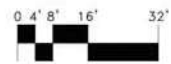
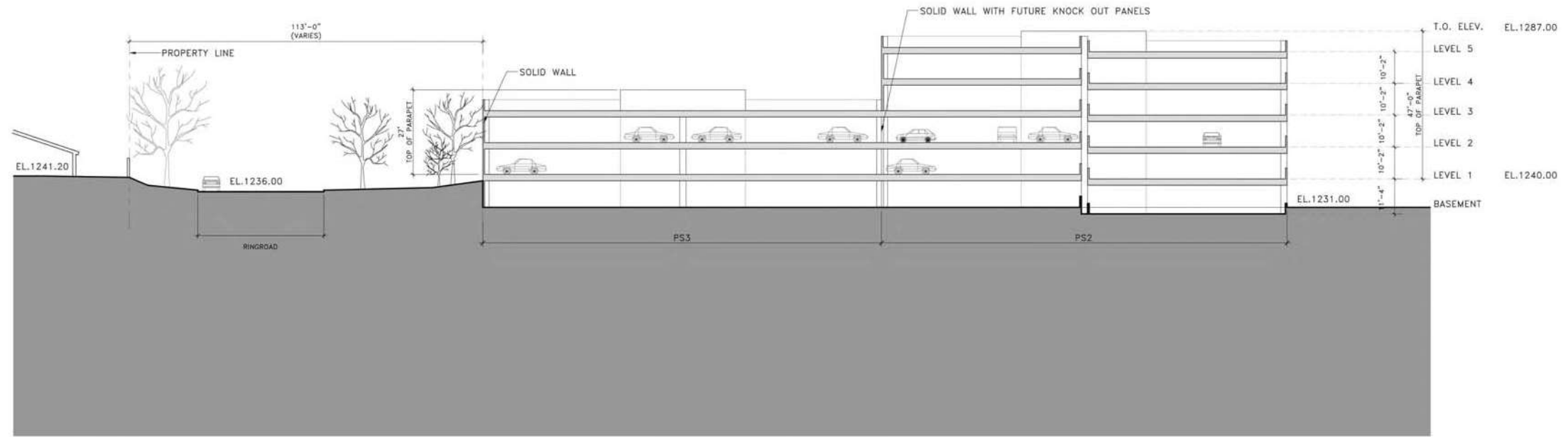
48 east holly street  
pasadena, ca 91103  
tel 626 791 9900 fax 626 791 9911



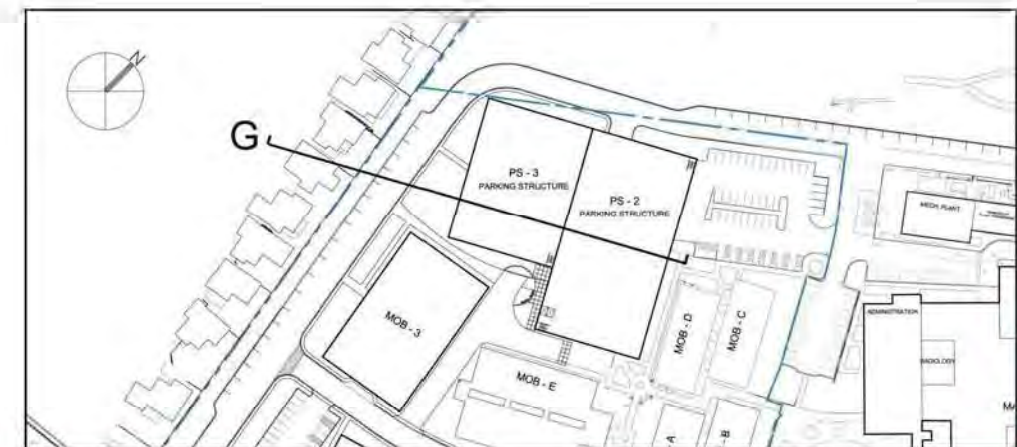
SECTION - F



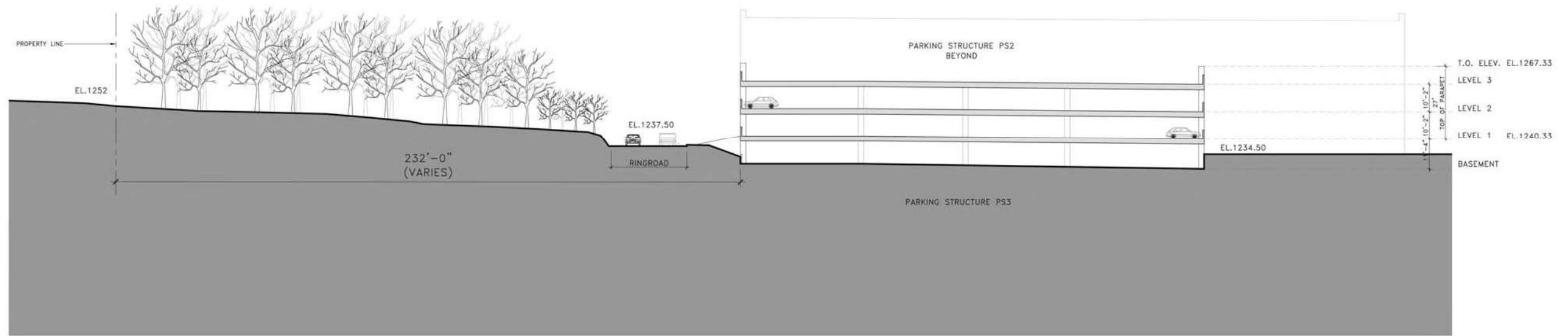
MOB3 / SECTION - F



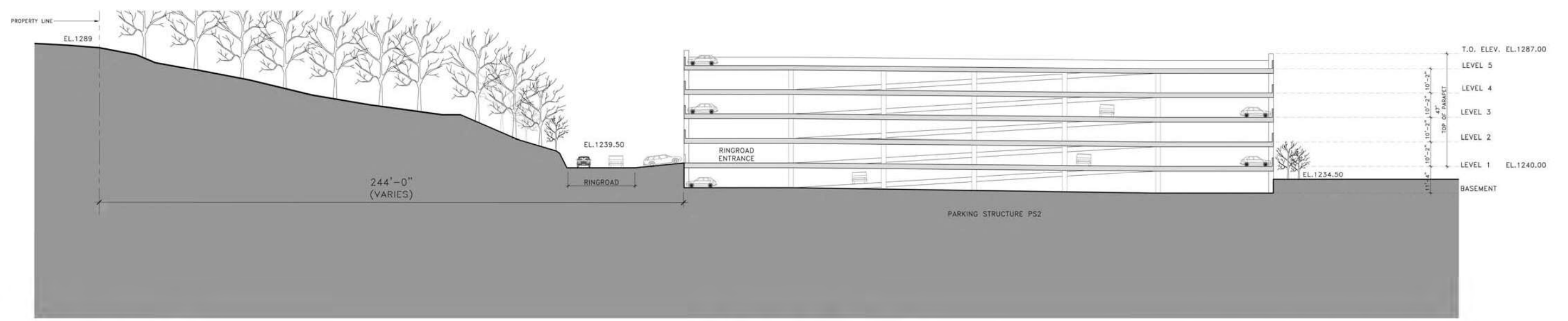
SECTION - G



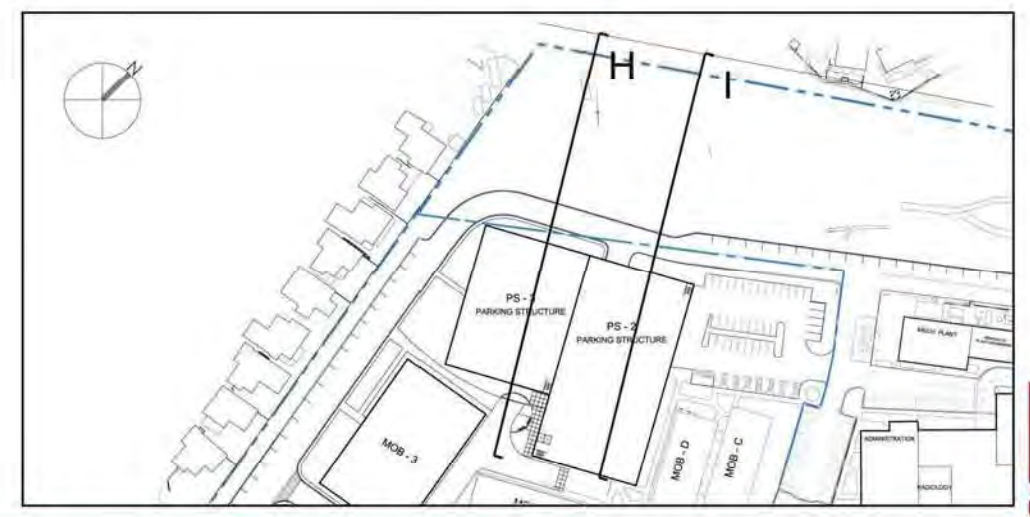
PS2 & PS3 / SECTION - G



SECTION - H

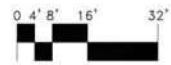
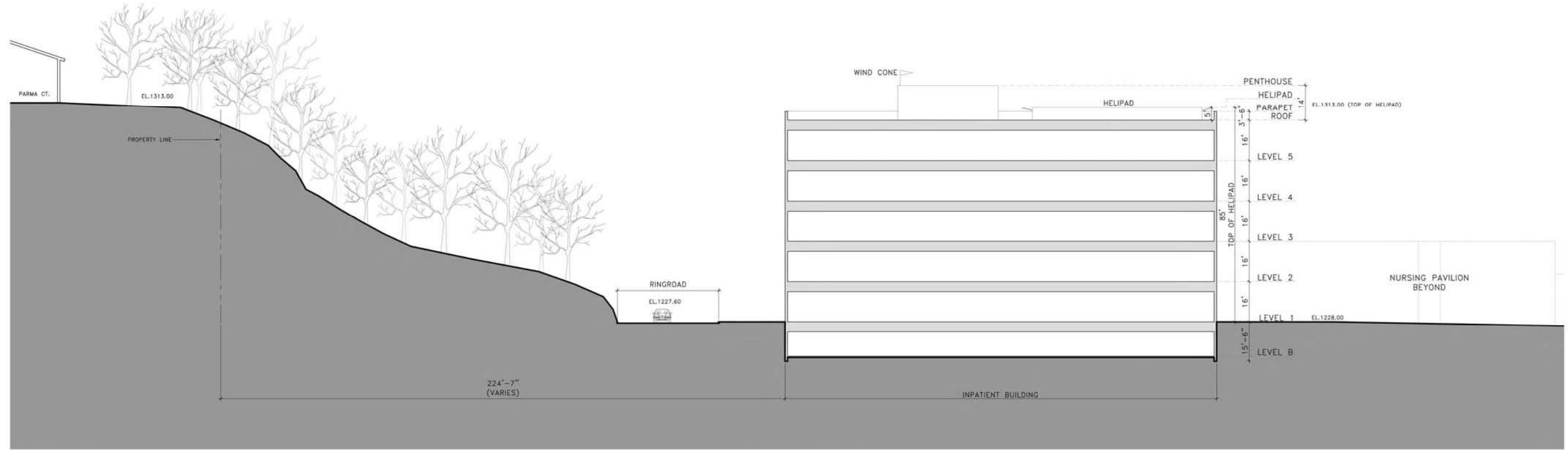


SECTION - I

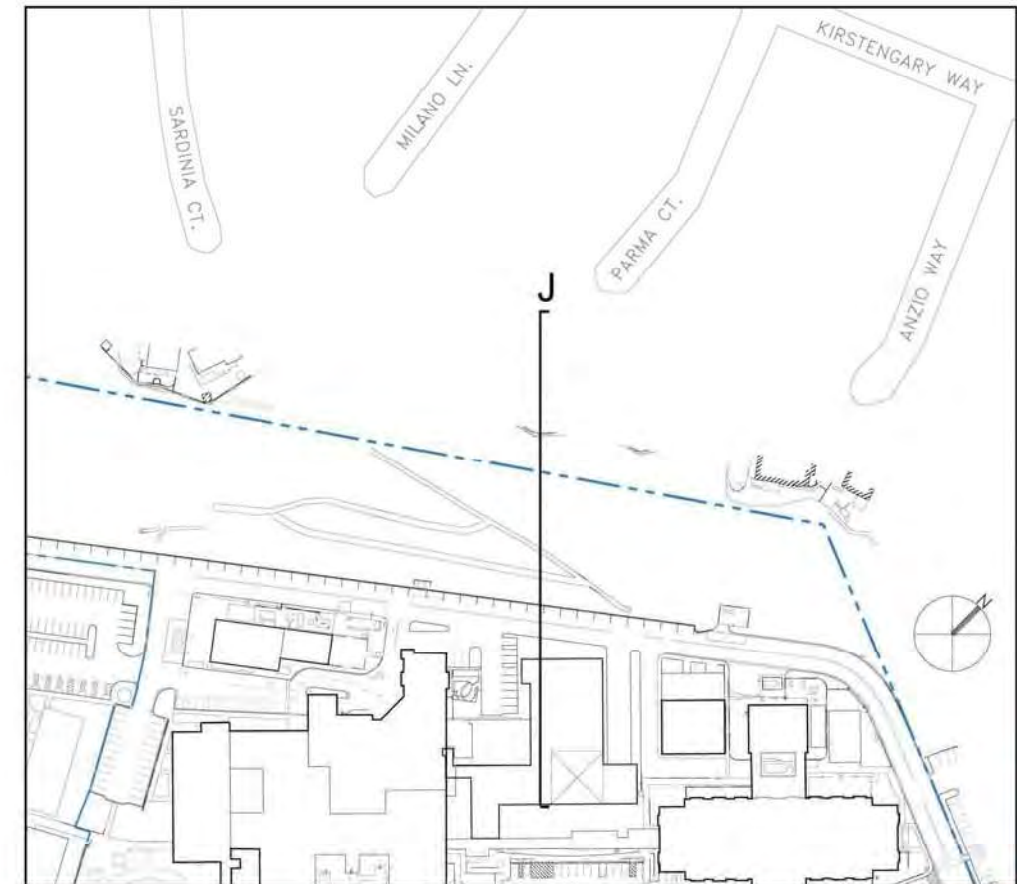


PS2 & PS3 / SECTION - H & I





SECTION - J



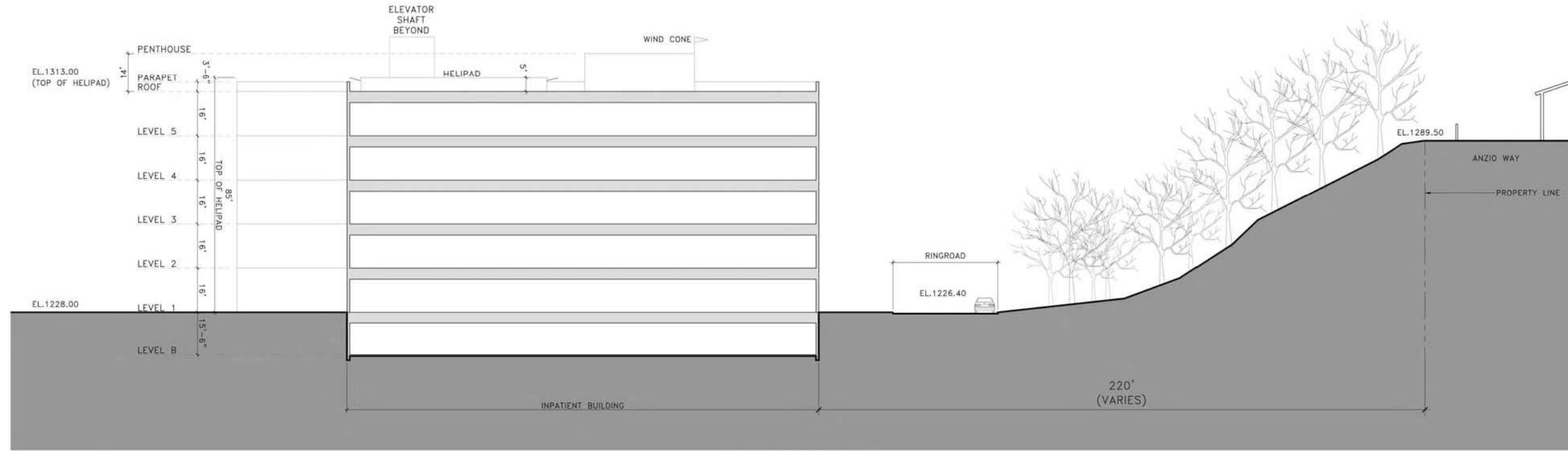
INPATIENT BUILDING / SECTION - J

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway, Santa Clarita, CA 91355

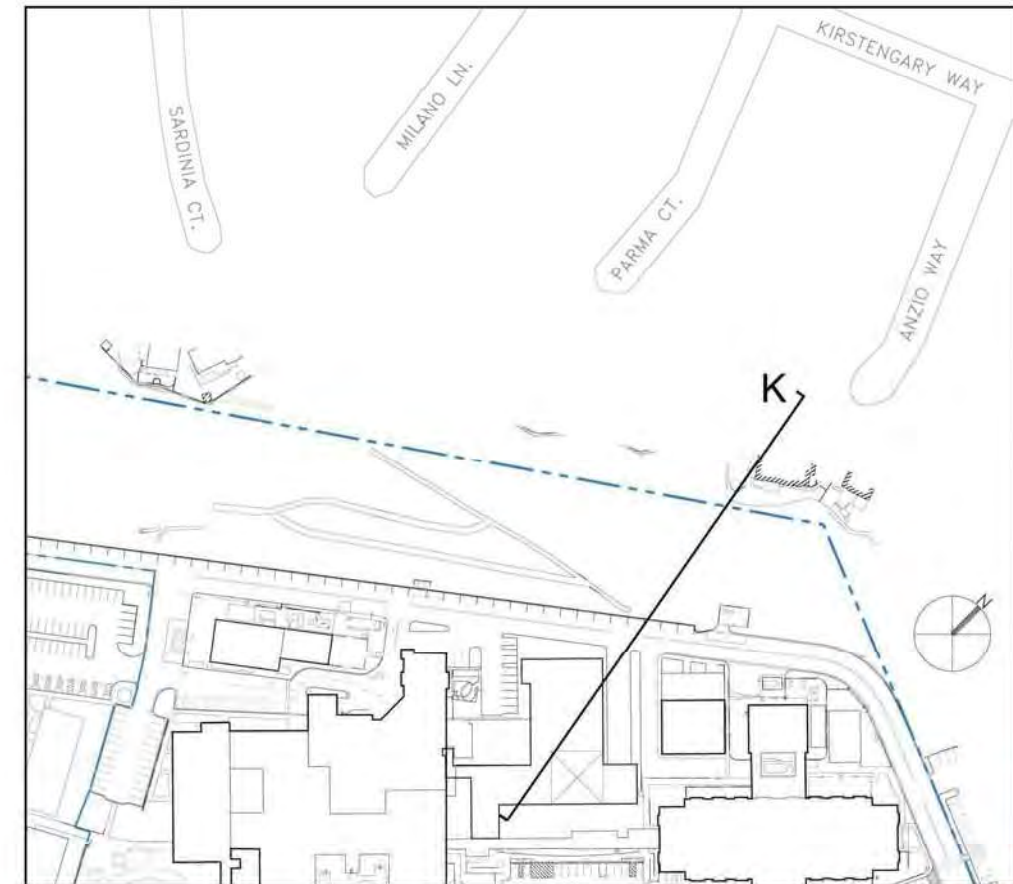
SWA # 060307 NOVEMBER 19, 2008 ARCHITECTS



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SECTION - K



INPATIENT BUILDING / SECTION - K

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23845 McBean Parkway, Santa Clarita, CA 91355

SWA # 060307 NOVEMBER 19, 2008



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pasadena, ca 91103  
wp 626 751 9878 - fx 626 791 5911



## TREE SUMMARY FOR THE FRONTAGE ALONG McBEAN PARKWAY

Date: NOVEMBER 19, 2008

Henry Mayo Newhall Memorial Hospital  
23845 McBean Parkway, Santa Clarita, CA 91355

	<b>Trees Along McBean Parkway - Street Median (From Avenida Navarre to Alegro Dr.) *</b>	<b>Trees Along McBean Parkway – HMNMH Frontage</b>
<b>TOTAL NUMBER OF EXISTING TREES: (Before Construction of The Master Plan)</b>	<b>58</b>	<b>57</b>
<b>TOTAL NUMBER OF EXISTING TREES TO BE REMOVED: (For Construction of The Master Plan)</b>	<b>-12</b>	<b>-34</b>
<b>TOTAL NUMBER OF EXISTING TREES TO REMAIN: (For Construction of The Master Plan)</b>	<b>46</b>	<b>23</b>
<b>TOTAL NUMBER OF NEW TREES: (For Construction of The Master Plan)</b>	<b>7</b>	<b>57</b>
<b>TOTAL NUMBER OF TREES: (After Construction of The Master Plan)</b>	<b>53</b>	<b>80</b>

\* INCLUDES REMOVAL OF 6 MEDIAN TREES LOCATED WEST OF HMNMH CAMPUS

### MASTER PLAN TREE SUMMARY

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway, Santa Clarita, CA 91355

SWA #: 060307 NOVEMBER 19, 2008







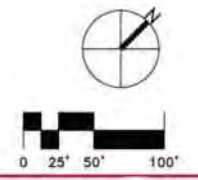
Tree Legend			Tree Legend			Tree Legend		
Symbol	Botanical Name	Common Name	Symbol	Botanical Name	Common Name	Symbol	Botanical Name	Common Name
	Cedrus deodara	Deodar Cedar		Fraxinus uhdei	Shamel Ash		Lagerstroemia indica	Crape Myrtle
	Chorisia Species	Floss Silk Tree		Gleditsia triacanthos	Honeylocust		Liquidambar styraciflua	American Sweet Gum
	Cinnamomum camphora	Camphor Tree		Grevillea robusta	Silky Oak		Platanus racemosa	Sycamore
	Eucalyptus rudis	Flooded Gum		Koelreuteria bipinnata	Chinese Flame Tree		Prunus 'Krauter Vesuvius'	Purple Leaf Plum

Color Legend	
Symbol	Action
	Remain
	Remove

## TREE REMOVAL PLAN (ALONG McBEAN PARKWAY)

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway Santa Clarita, CA 91355

SWA #: 060307 NOVEMBER 19, 2008



IAN DAVIDSON, LANDSCAPE ARCHITECTURE  
8744 TENTH STREET  
RIVERSIDE, CA 92501  
951 581-0282 FAX 951 581-0811  
WWW.IDLANDSCAPEARCHITECTURE.COM  
REGISTRATION NO. 2411

48 east holly street  
pasadena, ca 91103  
tel: 626.783.9855 fax: 626.783.9857





EXISTING TREE PROTECTED IN PLACE

PROPOSED TREE LEGEND



1 CINNAMOMUM CAMPHORA - CAMPHOR TREE



2 RHUS LANCEA - AFRICAN SUMAC



3 LAGERSTROEMIA INDICA - CRAPE MYRTLE



4 PLATANUS RACEMOSA - SYCAMORE



5 PRUNUS C 'KRAUTER VES' MULTI TRUNK - PURPLE LEAF PLUM

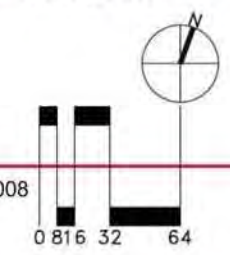


6 KOELREUTERIA BIPINNATA - CHINESE FLAME TREE

CONCEPTUAL LANDSCAPE PLAN - MOB 2 AND MOB F (ALONG McBEAN PARKWAY)

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway Santa Clarita, CA 91355

SWA #: 060307 NOVEMBER 19, 2008

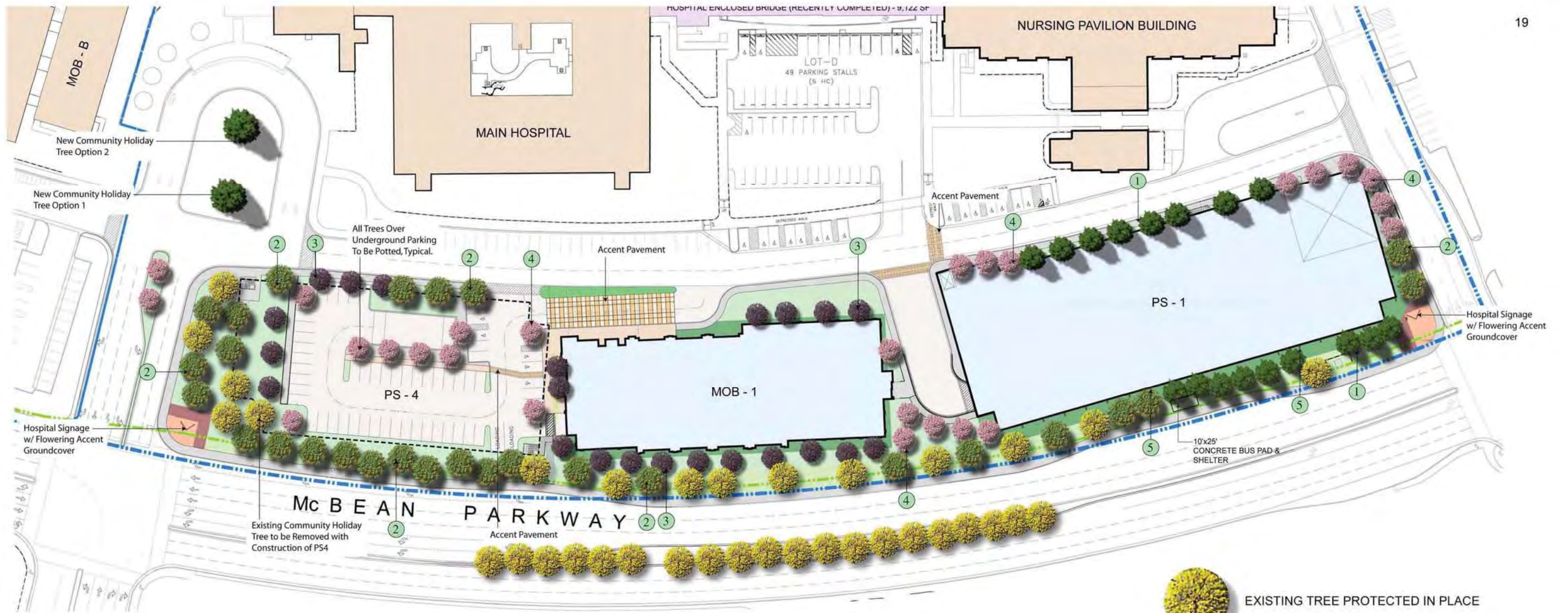


IAN DAVIDSON, LANDSCAPE ARCHITECTURE  
5847 TRENCH STREET  
KINGSTIDE, CA 92501  
PH: 951.825.4444 FAX: 951.825.4402  
E: IAN.DAVIDSON@LDLA.COM  
WWW.LDARCHITECTURE.COM



48 east holly street  
pasoadena, ca 91103  
tel: 805.793.8825 fax: 805.793.9877





PROPOSED TREE LEGEND



1 PINUS CANARIENSIS - CANARY ISLAND PINE



2 PLATANUS RACEMOSA - SYCAMORE



3 PRUNUS C 'KRAUTER VES' MULTI TRUNK - PURPLE LEAF PLUM



4 LAGERSTROEMIA INDICA - CRAPE MYRTLE

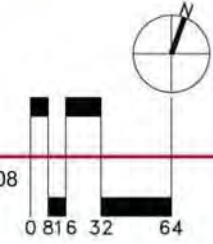


5 FICUS PUMILA VINE - CREEPING FIG

CONCEPTUAL LANDSCAPE PLAN - MOB 1 AND PS1 (ALONG McBEAN PARKWAY)

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway Santa Clarita, CA 91355

SWA #: 060307 NOVEMBER 19, 2008



**ID**  
LANDSCAPE ARCHITECTURE  
**LA**

IAN DAVIDSON, LANDSCAPE ARCHITECTURE  
304 Tenth Street  
Evanston, CA 94920

SWA # 060307 NOV 19 2008  
I.D.A. IAN DAVIDSON  
I.D.A. IAN DAVIDSON  
REGISTRATION NO. 2001



48 east holly street  
pasadena, ca 91103  
tel 626.793.9800 fax 626.793.9807







PROPOSED TREE LEGEND



1 CINNAMOMUM CAMPHORA - CAMPHOR TREE



2 RHUS LANCEA - AFRICAN SUMAC



3 PINUS ELДАРICA - AFGHAN PINE

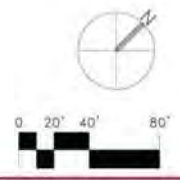


EXISTING TREE PROTECTED IN PLACE

CONCEPTUAL LANDSCAPE PLAN (ALONG HILLSIDE AND WESTERN BOUNDARY)

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway, Santa Clarita, CA 91355

SWA #: 060307 NOVEMBER 19, 2008



IGN DIVISION, Landscape Architecture  
3244 Tenth Street  
Riverside, CA 92501  
951-483-7283 FAX 951-883-4379  
E-MAIL: ign@ignswa.com  
www.ignswa.com

48 east holly street  
pasadena, ca 91103  
626.793.9905 fax 626.793.9907





SOUTH ELEVATION  
(McBEAN PARKWAY)



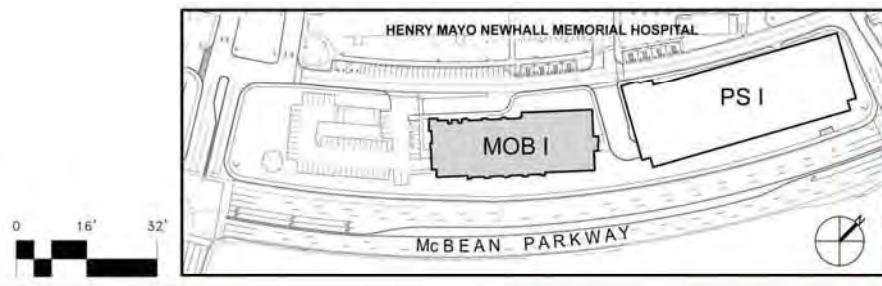
NORTH ELEVATION  
(HOSPITAL)



EAST ELEVATION



WEST ELEVATION



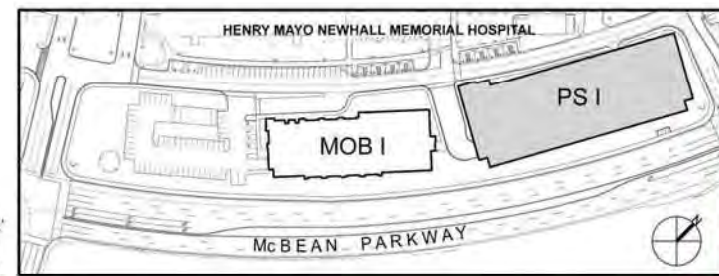
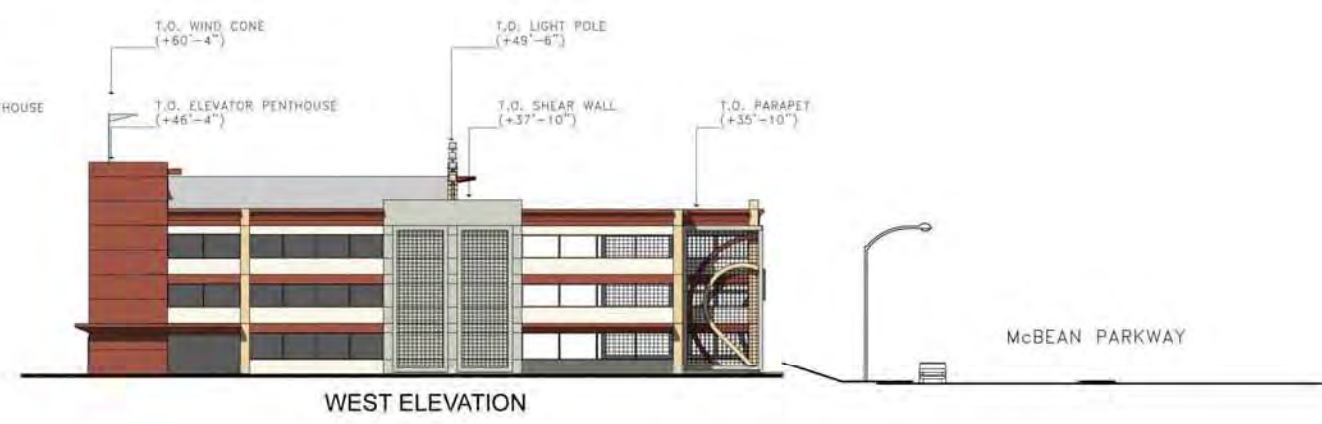
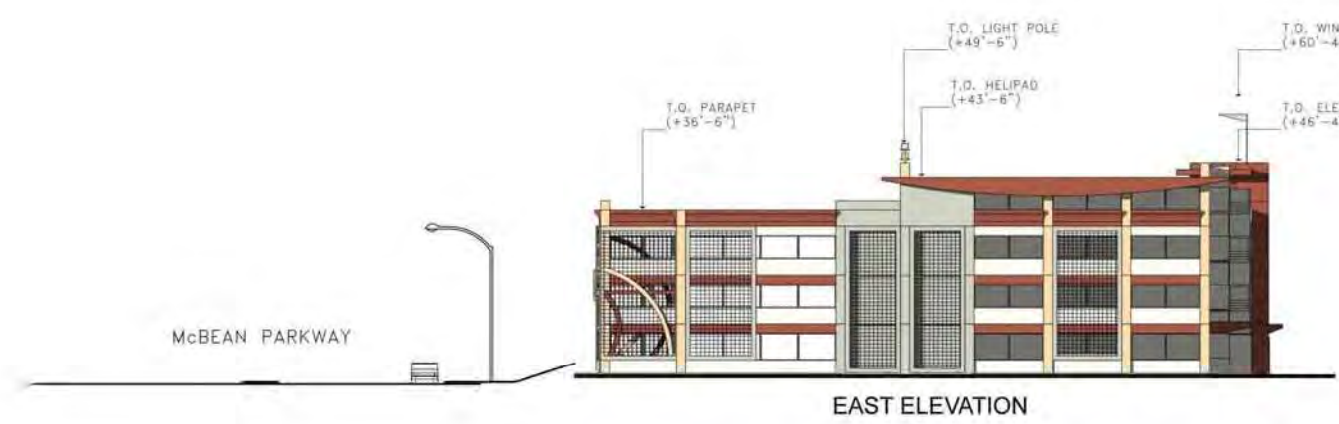
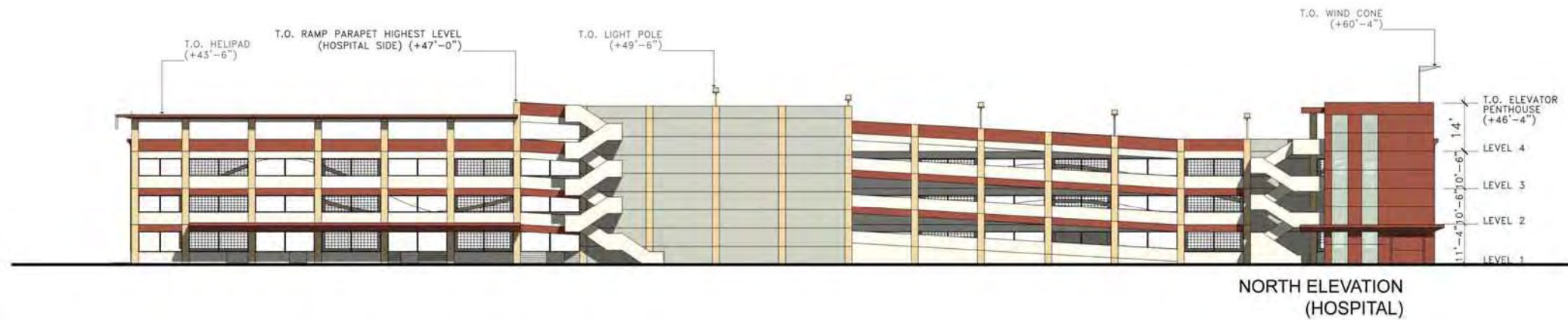
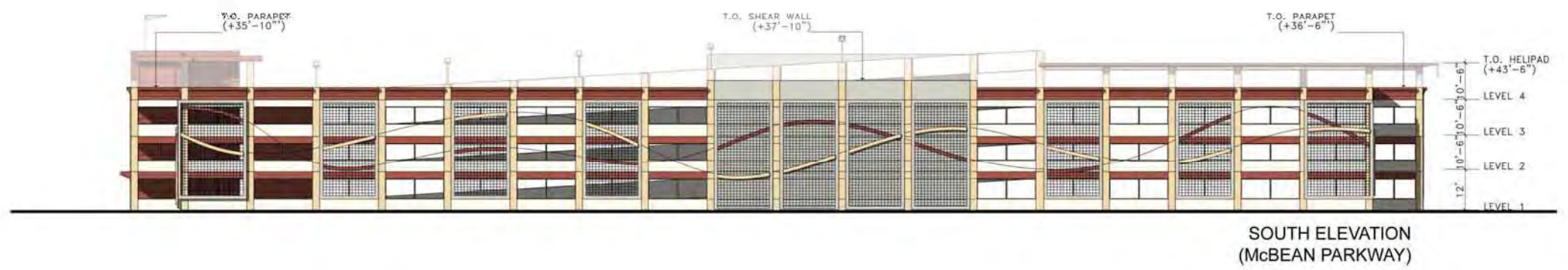
### MOB I: COLOR - BUILDING EXTERIOR ELEVATIONS

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway, Santa Clarita, CA 91355

SWA # 060307 NOVEMBER 19, 2008 ARCHITECTS



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pasadena, ca 91103  
tel: 626.793.9605 • fax: 626.793.9607



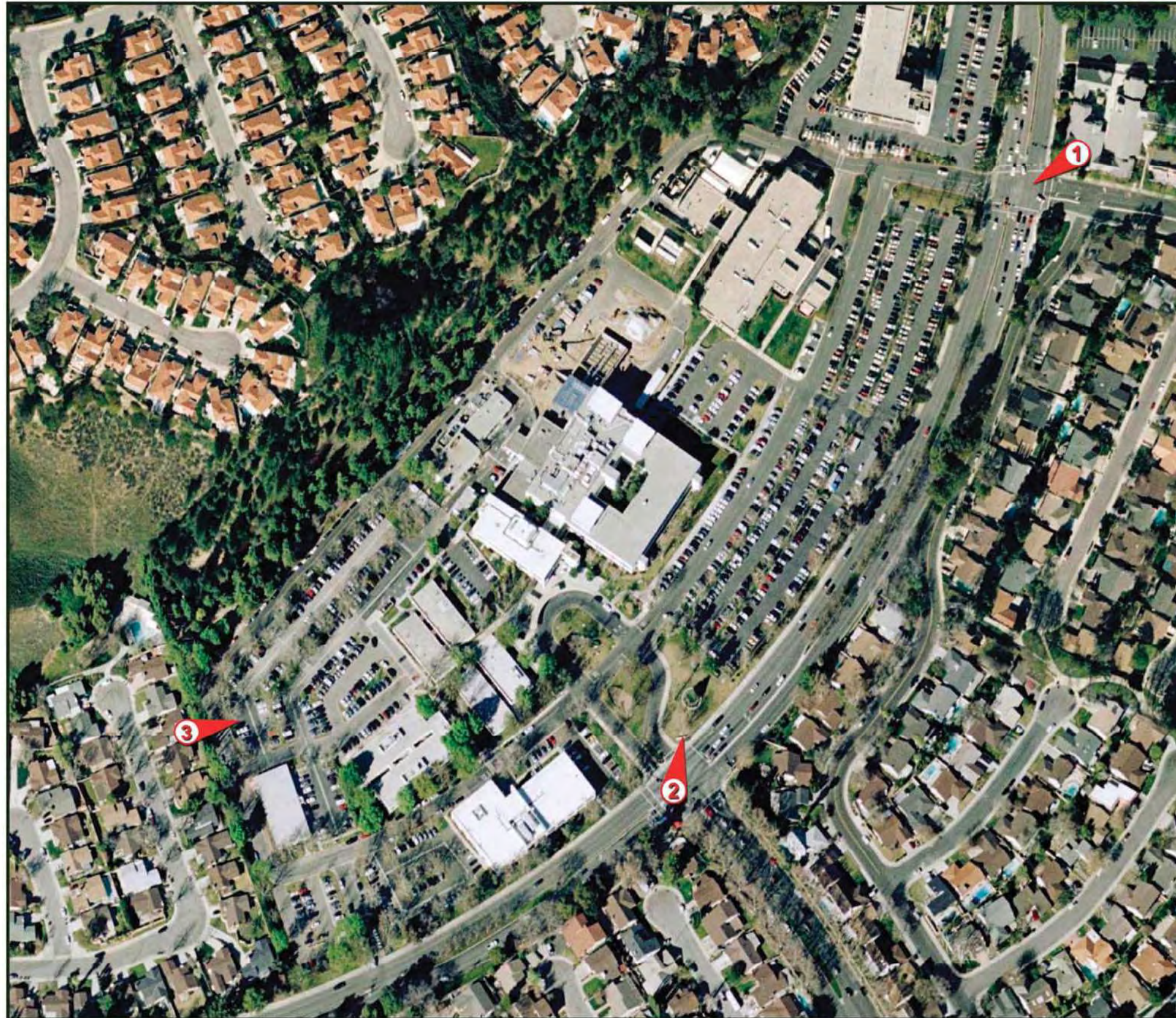
PS1: COLOR - BUILDING EXTERIOR ELEVATIONS

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway, Santa Clara, CA 91355

SWA # 060307 NOVEMBER 19, 2008 ARCHITECTS



48 east holly street  
pasadena, ca 91103  
tel: 626.793.9505 • fax: 626.793.9507



**VISUAL SIMULATION - KEY PLAN**

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway, Santa Clarita, CA 91355

SWA #: 060307 NOVEMBER 19, 2008



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pasadena, ca 91103  
tel: 626.793.9805 • fax: 626.793.9807





EXISTING VIEW



PROPOSED VIEW



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**VISUAL SIMULATION - VIEW 1 (WITH NEW AND EXISTING TREES AT THE TIME OF CONSTRUCTION COMPLETION FOR MOB 1 AND PS 1)**

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway, Santa Clarita, CA 91355

SWA #: 060307 NOVEMBER 19, 2008



EXISTING VIEW



PROPOSED VIEW

**VISUAL SIMULATION - VIEW 1 (LANDSCAPE: 15 YEAR GROWTH)**

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway, Santa Clara, CA 91355

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EXISTING VIEW



PROPOSED VIEW

**VISUAL SIMULATION - VIEW 1 (LANDSCAPE: 25 YEAR GROWTH)**

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway, Santa Clarita, CA 91355

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EXISTING VIEW



PROPOSED VIEW

**VISUAL SIMULATION - VIEW 2 (WITH NEW AND EXISTING TREES AT THE TIME OF CONSTRUCTION COMPLETION FOR THE INPATIENT BUILDING)**

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway, Santa Clarita, CA 91355

SWA #: 060307 NOVEMBER 19, 2008





EXISTING VIEW



PROPOSED VIEW

**VISUAL SIMULATION - VIEW 2 (LANDSCAPE: 15 YEAR GROWTH)**

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway, Santa Clarita, CA 91355

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EXISTING VIEW



PROPOSED VIEW

**VISUAL SIMULATION - VIEW 2 (LANDSCAPE: 25 YEAR GROWTH)**

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway, Santa Clarita, CA 91355

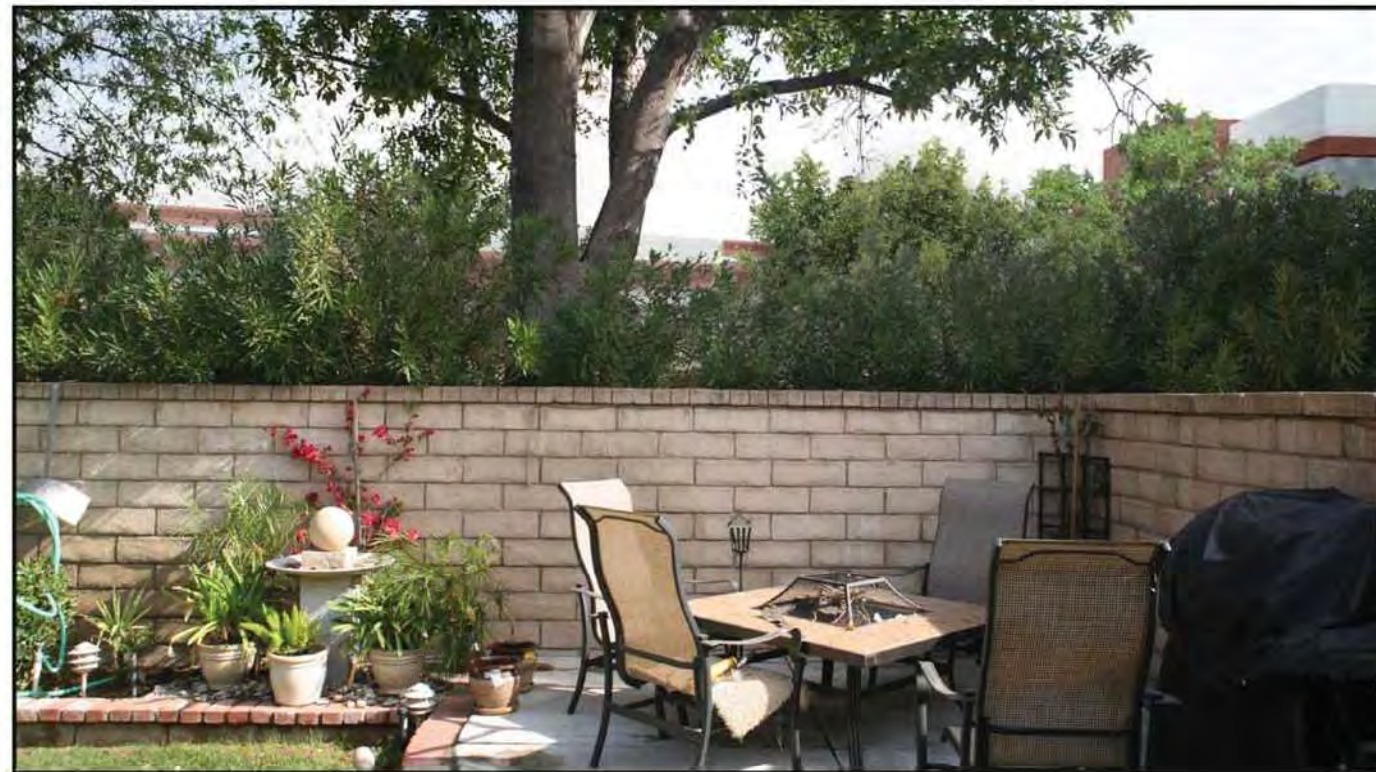
SWA #: 060307 NOVEMBER 19, 2008



48 east holly street  
pasadena, ca 91103  
tel: 626.793.9805 - fax: 626.793.9807



EXISTING VIEW



PROPOSED VIEW

**VISUAL SIMULATION - VIEW 3 (WITH NEW AND EXISTING TREES AT THE TIME OF CONSTRUCTION COMPLETION FOR MOB 3)**

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway, Santa Clarita, CA 91355

SWA #: 060307 NOVEMBER 19, 2008



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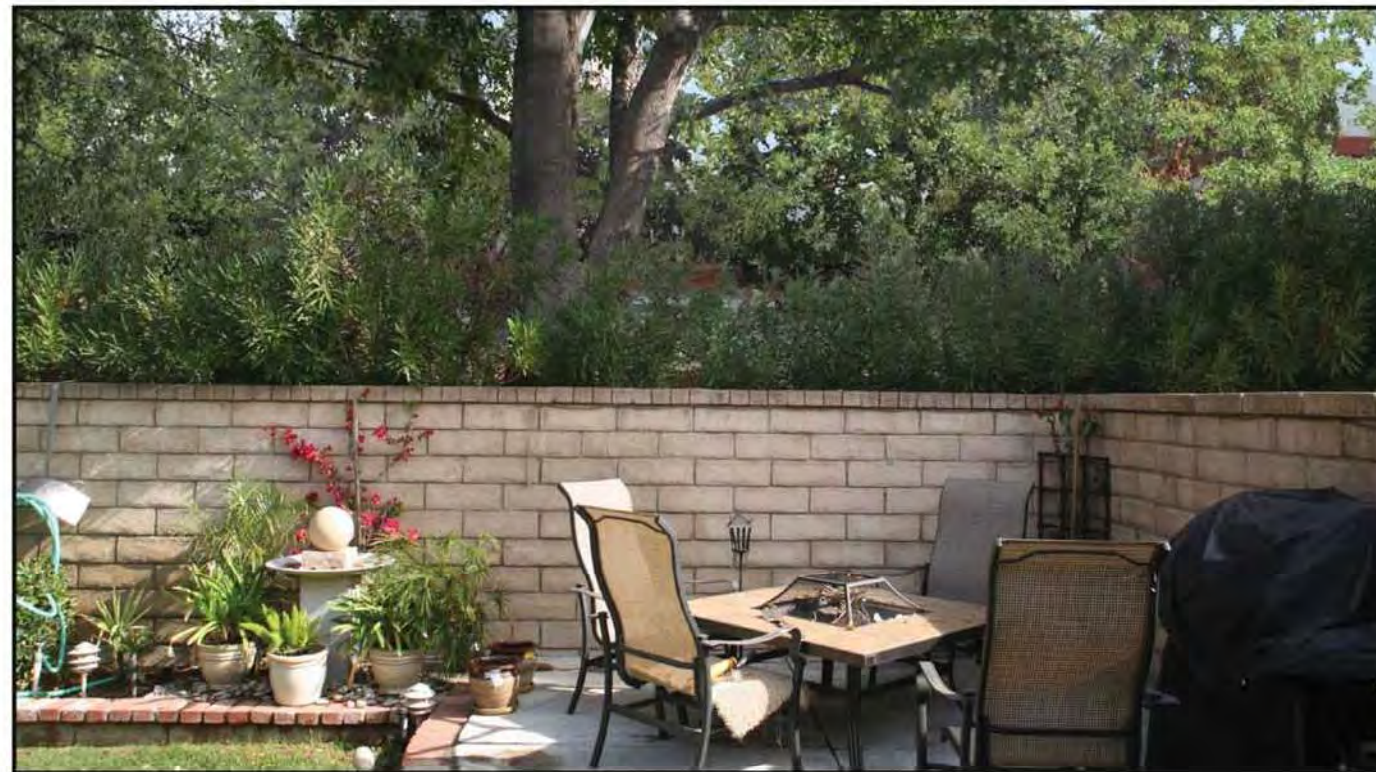


48 east holly street  
pasadena, ca 91103  
tel: 626.793.9805 • fax: 626.793.9807





EXISTING VIEW



PROPOSED VIEW

**VISUAL SIMULATION - VIEW 3 (LANDSCAPE: 15 YEAR GROWTH)**

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway, Santa Clarita, CA 91355

SWA #: 060307 NOVEMBER 19, 2008



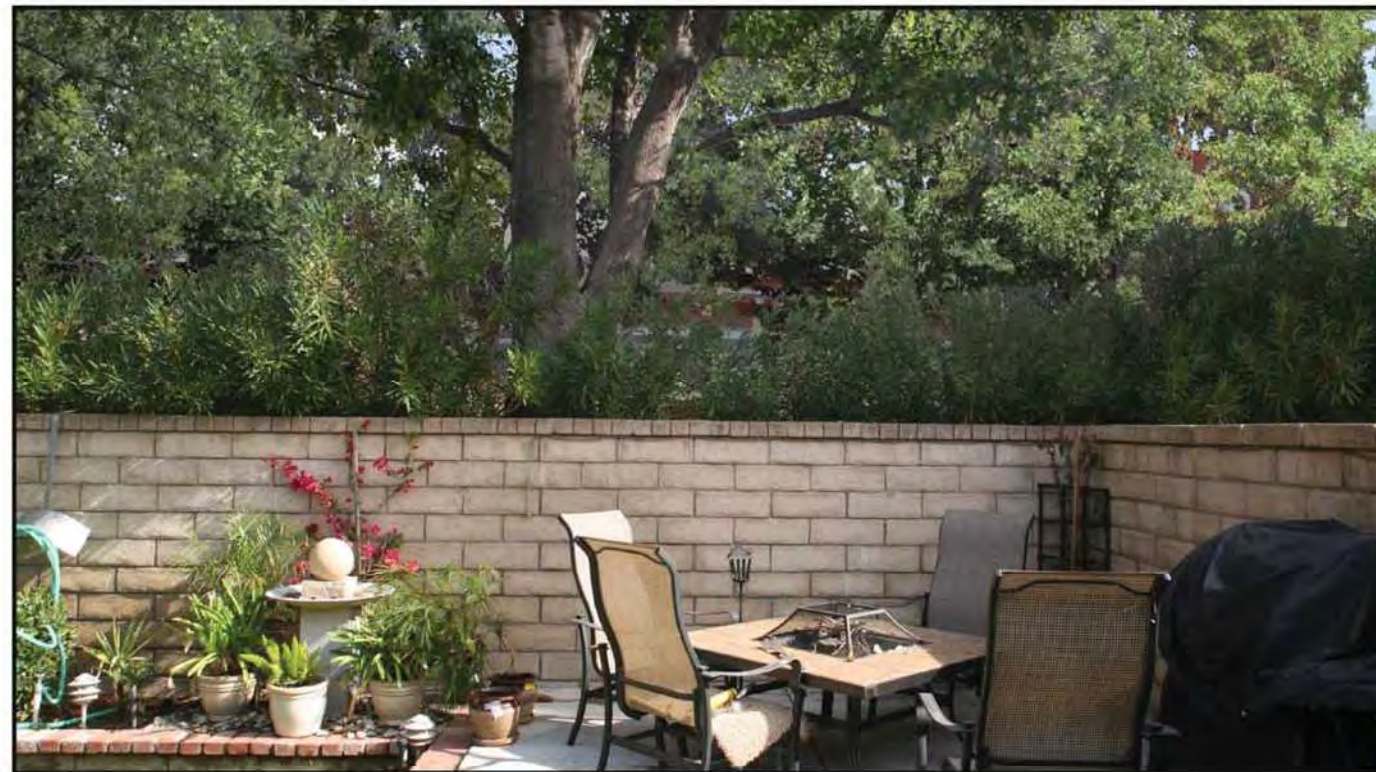
48 east holly street  
pasadena, ca 91103  
tel: 626.793.9605 • fax: 626.793.9607







EXISTING VIEW



PROPOSED VIEW

**VISUAL SIMULATION - VIEW 3 (LANDSCAPE: 25 YEAR GROWTH)**

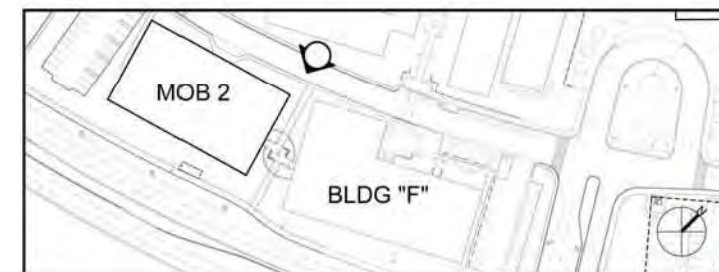
HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway, Santa Clarita, CA 91355

SWA #: 060307 NOVEMBER 19, 2008



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pasadena, ca 91103  
tel: 626.793.9605 - fax: 626.793.9607





**LANDSCAPE TRELLIS (AT MOB 2 AND BUILDING F): PERSPECTIVE**

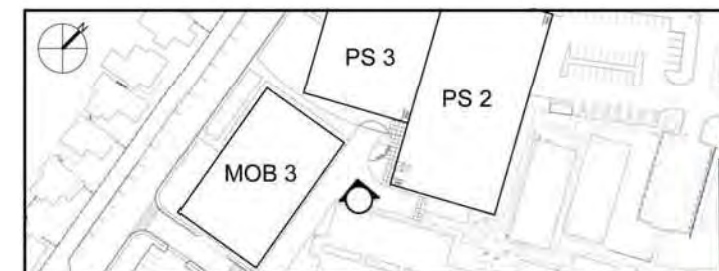
HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
 23845 McBean Parkway, Santa Clarita, CA 91355

SWA # 060307 NOVEMBER 19, 2008 ARCHITECTS



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 pasadena, ca 91103  
 tel 626.793.9805 fax 626.793.9607





**PEDESTRIAN PLAZA (AT MOB 3, PS 2 AND PS 3): PERSPECTIVE**

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
 23845 McBean Parkway, Santa Clarita, CA 91355

SWA # 060307 NOVEMBER 19, 2008 ARCHITECTS



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 pasadena, ca 91103  
 tel: 626.793.9805 • fax: 626.793.9807



FIRST AMENDMENT  
TO HENRY MAYO NEWHALL MEMORIAL HOSPITAL MASTER PLAN

MASTER PLAN 04-022

DATED: \_\_\_\_\_, 2016

---

Reference is made to the Henry Mayo Newhall Memorial Hospital Master Plan (Master Plan 04-022), approved by the City Council of the City of Santa Clarita, California, by the adoption of Resolution No. 08-104 on November 20, 2008 (the "Original Master Plan"). Capitalized terms not defined herein shall have the meanings ascribed to them in the Original Master Plan.

This First Amendment to the Henry Mayo Newhall Memorial Hospital Master Plan (the "First Amendment") shall modify the Original Master Plan as follows:

1. By deleting the Project Description in Exhibit "A" to the Original Master Plan and substituting the Project Description that is attached as Exhibit "A" to this First Amendment in its place.

2. By deleting the Master Plan map in Exhibit "A" to the Original Master Plan and substituting the Master Plan map that is attached as Exhibit "B" to this First Amendment in its place.

3. By amending the descriptions of the permitted project square footage, Inpatient Building and Central Plant that are listed on Pages 2 and 3 of 28 of the Conditions of Approval attached as Exhibit "B" to the Original Master Plan (the "Conditions of Approval") to read in full as follows:

A. Condition PL1, paragraph 2:

“The project is permitted to increase the existing square footage of the hospital campus from 340,071 square feet to 698,000 square feet, a 357,929 net square-foot increase.”

B. Condition PL1,1:

"1. INPATIENT BUILDING: A 162,329 square-foot, 142-bed Inpatient Building located in the central portion of the campus. This building will be 85 feet in height to the top of the parapet and 100 feet in height to the top of the wind sock and elevator shaft. This building will also include a rooftop helipad;"

C. Condition PL1.5:

"5. CENTRAL PLANT: A 3,600 square-foot Central Plant building measuring approximately 19 feet 4 inches in height to provide support services to the proposed Inpatient Building."

4. By amending Condition EN30 of the Conditions of Approval to read in full as follows:

5. "EN30. Prior to issuance of each building penult, the applicant shall pay the applicable Bridge and Thoroughfare (B&T) District Fee to implement the Circulation Element of the General Plan as a means of mitigating the traffic impact of this project.

This project is located in the Valencia B&T District. The B&T rate is subject to change and is based on the rate at the time of payment. The total square footage of the proposed buildings as provided by the applicant is as follows:

MOB 1 = 80,000 SF  
MOB 2 = 60,000 SF  
MOB 3 = 60,000 SF  
Inpatient Building = 162,329 SF  
Central Plant = 3,600 SF  
Total = 363,600 SF

The B&T Fee shall be calculated as follows: = Proposed building SF x 5 x District Fee 43,560



---

**EXHIBIT "A"**  
**Project Description**

---

The Project Description is on the following page

## **Project Description**

The approved Master Plan buildings include an “Inpatient Building”, projected to have 120 beds. The Master Plan provides that the Inpatient Building would have 125,363 square feet, and a building height of 85 feet to the top of the parapet and 100 feet to the top of the wind sock and elevator shaft. The Master Plan also includes a new “Central Plant”. The Master Plan provides that the Central Plant would have 10,000 square feet and a building height of 26 feet. However, the Hospital has reduced the size of the Central Plant to 3,600 square feet and its height to 19 feet, 4 inches and, as of the date of this Amendment, it is under construction. As of the date of this Amendment, the 8,872 square foot Loading Dock is also under construction.

The proposed project would:

1. Increase the square footage of the Inpatient Building from 125,363 square feet to 162,329 square feet (an increase of 36,966 square feet), which includes 153,457 square feet for the Inpatient Building, and 8,872 square feet for the Loading Dock.
2. Increase the aggregate square footage of the Master Plan from 327,363 square feet to 357,929 square feet (an increase of 30,566 square feet) to reflect the net increase in square footage in the Master Plan from the increase in square footage of the Inpatient Building and the decrease in square footage of the Central Plant.
3. Relocate the approved Helipad on the roof of the Inpatient Building from the southeast corner to the north side of the roof in order to allow direct elevator access from the roof to the emergency room.
4. Modify the existing Site Plan for the Master Plan to reflect: (i) changes in the footprints of the Inpatient Building and the Central Plant; and (ii) the relocation of the helipad to the rear of the roof of the Inpatient Building.

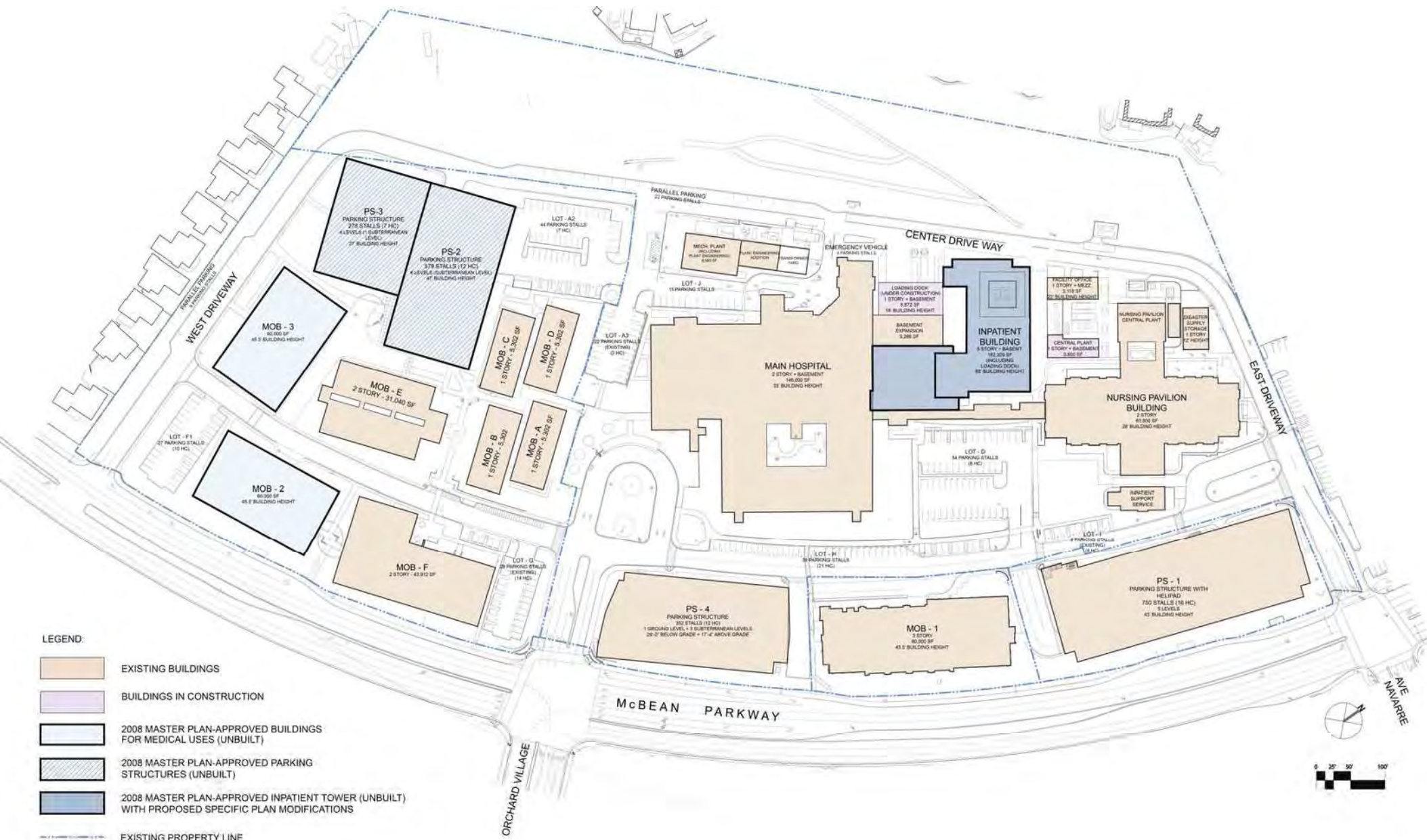
Under the proposed project, there would be no increases in permitted building heights, number of overall campus inpatient beds, programs or employees or square footage allocated to administration uses from the approved Master Plan.

---

EXHIBIT "B"  
Master Plan Map

---

The Master Plan Map is on the following page



**SPECIFIC PLAN**

HENRY MAYO NEWHALL HOSPITAL  
 23845 McBean Parkway, Santa Clara, CA 95135

MARCH 16, 2016

# APPENDIX B

*2020 Second Amendment to the Development Agreement, 2016 First Amendment to the Development Agreement and Development Agreement*



RECORDING REQUESTED BY  
AND WHEN RECORDED MAIL  
TO:

CITY OF SANTA CLARITA  
23920 Valencia Boulevard Suite 300  
Santa Clarita, CA 91355  
Attn: City Clerk

EXEMPT FROM RECORDERS FEES  
Pursuant to Government Code § 27383

---

SPACE ABOVE THIS LINE FOR RECORDER'S USE ONLY

SECOND AMENDMENT TO  
DEVELOPMENT AGREEMENT

by and among

THE CITY OF SANTA CLARITA  
a municipal corporation

and

HENRY MAYO NEWHALL MEMORIAL HOSPITAL,  
a California non-profit public benefit

corporation and

HCN G&L VALENCIA SUB, LLC,  
a Delaware limited liability company

THIS SECOND AMENDMENT SHALL BE RECORDED WITHIN TEN  
DAYS OF EXECUTION BY ALL PARTIES HERETO PURSUANT TO  
THE REQUIREMENTS OF GOVERNMENT CODE §65868.5

SECOND AMENDMENT TO  
DEVELOPMENT AGREEMENT

This Second Amendment to Development Agreement (the "Second Amendment") is made this day of \_\_\_\_\_, 2021 (the "Reference Date"), by and among the CITY OF SANTA CLARITA, a municipal corporation, organized and existing under the general laws of the State of California (the "City") and HENRY MAYO NEWHALL HOSPITAL, a California non-profit public benefit corporation, ("HMNH") and HCN G&L VALENCIA SUB, LLC, a Delaware limited liability company ("HCN Valencia"). HMNH and HCN Valencia are hereinafter sometimes collectively referred to as the "Developer." City and Developer are hereinafter sometimes collectively referred to as the "Parties" and each may be referred to as a "Party." The Parties enter into this First Amendment with reference to the following facts:

RECITALS

- A. The City, HMNH and HCN Valencia are parties to that certain Development Agreement, dated as of December 9, 2008 with respect to the future development of the Campus Property (the "Development Agreement"). The Development Agreement was recorded in the Official Records of Los Angeles County (the "Official Records") on February 2, 2009 as Document No. 09-135039.
- B. Pursuant to the following agreements, to which the City and HMNH consented in writing, all rights, title and interest of G&L in the Development Agreement with respect to the G&L Property have been transferred and assigned to HCN Valencia and all of terms, conditions, covenants and obligations required to be kept, performed and fulfilled by G&L under the Development Agreement with respect to the G&L Property have been assumed by HCN Valencia:
- (1) That certain Assignment and Assumption Agreement (Development Agreement) between G&L, as assignor, and Health Care REIT, Inc., a Delaware corporation ("HCN"), as assignee, dated as of September 24, 2014, which was recorded in the Official Records on September 26, 2014 as Document No. 20141017733; and
- (2) That certain Assignment and Assumption Agreement (Development Agreement) between HCN, as assignor, and HCN Valencia, as assignee, dated as of September 24, 2014, which was recorded in the Official Records on September 26, 2014 as Document No. 20141024132.
- C. Pursuant to Section 4.1 of the Development Agreement, during the Term of the Development Agreement, Developer was granted the right to develop on the Campus Property an aggregate of approximately 327,363 net new square feet of additional inpatient, outpatient, medical office and associated medical facilities and a new central plant building, as well as the provision of adequate parking facilities (the "Project") in accordance with: (i) the terms and conditions of the Project Approvals (as defined in the Development Agreement) and the Development Agreement; and (ii) the City Requirements (as defined in the Development Agreement) in effect on the Effective Date (as defined in the Development Agreement) of the Development Agreement. The Project Approvals include, without limitation, a Master Plan and the Conditions of Approval thereto for the Campus Property



(the "Master Plan"), which was approved by the City Council of the City by the adoption of Resolution No. 08-104 on November 20, 2008.

- D. In connection with the approval by the City Council of the Project Approvals for the Project and the adoption of the resolution approving the Development Agreement, the City Council of the City certified Environmental Impact Report SCH#2004111149 for the Master Plan and the Development Agreement on November 19, 2008 (the "EIR") pursuant to the requirements of the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA").
- E. In 2016, HMNH submitted applications to the City (collectively, the "Project Applications"), with the consent of HCN Valencia, to (i) amend the General Plan to change the Campus Property's land use designation from Public Institutional (PI) to Specific Plan (SP), (ii) amend the Santa Clarita Zoning Code to change the Campus Property's zone from PI to SP, (iii) adopt a Specific Plan, (iv) amend the Master Plan, and (v) amend the Development Agreement (the "First Amendment"), for the following purposes:
1. Increase the square footage of the Inpatient Building from 125,363 square feet to 162,329 square feet (an increase of 36,966 square feet);
  2. Modify the aggregate square footage increase of the Project from 327,363 square feet to 357,929 square feet (a net increase of 30,566 square feet) to reflect the net increase in square footage in the Project that would result from the increase in the square footage of the Inpatient Building and the decrease in the square footage of the Central Plant; and,
  3. Relocate the Helipad from the front to the rear of the roof of the Inpatient Building.
- F. As of the Reference Date of the First Amendment: (i) the Project Building (as defined in the Development Agreement) that is identified as "MOB1" in the Development Agreement had been developed and was fully occupied; (ii) the Parking Structures (as defined in the Development Agreement) that are identified as "PS1" and "PS4" had been developed and were open for use; (iii) the Project Building identified as the "Central Plant" at 3,600 square feet, representing a 6,400 square foot reduction from that approved in the Master Plan, had been completed with a building height of 19 feet and 4 inches reduced from 26 feet; (iv) a 8,872 square foot loading dock with an approximate height of 16 feet above ground was under construction.
- G. The Project Applications and the First Amendment were approved by the City Council upon the enactment of Ordinance No. 16-10 on June 28, 2016.
- H. The construction of the increased area of the Inpatient Building No. 1 described in Recital E has been completed and the Helipad described in Recital E(3) has been relocated.
- I. In 2017, HMNH submitted applications to the City (collectively, the "Project Applications Second Amendment"), with the consent of HCN Valencia, to amend the Master Plan, the Specific Plan and the Development Agreement for the following purposes:

1. To have the 2019 Amended HMNH Specific Plan replace the Specific Plan adopted in 2016 in its entirety. The 2019 Specific Plan does not change the maximum number of beds (368) permitted under the 2016 Specific Plan and the 2008 Master Plan;
2. To add 200,000 square feet of hospital building area consisting of the 115,700 square foot Inpatient Building No. 2 and the accessory 84,300 square foot Diagnostic and Treatment (D&T) Building;
3. To add up to 292 parking spaces to PS-4 through the construction of up to three new levels to the existing structure;
4. To construct a second central plant within the basement and on the roof of the D&T building;
5. To modify the parking plan to permit both paid and assigned parking and adopt the HMNH Parking Demand Study (October 2019) establishing the parking requirements for the campus; and
6. To extend the Term of the Development Agreement by 10 years.

- J. The Project Applications Second Amendment shall, upon their approval by the City Council, become part of the Project Approvals and included in the Applicable Rules.
- K. Pursuant to Sections 4.2.2, 4.2.3 and 4.2.5 the Development Agreement, any increase in the maximum height, any amendment requiring a subsequent or supplemental EIR and any increase in the overall square footage of the Project requires an amendment to the Development Agreement which must be reviewed by the Planning Commission of the City and approved by the City Council of the City.
- L. A supplemental environmental impact report (the “SEIR”) analyzing the impacts of the Project Applications Second Amendment has been prepared.
- M. The Planning Commission of the City and the City Council of the City have given notice of their intention to consider the Project Applications Second Amendment and the SEIR, have conducted public hearings thereon pursuant to the California Government Code and the Santa Clarita Municipal Code, and the City Council has found that the Project Applications Second Amendment and the Project (as revised by the Project Applications Second Amendment are: (i) consistent with the General Plan, adopted plans, codes, ordinances and policies of the City; (ii) consistent with all other ordinances, resolutions, rules, regulations, laws, plans and policies applicable to the Project; and (iii) in the best interest of the health, safety and general welfare of the City, its residents, and the general public.
- N. On \_\_\_\_\_, 2020, after conducting a public hearing and after considering all evidence, both oral and written, submitted for the record, all appropriate documentation and circumstances, and making all required findings, the Planning Commission of the City adopted resolutions recommending that the City Council: (a) certify the SEIR as having been prepared in full compliance with the California Environmental Quality Act (“CEQA”); and (b) approve the Project Applications Second Amendment.

O. On \_\_\_\_\_, 2021, after conducting a public hearing and after consideration of the Planning Commission's recommendation and all evidence, both written and oral, submitted for the record, all appropriate documentation and circumstances, and making all required findings, the City Council of the City adopted the following measures (collectively, the "Project Amendments"):

1. Resolution No. 21-\_\_\_\_\_ (adopted \_\_\_\_\_ 2021), certifying that the SEIR has been prepared in full compliance with CEQA.
2. Resolution No. 21-\_\_\_\_\_ (adopted \_\_\_\_\_ 2021), approving the 2019 Specific Plan Amendment.
3. Ordinance No. 21-\_\_\_\_\_ (adopted \_\_\_\_\_ 2021), adopting the Second Amendment to the Development Agreement.
4. Resolution No. 21-\_\_\_\_\_ (adopted \_\_\_\_\_ 2021), approving the Second Amendment to the Master Plan.

NOW, THEREFORE, with reference to the above Recitals, and in consideration of the mutual covenants and agreements contained in this Second Amendment, the City and the Developer agree as follows:

### AGREEMENT

1. Recitals. Each and all the foregoing recitals of background facts are incorporated herein by this reference as though set forth herein verbatim.
2. Definition of Terms Used Herein. The capitalized terms in this Second Amendment shall have the meanings set forth in the Original Development Agreement unless otherwise expressly indicated herein. The term "Original Development Agreement" means the Development Agreement approved in 2008. Where the context requires it, a reference to "this Agreement" in the Original Development Agreement shall include the First and Second Amendments to the Original Development Agreement
3. Second Amendment to the Original Development Agreement. The Original Development Agreement is hereby amended as follows:
  - A. Section 4.7.1 of the original Development Agreement is amended to read as follows: **4.7.1 Developer shall not be required to construct the Project Improvements in any particular order or pursuant to any particular schedule, provided, however, that the following prerequisites to the Project Improvements as described in the table below are met:**

<b>Prior To:</b>	<b>Developer Must:</b>
Issuance of Building Permit for either MOB 2, MOB 3, Inpatient Building No. 2 or the D&T Building	None required
Issuance of Certificate of Occupancy for either MOB 2, MOB 3, Inpatient Building No. 2 or the D&T Building	<p>Provide required parking for each phase of the development pursuant to the Parking Demand Study including any updates.</p> <p>Complete the traffic mitigation measures identified on Exhibit F.</p> <p>Complete the Realignment Improvements shown in Exhibit L.</p> <p>Issuance of a Certificate of Occupancy for MOB 2 shall be conditioned on Developer providing written documentation to the City that 20% of MOB2's leasable space has been leased to HMMH for Centers for Excellence or other hospital-related uses.</p>

- B. Section 4.7.2 of the Original Development Agreement is amended to read as follows: **4.7.2 For the purposes of Section 4.7.2 “City Required Parking” means the number of parking spaces required by the HMNH Parking Demand Study (October 2019) or as updated with each phase of development.**
- C. Section 5.6 of the Original Development Agreement is deleted in its entirety. **All requirements of Section 5.6 of the Original Development Agreements have been satisfied as of the date of this second amendment.**
- D. Section 5.7 of the Original Development Agreement is amended to add the following: **The Developer may implement gated, paid, and assigned parking on the Campus Property at the Developer’s sole discretion consistent with the requirements of the HMNH Parking Demand Study (October 2019) or as updated. This requirement shall supersede any previous conditions of approval or requirements imposed by the City.**
- E. Section 5.8 of the Original Development Agreement shall be amended as follows: **All requirements of Section 5.8 of the Original Development**

**Agreement have been satisfied as of the date of this Second Amendment.**

- F. Section 5.9 of the Original Development Agreement shall be amended as follows: **All requirements of Section 5.9 of the Original Development Agreement have been satisfied as of the date of this Second Amendment.**
- G. Section 6 of the Original Development Agreement is amended to read as follows: **The Developer may implement new gating and paid parking consistent with the requirements of the HMNH Parking Demand Study, including any updates. Locations for said gating shall be submitted to the City as part of a Development Review application.**
- H. Section 7.3.1 of the Original Development Agreement is deleted in its entirety. **All requirements of Section 7.3.1 of the Original Development Agreements have been satisfied as of the date of this second amendment.**
- I. Section 7.3.2 of the Original Development Agreement is amended to read as follows: **In addition to the required Traffic Mitigation Improvements listed on Exhibit “F” to this Agreement, Developer, or the City at Developer’s expense, shall also complete the construction of street improvements to McBean Parkway pursuant to the approved street design plans attached hereto as Exhibit “L”.**
- J. The first sentence of Section 9 of the Original Development Agreement is amended to read as follows: **Term of Agreement. This agreement shall be binding as and when the ordinance approving this Agreement has been approved by the City Council and the Agreement has been executed by the City and the Developer, and shall remain in effect until the twenty-fifth (25<sup>th</sup>) anniversary of the Effective Date (the “Term”).**
- K. Portions of Section 18 are amended to delete the information on the Hospital’s attorneys and to G&L and its attorneys and to replace as follows:

With a copy to:

Cox, Castle & Nicholson LLP  
2029 Century Park East,  
21<sup>st</sup> Floor  
Los Angeles, CA 90067-3284  
Attn: Kenneth B. Bley, Esq.

If to HCN Valencia:

With a copy to:

HCN G&L Valencia Sub, LLC  
c/o Welltower Inc.  
4500 Door Street  
Toledo, OH 43615-4040  
Attn: Charles W. Hiller, Esq.

Shumaker, Loop & Kendrick LLP  
1000 Jackson Street  
Toledo, OH 43604  
Attn: Joseph A. Rideout, Esq.

- L. Exhibits "J" and "K" to the Original Development Agreement are deleted in their entireties.
  - M. Exhibits "C", "D", "E", "F", "G", and "L" to the Original Agreement are replaced by Exhibits "C", "D", "E", "F", "G", and "L" attached to this Second Amendment.
4. Counterparts. This Second Amendment may be executed in three or more identical counterparts, each of which shall be deemed to be an original and each of which shall be deemed to be one and the same instrument when each Party signs each such counterpart.
5. Full Force and Effect. Except to the extent specifically amended in this Second Amendment, all other terms, provisions and conditions of the Original Development Agreement and First Amendment shall remain unmodified and in full force and effect. In the event of any conflict between the provisions of the Original Development Agreement, the First Amendment and the provisions of this Second Amendment, the provisions of the Second Amendment shall control.

IN WITNESS WHEREOF, the Parties hereto have duly executed this Second Amendment.

"CITY"  
CITY OF SANTA CLARITA,  
a municipal corporation

Dated: \_\_\_\_\_, 2021

By: \_\_\_\_\_  
City Manager

ATTEST:

City Clerk

APPROVED AS TO FORM:

City Attorney

"HMNH"  
HENRY MAYO NEWHALL HOSPITAL,  
a California nonprofit public benefit  
corporation

Dated: \_\_\_\_\_, 2021

By: \_\_\_\_\_  
Roger E. Seaver President/CEO

"HCN VALENCIA"  
HCN G&L VALENCIA SUB, LLC,  
a Delaware limited liability company

Dated: \_\_\_\_\_, 2021

By: \_\_\_\_\_  
Its: Authorized Signatory

**EXHIBIT "C"**  
**MAP-AERIAL OF CAMPUS PROPERTY**



**EXHIBIT "D"**  
**EXISTING IMPROVEMENTS**

**EXHIBIT "E"**  
**MASTER PLAN**

**EXHIBIT “F”**  
**TRAFFIC MITIGATION IMPROVEMENTS**

1. **Prior to the issuance of a building permit for MOB 2, MOB 3, Inpatient Building No. 2 or the D&T Building.** Pay fees established in the Valencia Bridge and Thoroughfare District in order to provide a fair-share contribution of funds for future traffic system improvements.
2. **Prior to the issuance of a certificate of occupancy for MOB 2, MOB 3, Inpatient Building No. 2 or the D&T Building.**
  - a. Complete the construction of a westerly driveway for the campus property and modify the existing median to include an eastbound left-turn pocket on McBean Parkway in accordance with the approved McBean Parkway realignment plans.
  - b. Construct a bus turn-out lane and the right-turn lane at the westerly driveway for the Campus Property in accordance with the approved McBean Parkway realignment plans.
  - c. Complete the remaining McBean realignment improvements in accordance with the City approved plans.
  - d. In order to address long-term (2030) impacts along McBean Parkway at the Valencia Boulevard intersection complete the following. Add a fourth westbound through lane (requires the widening of Valencia Boulevard). The project’s fair share equal 4.3 percent of the cost of this improvement. If a fair share program has been adopted or if these improvements have been added to a District (Valencia Bridge and Thoroughfare District) payment of fair share costs shall be made.
  - e. In order to address long-term impacts (2035) along Orchard Village at the Wiley Canyon Road intersection complete the following. Modify the existing traffic signal to include a right-turn overlap phase to the westbound approach of Wiley Canyon Road. The right-turn phase proposed for Wiley Canyon Road would overlap with the existing left-turn phase provided for the southbound Orchard Village approach. No physical improvements to the existing lane configurations are required.

**EXHIBIT “G”  
HMNH PARKING DEMAND STUDY**

**EXHIBIT "L"**  
**FRONTAGE DESIGN PLANS - MCBEAN PARKWAY APPROVED**  
**REALIGNMENT PLANS**

STREET IMPROVEMENT PLAN - STANDARD NOTES

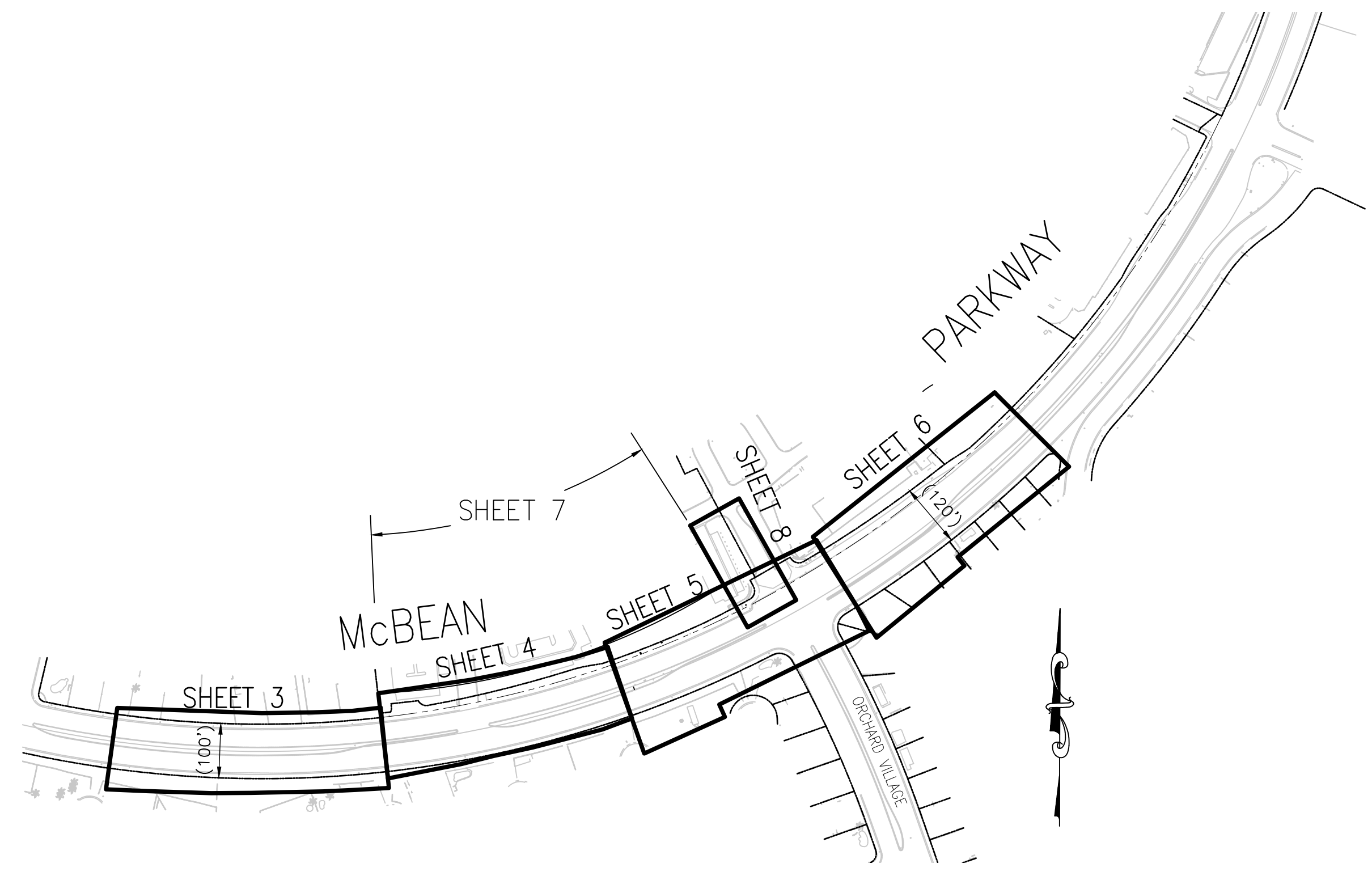
- 1. WORKING HOURS ON CITY STREETS: MONDAY - FRIDAY: 7:00AM - 4:30PM SATURDAY: 8:00AM - 5:00PM (OVERTIME INSPECTION FEES SHALL BE IN ADVANCE; 4 HOUR MINIMUM) NO WORK ON SUNDAYS OR HOLIDAYS. 2. LANE CLOSURES ARE ONLY PERMITTED FROM 8:00AM - 3:30PM. 3. A PERMIT FOR CONSTRUCTION INSPECTION SHALL BE OBTAINED AND FEES PAID TO THE CITY OF SANTA CLARITA, ENGINEERING SERVICES DIVISION, 23920 VALENCIA BOULEVARD, SUITE 140, SANTA CLARITA, CALIFORNIA 91355, PHONE: (661) 286-4060, AT LEAST 72 HOURS PRIOR TO START OF WORK. 4. A COPY OF THE ENCROACHMENT PERMIT AND APPROVED STREET PLANS MUST BE IN THE POSSESSION OF A RESPONSIBLE PERSON AND AVAILABLE AT THE SITE AT ALL TIMES. 5. CONTRACTOR SHALL CONTACT CITY OF SANTA CLARITA PUBLIC WORKS INSPECTION AT (661) 255-4060 TO ARRANGE FOR ACCEPTABLE CONSTRUCTION START DATE. 6. CONTRACTOR SHALL NOTIFY CITY OF SANTA CLARITA PUBLIC WORKS INSPECTION AT (661) 255-4060 AT LEAST TWENTY-FOUR (24) HOURS PRIOR TO THE START OF WORK. 7. CONTRACTOR SHALL NOTIFY UNDERGROUND SERVICE ALERT (USA) AT (800) 227-2600 AT LEAST FORTY-EIGHT (48) HOURS PRIOR TO START OF WORK. CONTRACTOR SHALL CONTACT UNDERGROUND SERVICE ALERT FOR UPDATES EVERY 28 DAYS. 8. A PERMIT MUST BE OBTAINED FROM THE LOS ANGELES COUNTY DEPARTMENT OF PUBLIC WORKS, CONSTRUCTION DIVISION, (626) 458-3129, FOR ALL STORM DRAIN CONNECTIONS. A COPY OF THE PERMIT MUST BE GIVEN TO THE CITY PUBLIC WORKS INSPECTOR PRIOR TO START OF WORK. 9. FOR ALL WORK WITHIN CALTRANS AND/OR LOS ANGELES COUNTY RIGHT-OF-WAY, OBTAIN A PERMIT FROM THE APPROPRIATE JURISDICTION PRIOR TO CONSTRUCTION WITHIN THEIR RIGHT-OF-WAY. 10. ALL WORK SHALL CONFORM TO THE APWA "STANDARD PLANS AND STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION" AND THE LOS ANGELES COUNTY DEPARTMENT OF PUBLIC WORKS "STANDARD PLANS" IN EFFECT AS OF THE APPROVAL DATE OF THESE PLANS. 11. WORK IN PUBLIC STREETS, ONCE BEGUN, SHALL BE PROSECUTED TO COMPLETION WITHOUT DELAY SO AS TO PROVIDE MINIMUM INCONVENIENCE TO ADJACENT PROPERTY OWNERS AND TO THE TRAVELING PUBLIC. FAILURE TO COMPLY WITH THIS REQUIREMENT IS A VIOLATION OF THE CITY OF SANTA CLARITA MUNICIPAL CODE. 12. CONTRACTOR SHALL TAKE ALL NECESSARY AND PROPER PRECAUTION TO PROTECT ADJACENT PROPERTIES FROM ALL DAMAGE THAT MAY OCCUR FROM STORM WATER RUNOFF AND/OR DEPOSITION OF DEBRIS RESULTING FROM ANY AND ALL WORK IN CONNECTION WITH CONSTRUCTION. 13. CONSTRUCT RESIDENTIAL DRIVEWAYS PER APWA STANDARD PLAN 110-1, TYPE A OR B, UNLESS OTHERWISE SHOWN ON PLAN. PRIOR TO FORMING CURB AND GUTTER, THE CONTRACTOR SHALL CONFER WITH THE CITY INSPECTOR TO SECURE APPROVAL FOR ALL DRIVEWAY LOCATIONS AND WIDTHS. 14. INSTALL SIX (6) INCHES OF CRUSHED AGGREGATE BASE UNDER COMMERCIAL DRIVEWAYS. 15. PROCESSED MISCELLANEOUS BASE OR SELECT MATERIAL WITH EXPANSION OF THREE (3) PERCENT OR LESS BY SOIL TEST MAY BE USED IN LIEU OF CRUSHED AGGREGATE BASE UNDER WALK. 16. ALL CONSTRUCTION JOINTS FOR PCC CURBS, GUTTERS, DRIVEWAYS, AND SIDEWALKS SHALL BE IN ACCORDANCE WITH A.P.W.A. STANDARD PLAN NO. 112-1. 17. DRIVEWAYS SHALL NOT BE LOCATED WITHIN TWENTY-FIVE (25) FEET UPSTREAM OF A CATCH BASIN WHEN STREET GRADE IS GREATER THAN SIX (6) PERCENT. 18. DRIVEWAYS SHALL NOT ENCR OACH INTO LOCAL DEPRESSIONS. 19. THERE SHALL BE NO ABOVE GROUND OBSTRUCTION THAT WOULD REDUCE THE WIDTH OF THE SIDEWALK TO LESS THAN 48 INCHES, EXCLUSIVE OF THE TOP OF CURB. 20. INSTALL MAILBOXES AND POSTS IN ACCORDANCE WITH CITY STANDARDS, AND SECURE APPROVAL OF THE U.S. POSTAL SERVICE PRIOR TO INSTALLATION. 21. CONSTRUCT STREET NAME SIGNS PER DETAIL HEREON. INSTALL STREET NAME SIGNS PER LOCATIONS SHOWN ON PLAN. 22. INSTALL STREET LIGHTS PER STREET LIGHT PLAN APPROVED BY THE CITY. 23. STREET TREES SHOWN ON THIS PLAN ARE FOR LOCATION REFERENCE ONLY. ALL PARKWAY AND MEDIAN LANDSCAPING, INCLUDING STREET TREES, SHALL BE CONSTRUCTED PER THE LANDSCAPE PLAN APPROVED BY THE CITY. 24. STREET TREES SHALL BE PLANTED PER THE LATEST EDITION OF THE "STANDARD PLANS FOR PUBLIC WORKS CONSTRUCTION" SECTION 5 - LANDSCAPING AND IRRIGATION SYSTEMS, AND DETAIL HEREON. 25. PAVEMENT MARKERS SHALL BE INSTALLED IN STREETS ADJACENT TO FIRE HYDRANTS IN ACCORDANCE WITH LOS ANGELES COUNTY FIRE DEPARTMENT STANDARDS. 26. PAVEMENT DROP-OFFS OVER ONE-INCH IN HEIGHT THAT WILL REMAIN OVERNIGHT SHALL BE RAMPED WITH TEMPORARY A.C. PAVEMENT. 27. THE THICKNESS OF ASPHALT AND BASE SHALL BE DETERMINED BY TESTING THE SOIL FOR R-VALUES. DEVELOPER'S SOIL ENGINEER SHALL PERFORM THE R-VALUE TESTS AFTER STREETS HAVE BEEN ROUGH GRADED, AND SUBMIT R-VALUE TESTS AND REPORT TO THE CITY OF SANTA CLARITA'S ENGINEERING SERVICES DIVISION. DEVELOPER'S ENGINEER MUST REVISE THE STREET PLANS TO SHOW ACTUAL THICKNESS OF ASPHALT AND BASE TO BE CONSTRUCTED. 28. POLYPROPYLENE GEOTEXTILE STABILIZATION FABRIC (MIRAFI HP370 OR CITY APPROVED EQUAL) AND/OR LIME TREATED SUBBASE SHALL BE UTILIZED WHEN THE R-VALUE IS LESS THAN SEVENTEEN (17) AND THE EXPANSION INDEX IS GREATER THAN THREE (3) PERCENT. 29. CONSTRUCTION OF STREETS SHALL BE PERFORMED PER EITHER OF THE FOLLOWING OPTIONS: OPTION A: THE APPLICANT SHALL CONSTRUCT THE FULL PAVEMENT SECTION INCLUDING THE FINAL LIFT OF ASPHALT TO FINISH GRADE IN TESTS AFTER CONFORMANCE WITH THE DESIGN TL. PRIOR TO OCCUPANCY, THE APPLICANT SHALL REFURBISH THE PAVEMENT TO THE SATISFACTION OF THE CITY ENGINEER. OPTION B: THE APPLICANT SHALL CONSTRUCT A PAVEMENT SECTION THAT IS A MINIMUM OF 1 1/2" LOWER THAN FINISH GRADE, IN CONFORMANCE WITH THE DESIGN TL. PRIOR TO OCCUPANCY, THE APPLICANT SHALL REFURBISH THE PAVEMENT, AND COMPLETE THE FINAL LIFT OF ASPHALT TO MEET FINISH GRADE TO THE SATISFACTION OF THE CITY ENGINEER.

STORMWATER POLLUTION PLAN NOTES

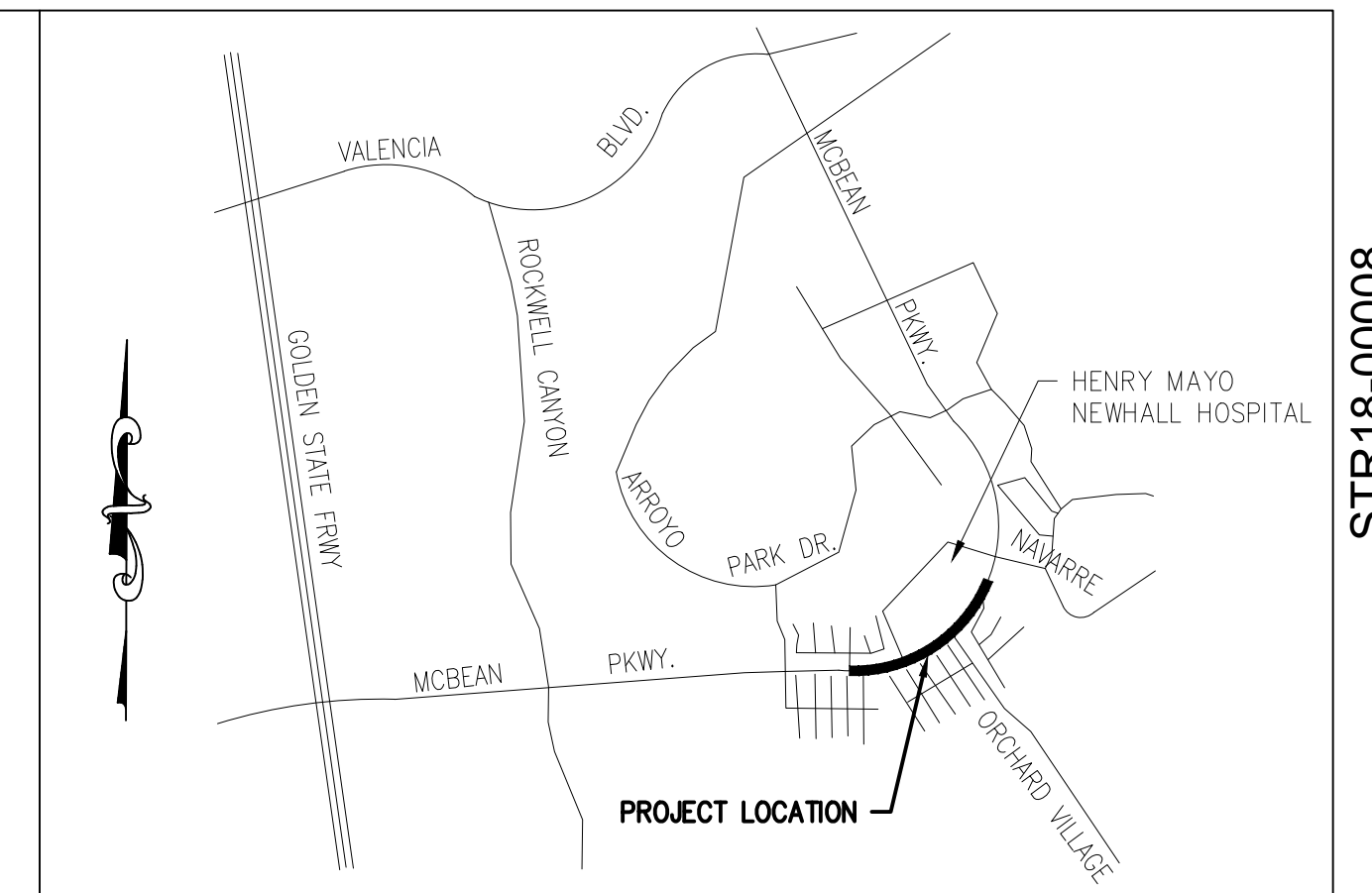
- 1. EVERY EFFORT SHOULD BE MADE TO ELIMINATE THE DISCHARGE OF NON-STORMWATER FROM THE PROJECT SITE AT ALL TIMES. 2. ERODED SEDIMENTS AND OTHER POLLUTANTS MUST BE RETAINED ON SITE AND MAY NOT BE TRANSPORTED FROM THE SITE VIA SHEET FLOW, SWALES, AREA DRAINS, NATURAL DRAINAGE COURSES, OR WIND. 3. STOCKPILES OF EARTH AND OTHER CONSTRUCTION-RELATED MATERIALS MUST BE PROTECTED FROM BEING TRANSPORTED FROM THE SITE BY THE FORCES OF WIND OR WATER. 4. FUELS, OILS, SOLVENTS, AND OTHER TOXIC MATERIALS MUST BE STORED IN ACCORDANCE WITH THEIR LISTING AND ARE NOT TO CONTAMINATE THE SOIL AND SURFACE WATERS. ALL APPROVED STORAGE CONTAINERS ARE TO BE PROTECTED FROM THE WEATHER. SPILLS MUST BE CLEANED UP IMMEDIATELY AND DISPOSED OF IN A PROPER MANNER. SPILLS MAY NOT BE WASHED INTO THE DRAINAGE SYSTEM. 5. EXCESS OR WASTE CONCRETE MAY NOT BE WASHED INTO THE PUBLIC RIGHT-OF-WAY OR ANY OTHER DRAINAGE SYSTEM. PROVISIONS SHALL BE MADE TO RETAIN CONCRETE WASTES ON SITE UNTIL THEY CAN BE DISPOSED OF AS SOLID WASTE. 6. TRASH AND CONSTRUCTION-RELATED SOLID WASTES MUST BE DEPOSITED INTO A COVERED RECEPTACLE TO PREVENT CONTAMINATION OF RAINWATER AND DISPERSAL BY WIND. 7. SEDIMENTS AND OTHER MATERIALS MAY NOT BE TRACKED FROM THE SITE BY VEHICLE TRAFFIC. THE CONSTRUCTION ENTRANCE ROADWAYS MUST BE STABILIZED SO AS TO INHIBIT SEDIMENTS FROM BEING DEPOSITED INTO THE PUBLIC RIGHT-OF-WAY. ACCIDENTAL DEPOSITIONS MUST BE SWEEPED UP IMMEDIATELY AND MAY NOT BE WASHED DOWN BY RAIN OR OTHER MEANS. 8. ANY SLOPES WITH DISTURBED SOILS OR DENUDED OF VEGETATION MUST BE STABILIZED SO AS TO INHIBIT EROSION BY WIND AND WATER. 9. THE FOLLOWING BMP'S AS OUTLINED IN, BUT NOT LIMITED TO, THE "BEST MANAGEMENT PRACTICE HANDBOOK, CALIFORNIA STORMWATER QUALITY TASK FORCE, SACRAMENTO, CALIFORNIA, 2009", AT THE LATEST REVISED EDITION, MAY APPLY DURING THE CONSTRUCTION OF THIS PROJECT (ADDITIONAL MEASURES MAY BE REQUIRED IF DEEMED APPROPRIATE BY CITY INSPECTORS):

Table with 4 columns: NON-STORMWATER MANAGEMENT, EROSION CONTROL, TEMPORARY SEDIMENT CONTROL, WASTE MANAGEMENT & MATERIAL POLLUTION CONTROL. Lists various engineering practices and controls such as scheduling, mulch, sediment traps, and erosion control measures.

CITY OF SANTA CLARITA STREET IMPROVEMENT PLANS McBEAN PARKWAY 1200FT S'LY OF ORCHARD VILLAGE TO 500FT N'LY OF ORCHARD VILLAGE



KEY MAP 1" = 200'



VICINITY MAP 1" = 2000'

ABBREVIATIONS

Table of abbreviations including B.C. BEGIN CURVE, BTM BOTTOM, B.V.C. BEGIN VERTICAL CURVE, C.B. CATCH BASIN, C.F. CURB FACE, C.L. CENTERLINE, DEP. DEPRESSION, E.C. END CURVE, E.V.C. END VERTICAL CURVE, F.F. FINISHED FLOOR, F.G. FINISHED GRADE, F.L. FLOWLINE, F.S. FINISHED SURFACE, G.B. GRADE BREAK, H.P. HIGH POINT, I.E. INVERT ELEVATION, L.T. LEFT, M.H. MANHOLE, N.T.S. NOT TO SCALE, O.C. ON CURB, P.I. POINT OF INTERSECTION, P.V.C. POLYVINYL CHLORIDE, R.C.P. REINFORCED CONCRETE PIPE, RT. RIGHT, T.C. TOP OF CURB, T.D. TOP OF DIKE, T.G. TOP OF GRATE, T.P. TOP OF PIPE, T.W. TOP OF WALL, T.R.W. TOP OF RETAINING WALL, V.C. VERTICAL CURVE, W.W.M. WELDED WIRE MESH.

SHEET INDEX

Table with 2 columns: SHEET, DESCRIPTION. Lists sheets 1 through 22, including Vicinity Map, Typical Section & Details, Street Improvement Plan (Sta. 3+00.76 to 20+50.00), Off-site Details, Signing and Striping Plan, Traffic Signal Modification Plan, Hardscape and Planting Notes and Legends, and Irrigation Calculations.

CONTRACTOR'S RESPONSIBILITY

THE CONTRACTOR IS REQUIRED TO TAKE DUE PRECAUTIONARY MEASURES TO PROTECT THE UTILITY LINES SHOWN ON THESE DRAWINGS. THE CONTRACTOR FURTHER ASSUMES ALL LIABILITY AND RESPONSIBILITY FOR THE UTILITY PIPES, CONDUITS OR STRUCTURES SHOWN OR NOT SHOWN ON THESE DRAWINGS.

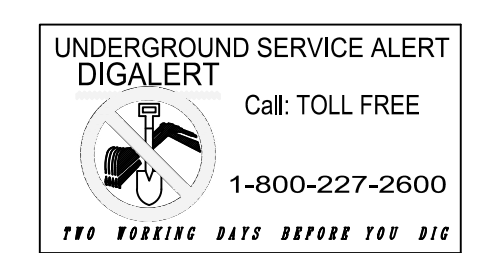
THE CONTRACTOR AGREES THAT HE SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOBSITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY THAT THIS REQUIREMENT SHALL APPLY CONTINUOUS AND NOT BE LIMITED TO NORMAL WORKING HOURS; AND THAT THE CONTRACTOR SHALL DEFEND, INDEMNIFY AND HOLD THE OWNER AND ENGINEER HARMLESS FROM ANY LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF OWNER OR ENGINEER.

PRIVATE ENGINEER'S NOTES TO CONTRACTORS

THE UNDERSIGNED CIVIL ENGINEER CERTIFIES THAT THE GRADING WORK WILL BE SUPERVISED IN ACCORDANCE WITH SECTION 17.88.010 OF THE MUNICIPAL CODE.

THE EXISTENCE AND LOCATION OF ANY UNDERGROUND UTILITY PIPES, CONDUITS OR STRUCTURES SHOWN ON THESE PLANS IS OBTAINED BY A SEARCH OF THE AVAILABLE RECORDS AND TO THE BEST OF MY KNOWLEDGE THERE ARE NO EXISTING UTILITIES EXCEPT AS SHOWN ON THESE PLANS.

Signature of Mike Hennawy, Registered Civil Engineer No. 45531, dated September 17, 2020.



BENCHMARK: L 4236 DESCRIPTION: RD.B.M. TAG LOCATION: N. CB @ N.E. COR. AVENIDA NAVARRRE & McBeAn PKWY, 77' E. & 30' N/O C/L INT. 10' E/O BCR ELEVATION: 1,213.91 DATUM: NAVD 1988

PLANS PREPARED FOR: ANDERSON PENNA PARTNERS 3737 BIRCH ST, SUITE 250 NEWPORT BEACH, CA 92660 P: (949) 428-1500 F: (949) 258-5053

PLANS PREPARED UNDER THE DIRECTION OF: JOE R. BUCKNER RCE 45531 9-17-20 DATE



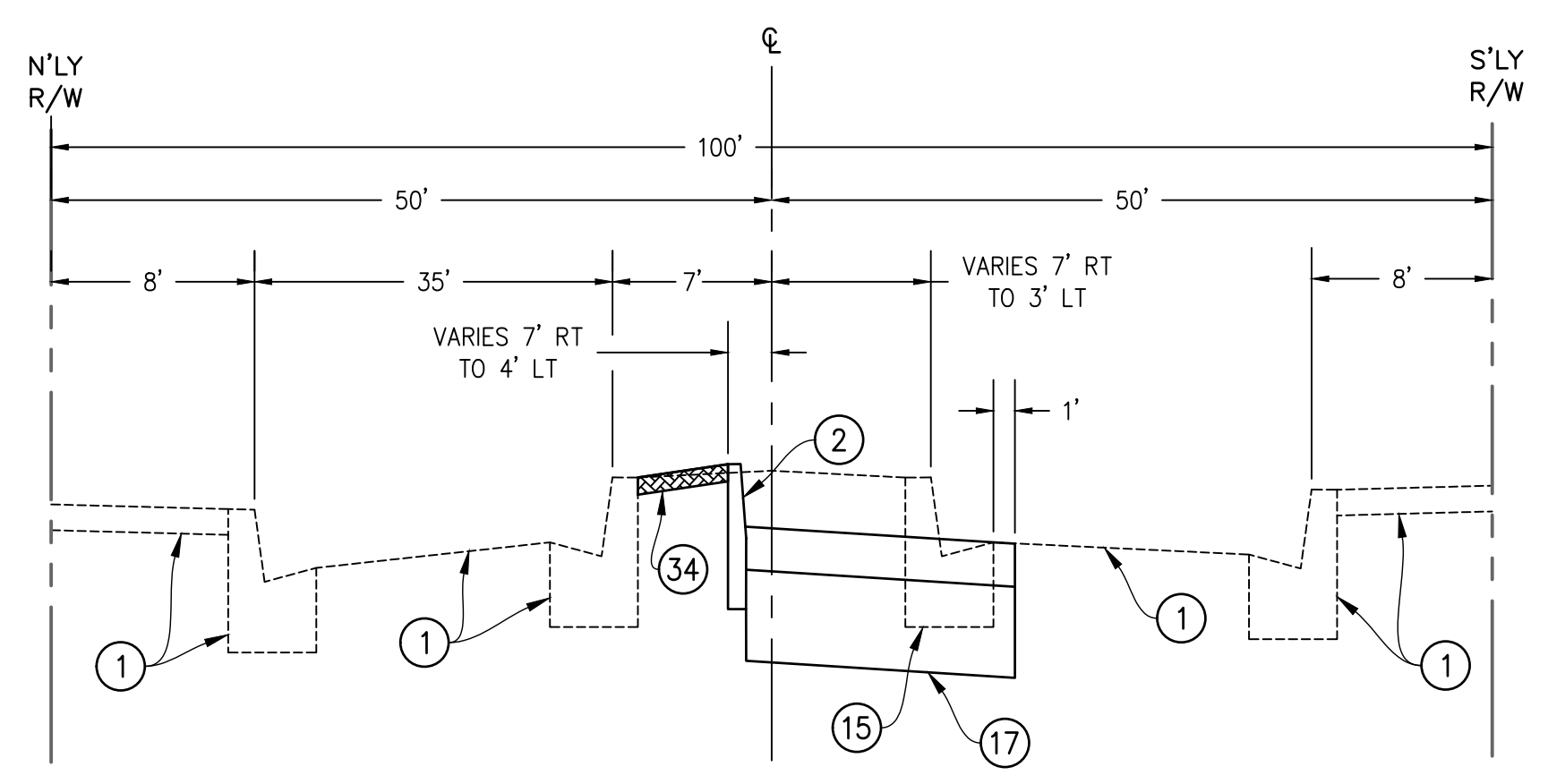
REVIEWED BY: Adwan Saba 10/19/20 PLAN CHECKER DATE OF EXECUTION APPROVED FOR CONSTRUCTION BY: SP Mike Hennawy 10/19/2020 MIKE HENNAWY CITY ENGINEER DATE OF EXECUTION

IN ACCORDANCE WITH RESOLUTION No. 19-6, AS A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF SANTA CLARITA DELEGATING DISCRETIONARY AUTHORITY TO THE DIRECTOR OF PUBLIC WORKS, DIRECTOR OF RECREATION AND COMMUNITY SERVICES, DIRECTOR OF NEIGHBORHOOD SERVICES, CITY ENGINEER, AND ASSISTANT CITY ENGINEERS TO APPROVE PLANS AND DESIGNS FOR THE CONSTRUCTION OF OR IMPROVEMENTS TO PUBLIC PROPERTY FOR PURPOSES OF DESIGN IMMUNITY PURSUANT TO GOVERNMENT CODE SECTION 830.6 FOR THE CITY OF SANTA CLARITA, I DO HEREBY EXERCISE THE DISCRETION DELEGATED TO ME AND APPROVE THE PLAN OR DESIGN, OR AMENDMENT OR MODIFICATION TO THE PLAN OR DESIGN, OR A CONSTRUCTION OF, OR AN IMPROVEMENT TO THE PUBLIC FACILITY, STRUCTURE, OR PROPERTY TO WHICH THIS STATEMENT, MY SIGNATURE AND DATE IS AFFIXED.

Table with 5 columns: REVISION, REVISED BY (PRINT NAME), CITY APPROVALS, EXECUTION DATE. Includes a revision entry for 'UNDERGROUND SERVICE ALERT DIGALERT'.

CITY OF SANTA CLARITA STREET IMPROVEMENT PLANS VICINITY MAP, SHEET INDEX BENCHMARK, STANDARD NOTES

STR18-00008



**TYPICAL SECTION - McBean Parkway (T.I. = 10)**  
NTS  
STA 3+00.76 TO STA 6+98.68

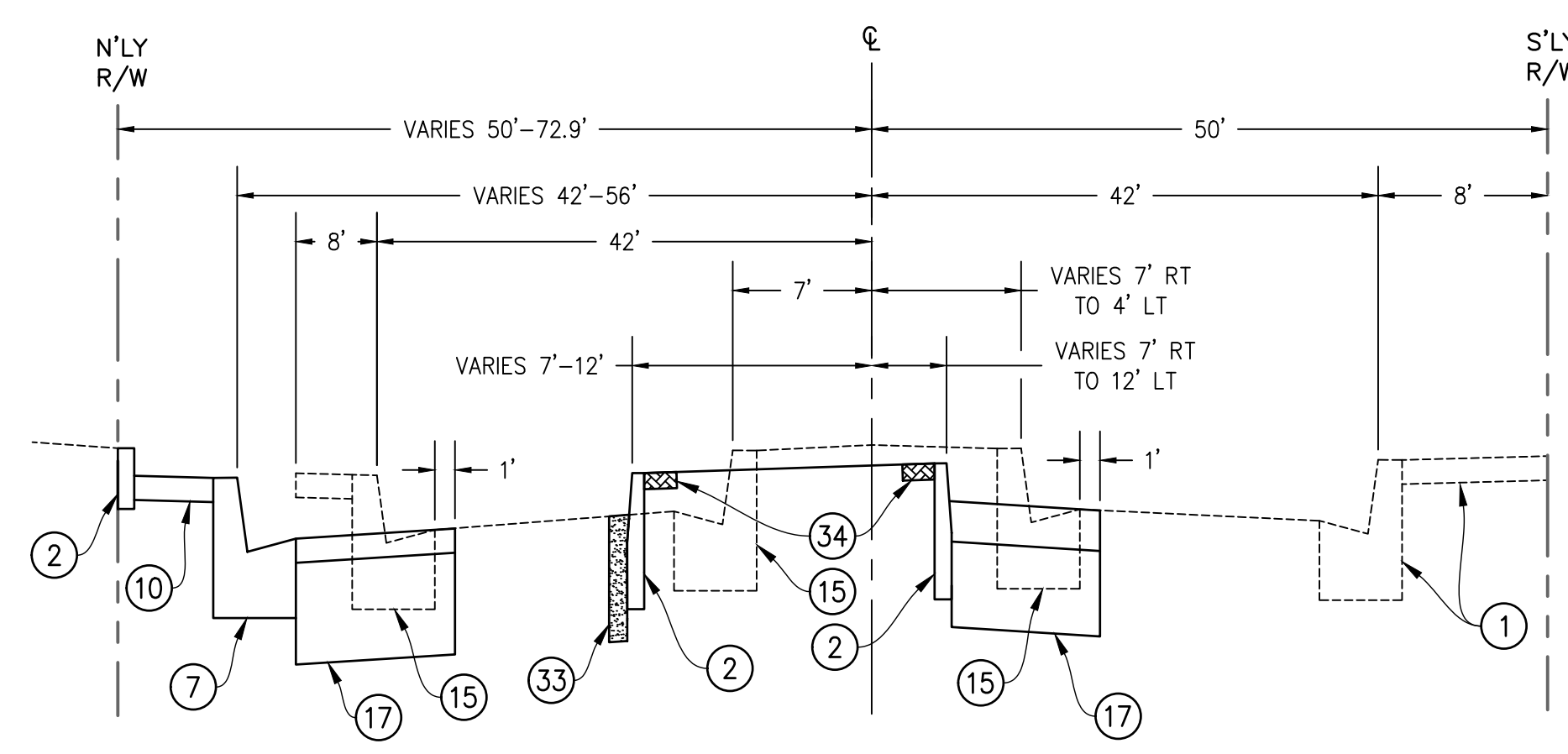


**CURB RAMP NW CORNER PROPOSED ENTRANCE**  
SCALE: 1"=5'

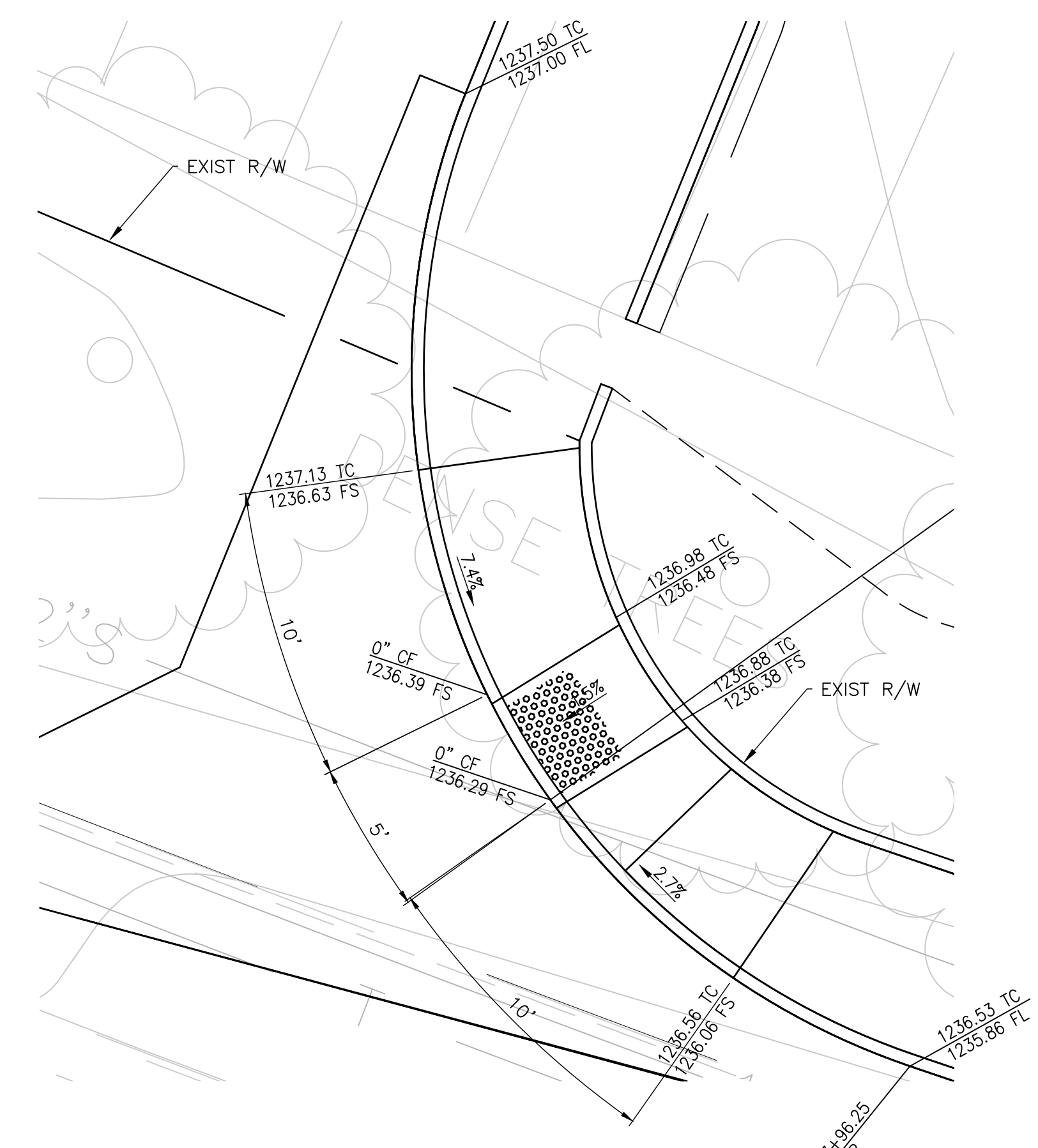
**CONSTRUCTION NOTES**

- 1 PROTECT IN PLACE
- 2 CONSTRUCT TYPE A1-6 PCC CURB PER SPPWC STD. PLAN NO. 120-2
- 3 CONSTRUCT 12" HDPE STORM DRAIN
- 4 CONSTRUCT MEDIAN TAPER PER SPPWC STD. PLAN NO. 140-3
- 5 CONSTRUCT CURB OPENING CATCH BASIN PER SPPWC STD. PLAN NO. 300-3 (W&V PER PLAN)
- 6 CONSTRUCT LOCAL DEPRESSION AT CATCH BASIN PER SPPWC STD. PLAN NO. 313-3 (CASE E, H=2")
- 7 CONSTRUCT A2-8 CURB AND GUTTER PER SPPWC STD. PLAN NO. 120-2
- 8 CONSTRUCT CROSS GUTTER PER SPPWC STD. PLAN NO. 122-2
- 9 CONSTRUCT TYPE A2-6 CURB AND GUTTER (W=1.5') PER SPPWC STD. PLAN NO. 120-2
- 10 CONSTRUCT 4" THICK PCC SIDEWALK, JOINTS PER SPPWC STD. PLAN NO. 112-2
- 11 CONSTRUCT CURB RAMP PER CALTRANS STD. PLAN NO. A88A (CASE PER PLAN). SEE SHEET 2 FOR DETAILS
- 12 ADJUST TO GRADE (ITEM PER PLAN)
- 13 REGRADE EXISTING LANDSCAPING TO NEW CURB ELEVATIONS. RESTORE IRRIGATION AND SOD
- 14 REMOVE, ITEM AS INDICATED
- 15 SAWCUT AND REMOVE EXISTING AC, CURB, GUTTER AND/OR LANDSCAPE
- 16 REMOVE EXISTING TREE (DIAMETER PER PLAN)
- 17 CONSTRUCT 7" ASPHALT CONCRETE OVER 8" AGGREGATE BASE OVER 6" COMPACTED SUBGRADE (90% COMPACTION)
- 18 CONSTRUCT MANHOLE PIPE-TO-PIPE PER SPPWC STD. PLAN 321-2
- 19 REMOVE EXISTING & CONSTRUCT 30" DEEP FILL WITH SOIL PER LANDSCAPE PLANS
- 20 CONSTRUCT MEDIAN CURB OPENING PER DETAIL B ON SHEET 2 NOT USED
- 21 RELOCATE, AS NOTED
- 22 CONSTRUCT CONCRETE COLLAR PER SPPWC STD. PLAN 380-4
- 23 CONSTRUCT TYPE A2-8 (W=12") PCC CURB AND GUTTER PER SPPWC PLAN NO. 120-2
- 24 REMOVE EXISTING SIGN
- 25 REMOVE AND REPLACE EXISTING SIGN RELATIVE TO NEW CONSTRUCTION
- 26 CONSTRUCT TYPE A1-6 PCC CURB PER SPPWC PLAN NO. 120-2
- 27 CONSTRUCT 6" TO 8" CURB FACE TAPER PER DETAIL ON SHEET 2 NOT USED
- 28 CONSTRUCT 11" AC DEEP LIFT WITH EACH PASS OF NOT MORE THAN 4" OVER 6" COMPACTED SUBGRADE (95% MIN.)
- 29 CONSTRUCT 21" RCP STORM DRAIN (D=2000).
- 30 REMOVE EXISTING CATCH BASIN AND INTERFERING PORTIONS OF EXIST 12" STORM DRAIN PIPE
- 31 RELOCATE BY OTHERS, ITEM AS INDICATED
- 32 CONSTRUCT ASPHALT CONCRETE TO MATCH EXISTING OR 6" MIN. OVER COMPACTED SUBGRADE (90% COMPACTION)
- 33 CONSTRUCT SLOT PAVE (2 SACK SLURRY) TO A DEPTH OF 12"
- 34 INSTALL CONCRETE UNIT PAVERS (REFER TO LANDSCAPE ARCHITECT PLANS)
- 35 INSTALL STEEL FALSE CURB OVER SEWER MANHOLE PER LBIV LB 750 SERIES AND DETAIL ON SHEET 8

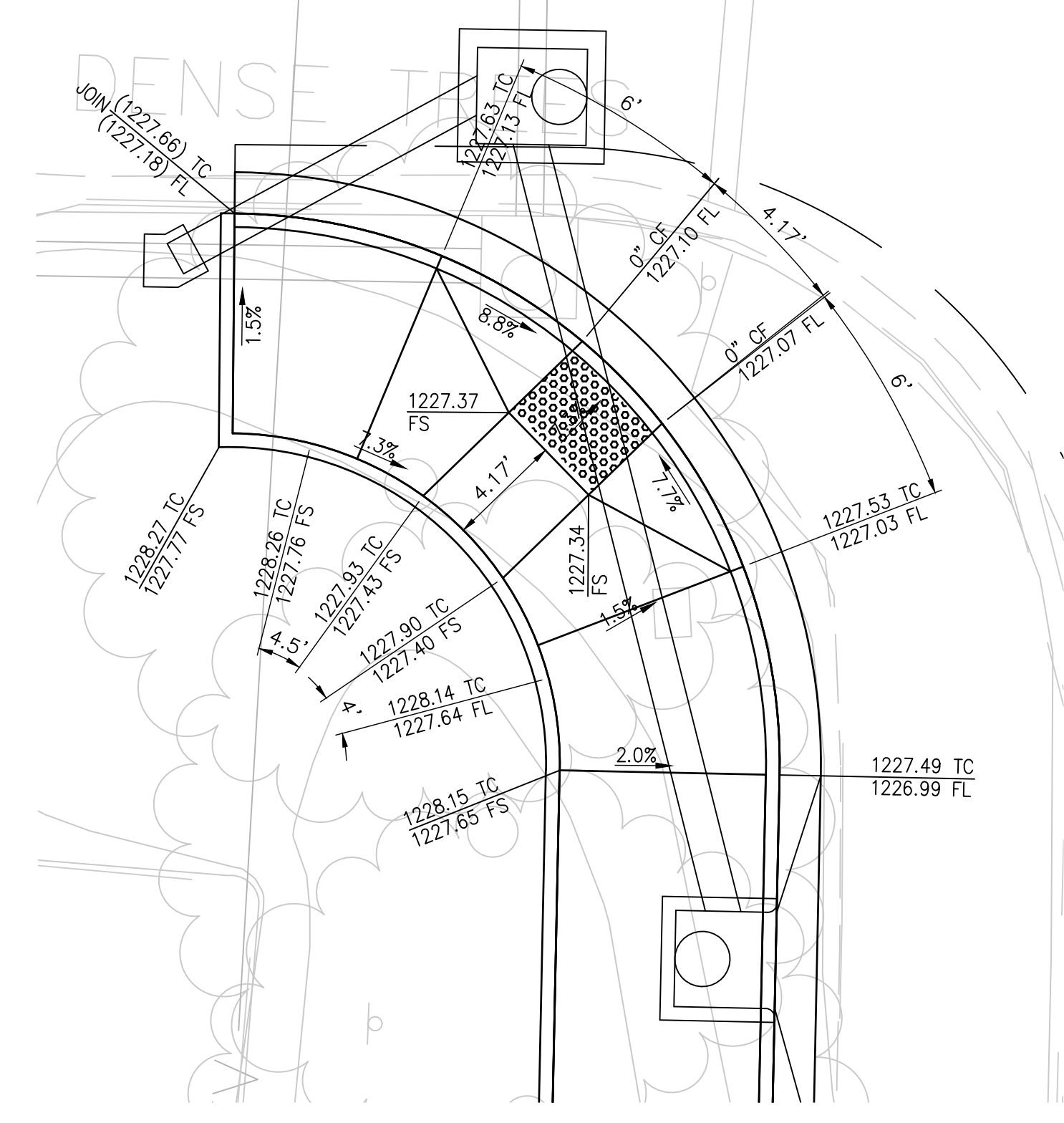
★ NOTE:  
REFER TO STRUCTURAL PAVEMENT INVESTIGATION REPORT PREPARED BY LA BELLE MARVIN DATED SEPTEMBER 2018, PROJECT NO. 43915. ACTUAL PAVEMENT SECTION TO BE DETERMINED AFTER R-VALUE TAKEN ONCE ROUGH GRADING IS COMPLETE.



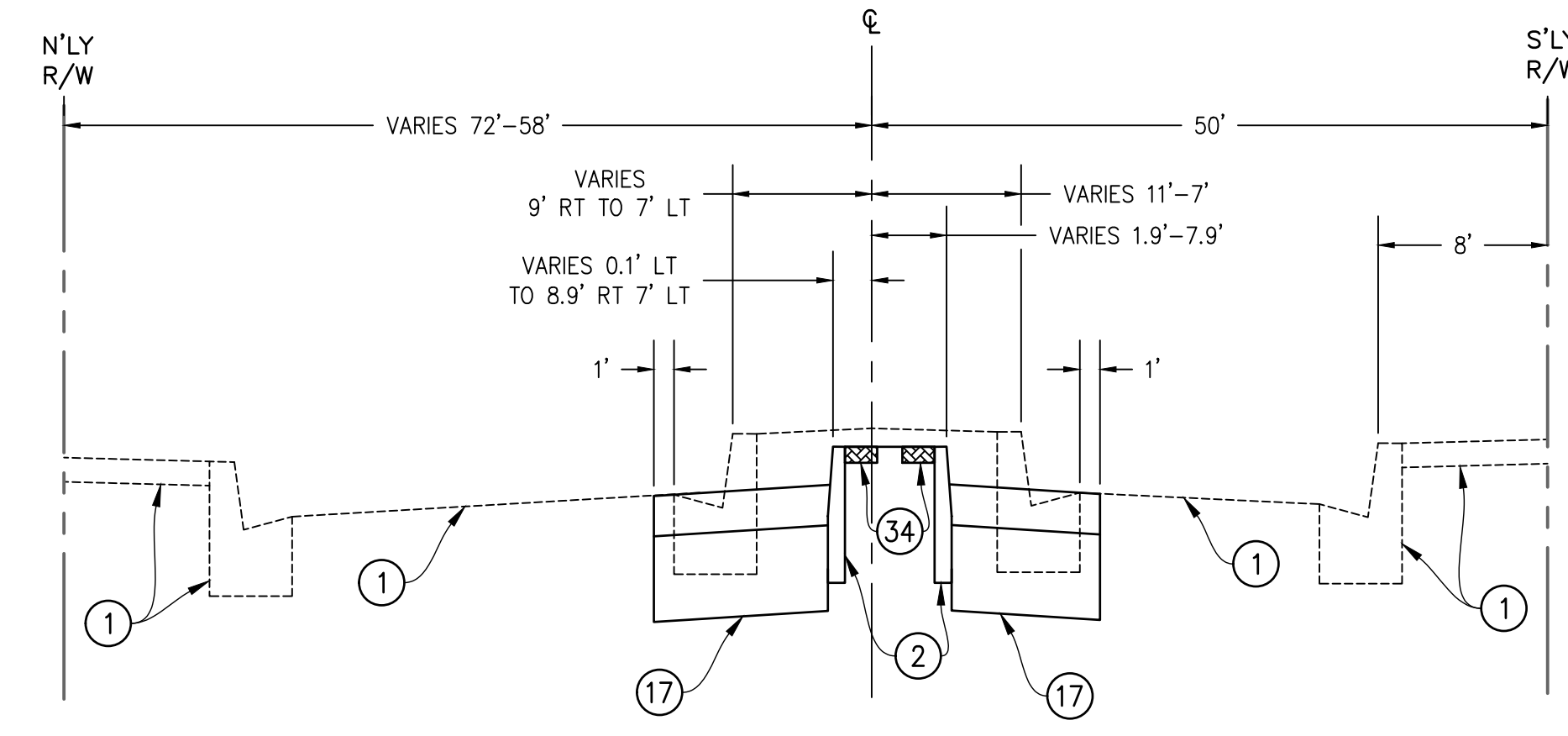
**TYPICAL SECTION - McBean Parkway (T.I. = 10)**  
NTS  
STA 6+98.68 TO STA 14+39.85



**CURB RAMP NE CORNER PROPOSED ENTRANCE**  
SCALE: 1"=5'



**CURB RAMP ON ORCHARD VILLAGE NORTH OF McBEAN OFFSITE**  
SCALE: 1"=5'



**TYPICAL SECTION - McBean Parkway (T.I. = 10)**  
NTS  
STA 15+63.95 TO STA 20+33.31

UNDERGROUND SERVICE ALERT DIGALERT  
Call: TOLL FREE 1-800-227-2600  
FOR WORKING DAYS BEFORE YOU DIG

PLANS PREPARED FOR:  
ANDERSON PENNA PARTNERS  
3737 BIRCH ST, SUITE 250  
NEWPORT BEACH, CA 92660  
P: (949) 428-1500  
F: (949) 258-5053

PLANS PREPARED UNDER THE DIRECTION OF:  
JOE R. BUCKNER RCE 45531  
9-17-20 DATE

REVIEWED BY:  
Adwan Saba 10/19/20  
DATE OF EXECUTION

APPROVED FOR CONSTRUCTION BY:  
Mike Hennawy 10/19/2020  
DATE OF EXECUTION

APPROVED FOR CONSTRUCTION BY:  
Mike Hennawy  
CITY ENGINEER

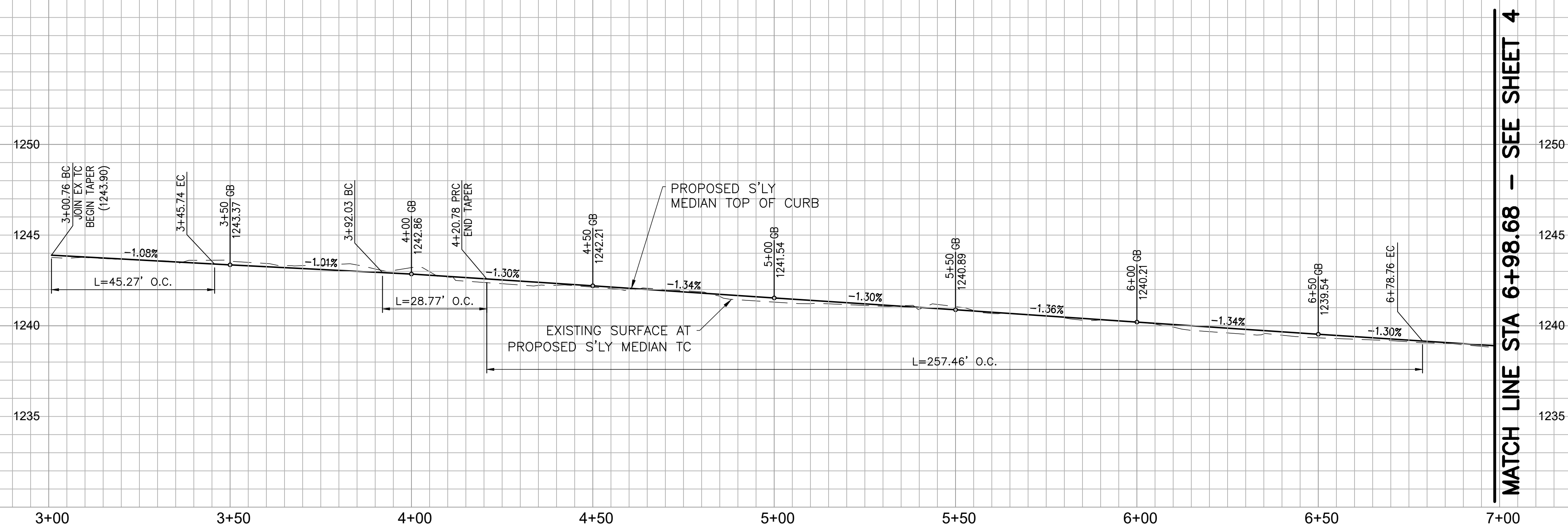
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REVISION	REVISED BY (PRINT NAME)	CITY APPROVALS	EXECUTION DATE

**CITY OF SANTA CLARITA**  
**STREET IMPROVEMENT PLANS**

TYPICAL SECTION & DETAILS

SHEET: **2** OF 22



CURVE DATA			
SEGMENT	RADIUS	DELTA	LENGTH
C1	275.00'	09°25'53"	45.27'
C2	275.00'	05°59'36"	28.77'
C3	1996.00'	07°23'26"	257.46'

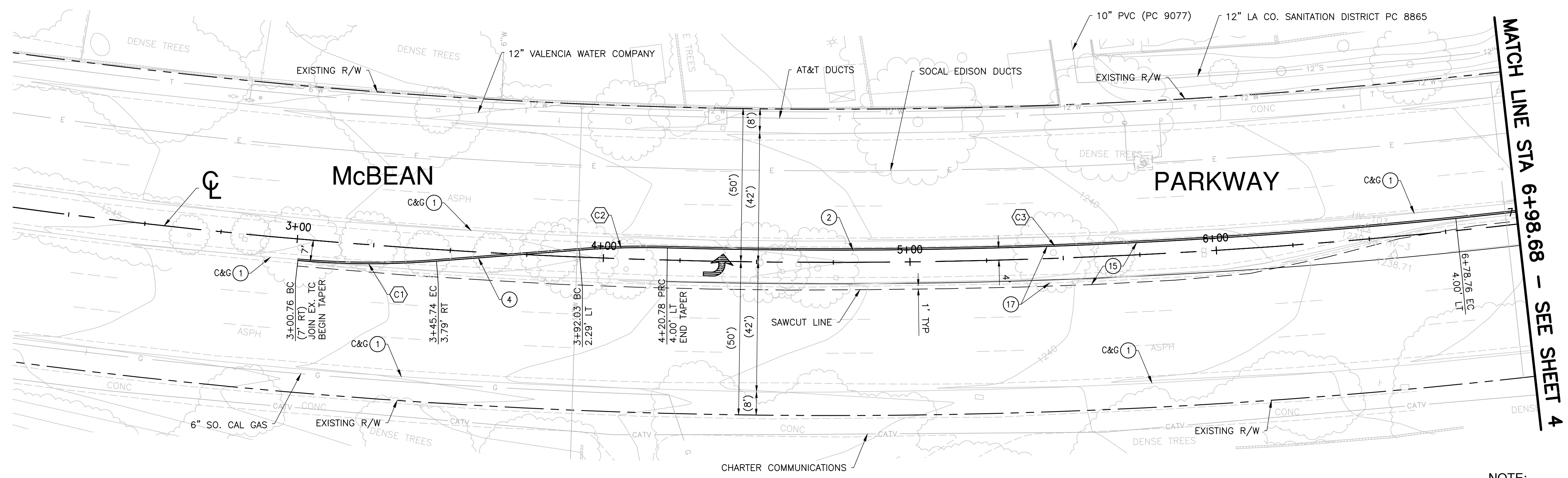
**PROFILE SCALE**  
 HORIZ.: 1" = 20'  
 VERT.: 1" = 4'

MATCH LINE STA 6+98.68 - SEE SHEET 4

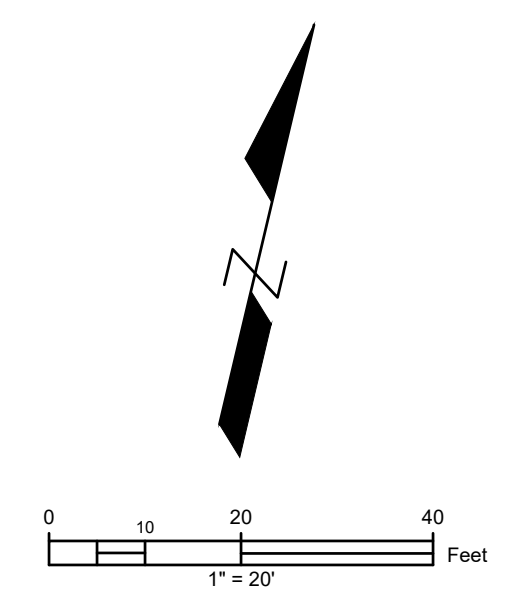
**CONSTRUCTION NOTES**

- ① PROTECT IN PLACE
- ② CONSTRUCT TYPE A1-6 PCC CURB PER SPPWC STD. PLAN NO. 120-2
- ④ CONSTRUCT MEDIAN TAPER PER SPPWC STD. PLAN NO. 140-3
- ⑮ SAWCUT AND REMOVE EXISTING AC, CURB, GUTTER AND/OR LANDSCAPE
- \* ⑰ CONSTRUCT 7" ASPHALT CONCRETE OVER 8" AGGREGATE BASE OVER 6" COMPACTED SUBGRADE (90% COMPACTION)

\* NOTE:  
 REFER TO STRUCTURAL PAVEMENT INVESTIGATION REPORT PREPARED BY LA BELLE MARVIN DATED SEPTEMBER 2018, PROJECT NO. 43915. ACTUAL PAVEMENT SECTION TO BE DETERMINED AFTER R-VALUE TAKEN ONCE ROUGH GRADING IS COMPLETE.



MATCH LINE STA 6+98.68 - SEE SHEET 4



NOTE:  
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PLANS PREPARED UNDER THE DIRECTION OF:  
 JOE R. BUCKNER  
 RCE 45531  
 9-17-20  
 DATE



REVIEWED BY:  
 Adwan Shuaib 10/19/20  
 PLAN CHECKER DATE OF EXECUTION

APPROVED FOR CONSTRUCTION BY:  
 SP Mike Hennawy 10/19/2020  
 MIKE HENNAWY  
 CITY ENGINEER DATE OF EXECUTION

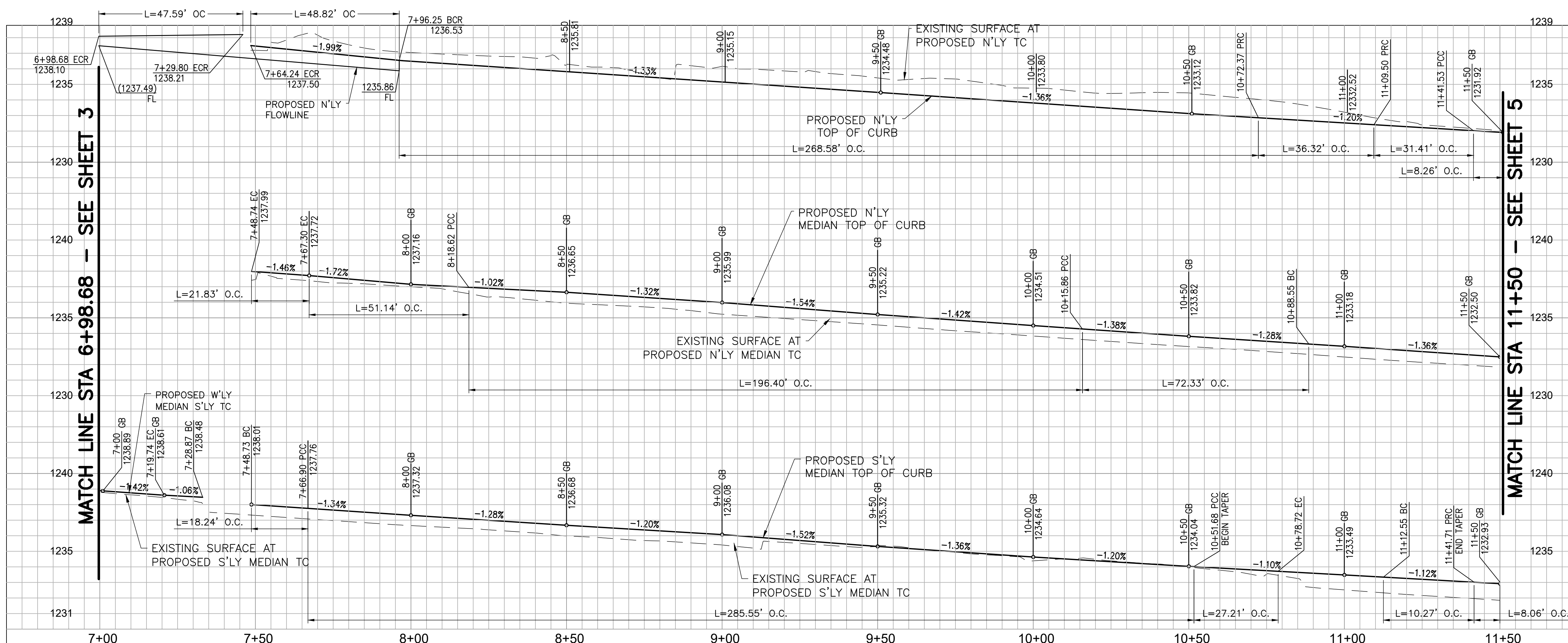
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REVISION	REVISED BY (PRINT NAME)	CITY APPROVALS	EXECUTION DATE

**CITY OF SANTA CLARITA**  
**STREET IMPROVEMENT PLANS**  
 STA. 3+00.76 TO STA. 6+98.68

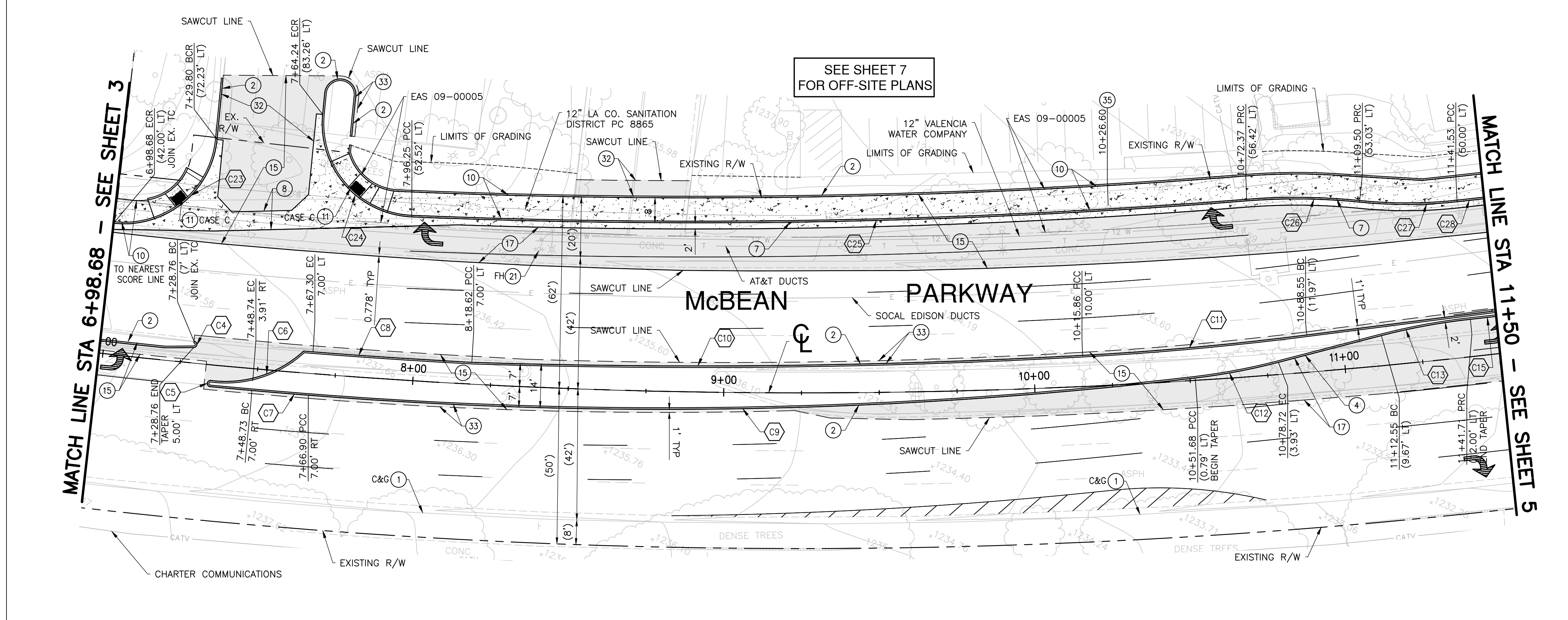
SHEET:  
**3**  
 OF 22





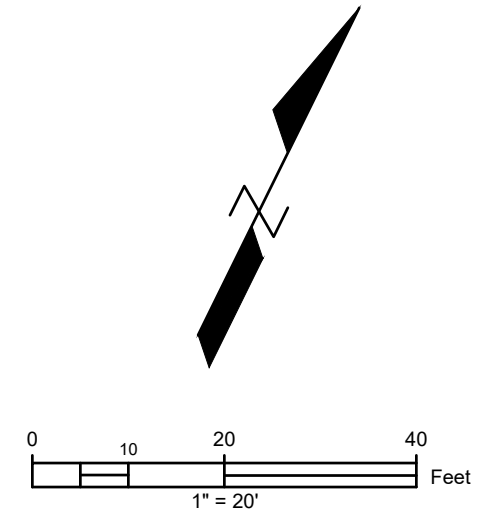
CURVE DATA			
SEGMENT	RADIUS	DELTA	LENGTH
C3	1996.00'	08°33'53"	298.36'
C4	1.00'	173°49'27"	3.03'
C5	1.00'	180°00'00"	3.17'
C6	37.00'	47°53'21"	32.26'
C7	2007.00'	00°54'15"	31.67'
C8	1993.00'	01°28'13"	51.14'
C9	1450.00'	11°17'00"	285.55'
C10	2270.21'	04°57'24"	196.40'
C11	1000.00'	04°08'39"	72.33'
C12	200.00'	07°47'37"	27.20'
C13	200.00'	08°20'22"	29.11'
C15	1988.00'	08°31'59"	296.07'
C23	30.00'	90°53'30"	47.59'
C24	30.00'	93°13'49"	48.82'
C25	2224.21'	06°55'07"	268.58'
C26	201.01'	10°21'05"	36.32'
C27	150.00'	11°59'51"	31.41'
C28	1950.00'	05°39'01"	192.30'

**PROFILE SCALE**  
 HORIZ.: 1" = 20'  
 VERT.: 1" = 4'



**CONSTRUCTION NOTES**

- 1 PROTECT IN PLACE
  - 2 CONSTRUCT TYPE A1-6 PCC CURB PER SPPWC STD. PLAN NO. 120-2
  - 4 CONSTRUCT MEDIAN TAPER PER SPPWC STD. PLAN NO. 140-3
  - 7 CONSTRUCT A2-8 CURB AND GUTTER PER SPPWC STD. PLAN NO. 120-2
  - 8 CONSTRUCT CROSS GUTTER PER SPPWC STD. PLAN NO. 122-2
  - 10 CONSTRUCT 4" THICK PCC SIDEWALK, JOINTS PER SPPWC STD. PLAN NO. 112-2
  - 11 CONSTRUCT CURB RAMP PER CALTRANS STD. PLAN NO. AB8A (CASE PER PLAN). SEE SHEET 2 FOR DETAILS
  - 15 SAWCUT AND REMOVE EXISTING AC, CURB, GUTTER AND/OR LANDSCAPE
  - \* 17 CONSTRUCT 7" ASPHALT CONCRETE OVER 8" AGGREGATE BASE OVER 6" COMPACTED SUBGRADE (90% COMPACTION)
  - 21 RELOCATE, AS NOTED
  - 32 CONSTRUCT ASPHALT CONCRETE TO MATCH EXISTING OR 6" MIN. OVER COMPACTED SUBGRADE (90% COMPACTION)
  - 33 CONSTRUCT SLOT PAVE (2 SACK SLURRY) TO A DEPTH OF 12"
  - 35 INSTALL STEEL FALSE CURB OVER SEWER MANHOLE PER LBW LB 750 SERIES AND DETAIL ON SHEET 8
- \* NOTE:  
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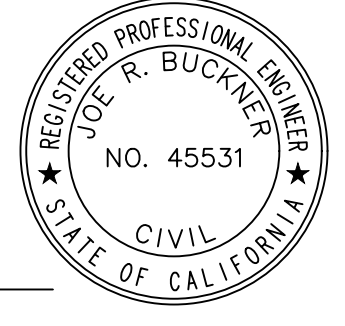


**NOTE:**  
 SEE LANDSCAPE PLAN FOR DISPOSITION OF TREES



PLANS PREPARED FOR:  
 ANDERSON PENNA PARTNERS  
 3737 BIRCH ST, SUITE 250  
 NEWPORT BEACH, CA 92660  
 P: (949) 428-1500  
 F: (949) 258-5053

PLANS PREPARED UNDER THE DIRECTION OF:  
 JOE R. BUCKNER RCE 45531  
 9-17-20  
 DATE



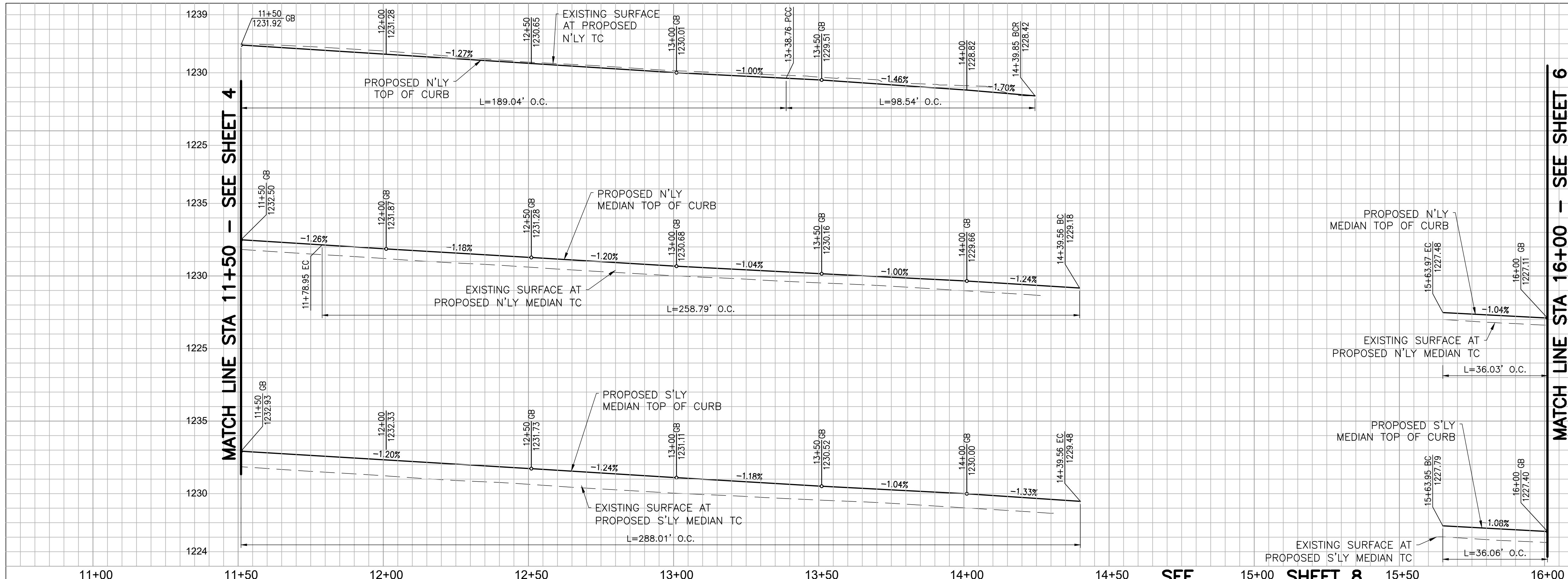
REVIEWED BY:  
 Adwan Shuaib 10/19/20  
 PLAN CHECKER DATE OF EXECUTION

APPROVED FOR CONSTRUCTION BY:  
 SP Mike Hennawy 10/19/2020  
 MIKE HENNAWY DATE OF EXECUTION  
 CITY ENGINEER

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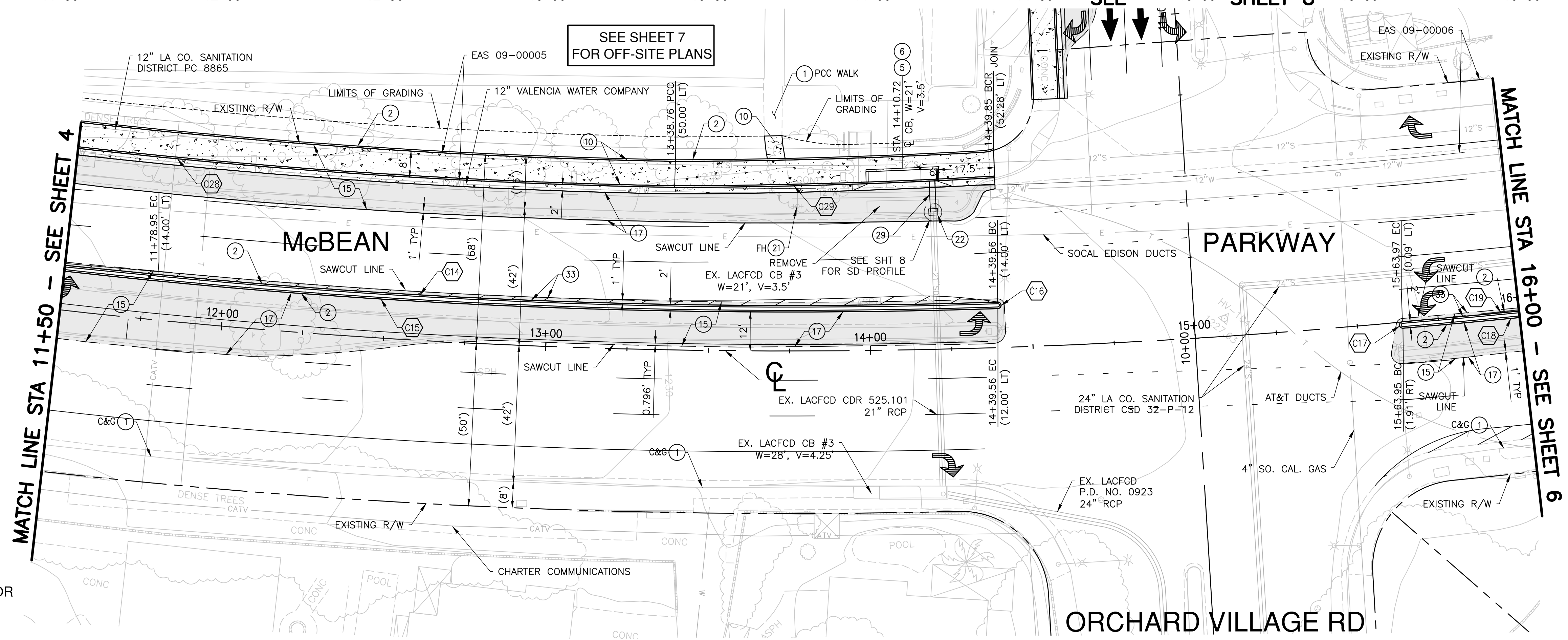
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**CITY OF SANTA CLARITA**  
**STREET IMPROVEMENT PLANS**  
 STA. 6+98.68 TO STA. 11+50.00  
 SHEET: **4** OF 22



CURVE DATA			
SEGMENT	RADIUS	DELTA	LENGTH
C14	1986.00'	7°27'58"	258.79'
C15	1988.00'	8°31'59"	296.07'
C16	1.00'	180°00'00"	3.14'
C17	1.00'	180°00'00"	3.14'
C18	2191.75'	8°20'13"	318.91'
C19	2189.75'	9°55'16"	379.17'
C28	1950.00'	5°39'01"	192.30'
C29	1769.44'	3°11'27"	98.54'

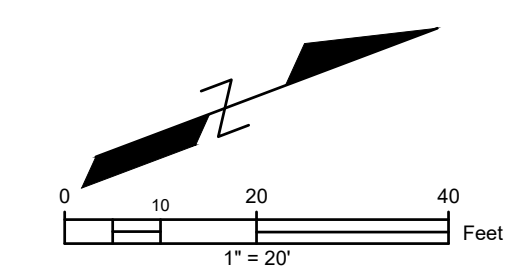
**PROFILE SCALE**  
 HORIZ.: 1" = 20'  
 VERT.: 1" = 4'



**CONSTRUCTION NOTES**

- 1 PROTECT IN PLACE
- 2 CONSTRUCT TYPE A1-6 PCC CURB PER SPPWC STD. PLAN NO. 120-2
- 5 CONSTRUCT CURB OPENING CATCH BASIN PER SPPWC STD. PLAN NO. 300-3 (W&V PER PLAN)
- 6 CONSTRUCT LOCAL DEPRESSION AT CATCH BASIN PER SPPWC STD. PLAN NO. 313-3 (CASE E, H=2")
- 7 CONSTRUCT A2-8 CURB AND GUTTER PER SPPWC STD. PLAN NO. 120-2
- 10 CONSTRUCT 4" THICK PCC SIDEWALK, JOINTS PER SPPWC STD. PLAN NO. 112-2
- 15 SAWCUT AND REMOVE EXISTING AC, CURB, GUTTER AND/OR LANDSCAPE
- \* 17 CONSTRUCT 7" ASPHALT CONCRETE OVER 8" AGGREGATE BASE OVER 6" COMPACTED SUBGRADE (90% COMPACTION)
- 21 RELOCATE, AS NOTED
- 22 CONSTRUCT CONCRETE COLLAR PER SPPWC STD. PLAN 380-4
- 29 CONSTRUCT 21" RCP STORM DRAIN (D=2000).
- 33 CONSTRUCT SLOT PAVE (2 SACK SLURRY) TO A DEPTH OF 12"

\* NOTE:  
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 FOR WORKING DAYS BEFORE YOU DIG

NOTE:  
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PLANS PREPARED FOR:  
 ANDERSON PENNA PARTNERS  
 3737 BIRCH ST, SUITE 250  
 NEWPORT BEACH, CA 92660  
 P: (949) 428-1500  
 F: (949) 238-5053

PLANS PREPARED UNDER THE DIRECTION OF:  
 JOE R. BUCKNER  
 RCE 45531  
 DATE 9-17-20



REVIEWED BY:  
 Adwan Sha 10/19/20  
 PLAN CHECKER DATE OF EXECUTION

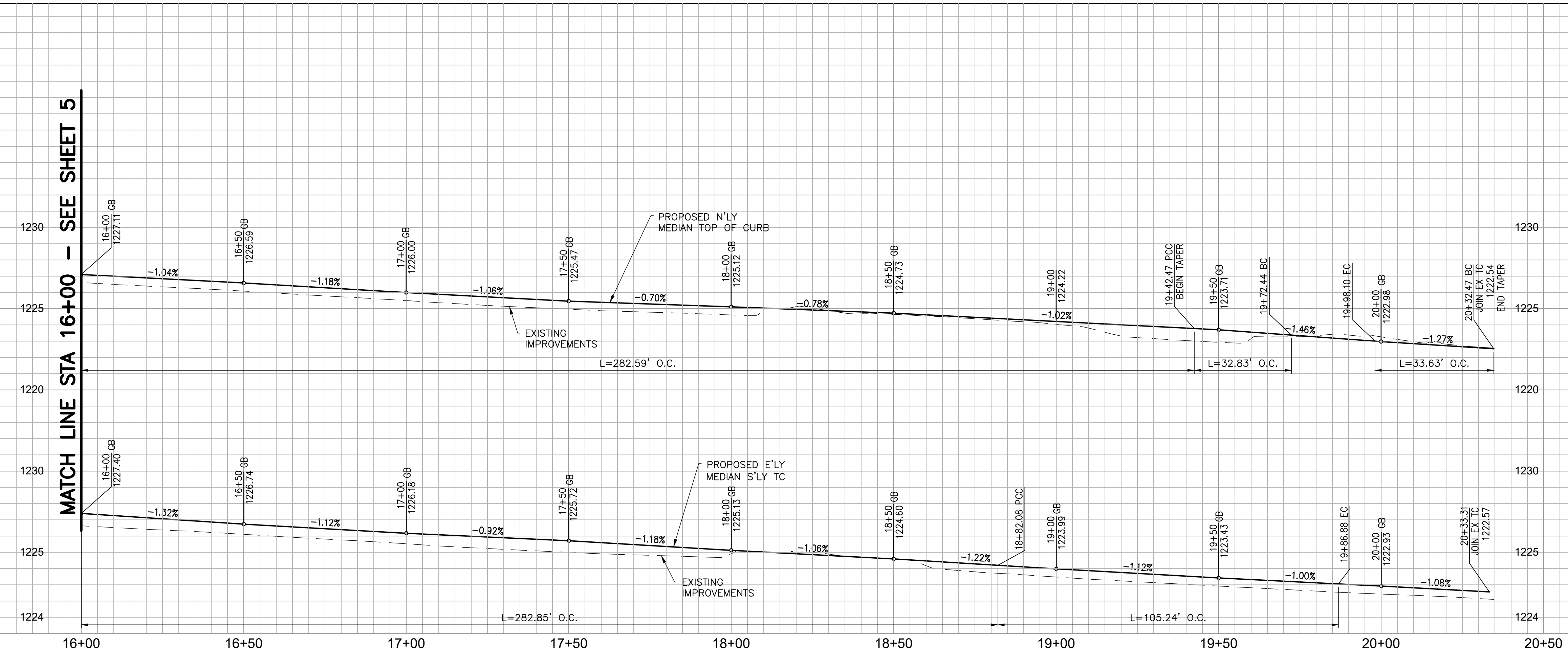
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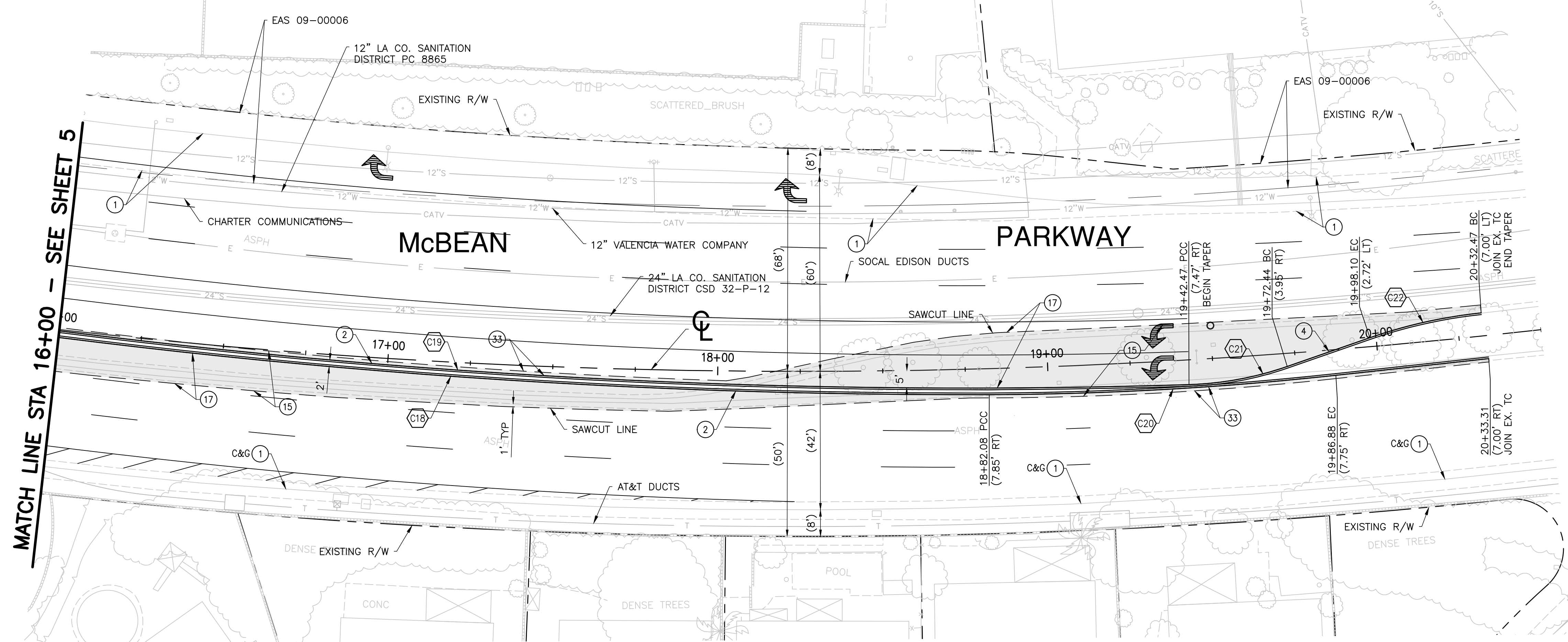
**CITY OF SANTA CLARITA**  
**STREET IMPROVEMENT PLANS**  
 STA. 11+50.00 TO STA. 16+00.00

SHEET:  
**5**  
 OF 22



CURVE DATA			
SEGMENT	RADIUS	DELTA	LENGTH
C18	2191.75'	8°20'13"	318.91'
C19	2189.75'	9°55'16"	379.17'
C20	1000.00'	6°01'47"	105.24'
C21	100.00'	17°24'27"	30.38'
C22	150.00'	13°13'46"	34.63'

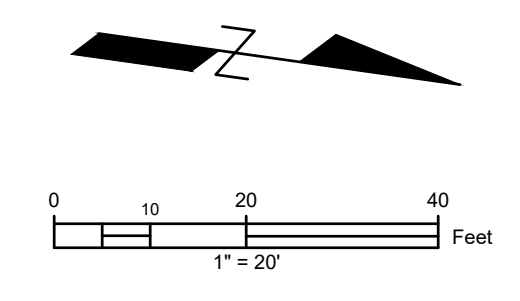
**PROFILE SCALE**  
 HORIZ.: 1" = 20'  
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**CONSTRUCTION NOTES**

- ① PROTECT IN PLACE
- ② CONSTRUCT TYPE A1-6 PCC CURB PER SPPWC STD. PLAN NO. 120-2
- ④ CONSTRUCT MEDIAN TAPER PER SPPWC STD. PLAN NO. 140-3
- ⑮ SAWCUT AND REMOVE EXISTING AC, CURB, GUTTER AND/OR LANDSCAPE
- \* ⑰ CONSTRUCT 7" ASPHALT CONCRETE OVER 8" AGGREGATE BASE OVER 6" COMPACTED SUBGRADE (90% COMPACTION)
- ③③ CONSTRUCT SLOT PAVE (2 SACK SLURRY) TO A DEPTH OF 12"

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 DATE: 9-17-20



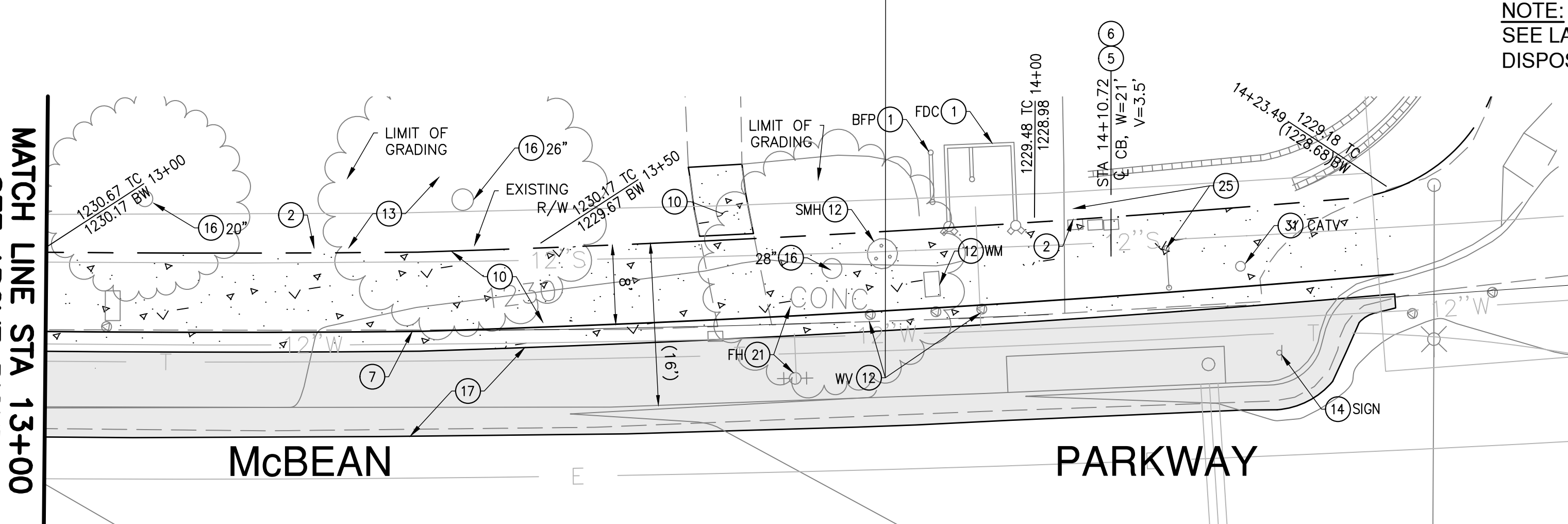
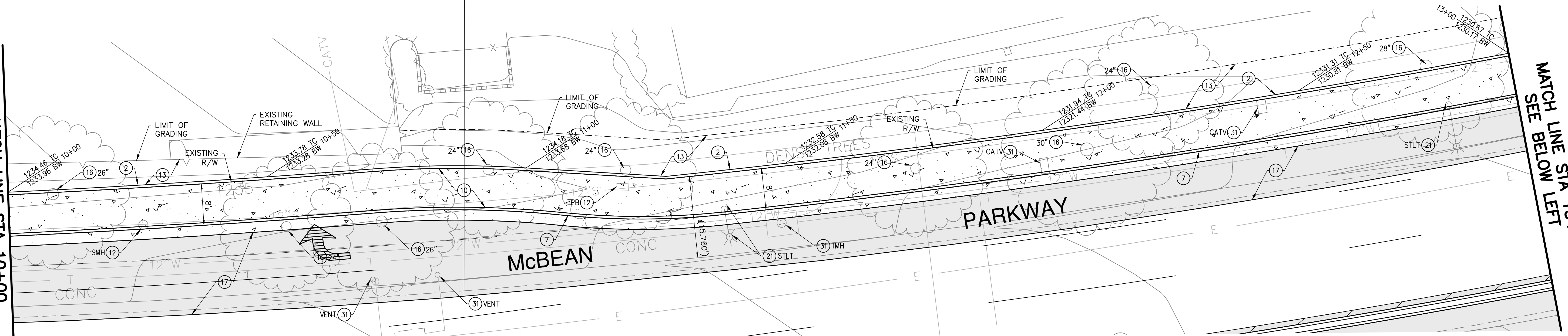
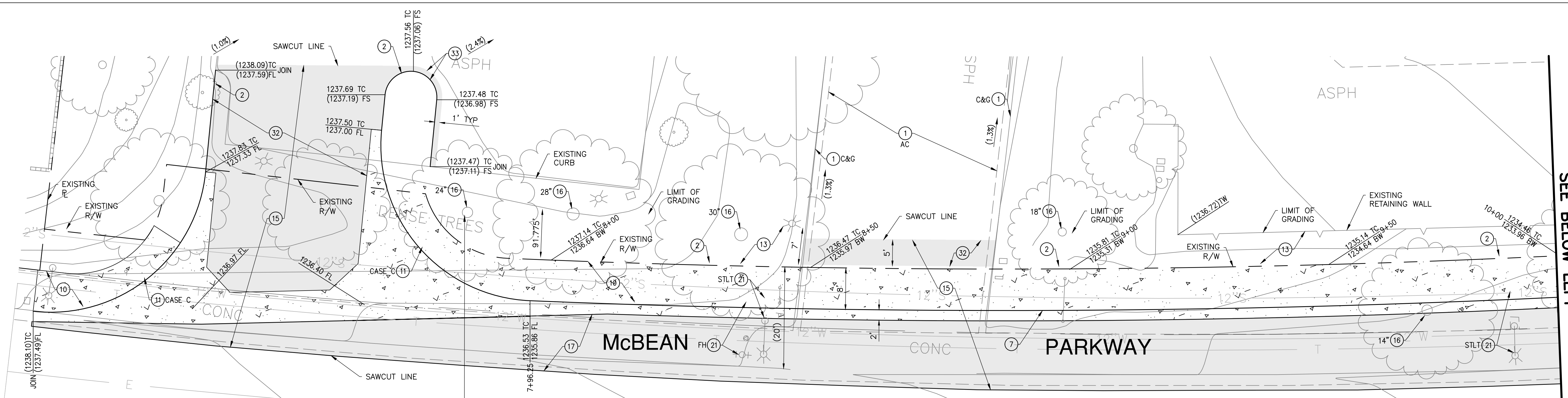
REVIEWED BY:  
 Adwan Sha 10/19/20  
 PLAN CHECKER DATE OF EXECUTION

APPROVED FOR CONSTRUCTION BY:  
 Mike Hennawy 10/19/2020  
 MIKE HENNAWY DATE OF EXECUTION  
 CITY ENGINEER

IN ACCORDANCE WITH RESOLUTION No. 19-6, AS A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF SANTA CLARITA DELEGATING DISCRETIONARY AUTHORITY TO THE DIRECTOR OF PUBLIC WORKS, DIRECTOR OF RECREATION AND COMMUNITY SERVICES, DIRECTOR OF NEIGHBORHOOD SERVICES, CITY ENGINEER, AND ASSISTANT CITY ENGINEERS TO APPROVE PLANS AND DESIGNS FOR THE CONSTRUCTION OF OR IMPROVEMENTS TO PUBLIC PROPERTY FOR PURPOSES OF DESIGN IMMUNITY PURSUANT TO GOVERNMENT CODE SECTION 830.6 FOR THE CITY OF SANTA CLARITA, I DO HEREBY EXERCISE THE DISCRETION DELEGATED TO ME AND APPROVE THE PLAN OR DESIGN, OR AMENDMENT OR MODIFICATION TO THE PLAN OR DESIGN, OR A CONSTRUCTION OF, OR AN IMPROVEMENT TO THE PUBLIC FACILITY, STRUCTURE, OR PROPERTY TO WHICH THIS STATEMENT, MY SIGNATURE AND DATE IS AFFIXED.

REVISION	REVISED BY (PRINT NAME)	CITY APPROVALS	EXECUTION DATE

**CITY OF SANTA CLARITA**  
**STREET IMPROVEMENT PLANS**  
 STA. 16+00.00 TO STA. 20+33.31



**NOTE:**  
SEE LANDSCAPE PLAN FOR  
DISPOSITION OF TREES

**CONSTRUCTION NOTES**

- 1 PROTECT IN PLACE
- 2 CONSTRUCT TYPE A1-6 PCC CURB PER SPPWC STD. PLAN NO. 120-2
- 5 CONSTRUCT CURB OPENING CATCH BASIN PER SPPWC STD. PLAN NO. 300-3 (W&V PER PLAN)
- 6 CONSTRUCT LOCAL DEPRESSION AT CATCH BASIN PER SPPWC STD. PLAN NO. 313-3 (CASE E, H=2")
- 7 CONSTRUCT A2-8 CURB AND GUTTER PER SPPWC STD. PLAN NO. 120-2
- 10 CONSTRUCT 4" THICK PCC SIDEWALK, JOINTS PER SPPWC STD. PLAN NO. 112-2
- 11 CONSTRUCT CURB RAMP PER CALTRANS STD. PLAN NO. AB8A (CASE PER PLAN). SEE SHEET 2 FOR DETAILS
- 12 ADJUST TO GRADE (ITEM PER PLAN)
- 13 REGRADE EXISTING LANDSCAPING TO NEW CURB ELEVATIONS. RESTORE IRRIGATION AND SOD
- 14 REMOVE, ITEM AS INDICATED
- 15 SAWCUT AND REMOVE EXISTING AC, CURB, GUTTER AND/OR LANDSCAPE
- 16 REMOVE EXISTING TREE (DIAMETER PER PLAN)
- \* 17 CONSTRUCT 7" ASPHALT CONCRETE OVER 8" AGGREGATE BASE OVER 6" COMPACTED SUBGRADE (90% COMPACTION)
- 21 RELOCATE, AS NOTED
- 25 REMOVE AND REPLACE EXISTING SIGN RELATIVE TO NEW CONSTRUCTION
- 31 RELOCATE BY OTHERS, ITEM AS INDICATED
- 32 CONSTRUCT ASPHALT CONCRETE TO MATCH EXISTING OR 6" MIN. OVER COMPACTED SUBGRADE (90% COMPACTION)
- 33 CONSTRUCT SLOT PAVE (2 SACK SLURRY) TO A DEPTH OF 12"

\* NOTE:  
REFER TO STRUCTURAL PAVEMENT INVESTIGATION REPORT PREPARED BY LA BELLE MARVIN DATED SEPTEMBER 2018, PROJECT NO. 43915. ACTUAL PAVEMENT SECTION TO BE DETERMINED AFTER R-VALUE TAKEN ONCE ROUGH GRADING IS COMPLETE.

UNDERGROUND SERVICE ALERT  
DIGALERT  
Call: TOLL FREE  
1-800-227-2600  
FOR WORKING DAYS BEFORE YOU DIG

PLANS PREPARED FOR:  
  
PLANS PREPARED BY:  
ANDERSON PENNA PARTNERS  
3737 BIRCH ST, SUITE 250  
NEWPORT BEACH, CA 92660  
P: (949) 428-1500  
F: (949) 258-5053

PLANS PREPARED UNDER THE DIRECTION OF:  
JOE R. BUCKNER RCE 45531  
9-17-20  
DATE



REVIEWED BY:  
Adwan Sha 10/19/20  
PLAN CHECKER DATE OF EXECUTION

APPROVED FOR CONSTRUCTION BY:  
SP Mike Hennawy 10/19/2020  
MIKE HENNAWY DATE OF EXECUTION  
CITY ENGINEER

IN ACCORDANCE WITH RESOLUTION No. 19-6, AS A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF SANTA CLARITA DELEGATING DISCRETIONARY AUTHORITY TO THE DIRECTOR OF PUBLIC WORKS, DIRECTOR OF RECREATION AND COMMUNITY SERVICES, DIRECTOR OF NEIGHBORHOOD SERVICES, CITY ENGINEER, AND ASSISTANT CITY ENGINEERS TO APPROVE PLANS AND DESIGNS FOR THE CONSTRUCTION OF OR IMPROVEMENTS TO PUBLIC PROPERTY FOR PURPOSES OF DESIGN IMMUNITY PURSUANT TO GOVERNMENT CODE SECTION 830.6 FOR THE CITY OF SANTA CLARITA, I DO HEREBY EXERCISE THE DISCRETION DELEGATED TO ME AND APPROVE THE PLAN OR DESIGN, OR AMENDMENT OR MODIFICATION TO THE PLAN OR DESIGN, OR A CONSTRUCTION OF, OR AN IMPROVEMENT TO THE PUBLIC FACILITY, STRUCTURE, OR PROPERTY TO WHICH THIS STATEMENT, MY SIGNATURE AND DATE IS AFFIXED.

REVISION	REVISED BY (PRINT NAME)	CITY APPROVALS	EXECUTION DATE

CITY OF SANTA CLARITA  
STREET IMPROVEMENT PLANS  
OFF SITE DETAILS  
SHEET:  
7  
OF 22



RECORDING REQUESTED BY  
AND WHEN RECORDED MAIL TO:

CITY OF SANTA CLARITA  
23920 Valencia Boulevard Suite 300  
Santa Clarita, CA 91355  
Attn: City Clerk

EXEMPT FROM RECORDERS FEES  
Pursuant to Government Code § 6103

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SPACE ABOVE THIS LINE FOR RECORDER'S USE ONLY

FIRST AMENDMENT TO  
DEVELOPMENT AGREEMENT

by and among

THE CITY OF SANTA CLARITA  
a municipal corporation

and

HENRY MAYO NEWHALL MEMORIAL HOSPITAL,  
a California non-profit public benefit corporation

and

HCN G&L VALENCIA SUB, LLC,  
a Delaware limited liability company

THIS FIRST AMENDMENT SHALL BE RECORDED WITHIN TEN DAYS  
OF EXECUTION BY ALL PARTIES HERETO PURSUANT TO  
THE REQUIREMENTS OF GOVERNMENT CODE §65868.5

FIRST AMENDMENT TO  
DEVELOPMENT AGREEMENT

This First Amendment to Development Agreement (the "First Amendment") is made this \_\_\_\_ day of \_\_\_\_\_, 2016 (the "Reference Date"), by and among the CITY OF SANTA CLARITA, a municipal corporation, organized and existing under the general laws of the State of California (the "City") and HENRY MAYO NEWHALL HOSPITAL, a California non-profit public benefit corporation, formerly known as Henry Mayo Newhall Memorial Hospital ("HMNH") and HCN G&L VALENCIA SUB, LLC, a Delaware limited liability company ("HCN Valencia"). HMNH and HCN Valencia are hereinafter sometimes collectively referred to as the "Developer." City and Developer are hereinafter sometimes collectively referred to as the "Parties" and each may be referred to as a "Party." The Parties enter into this First Amendment with reference to the following facts:

RECITALS

A. On October 19, 2015, pursuant to filings with the Secretary of State of the State of California, Henry Mayo Newhall Memorial Hospital became known as Henry Mayo Newhall Hospital. HMNH is the owner of certain real property located in the City of Santa Clarita, County of Los Angeles, State of California, that is legally described in Exhibit "A" to this First Amendment (the "HMNH Property"). HCN Valencia is the owner of certain real property, immediately adjacent to the HMNH Property, located in the City of Santa Clarita, County of Los Angeles, State of California, that is legally described in Exhibit "B" to this First Amendment (the "G&L Property"). HCN Valencia is the current successor to G&L Valencia, LLC, a California limited liability company ("G&L") as the fee owner of the G&L Property. The HMNH Property and the G&L Property are hereinafter sometimes collectively referred to as the "Campus Property."

B. The City, HMNH and G&L are parties to that certain Development Agreement, dated as of December 9, 2008 with respect to the future development of the Campus Property (the "Development Agreement"). The Development Agreement was recorded in the Official Records of Los Angeles County (the "Official Records") on February 2, 2009 as Document No. 09-135039.

C. Pursuant to the following agreements, to which the City and HMNH consented in writing, all rights, title and interest of G&L in the Development Agreement with respect to the G&L Property have been transferred and assigned to HCN Valencia and all of terms, conditions, covenants and obligations required to be kept, performed and fulfilled by G&L under the Development Agreement with respect to the G&L Property have been assumed by HCN Valencia:

(1) That certain Assignment and Assumption Agreement (Development Agreement) between G&L, as assignor, and Health Care REIT, Inc., a Delaware corporation ("HCN"), as assignee, dated as of September 24, 2014, which was recorded in the Official Records on September 26, 2014 as Document No. 20141017733; and

(2) That certain Assignment and Assumption Agreement (Development Agreement) between HCN, as assignor, and HCN Valencia, as assignee, dated as of September 24, 2014, which was recorded in the Official Records on September 26, 2014 as Document No. 20141024132.

D. Pursuant to Section 4.1 of the Development Agreement, during the Term of the Development Agreement, Developer was granted the right to develop on the Campus Property an aggregate of approximately 327,363 net new square feet of additional inpatient, outpatient, medical office and associated medical facilities and a new central plant building, as well as the provision of adequate parking facilities (the "Project") in accordance with: (i) the terms and conditions of the Project Approvals (as defined in the Development Agreement) and the Development Agreement; and (ii) the City Requirements (as defined in the Development Agreement) in effect on the Effective Date (as defined in the Development Agreement) of the Development Agreement. The Project Approvals include, without limitation, a Master Plan and the Conditions of Approval attached as Exhibits "A" and "B" thereto for the Campus Property (the "Master Plan"), which was approved by the City Council of the City by the adoption of Resolution No. 08-104 on November 20, 2008.

E. In connection with the approval by the City Council of the Project Approvals for the Project and the adoption of the resolution approving the Development Agreement, the City Council of the City certified Environmental Impact Report SCH#2004111149 for the Master Plan and the Development Agreement on November 19, 2008 (the "EIR") pursuant to the requirements of the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA").

F. As of the Reference Date of this First Amendment: (i) the Project Building (as defined in the Development Agreement) that is identified as "MOB1" in the Development Agreement has been developed and is fully occupied; (ii) the Project Structures (as defined in the Development Agreement) that are identified as "PS1" and "PS4" have been developed and are open for use; (iii) the Project Building identified as the "Central Plant" is under construction at 3,600 square feet, representing a 6,400 square foot reduction from that approved in the Master Plan, with a building height of 19 feet and 4 inches reduced from 26 feet; and (iv) a 8,872 square foot loading dock with an approximate height of 16 feet above ground is under construction.

G. One of the approved Project Buildings under the Development Agreement and the Master Plan includes an "Inpatient Building" on the HMNH Property at the location shown on the Site Plan attached as Exhibit "E" to the Development Agreement. The Master Plan provides that the Inpatient Building would have 125,363 square feet and a building in height of 85 feet to the top of the parapet and 100 feet in height to the top of the wind sock and elevator shaft. The Inpatient Building would also include a rooftop helipad (the "Helipad").

H. In 2011, the City adopted a General Plan Update, which resulted in a change in land use designation for the Campus Property from Residential Low (RL) to Public Institutional (PI).

I. In 2013, the City adopted a Zoning Code Update, which resulted in the Campus Property being rezoned from RL to PI.



J. Subsequent to approval of the Master Plan and Development Agreement, the California Building Standards Code was amended to impose mandatory new requirements related to design and construction of new hospitals. Compliance with the new regulations requires an increase in the square footage of the Inpatient Building to increase the number and width of exiting corridors between departments, provide two separate elevators for clean items delivered and soiled items hauled, and a dedicated corridor between food storage areas and the kitchen.

K. HMNH desires to expand its Obstetric Services Unit through construction of new Caesarean section rooms in order to accommodate the growing need for obstetric services in the Santa Clarita Valley and to better contract services from insurance providers. Two new Caesarean sections rooms are anticipated to be constructed. This expansion requires an increase in square footage of the Inpatient Building.

L. The Helipad is to be used for incoming flights for trauma and other transfers to HMNH and departing flights for specialty care, such as advanced neonatal intensive-care unit and seriously ill pediatric patients. HMNH desires to relocate the Helipad from the front to the rear of the roof of the Inpatient Building in order to accommodate direct elevator access between the Helipad and the emergency room and comply with Federal Aviation Administration (FAA) and State required airspace obstruction clearance criteria for operational safety.

M. In order to modify the Master Plan to implement the mandatory new California Building Standards Code requirements, permit the additional Caesarean section rooms and relocation of the Helipad, it was determined that as well as amending the Master Plan and the Development Agreement, a Specific Plan was needed to outline existing uses, including those approved under the Master Plan, as amended, future uses approved under the Master Plan, as amended, as well as to include the amendments to the Master Plan, and provide a cohesive set of standards and guidelines for how the amendments would occur on the Campus Property.

N. HMNH has submitted applications to the City (collectively, the "Project Applications"), with the consent of HCN Valencia, to (i) amend the General Plan to change the Campus Property's land use designation from Public Institutional (PI) to Specific Plan (SP), (ii) amend the Santa Clarita Zoning Code to change the Campus Property's zone from PI to SP, (iii) adopt a Specific Plan, (iv) amend the Master Plan, and (v) amend the Development Agreement, for the following purposes:

(1) Increase the square footage of the Inpatient Building from 125,363 square feet to 162,329 square feet (an increase of 36,966 square feet);

(2) Modify the aggregate square footage increase of the Project from 327,363 square feet to 357,929 square feet (a net increase of 30,566 square feet) to reflect the net increase in square footage in the Project that would result from the increase in the square footage of the Inpatient Building and the decrease in the square footage of the Central Plant.

(3) Relocate the Helipad from the front to the rear of the roof of the Inpatient Building.

O. Under the Project Applications no increases in the permitted building height of the Inpatient Building is requested, and no new uses, increases in the density or intensity of existing uses or the number of programs, employees, licensed beds overall on campus or square footage allocated to administration uses in the Inpatient Building, are requested. HMNH has paid all necessary costs and fees associated with the filing and the City's processing of the Project Applications.

P. The Project Applications are more fully described in the Addendum to the Henry Mayo Newhall Memorial Hospital Master Plan Certified Environmental Impact Report SCH#2004111149, dated March \_\_, 2016 (the "EIR Addendum") prepared by Dudek. The EIR Addendum contains findings that: (i) the proposed Project Applications require only minor, technical changes in the EIR; and (ii) the proposed Project Applications do not trigger any of the conditions identified in Section 15162 of the CEQA Guidelines, 14 Cal. Code Regs. Section 15000 et seq., that would require a subsequent or supplemental EIR.

Q. Pursuant to Section 4.2.5 of the Development Agreement, any increase in the overall square footage of the Project requires an amendment to the Development Agreement which must be reviewed by the Planning Commission of the City and approved by the City Council of the City.

R. The Planning Commission of the City and the City Council of the City have given notice of their intention to consider the Project Applications and the Addendum, have conducted public hearings thereon pursuant to the California Government Code and the Santa Clarita Municipal Code, and the City Council has found that the Project Applications and the Project (as revised by the Project Applications) are: (i) consistent with the General Plan, adopted plans, codes, ordinances and policies of the City; (ii) consistent with all other ordinances, resolutions, rules, regulations, laws, plans and policies applicable to the Project; and (iii) in the best interest of the health, safety and general welfare of the City, its residents, and the general public.

S. On \_\_\_\_\_, 2016, after conducting a public hearing and after considering all evidence, both oral and written, submitted for the record, all appropriate documentation and circumstances, and making all required findings, the Planning Commission of the City adopted resolutions recommending that the City Council: (a) approve the EIR Addendum for the Project; and (b) approve the Project Applications.

T. On \_\_\_\_\_, 2016, after conducting a public hearing and after consideration of the Planning Commission's recommendation and all evidence, both written and oral, submitted for the record, all appropriate documentation and circumstances, and making all required findings, the City Council of the City adopted the following measures (collectively, the "Project Amendments"):

1. Resolution No. 16- \_\_\_\_\_ (adopted, \_\_\_\_\_ 2016),  
Approving the EIR Addendum for the Project Applications
2. Resolution No. 16- \_\_\_\_\_ (adopted, \_\_\_\_\_ 2016),  
Approving the General Plan Amendment.

3. Ordinance No 16- \_\_\_\_\_ (adopted, \_\_\_\_\_ 2016),  
Adopting the Zone Change.
4. Ordinance No. 16- \_\_\_\_\_ (adopted \_\_\_\_\_, 2016),  
Adopting the First Amendment to the Development Agreement.
5. Resolution No. 16- \_\_\_\_\_ (adopted \_\_\_\_\_, 2016),  
Approving the First Amendment to the Master Plan.

NOW, THEREFORE, with reference to the above Recitals, and in consideration of the mutual covenants and agreements contained in this First Amendment, the City and the Developer agree as follows:

### **AGREEMENT**

1. Recitals. Each and all of the foregoing recitals of background facts are incorporated herein by this reference as though set forth herein verbatim.

2. Definition of Terms Used Herein. The capitalized terms in this First Amendment shall have the meanings set forth in the Development Agreement unless otherwise expressly indicated herein.

3. Amendments to Development Agreement. The Development Agreement is hereby amended as follows:

3.1 The overall square footage of the Project is hereby increased to 357,929 square feet. The maximum square footage of the Inpatient Building is hereby increased to 162,329 square feet.

3.2 The Site Plan for the Project attached as Exhibit "C" to this First Amendment shall replace and supersede the Site Plan that was attached as Exhibit "E" to the Development Agreement.

4. Notices to HCN Valencia. Section 18 of the Development Agreement is hereby amended to delete the addresses for notices to G&L and to add the following addresses for notices to HCN Valencia:

If to HCN Valencia:  
HCN G&L Valencia Sub, LLC  
c/o Welltower Inc.  
4500 Dorr Street  
Toledo, OH 43615-4040  
Attn.: Charles W. Hiller, Esq.

With a copy to:  
Joseph A. Rideout, Esq.  
Shumaker, Loop & Kendrick LLP  
1000 Jackson Street  
Toledo, OH 43604

5. Successors and Assigns. The provisions of this First Amendment shall be binding upon and inure to the benefit of the Parties, and any subsequent owners of all or any portion of the Campus Property and their respective successors and assigns. Any successors in interest to the City shall be subject to the provisions set forth in Sections 65865.4 and 65868.5 of the Government Code.

6. Constructive Notice and Acceptance. Every person who, now or hereafter, owns or acquires any right, title or interest in or to any portion of the Campus Property is, and shall be, conclusively deemed to have consented and agreed to every provision contained herein, whether or not any reference to this First Amendment is contained in the instrument by which such person acquired an interest in the Campus Property.

7. Counterparts. This First Amendment may be executed in two or more identical counterparts, each of which shall be deemed to be an original and each of which shall be deemed to be one and the same instrument when each Party signs each such counterpart.

8. Incorporation of Attachments. All attachments to this First Amendment, including all Exhibits referenced herein, and all subparts thereto, are incorporated herein by this reference.

9. Full Force and Effect. Except to the extent specifically amended in this First Amendment, all other terms, provisions and conditions of the Development Agreement shall remain unmodified and in full force and effect. In the event of any conflict between the provisions of the Development Agreement and the provisions of this First Amendment, the provisions of this First Amendment shall control.

IN WITNESS WHEREOF, the Parties hereto have duly executed this First Amendment.

"CITY"  
CITY OF SANTA CLARITA,  
a municipal corporation

Dated: \_\_\_\_\_, 2016

By: \_\_\_\_\_  
City Manager

ATTEST:  
  
\_\_\_\_\_  
City Clerk

APPROVED AS TO FORM:  
  
\_\_\_\_\_  
City Attorney

"HMNH"  
HENRY MAYO NEWHALL HOSPITAL,  
a California nonprofit public benefit corporation

Dated: \_\_\_\_\_, 2016

By: \_\_\_\_\_  
Roger E. Seaver  
President/CEO

"HCN VALENCIA"  
HCN G&L VALENCIA SUB, LLC,  
a Delaware limited liability company

Dated: \_\_\_\_\_, 2016

By: \_\_\_\_\_  
Its: Authorized Signatory

**ACKNOWLEDGMENT**

A notary public or other officer completing this certificate verifies only the identity of the individual who signed the document to which this certificate is attached, and not the truthfulness, accuracy, or validity of that document.

STATE OF OHIO )  
 ) ss:  
COUNTY OF LUCAS )

The foregoing instrument was acknowledged before me this \_\_\_\_ day of \_\_\_\_\_, 2016, by \_\_\_\_\_, the Authorized Signatory of HCN G&L Valencia Sub, LLC, a Delaware limited liability company, on behalf of the company.

\_\_\_\_\_

Notary Public

My Commission Expires: \_\_\_\_\_

[Seal]

ACKNOWLEDGMENT

A notary public or other officer completing this certificate verifies only the identity of the individual who signed the document to which this certificate is attached, and not the truthfulness, accuracy, or validity of that document.

STATE OF CALIFORNIA )  
 ) ss:  
COUNTY OF LOS ANGELES )

On \_\_\_\_\_ before me, \_\_\_\_\_  
Notary Public (insert name and title of the officer),

personally appeared \_\_\_\_\_, who proved to me on the basis of satisfactory evidence to be the person(s) whose name(s) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing paragraph is true and correct.

WITNESS my hand and official seal.

Signature: \_\_\_\_\_

[Seal]

ACKNOWLEDGMENT

A notary public or other officer completing this certificate verifies only the identity of the individual who signed the document to which this certificate is attached, and not the truthfulness, accuracy, or validity of that document.

STATE OF CALIFORNIA )  
 ) ss:  
COUNTY OF LOS ANGELES )

On \_\_\_\_\_ before me, \_\_\_\_\_  
Notary Public (insert name and title of the officer),

personally appeared \_\_\_\_\_, who proved to me on the basis of satisfactory evidence to be the person(s) whose name(s) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing paragraph is true and correct.

WITNESS my hand and official seal.

Signature: \_\_\_\_\_

[Seal]



EXHIBIT "A"  
Legal Description of HMNH Property

PARCELS 1 AND 2 OF PARCEL MAP NO. 3083, IN THE CITY OF SANTA CLARITA, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP FILED IN BOOK 45, PAGE 91 OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY, EXCEPTING THEREFROM THE FOLLOWING DESCRIBED PARCEL:

BEGINNING AT THE MOST SOUTHERLY CORNER OF SAID PARCEL 1, SAID CORNER LYING ON THE NORTHWESTERLY LINE OF MC BEAN PARKWAY, 100.00 FEET WIDE, SAID NORTHWESTERLY LINE ALSO BEING A CURVE, CONCAVE NORTHWESTERLY AND HAVING A RADIUS OF 1950.00 FEET; THENCE NORTHWESTERLY ALONG THE SOUTHWESTERLY LINE OF SAID PARCEL 1, NORTH 19 DEGREES 37 MINUTES 08 SECONDS WEST, 551.90 FEET, SAID SOUTHWESTERLY LINE ALSO BEING A RADIAL LINE TO SAID CURVE; THENCE NORTH 42 DEGREES 30 MINUTES 21 SECONDS EAST, 510.04 FEET; THENCE SOUTH 47 DEGREES 43 MINUTES 44 SECONDS EAST, 103.96 FEET; THENCE SOUTH 42 DEGREES 24 MINUTES 24 SECONDS EAST, 37.17 FEET; THENCE SOUTH 38 DEGREES 57 MINUTES 00 SECONDS EAST, 24.83 FEET; THENCE SOUTH 51 DEGREES 19 MINUTES 23 SECONDS WEST, 20.00 FEET; THENCE SOUTH 38 DEGREES 35 MINUTES 36 SECONDS EAST, 108.15; THENCE SOUTH 33 DEGREES 01 MINUTES 43 SECONDS EAST, 45.04 FEET; THENCE NORTH 51 DEGREES 32 MINUTES 59 SECONDS EAST, 18.85 FEET; THENCE SOUTH 38 DEGREES 36 MINUTES 22 SECONDS EAST, 118.74 FEET; THENCE NORTH 51 DEGREES 40 MINUTES 23 SECONDS EAST, 24.00 FEET; THENCE SOUTH 41 DEGREES 24 MINUTES 03 SECONDS EAST, 250.71 FEET; THENCE SOUTH 03 DEGREES 50 MINUTES 00 SECONDS EAST, 21.88 FEET TO A POINT ON SAID NORTHWESTERLY LINE OF MC BEAN PARKWAY, 100.00 FEET WIDE AND SAID CURVE, A RADIAL LINE TO SAID CURVE BEARS NORTH 41 DEGREES 08 MINUTES 36 SECONDS WEST; THENCE SOUTHWESTERLY ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 21 DEGREES 31 MINUTES 28 SECONDS FOR AN ARC LENGTH OF 732.56 FEET TO THE POINT OF BEGINNING FOR THIS DESCRIPTION.

EXHIBIT "B"  
Legal Description of [G&L Property]

THOSE PORTIONS OF PARCELS 1 AND 2 OF PARCEL MAP NO. 3083, IN THE CITY OF SANTA CLARITA, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP FILED IN BOOK 45, PAGE 91 OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY, BEING DESCRIBED AS FOLLOWS:

BEGINNING AT THE MOST SOUTHERLY CORNER OF SAID PARCEL 1, SAID CORNER LYING ON THE NORTHWESTERLY LINE OF MC BEAN PARKWAY, 100.00 FEET WIDE, SAID NORTHWESTERLY LINE ALSO BEING A CURVE, CONCAVE NORTHWESTERLY AND HAVING A RADIUS OF 1950.00 FEET; THENCE NORTHWESTERLY ALONG THE SOUTHWESTERLY LINE OF SAID PARCEL 1, NORTH 19 DEGREES 37 MINUTES 08 SECONDS WEST, 551.90 FEET, SAID SOUTHWESTERLY LINE ALSO BEING A RADIAL LINE TO SAID CURVE; THENCE NORTH 42 DEGREES 30 MINUTES 21 SECONDS EAST, 510.04 FEET; THENCE SOUTH 47 DEGREES 43 MINUTES 44 SECONDS EAST, 103.96 FEET; THENCE SOUTH 42 DEGREES 24 MINUTES 24 SECONDS EAST, 37.17 FEET; THENCE SOUTH 38 DEGREES 57 MINUTES 00 SECONDS EAST, 24.83 FEET; THENCE SOUTH 51 DEGREES 19 MINUTES 23 SECONDS WEST, 20.00 FEET; THENCE SOUTH 38 DEGREES 35 MINUTES 36 SECONDS EAST, 108.15; THENCE SOUTH 33 DEGREES 01 MINUTES 43 SECONDS EAST, 45.04 FEET; THENCE NORTH 51 DEGREES 32 MINUTES 59 SECONDS EAST, 18.85 FEET; THENCE SOUTH 38 DEGREES 36 MINUTES 22 SECONDS EAST, 118.74 FEET; THENCE NORTH 51 DEGREES 40 MINUTES 23 SECONDS EAST, 24.00 FEET; THENCE SOUTH 41 DEGREES 24 MINUTES 03 SECONDS EAST, 250.71 FEET; THENCE SOUTH 03 DEGREES 50 MINUTES 00 SECONDS EAST, 21.88 FEET TO A POINT ON SAID NORTHWESTERLY LINE OF MC BEAN PARKWAY, 100.00 FEET WIDE AND SAID CURVE, A RADIAL LINE TO SAID CURVE BEARS NORTH 41 DEGREES 08 MINUTES 36 SECONDS WEST; THENCE SOUTHWESTERLY ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 21 DEGREES 31 MINUTES 28 SECONDS FOR AN ARC LENGTH OF 732.56 FEET TO THE POINT OF BEGINNING FOR THIS DESCRIPTION.

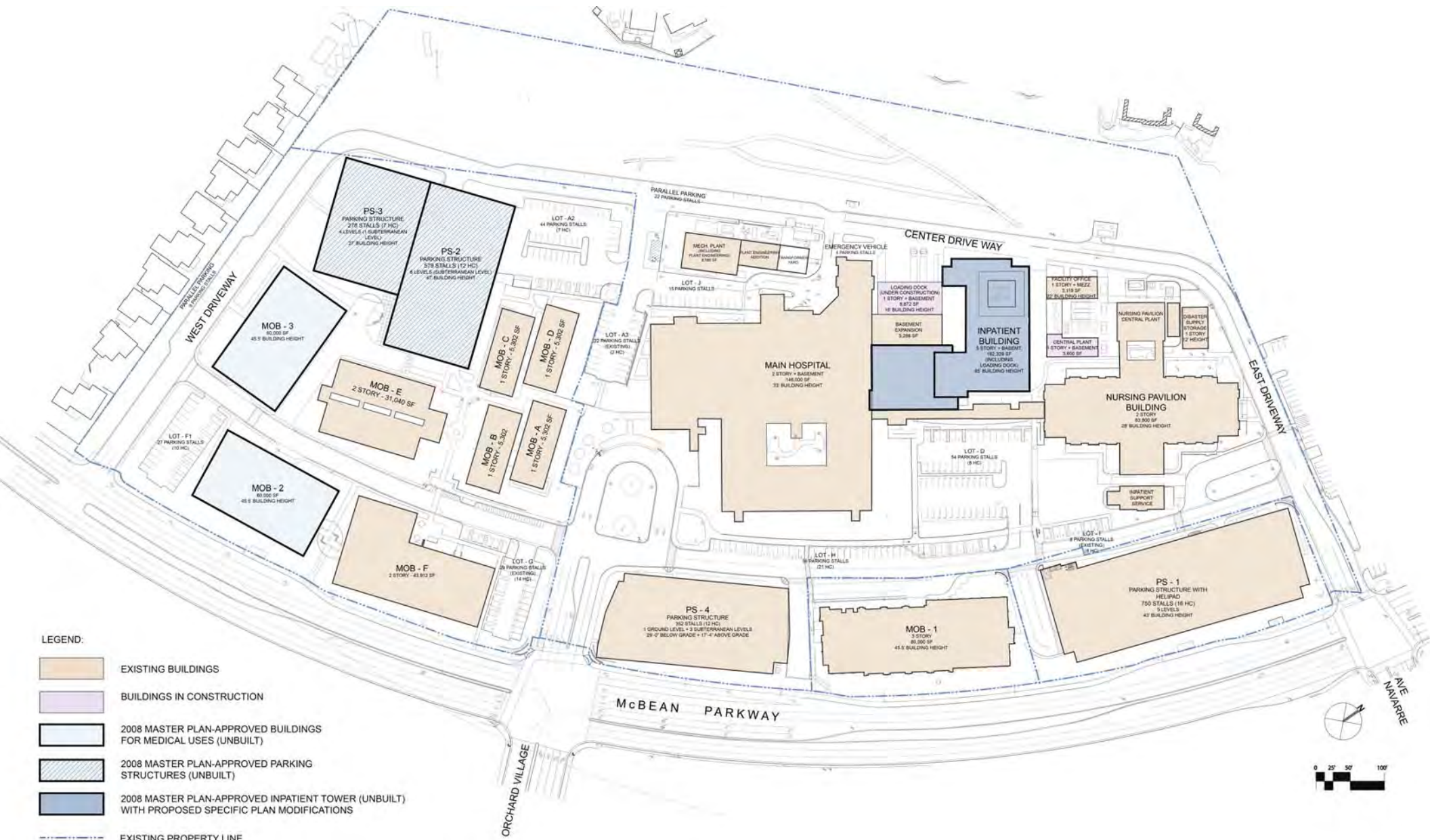
EXHIBIT "C"  
Site Plan  
Page C-1

- LEGEND:**
- EXISTING BUILDINGS
  - BUILDINGS IN CONSTRUCTION
  - 2008 MASTER PLAN-APPROVED BUILDINGS FOR MEDICAL USES (UNBUILT)
  - 2008 MASTER PLAN-APPROVED PARKING STRUCTURES (UNBUILT)
  - 2008 MASTER PLAN-APPROVED INPATIENT TOWER (UNBUILT) WITH PROPOSED SPECIFIC PLAN MODIFICATIONS
  - EXISTING PROPERTY LINE

## SPECIFIC PLAN

HENRY MAYO NEWHALL HOSPITAL  
23845 McBean Parkway, Santa Clarita, CA 91355

MARCH 16, 2016



RECORDING REQUESTED BY:

AND WHEN RECORDED MAIL TO:

CITY OF SANTA CLARITA  
23920 Valencia Boulevard  
Suite 300  
Santa Clarita, CA 91355  
Attn: City Clerk

EXEMPT FROM RECORDER'S FEES  
Pursuant to Government Code  
§ 6103  
ONLY

SPACE ABOVE THIS LINE FOR RECORDER'S USE

**EXHIBIT A**

DEVELOPMENT AGREEMENT

by and between

THE CITY OF SANTA CLARITA  
a municipal corporation

and

HENRY MAYO NEWHALL MEMORIAL HOSPITAL,  
a California non-profit public benefit corporation

and

G&L VALENCIA, LLC  
a California limited liability company

THIS AGREEMENT SHALL BE RECORDED WITHIN TEN DAYS  
OF EXECUTION BY ALL PARTIES HERETO PURSUANT TO  
THE REQUIREMENTS OF GOVERNMENT CODE §65868.5

## DEVELOPMENT AGREEMENT

This Development Agreement (the “**Agreement**”) is made this 9th day of December, 2008, by and between the CITY OF SANTA CLARITA, a municipal corporation, organized and existing under the general laws of the State of California (the “**City**”) and HENRY MAYO NEWHALL MEMORIAL HOSPITAL, a California non-profit public benefit corporation (“**HMNMH**”) and G&L VALENCIA, LLC, a California limited liability company (“**G&L**”). HMNMH and G&L are hereinafter sometimes collectively referred to as the “**Developer**”. City and Developer are hereinafter sometimes collectively referred to as the “**Parties**” and each may be referred to as a “**Party**”.

### RECITALS

A. Pursuant to Section 65864 through 65869.5 of the California Government Code (the “**Government Code**”) and Section 17.03.010 of the Santa Clarita Municipal Code (the “**Santa Clarita Code**”), the City is authorized to enter into binding development agreements with persons having legal or equitable interest in real property for the development of such real property.

B. The Hospital is the owner of certain real property located in the City of Santa Clarita, County of Los Angeles, State of California, that is legally described in Exhibit “A” to this Agreement and is diagrammed on Exhibit “C” to this Agreement (the “**HMNMH Property**”). G&L is the owner of certain real property, immediately adjacent to the HMNMH Property, located in the City of Santa Clarita, County of Los Angeles, State of California, that is legally described in Exhibit “B” to the Agreement and is diagrammed on Exhibit “C” to this Agreement (the “**G&L Property**”). The HMNMH Property and the G&L Property are sometimes collectively referred to as the “**Campus Property**”. The Campus Property contains approximately 30.4 acres.

C. As of the Effective Date (as hereinafter defined) of this Agreement:

(1) Existing Campus Facilities & Operations

HMNMH is a full-service community hospital that provides advanced life support services on approximately 30.4 acres of land. A number of medical office buildings, both on- and off-site, provide support to the hospital facility. Currently, there are six existing medical office buildings, along with the foundation building.

Specifically, the existing 340,071 square foot HMNMH medical campus is comprised of the following facilities:

- (a) The main hospital facility comprises approximately half of the on-site buildings with 146,000 square feet. There are 121 beds currently in this facility along with the Emergency Department. A basement expansion totals 5,286 square feet.

- (b) The Nursing Pavilion totals 63,800 square feet with a maximum capacity of 109 beds.
  - (c) The Mechanical Plant and Facilities Building comprise 11,703 square feet and include the mechanical operations of the hospital in these two structures.
  - (d) There are six medical office buildings on the western portion of the campus. These office buildings comprise 96,160 square feet of floor area.
  - (e) The Hospital Foundation currently occupies 8,000 square feet of modular office space on the far western portion of the site.
  - (f) A 9,122 square-foot hospital bridge which links the main hospital building to the Nursing Pavilion.
- (2) Constructed Improvements.

Certain improvements recently have been constructed (collectively, the **“Constructed Improvements”**), including: (a) the construction of a new facilities building; and (b) a remodel of the plant engineering building.

The Current Improvements and the Constructed Improvements, are hereinafter sometimes collectively referred to as the **“Existing Improvements”**. The Existing Improvements are depicted on the site plan that is attached as Exhibit “D” to this Agreement.

D. The following applications (collectively, the **“Project Applications”**) have been filed by Developer with the City for the proposed development on the Campus Property of approximately 327,363 net new square feet of additional inpatient, outpatient, medical office and associated medical facilities and a new central plant building, as well as the provision of adequate parking facilities (the **“Project”**) in order to provide enhanced inpatient and outpatient treatment capacity:

- (1) An Application for a Master Plan (the **“Master Plan”**) pursuant to Santa Clarita Municipal Code Section 17.03.025.
- (2) An application for a Development Agreement, pursuant to Santa Clarita Code Section 17.03.010.

Developer has paid all necessary costs and fees associated with the filing and the City’s processing of the Project Applications.

E. The locations of the new buildings (sometimes collectively, the **“Project Buildings”**) and the parking structures (sometimes, collectively, the **“Parking Structures”**) of the Project along with the maximum heights for each are depicted on the site plan that is attached

as Exhibit "E" to this Agreement (the "**Site Plan**"). The Project Buildings and the Parking Structures are hereinafter sometimes collectively referred to as the "**Project Improvements**". The Project Buildings consist of a new central plant building, three medical buildings labeled as "MOB1", "MOB2" and "MOB3" on the Site Plan, and an inpatient hospital building labeled as "Inpatient Building" on the Site Plan. The Parking Structures consist of four structures labeled as "PS1", "PS2", "PS3", and "PS4" on the Site Plan. The "**Medical Buildings**" consist of the three medical buildings labeled as "MOB1", "MOB2" and "MOB3" on the Site Plan.

F. The Project is more fully described in the Henry Mayo Newhall Memorial Hospital Master Plan Final Environmental Impact Report (the "**EIR**") prepared by RBF Consulting pursuant to the requirements of the California Environmental Quality Act (Public Resources Code Section 21000 *et seq.*) and the Guidelines thereunder (14 California Code of Regulations Section 15000, *et seq.* (collectively, "**CEQA**").

G. Based upon representations of the Developer, the new Inpatient Building and three new Medical Buildings contemplated as Project Improvements will allow HMNMH to provide expanded and additional medical services not currently provided on the Campus Property.

H. The Planning Commission and the City Council of the City have given notice of their intention to consider the Project Applications, have conducted public hearings thereon pursuant to the Government Code and the Santa Clarita Code, and the City Council has found that the Project Approvals and the Project are (i) consistent with the General Plan, adopted plans, codes, ordinances and policies of the City, (ii) consistent with all other ordinances, resolutions, rules, regulations, laws, plans and policies applicable to the Campus Project, and (iii) in the best interest of the health, safety and general welfare of the City, its residents, and the general public.

I. On February 6, 2007, at a public meeting and after considering all appropriate documentation and circumstances, the Planning Commission of the City adopted resolutions recommending that the City Council: (a) certify the EIR for the Project; (b) approve the Master Plan; and (c) adopt a Statement of Overriding Considerations pursuant to Section 21081(a)(3) of CEQA with respect to Aesthetic, Light and Glare, Traffic and Circulation, Solid Waste and Air Quality. At that same meeting, the Planning Commission voted to reject an applied for Development Agreement, which denial was subsequently appealed by the Developer to the City Council.

J. On November 20, 2008 and December 9, 2008, at a public meeting and after considering all appropriate documentation and circumstances, and making all required findings, the City Council of the City adopted the following measures (collectively, the "**Project Approvals**"):

- (1) Resolution No. 08-101 (adopted November 20, 2008), certifying the EIR for the Project and adopting a Statement of Overriding Considerations pursuant to Section 21081(a)(3) of CEQA with respect to Solid Waste, Air Quality, Cumulative Global Climate Change and Construction Noise;



- (2) Resolution No. 08-102 (adopted November 20, 2008), approving the Master Plan with the Conditions of Approval that are attached as Exhibit "A" thereto (the "Conditions of Approval"); and
- (3) Ordinance No. 08-17 (adopted December 9, 2008), adopting this Agreement (as modified subsequent to denial by the Planning Commission, consistent with Santa Clarita Municipal Code Section 17.03.010F) for the Project.

K. The Hospital is a full-service community hospital. It is the only hospital in the Santa Clarita Valley and serves a population of more than 250,000 people within a 680 square-mile trauma service area. The Hospital currently operates the only 24-hour Emergency Department and trauma service in the Santa Clarita Valley.

L. The City Council has determined that a development agreement is appropriate for the proposed development of the Campus Property. This Agreement shall: (1) eliminate uncertainty in planning for, and securing orderly development of, the Project; (2) assure installation of necessary on-site and off-site improvements; (3) provide for public infrastructure and services appropriate to development of the Project; (4) allow the development of new, needed medical services in cardiac care, neonatal and high risk pregnancy care, intensive care and surgical care; (5) create new long-term employment opportunities in high paying healthcare jobs to add to the 1,200 people currently employed by HMNMH, creating a positive economic benefit to the community; (6) improve the community's readiness for disaster by adding capability for support of other first responder agencies and dedicating additional permanent physical assets for emergency services; and (7) otherwise achieve the goals and purposes for which Government Code Sections 65864 et seq., and Section 17.03.010 of the Santa Clarita Municipal Code were enacted.

M. The City Council, in its adoption of Ordinance No. 08-17, has made all of the findings with respect to this Agreement that are required under Section 17.03.010E of the Santa Clarita Municipal Code.

NOW, THEREFORE, with reference to the above Recitals, and in consideration of the mutual covenants and agreements contained in this Agreement, the City and the Developer agree as follows:

#### A G R E E M E N T

1. Interest of Developer. HMNMH represents to the City that, as of the Effective Date of this Agreement, HMNMH owns the HMNMH Property in fee, subject only to encumbrances, easements, covenants, conditions, restrictions, and other matters of record. G&L represents to the City that, as of the Effective Date of this Agreement, G&L owns the G&L Property in fee, subject only to encumbrances, easements, covenants, conditions, restrictions, and other matters of record.

2. Binding Effect. All the terms and conditions of this Agreement shall bind and run with the Campus Property and shall be binding upon and inure to the benefit of the Parties and

their respective assigns and other successors in interest. Nothing in this Agreement is a dedication or transfer of any right or interest in, or creating a lien upon, the Campus Property.

3. Negation of Agency. The development of the Project is a private and not a public sector development nor is it receiving public funding, neither party is acting as the agent of the other in any respect hereunder, and each party is an independent contracting entity with respect to the terms, covenants and conditions contained in this Agreement. No partnership, joint venture or other association of any kind is formed by this Agreement. The only relationship between the City and the Developer is that of a government entity regulating the development of private property by the owner of such property.

4. Development of the Project.

4.1 Applicable Rules. Except for such changes as may in the future be mutually agreed upon between the City and Developer or as specified in Section 4.4, Developer shall have the right to develop the Project during the Term (as hereinafter defined) of this Agreement in accordance with the following (collectively, the "**Applicable Rules**"): (a) the terms and conditions of the Project Approvals; (b) the terms and conditions of this Agreement; (c) the Santa Clarita Code and all rules, regulations and official policies of the City governing development, subdivision and zoning (sometimes, collectively, the "**City Requirements**"), that are in effect as of December 9, 2008, the date on which the Ordinance approving this Agreement was adopted by the City Council (the "**Effective Date**"). The City Requirements include requirements governing building height, maximum floor area, permitted and conditionally permitted uses, maximum lot coverage, building setbacks and stepbacks, landscaping, exactions and dedications, and design criteria. In the event of any conflict between the provisions in this Agreement, the Project Approvals and the City Requirements, such conflict shall be resolved in the following order of priority: (i) first, this Agreement; (ii) then, the Project Approvals; and (iii) finally, the City Requirements. Notwithstanding the foregoing, Applicable Rules shall include building code provisions in effect at the time of construction and subdivision map act provisions in effect at the time of any map application submittal. The rules of the City as of the Effective Date shall be subject to the reasonable interpretation of the City's Director of Community Development.

4.2 Administrative Changes and Amendments. The parties acknowledge that further planning and development of the Project may demonstrate that refinements and changes are appropriate with respect to the details and performance of the parties under this Agreement. The parties desire to retain a certain degree of flexibility with respect to the details of the development of the Project and with respect to those items covered in general terms under this Agreement. If and when the parties find that Minor Changes (as hereinafter defined) are necessary or appropriate, they shall, unless otherwise required by law, effectuate such changes or adjustments through administrative amendments executed by the Developer and the City Manager or his or her designee, which, after execution, shall be attached hereto as addenda and become a part hereof, and may be further changed and amended from time to time as necessary, with approval by the City Manager and the Developer. In the event that the Developer and the City Manager cannot agree on whether certain changes proposed by Developer constitute Minor Changes or whether such proposed changes are necessary or appropriate, the Developer shall

have the right to appeal the determination of the City Manager to the Planning Commission and shall have the further right to appeal any determination of the Planning Commission to the City Council. The term “**Minor Changes**” means changes, modifications or adjustments which are consistent with the overall intent of the Project Approvals and which do not materially alter the overall nature, scope, or design of the Project, including, without limitation, minor changes in locations of the Project Improvements or infrastructure, the construction or provision of additional parking spaces within the building envelope of any Parking Structure shown on the Project Approval, and the configuration of internal circulation elements. In effecting these modifications, the City shall fully cooperate with the Developer, provided that the aggregate total density and intensity of the Project are not increased, the permitted uses are not modified from those in the Project Approvals and any changes to the Project Improvements are in accordance with the Applicable Rules. Minor Changes shall not be deemed to be an amendment to this Agreement under Government Code Section 65868, and unless otherwise required by law, no such administrative amendments shall require prior notice or hearing by the Planning Commission and City Council. The following matters shall not be considered Minor Changes, but shall be considered substantive amendments which shall be reviewed by the Planning Commission and approved by the City Council:

4.2.1 Any addition of permitted uses not substantially similar to those set forth in the Project Approvals;

4.2.2 Any increase in the maximum height of any permitted Project Improvements;

4.2.3 Any amendment or change requiring a subsequent or supplemental environmental impact report pursuant to CEQA.

4.2.4 Any reduction in the minimum building setbacks and setbacks for any of the Project Improvements.

4.2.5 Any increase in the overall aggregate square footage of the Project Buildings.

4.3 Material Project Modifications. The Developer reserves the right to apply to the City for permits, variances or other approvals to develop portions of the Project in a manner which may be materially inconsistent with the Project Approvals. In such event, such portions of the Project shall be reviewed and approved pursuant to the rules, regulations, and procedures of the City in effect at the time the Developer makes application to the City for such development, and to the extent any such application is inconsistent with this Agreement, such application shall include an application to amend this Agreement.

4.4 New Rules. This Agreement shall not prevent the City from applying to the Project the following new rules, regulations and policies, if uniformly applied on a City-wide basis:

4.4.1 Procedural regulations relating to hearing bodies, petitions, applications, notices, findings, records, hearings, reports,

recommendations, appeals and any other matter of procedure, provided that the City's Director of Community Development makes an affirmative finding that such changes in procedural regulations do not have the effect of materially interfering with the substantive benefits conferred to Developer by this Agreement.

4.4.2 Regulations which are not in conflict with this Agreement provided that the City's Director of Community Development makes an affirmative finding that such new regulations would not, alone or in the aggregate, cause development of the Project to be materially different, more burdensome, time consuming or expensive.

4.4.3 Regulations which are necessary to avoid serious threats to the public health and safety, provided that the City's Director of Community Development makes an affirmative finding that, to the maximum extent possible, such regulations have been construed and applied in a manner to preserve the substantive benefits to the Developer of this Agreement.

4.4.4 Mandatory regulations of the County of Los Angeles, State of California and the United States of America applicable to the Project, provided that the City's Director of Community Development makes an affirmative finding that, to the maximum extent possible, such regulations have been construed and applied in a manner to preserve to the Developer the substantive benefits of this Agreement.

If the Developer does not agree with a determination by the City's Director of Community Development under this Section 4.4, the Developer may appeal such determination to the Planning Commission. If the Developer does not agree with the determination of the Planning Commission, Developer may appeal to the City Council. If Developer does not agree with a determination of the City Council, Developer shall have the right to contest or challenge such determination.

4.5 Discretionary Approvals. The development of the Project for specified allowable uses and as described in this Agreement shall require no subsequent discretionary approvals other than the Project Approvals, and no ministerial approvals by the City except for: (a) review and approval by the Community Development Director of the exterior elevations of any Project Building or Parking Structure for a determination as to consistency with the architecture of MOB1 and PS1; (b) design review, plan checking, grading and building permits solely to evaluate the proposed development for conformity to the Applicable Rules; and (c) any subdivision or parcel map approvals with respect to the Campus Property that may be requested or required by the Developer subsequent to the Effective Date of this Agreement. Prior to the issuance of a building permit for MOB2/PS2 and MOB3/PS3, the Director of Community Development shall conduct a review of the compliance by Developer with its obligations under this Agreement and shall report the findings of that review to the City Council. If the Developer is found to not be in compliance with its obligations under this Agreement, the building permit

for MOB2/PS2 or MOB3/PS3, as applicable, shall not be issued until Developer cures or corrects the items of non-compliance.

4.6 No Obligation to Develop. Nothing in this Agreement is intended, should be construed nor shall require Developer to proceed with the construction of any Project Improvements on the Campus Property; provided that any Project Improvements constructed shall comply with the requirements for timing and usage set forth in Sections 4.7 and 5 herein. The decision to proceed or to forbear or delay in proceeding with the implementation or construction of the Project or any Project Improvements shall be in the sole discretion of Developer and the failure of Developer to proceed with construction of any Project Improvements shall not (i) give rise to any rights of the City to terminate this Agreement or (ii) constitute an event of default or give rise to any liability, claim for damages or cause of action against Developer.

4.7 Timing of Construction of Project Improvements.

4.7.1 Developer shall not be required to construct the Project Improvements in any particular order or pursuant to any particular schedule, provided, however, that the following prerequisites to the Project Improvements as described in the table below are met:

Prior To:	Developer Must:
<p>Issuance of a Certificate of Occupancy for MOB1</p>	<p>Complete the construction of the traffic mitigation improvements identified in the EIR (collectively, the “<b>Traffic Mitigation Improvements</b>”) that are listed in Paragraph 1 on <u>Exhibit “F”</u> attached hereto.</p> <p>Complete the construction of the Realignment Improvements (as hereinafter defined) that are listed in Paragraph 1 on <u>Exhibit “K”</u> attached hereto.</p> <p>Obtain the issuance of a Certificate of Occupancy for PS1.</p> <p>Complete all (i) Conditions of Approval that are required for the construction of MOB1 and PS1, and (ii) CEQA mitigation measures identified in the EIR that are associated with the construction of MOB1 and PS1.</p>

<p>Issuance of a Certificate of Occupancy for the Inpatient Building</p>	<p>Obtain the issuance of Certificates of Occupancy for PS1 and MOB1.</p> <p>Complete the construction of the Traffic Mitigation Improvements that are listed in Paragraph 2 on <u>Exhibit "F"</u> attached hereto.</p> <p>Complete the construction of the Realignment Improvements that are listed in Paragraph 2 on <u>Exhibit "K"</u> attached hereto.</p> <p>Provide City Required Parking (as hereinafter defined) on the Campus Property for (i) the Existing Improvements, (ii) any Prior Project Buildings (as hereinafter defined) and (iii) the Inpatient Building.</p> <p>Complete all (i) Conditions of Approval that are required for the construction of the Inpatient Building, and (ii) CEQA mitigation measures identified in the EIR that are associated with the construction of the Inpatient Building and any related parking structures.</p>
<p>Issuance of a Building Permit for MOB2</p>	<p>Obtain the issuance of Certificates of Occupancy for PS1 and MOB1.</p> <p>Complete the relocation of hospital functions into MOB1 as specified in Section 5.6 herein.</p> <p>Provide City with written verification that plans for the Inpatient Building have been submitted to OSHPD for approval.</p>
<p>Issuance of a Certificate of Occupancy for MOB2</p>	<p>Provide City Required Parking on the Campus Property for (i) the Existing Improvements, (ii) any Prior Project Buildings and (iii) MOB2.</p> <p>Complete the construction of the Traffic Mitigation Improvements that are listed in Paragraph 2 on <u>Exhibit "F"</u> attached hereto.</p> <p>Complete the construction of the Realignment Improvements that are listed in Paragraphs 2 and 3 on <u>Exhibit "K"</u> attached hereto.</p>

	<p>Complete all (i) Conditions of Approval that are required for the construction of MOB2, and (ii) CEQA mitigation measures identified in the EIR that are associated with the construction of MOB2 and any related parking structures.</p> <p>Issuance of a Certificate of Occupancy for MOB2 shall be conditioned on Developer providing written documentation to the City Council that 20% of MOB2's leasable space has been leased to HMNMH for Centers of Excellence (as hereinafter defined) or other hospital-related uses.</p>
<p>Issuance of a Building Permit for MOB 3</p>	<p>Obtain issuance of Certificates of Occupancy for PS1 and MOB1.</p> <p>Complete the relocation of hospital functions into MOB1 as specified in Section 5.6 herein.</p> <p>Complete the construction of the Traffic Mitigation Improvements that are listed in Paragraph 2 on <u>Exhibit "F"</u> attached hereto.</p> <p>Complete the construction of the Realignment Improvements that are listed in Paragraphs 2 and 3 on <u>Exhibit "K"</u> attached hereto.</p> <p>Foundations for the Inpatient Building shall be complete. In addition, either vertical steel rebar must be in place for the first structural column section or, if a steel structural frame is to be used, the first vertical steel column section must be in place.</p> <p>Developer to provide proof that an agreement with a contractor(s) for completing the Inpatient Building has been entered into.</p> <p>Obtain issuance of a building permit for PS3 and provide written documentation satisfactory to the Director of Community Development that, upon issuance of a Certificate of Occupancy for PS3, Developer shall provide City Required Parking on the Campus Property for (i) the Existing Improvements, (ii) any Prior Project Buildings and (iii) MOB 3. The City shall impose as a condition to the issuance</p>

	of a certificate of occupancy for MOB3 that a certificate of occupancy has been issued for PS3.
Issuance of a Certificate of Occupancy for MOB3	<p>Complete all (i) Conditions of Approval that are required for the construction of MOB3, and (ii) CEQA mitigation measures identified in the EIR that are associated with the construction of MOB3.</p> <p>Complete the Traffic Mitigation Improvement that is listed in Paragraph 4 on <u>Exhibit "F"</u> attached hereto.</p>

4.7.2 For purposes of Section 4.7.1: (a) the term “**City Required Parking**” means the number of parking spaces set forth on Exhibit “G” to this Agreement ; and (b) the term “**Prior Project Buildings**” means any Project Buildings under this Agreement (i) for which a certificate of occupancy has been issued by the City or (ii) which are then under construction.

4.7.3 For purposes of Section 4.7.1, the term “**Centers of Excellence**” means the provision of highly specialized health care services via physician and/or hospital-authorized providers or hospital collaboration around a disease category (e.g. – cancer, heart, maternity or orthopedic or spine) or a service area (e.g. – outpatient imaging) in a central location. “Centers of Excellence” include diagnostics, treatment, rehabilitation, nursing, physician or community educational programs, clinical research and advanced medical technologies.

4.8 Additional Subterranean Parking Spaces. Developer shall be permitted to add additional subterranean parking spaces in any Parking Structure beyond the number of City Required Parking spaces required for that Parking Structure (the “**Additional Subterranean Spaces**”), without the necessity of an amendment to this Agreement or the modification of the Project Approvals, unless the exportation of dirt required for any such Additional Subterranean Spaces shall cause the aggregate cubic yards of dirt export in connection with the development of the Project to exceed 93,293 cubic yards. Should the Developer provide these Additional Subterranean Spaces in any Parking Structure, the Developer may reduce the number of City Required Parking spaces in a subsequently constructed Parking Structure by no more than the aggregate number of Additional Subterranean Spaces.

5. Restrictions on Use. Developer agrees that the use of the Campus Property shall be restricted as follows during the Term of this Agreement:

5.1 all Existing Improvements on the G&L Property and any Project Buildings developed on the G&L Property during the Term of this Agreement shall be used solely for the



purpose of (i) the erection, maintenance and operation of medical office buildings, which may include, but not be limited to, the operation of doctors' offices, pharmacies, diagnostic imaging facilities, lab specimen collection, doctor billing services, and such other health care services as may be provided by doctors or HMNMH or its affiliates, successors and assigns, and (ii) such other medical uses approved in writing in advance by the Hospital, its affiliates, successors and assigns; and

5.2 unless otherwise approved by HMNMH in writing (which approval may be given or withheld in HMNMH's sole and absolute discretion), all new tenants executing a new lease during the Term of this Agreement in any Existing Improvements and Project Buildings located on the G&L Property shall be limited to physicians who, or professional entities comprised of physicians the majority of whom, have privileges to admit and treat patients at HMNMH; and

5.3 HMNMH shall have a right of first offer to lease any space in any Existing Improvements and Project Buildings of the G&L Property in accordance with the procedures set forth in Exhibit "H"; and

5.4 HMNMH shall have a right of first refusal to purchase all or any part of the G&L Property and the Existing Improvements and the Project Buildings thereon in accordance with the procedures set forth in Exhibit "I"; and

5.5 The HMNMH Property and buildings located thereon (including the Existing Improvements thereon and the Inpatient Building, but excluding MOB1) shall be limited to hospital and hospital-related uses during the Term of this Agreement.

5.6 HMNMH shall relocate some or all of the following hospital functions to MOB1: administration, nursing administration, human resources, information technology, quality and medical staff services, education, board and educational conference rooms, business services including billing and collections, accounting services, material management and logistics; clinical case management, social services, risk management, medical library, medical staff conference rooms, marketing, public relations and community relations, security and safety and other support offices. These hospital functions will occupy 40,000 rentable square feet in MOB1 as part of the initial occupancy/leasing of MOB1.

5.7 Except for the Existing Gated Areas (as hereinafter defined) or as otherwise provided in the Conditions of Approval, all parking on the Campus Property shall be available for all uses on the Campus Property, and, where appropriate, reciprocal parking and access easements/agreements either have been or hereafter shall be executed by Developer prior to certificates of occupancy being issued for each parking structure to effectuate this requirement.

5.8 Subject to completion of the improvements referenced below, expanded services available on the Campus Property shall include:

5.8.1 A 50% increase in the current number of intensive care beds from

12 beds to 18 beds in an improved critical care center, to be located in the current Hospital facility within two years after issuance of a certificate of occupancy of MOB1.

5.8.2 Neonatal intensive care services to address medical needs of high risk pregnancies and high risk infants, to be developed within the main hospital building or located in the new Inpatient Building within two years following the issuance of the certificate of occupancy for the Inpatient Building, unless prior to this time another hospital has been located in the Santa Clarita Valley which duplicates full-service obstetric care.

5.8.3 A women's services unit to include private labor and delivery suites and dedicated operating rooms for scheduled and emergency c-section deliveries, along with post-operative and post-partum private rooms, to be located in the new Inpatient Building within two years following the issuance of the certificate of occupancy for the Inpatient Building.

5.8.4 A minimum 50% increase in inpatient operating room capacity from 4 operating rooms to at least 6 operating rooms. Additional operating rooms to occur upon the issuance of the certificate of occupancy for the new Inpatient Building.

5.8.5 Additional post coronary care private rooms to complement interventional cardiac services, to occur in the existing Hospital facility if services are able to be moved to the new Medical Buildings, or otherwise in the new Inpatient Building upon issuance of the certificate of occupancy for the Inpatient Building.

5.8.6 Expansion of post-surgical care services with additional private room accommodations in the new Inpatient Building, to occur within two years following the issuance of the certificate of occupancy for the Inpatient Building.

5.8.7 Replacement and expansion of campus educational and training facilities for both Hospital staff and community health education, to be provided in the new Medical Buildings within one (1) year of the certificate of occupancy of MOB1.

5.9 TCU Task Force. HMNMH will continue to actively participate in the City's Transitional Care Unit (TCU) Task Force designed to insure that a suitable location and operator for a TCU facility is provided within the Santa Clarita Valley. In addition, the Developer shall contribute Two Hundred and Fifty Thousand Dollars (\$250,000) to the City, to be used at the discretion of the City Council, following a recommendation of the City's TCU Task Force, for the feasibility, siting and construction of a facility or other senior health care needs. The sum of \$50,000 shall be paid upon the Effective Date of this Agreement. The

remaining balance shall be paid in equal installments of \$50,000 per year on the anniversary date of the Effective Date of this agreement until the entire amount is paid. If a facility is found and the funds are needed for the purchase and development of the facility, these funds shall be provided within 30 days upon a written request from the City Manager.

6. Parking. During the Term of this Agreement, Developer shall not (i) gate any entrances to surface parking areas (other than the Existing Gated Areas for physician parking) or Parking Structures on the Campus Property or (ii) charge any patients or visitors for parking on the Campus Property unless: (a) in the case of any proposed gating of parking entrances, Developer hereafter files an application with the City for a minor use permit of such gating, which application must be submitted to the City Council for its review and approval; and (b) in the case that Developer hereafter proposes to charge patients or visitors for parking on the Campus Property, Developer files an application with the City for approval of the right to institute such parking charges, which application must be submitted to the City Council for its review and approval. In conjunction with filing an application under either clause (a) or (b) above, such application shall be accompanied by a study that analyzes the potential impacts and benefits of the proposed actions that are the subject of the application. The term “**Existing Gated Areas**” means: (x) the existing surface lot for physician parking located adjacent to the north side of the new emergency room for the Hospital, which contains 25 parking spaces; and (y) the existing surface lot for physician parking located adjacent to the west side of the main Hospital building, which contains 24 parking spaces.

7. Exactions, Dedications, Assessments, Fees, Reservations, Dedications and Public Improvements.

7.1 Bridge and Thoroughfare District Fees. For purposes of this Agreement, the term “**B&T Fees**” means any Bridge and Thoroughfare District Fees that are established pursuant to Santa Clarita Code Section 16.21.190. During the Term of this Agreement, Developer shall be responsible for the payment of all B&T Fees that are imposed by the City in connection with the issuance by the City of a building permit for the construction of any Project Buildings. The B&T Fees with respect to each Project Building shall be calculated at the rates in effect on the date that Developer submits its application for the building permit for that Project Building, provided that such rates are uniformly applied throughout the District. Subject to section 7.3 herein, and subject to approval by the District of an application submitted by the Developer in accordance with the District’s guidelines and procedures, Developer shall be entitled to a credit against the B&T Fee for any eligible out-of-pocket costs incurred by Developer in the performance of any required Traffic Mitigation Improvements.

7.2 Exactions and Fees. The City agrees that no conditions, exactions, dedications, assessments, fees, reservations or public improvements whatsoever shall be imposed or required by the City in connection with any Project Approvals or the development of the Project or any portion thereof except for: (a) the B&T Fees, (b) the Conditions of Approval, (c) the Traffic Mitigation Improvements set forth on Exhibits “F” and “K”, (d) the obligations of Developer under Section 7.3, and (e) any fees or exactions that the City is mandated to impose under a law or regulation adopted after the Effective Date of this Agreement by the federal government, the State of California or the County of Los Angeles. Where the Developer must

provide for construction of improvements or dedication of land, or both, in lieu of payment of a regulatory fee or development imposition, and such construction and/or dedication constitutes, by agreement of the City, full and complete discharge of the obligation of Developer and the Campus Property for the impact or matter at issue, no future development fee or regulatory imposition may be imposed upon the Campus Property or the development for all or any portion thereof for the same or similar purpose.

7.3 Realignment and Widening of McBean Parkway. The City desires to widen and realign McBean Parkway in the future in order to improve overall traffic circulation (the "**McBean Frontage Realignment**"). The City has requested that the Developer assist the City's efforts to effectuate the McBean Frontage Realignment. Consistent with Santa Clarita Municipal Code section 17.03.010(E)(4) (requirement that a development agreement provide for clear and substantial public benefit) and in addition to the other public benefits provided by Developer herein, Developer further agrees to provide assistance toward the McBean Frontage Realignment by taking the following actions and providing the following payments and dedications, pursuant to the terms and conditions set forth below:

7.3.1 Prior to the issuance of the building permit for MOB1 or PS1, Developer shall dedicate to the City, at no cost to the City, the portion of the Campus Property that fronts McBean Parkway which is depicted and legally described on Exhibit "J" to this Agreement for use by the City as right-of-way for the future McBean Frontage Realignment (the "**Dedicated Area**").

7.3.2 In addition to the required Traffic Mitigation Improvements listed on Exhibit "F" to this Agreement, Developer also shall complete the construction of the street improvements to McBean Parkway listed on Exhibit "K" to this Agreement (collectively, the "**Realignment Improvements**"), as and when required under Section 4.7 and Exhibit "K", at the respective locations and configurations (collectively, the "**Designated Configuration**") shown on that certain preliminary engineering plan depicting the McBean Frontage Realignment, prepared by DCA Engineers, dated July 24, 2008 (the "**Frontage Design Plan**"), which is attached as Exhibit "L" to this Agreement. The Frontage Design Plan has been reviewed by the City. Exhibit F to this Agreement sets forth the traffic improvements imposed by the City as mitigation measures pursuant to the EIR, which are to be performed by Developer as provided therein. Exhibit K sets forth the Realignment Improvements which Developer has agreed to perform pursuant to this Agreement Prior to the issuance by the City of any permit required for the construction of any Realignment Improvement, Developer shall submit to the City, for its review and approval, detailed street improvement plans for such Realignment Improvement based upon the Frontage Design Plan.

7.3.3 On or prior to the fifth (5<sup>th</sup>) anniversary of the Effective Date, Developer shall pay to the City the sum of \$500,000 to be utilized by the

City for the McBean Frontage Realignment.

In the event that the McBean Frontage Realignment is a project that is or becomes eligible for the use of B&T Fees pursuant to Santa Clarita Code Section 16.21.190 , Developer shall be entitled to a credit against the B&T Fee for eligible out-of-pocket costs as (as determined by the rules and regulations governing such credits that are then generally in effect in the City) for (i) those Traffic Mitigation Improvements described on Exhibit F as items 1(a) and 2(a), and (ii) those Realignment Improvements described on Exhibit K as items 1(d), 2(a) and 2(b), that are incurred by Developer.

7.4 No Eminent Domain. The City and Developer expressly acknowledge and agree that the City shall not initiate nor prosecute any condemnation or eminent domain action to acquire any residential real property in connection with the development of the Project or in order to facilitate the construction of any Traffic Mitigation Improvements identified on Exhibit "F" or "K" to this Agreement.

8. Cooperation and Implementation by the City and Developer.

8.1 Processing. Upon execution of this Agreement, the City shall commence and proceed to complete all steps required of the City necessary or appropriate for the implementation of this Agreement and the development of the Project in accordance with the terms of this Agreement, including, but not limited to, the processing and checking of any and all subdivision or plat maps, improvement plans, grading plans, building plans and specifications and any other plans necessary for the development of the Project and the issuance of all necessary building permits, occupancy certificates, or other required permits for the construction, use, and occupancy of the Project. The City acknowledges that HMNMH intends to apply to the City for approval to subdivide the HMNMH Property so as to create a separate legal parcel for MOB1 and the City agrees to process such application in accordance with this Section 8.1. The City's obligations pursuant to this Section 8.1 are conditioned upon the Developer providing the City with all documents, plans, fees and other information necessary for the City to carry out its obligations under this Agreement, consistent with the City's application procedures, codes, ordinances and standards.

8.2. Other Governmental Permits. City agrees to cooperate with Developer in Developer's endeavors to obtain permits and approvals as may be required from other governmental or quasi-governmental agencies having jurisdiction over the Property or portions thereof (such as, for example, but not by way of limitation, public utilities or utility districts and agencies having jurisdiction over transportation facilities and air quality issues) so long as the cooperation by City will not require City to incur any cost, liability or expense without adequate indemnity against or right of reimbursement therefor from Developer.

9. Term of Agreement. This Agreement shall be binding as and when the ordinance approving this Agreement has been approved by the City Council and the Agreement has been executed by the City and Developer, and shall remain in effect until the fifteenth (15<sup>th</sup>) anniversary of the Effective Date (the "**Term**"). Expiration or termination of this Agreement shall not affect any right arising from permits or approvals on the Campus Property issued by the

City prior to such expiration or termination, nor shall such expiration affect any right the City may have by reason of the Developer's covenants to dedicate land or provide public improvements in conjunction with any portion of the Campus Property which is under construction at such time.

10. Vesting.

10.1 Existing Rules to Govern. Except as otherwise provided in this Agreement, no amendment to, revision of, or addition to any of the Applicable Rules without the Developer's written approval, whether adopted or approved by the City Council or any office, board, commission or other agency of the City, or by the people of the City through charter amendment, referendum or initiative measure, shall be effective or enforceable by the City with respect to the Project, and the design, density, intensity, signage, grading, zoning, construction, remodeling, or use of the Project. No future modification of City's codes or ordinances, or adoption of any code, ordinance, regulation or other action that purports to limit the rate of development over time or directly or indirectly limit the number of building permits issued or obtainable during any period within the Term (whether adopted or imposed by the City Council or through the initiative or referendum process) shall apply to the Project or any part thereof; nor shall any such modification or adoption of a code, ordinance or regulation modify the rights held by Developer hereunder.

10.2 Subsequent "Slow/No Growth" Measures. To the fullest extent legally permissible, any subsequently enacted initiatives, referenda, moratoria or amendments to the General Plan and/or ordinances which contain "slow/no growth" measures, or which by their terms are intended to or by operation have such effect, shall have no application to the Project.

11. OSHPD Regulations. The City and Developer mutually acknowledge and agree that Part 7 of the California Health and Safety Code and Part 1, Chapter 7 of Title 24 of the California Code of Regulations grant exclusive authority to the California Office of Statewide Health Planning & Development over the construction of the Inpatient Building and any other "hospital building" as defined in California Health and Safety Code Sections 129675-129680 and Section 7-111 of Part 1, Chapter 7 of Title 24 of the California Code of Regulations.

12. Review of Compliance. Developer shall request annual review of this Agreement in accordance with Government Code Section 65865.1, Santa Clarita Code Section 17.03.010.J., and this Agreement, in order to ascertain compliance by the Developer with the terms of this Agreement.

13. Mortgages.

13.1 Mortgagee Protection. No breach of this Agreement shall defeat, render invalid, diminish or impair the lien of any mortgage or deed of trust made in good faith and for value affecting any portion of the Campus Property or any Existing Improvements or Project Improvements thereon (collectively, a "**Mortgage**"); and any acquisition or acceptance of title or any right or interest in or with respect to the Campus Property or any portion thereof pursuant to a Mortgage, foreclosure, trustee's sale, deed in lieu of foreclosure or otherwise shall be subject to

all of the terms and conditions contained in this Agreement and entitled to all of its benefits. The Parties agree that they will make reasonable amendments to this Agreement to meet the requirements of any lender for the Project.

13.2 Mortgagee Not Obligated. No mortgagee under any Mortgage (collectively, a “**Mortgagee**”) shall have an obligation or duty under this Agreement to perform the Developer’s obligations or other affirmative covenants of either hereunder, or to guarantee such performance, except where such Mortgagee attempts to exercise any rights hereunder associated with any such obligation or duty.

13.3 Notice of Default to Mortgagee: Right of Mortgagee to Cure. If the City receives notice from a Mortgagee requesting a copy of any notice of default given hereunder and specifying the address for service thereof, and the said Mortgagee has recorded a copy of such request in the official records of Los Angeles County in the manner required under California Civil Code Section 2924b with respect to Requests for Notices of Default, then the City shall deliver to such Mortgagee, concurrently with service thereon to the applicable Party, any notice given to the applicable Party with respect to any claim by such Party that it has not complied in good faith with the terms of this Agreement or has committed an event of default. Each Mortgagee shall have the right (but not the obligation) for a period of ninety (90) days after the receipt of such notice from such Party to cure or remedy, or to commence to cure or remedy, the claimed default or act of noncompliance set forth in such Party’s notice. If the default is of a nature which can only be remedied or cured by such Mortgagee upon obtaining possession, such Mortgagee may (but is not obligated to) seek to obtain possession with diligence and continuity through foreclosure, a receiver or otherwise, and may (but is not obligated to) thereafter remedy or cure the default or noncompliance within thirty (30) days after obtaining possession. If any such default or noncompliance cannot, with diligence, be remedied or cured within such thirty (30) day period, then such Mortgagee shall have such additional time as may be reasonably necessary to remedy or cure such default or noncompliance if such Mortgagee commences cure during such thirty (30) day period, and thereafter diligently pursues and completes such cure.

13.4 Bankruptcy. Notwithstanding the foregoing provisions of this Section 13, if any Mortgagee is prohibited from commencing or prosecuting foreclosure, or other appropriate proceedings in the nature thereof, by any injunction issued by any court or by reason of any action by any court having jurisdiction of any bankruptcy or insolvency proceeding involving the Developer, the times specified in Section 13.3 for commencing or prosecuting foreclosure or other proceedings shall be extended for the period of the prohibition.

#### 14. Default and Remedies.

##### 14.1 Notice and Cure.

14.1.1 In the event of failure by either party hereto substantially to perform any terms, covenant or condition of this Agreement which is required on its part to be performed (“**Default**”), the non-defaulting party shall have those rights and remedies provided in this Agreement, provided that such non-defaulting party has first sent a written notice of Default, in

the manner required by Section 18, by registered or certified mail, return receipt requested, identifying with specificity the nature of the alleged Default and the manner in which the alleged Default may be satisfactorily cured (“**Notice of Default**”). In the event of a Default by Developer, the Notice of Default shall also be provided to any Mortgagee of Developer which has delivered a Request for Notice to the City in accordance with Section 13.3.

14.1.2 In the case of a monetary Default by Developer, Developer shall promptly commence to cure the identified Default and shall complete the cure of such Default within ten (10) business days after receipt by Developer of the Notice of Default. In the case of a non-monetary Default by either party, the alleged defaulting party shall promptly commence to cure the identified Default and shall complete the cure within thirty (30) days after receipt of the Notice of Default. The thirty (30) day cure period for a non-monetary Default shall be extended as is reasonably necessary to remedy such Default, provided that the alleged defaulting party commences such cure promptly after receiving the Notice of Default and continuously and diligently pursues such remedy at all times until such Default is cured.

14.2 Remedies for Monetary Default. In the event of Default by Developer in the performance of any of its monetary obligations under this Agreement which remains uncured (i) ten (10) business days after receipt by Developer of a written notice of default from the City and (ii) after expiration of Mortgagee’s cure period under Section 13.3 (if a Mortgagee of Developer has delivered a Request for Notice to the City in accordance with Section 13.3), the City shall have available any right or remedy provided in this Agreement, at law or in equity. All of said remedies shall be cumulative and not exclusive of one another, and the exercise of any one or more of said remedies shall not constitute a waiver or election in respect to any other available remedy.

14.3 Remedies for Non-Monetary Default.

14.3.1 In the event of non-monetary Default by either party hereunder which remains uncured (i) after expiration of all applicable notice and cure periods and (ii) in the case of a Default by Developer, after the expiration of Mortgagee’s cure period under Section 13.3 (if a Mortgagee of Developer has delivered a Request for Notice to the City in accordance with Section 13.3), the non-defaulting party shall have available any right or remedy provided in this Agreement, or provided at law or in equity except as prohibited by this Agreement. All of said remedies shall be cumulative and not exclusive of one another, and the exercise of any one or more of said remedies shall not constitute a waiver or election in respect to any other available remedy.

14.3.2 The City and Developer acknowledge that monetary damages and



remedies at law generally are inadequate and that specific performance is an appropriate remedy for the enforcement of this Agreement. Therefore, the remedy of specific performance shall be available to both the City and Developer under this Agreement in the event of a non-monetary Default.

14.3.3 The City and Developer hereby stipulate that Developer shall be entitled to obtain relief in the form of a writ of mandate in accordance with Code of Civil Procedure Section 1085 or Section 1094.5, as appropriate, to remedy any non-monetary Default by the City of its obligations and duties under this Agreement.

14.3.4 Neither the City nor Developer shall have the right to sue for monetary damages as a result of a non-monetary Default under this Agreement.

#### 14.4 Termination of Agreement by City.

14.4.1 In the event that (i) the City finds and determines pursuant to Section 12, on the basis of substantial evidence, that Developer has not been in good faith compliance with the terms and conditions of this Agreement, or (b) the City finds and determines that there has been a Default by Developer of its obligations under this Agreement, the City may commence proceedings to terminate this Agreement pursuant to this Section 14.4.

14.4.2 The procedures for termination of this Agreement by the City for the grounds set forth in Section 14.4.1 are as follows:

14.4.2(a) The City shall provide a written notice to Developer (and to any Mortgagee of Developer which has delivered a Request for Notice to the City in accordance of Section 13.3) of its intention to terminate this Agreement unless Developer (or the Mortgagee) cures or corrects the acts or omissions that constitute the basis of such determinations by the City (the **"Hearing Notice"**). The Hearing Notice shall be delivered by the City to Developer in accordance with Section 18 and shall contain the time and place of a public hearing to be held by the City Council on the determination of the City to proceed with termination or modification of this Agreement. The public hearing shall not be held earlier than: (i) thirty-one (31) days after delivery of the Hearing Notice to Developer, or (ii) if a Mortgagee has delivered a Request for Notice in accordance with Section 13.3, the day following the expiration of the Mortgagee's cure period.

14.4.2(b) If, following the conclusion of the public hearing, the City Council: (i) determines that Developer is in Default of its

obligations under this Agreement or has not been in good faith compliance with this Agreement pursuant to Section 12, as applicable; and (ii) further determines that Developer (or the Mortgagee, if applicable) has not cured the acts or omissions that constitute the basis of the determination under subsection (i) or, if those acts or omissions could not be reasonably remedied prior to the public hearing, that Developer (or the Mortgagee) has not in good faith commenced to cure or correct such acts or omissions prior to the public hearing or is not diligently and continuously proceeding therewith to completion, the City Council may terminate this Agreement.

15. Project Approvals Independent. If any provision of this Agreement or the application of any provision of this Agreement to a particular situation is held by a court of competent jurisdiction to be invalid or unenforceable, or if this Agreement is terminated for any reason, then such invalidity, unenforceability or termination of this Agreement, or any part hereof, shall not affect the validity or effectiveness of any Project Approvals or land use approvals which have been issued or granted by the City prior to that time. In such cases, such Project Approvals or land use approvals will remain in effect pursuant to their own terms, provisions, and conditions of approval.

16. Required Actions of Parties; Further Assurances. The City and the Developer shall execute all such instruments and documents and take in good faith all actions necessary or convenient to consummate the transactions herein contemplated.

17. Assignment. The rights of the Developer under this Agreement may be transferred or assigned in whole or in part to any person acquiring all or any portion of the Campus Property or the Project subject only to the City's written approval of the assignee or transferee, which shall not be unreasonably withheld. Express assumption of any of the Developer's obligations under this Agreement by any such transferee or assignee shall release the Developer from the obligations so assigned and the City shall look solely to the transferee or assignee for performance of the assigned obligations under this Agreement.

18. Notices. All notices under this Agreement shall be in writing and shall be effective when personally delivered or upon receipt after deposit in the United States mail as registered or certified mail, postage prepaid, return receipt requested, to the following representatives of the Parties at the addresses indicated below or to such other addresses as one Party may provide to the other from time to time:

If to the City:

City of Santa Clarita  
23920 Valencia Boulevard, Suite 300  
Santa Clarita, California 91355  
Attention: City Manager

With a copy to:

Burke, Williams & Sorensen  
611 West Sixth Street, Suite 2500  
Los Angeles, California 90017  
Attention: Carl K. Newton, City Attorney

If to the Hospital:

Henry Mayo Newhall Memorial Hospital  
23845 McBean Parkway  
Valencia, CA 91355  
Attention: Roger E. Seaver, President & CEO

With a copy to:

Hooper, Lundy and Bookman, Inc.  
101 West Broadway, Suite 1330  
San Diego, CA 92101-3890  
Attention: Stephen Treadgold, Esq.

If to G&L:

G&L Valencia, LLC  
439 Bedford Drive  
Beverly Hills, CA 90210  
Attention: Steven D. Lebowitz

With a copy to:

Law Offices of Richard A. Lawrence  
2815 Townsgate Road, Suite 140  
Westlake Village, CA 91361  
Attention: Richard A. Lawrence, Esq.

19. Amendment or Cancellation. Subject to meeting the notice and hearing requirements of Section 65867 of the Government Code, this Agreement may be amended from time to time, or canceled in whole or in part, by mutual consent of the City and Developer, or their respective successors in interest in accordance with the provisions of Section 65868 of the California Government Code; provided, however, that any amendment which does not relate to the Term, permitted uses, density or intensity of use, height or size of Project Improvements, provisions for reservation and dedication of land, conditions, terms, restrictions and requirements relating to subsequent discretionary actions, or any conditions or covenants relating to the use of the Campus Property, shall not require notice or public hearing before the Parties may execute an amendment hereto.

20. Waiver. No waiver of any provision of this Agreement shall be effective unless in writing and signed by a duly authorized representative of the party against whom enforcement of a waiver is sought and referring expressly to this Section. No waiver of any right or remedy in respect of any occurrence or event shall be deemed a waiver of any right or remedy in respect of any other occurrence or event.

21. Successor and Assigns. The provisions of this Agreement shall be binding upon and inure to the benefit of the Parties, and any subsequent owners of all or any portion of the Campus Property and their respective successors and assigns. Any successors in interest to the City shall be subject to the provisions set forth in Sections 65865.4 and 65868.5 of the Government Code.

22. Interpretation and Governing State Law. This Agreement and any dispute arising

hereunder shall be governed and interpreted in accordance with the laws of the State of California. This Agreement shall be construed as a whole according to its fair language and common meaning to achieve the objective and purposes of the Parties hereto, and the rule of construction to the effect that ambiguities are to be resolved against the drafting party shall not be employed in interpreting this Agreement, both Parties having been represented by counsel in the negotiation and preparation hereof. All legal actions brought to enforce the terms of this Agreement shall be brought and heard in the Superior Court of the State of California, County of Los Angeles.

23. Constructive Notice and Acceptance. Every person who, now or hereafter, owns or acquires any right, title or interest in or to any portion of the Campus Property is, and shall be, conclusively deemed to have consented and agreed to every provision contained herein, whether or not any reference to this Agreement is contained in the instrument by which such person acquired an interest in the Campus Property.

24. No Third Party Beneficiaries. This Agreement is made and entered into for the sole protection and benefit of the Parties and their successors and assigns. No other person shall have any right of action based upon any provision of this Agreement.

25. Attorneys' Fees. If either Party commences any action for the interpretation, enforcement, termination, cancellation or rescission hereof, or for specific performance of the breach hereof, the prevailing party shall be entitled to its reasonable attorneys' fees and costs.

26. Counterparts. This Agreement may be executed in two or more identical counterparts, each of which shall be deemed to be an original and each of which shall be deemed to be one and the same instrument when each Party signs each such counterpart.

27. Incorporation of Attachments. All recitals and attachments to this Agreement, including all Exhibits referenced herein, and all subparts thereto, are incorporated herein by this reference.

28. Determinations. Whenever in this Agreement the consent or approval of any party to this Agreement is required, such consent or approval shall not be unreasonably withheld or delayed. In addition, unless a contrary standard or right is set forth herein, whenever any party hereto is granted a right to take action, exercise discretion, or make an allocation, judgment or other determination, each party hereto shall act reasonably and in good faith and take no action which might result in the frustration of the expectations of the other Parties concerning the benefits to be enjoyed under this Agreement as expressed in this Agreement.

29. Defense of Actions.

29.1 If any legal action or other proceeding is instituted by a third party or parties (including without limitation, another governmental entity or official), challenging the validity of any provision of the Project Approvals, the EIR or other CEQA actions related to the Project, or this Agreement, Developer and the City shall cooperate in defending any such action. The City shall promptly notify Developer of any such legal action against City within five (5)

business days after the City receives service of process, except for any petition for injunctive relief, in which case the City shall notify Developer immediately upon receipt of notice thereof. Developer shall indemnify, hold harmless and defend the City, and any of its officers, employees or agents for any claim or lawsuit brought to challenge the validity or enforcement of the Project Approvals, the EIR or other CEQA actions related to the Project, or this Agreement, instituted by a third party or another governmental entity or official; provided, however, that if the City fails to cooperate in the defense, Developer shall not thereafter be responsible for the City's defense costs. Developer shall reimburse all of the City's defense costs including, without limitation, court costs, attorneys fees and expert witness fees. Developer shall promptly pay all monetary awards, judgments, verdicts, court costs and attorney's fees that may be awarded in such action. The City shall be entitled to select counsel to conduct its defense in any such action; provided, however, that the City shall instruct such counsel to cooperate with Developer as provided in this Section 29.1.

29.2 The filing of any lawsuit(s) by a third party (not a party to this Agreement) after the Effective Date against the City and/or Developer relating to this Agreement or to other development issues affecting the Project shall not delay or stop the processing or issuance of any permit or authorization necessary for development of the Project, unless the City in good faith determines that such delay is legally required.

30. Estoppel Certificate. Either party may, at any time, and from time to time, (but no more frequently than four (4) times in any calendar year) deliver written notice to the other party requesting such party to certify in writing that, to the knowledge of the certifying party, (i) this Agreement is in full force and effect and a binding obligation of the parties, (ii) this Agreement has not been amended or modified either orally or in writing, or if so amended, identifying the amendments, and (iii) the requesting party is not in default in the performance of its obligations under this Agreement, or if in default, to describe therein the nature and amount of any such defaults. A party receiving a request hereunder shall execute and return such certificate or give a written detailed response explaining why it will not do so within thirty (30) days following the receipt thereof. Each party acknowledges that such a certificate may be relied upon by third parties acting in good faith. A certificate provided by City establishing the status of this Agreement with respect to the HMNMH Property or the G&L Property shall be in recordable form and may be recorded with respect to the affected parcels at the expense of the recording party. Failure to deliver such a certificate or a written denial within the time specified above shall constitute a conclusive presumption against the party failing to provide the certificate that this Agreement is in full force and effect, without modification, except as may be represented by the requesting party; and that there are no uncured defaults in the performance of the requesting party except as may be so represented. All costs incurred in providing the notice(s) anticipated by this section including reasonable attorney's fees shall be borne by the requesting party.

31. Authorized Delays. Performance by any Party of its obligations hereunder shall be excused during any period of Excusable Delay, as hereinafter defined, provided that the Party claiming the delay gives written notice of the delay to the other Parties as soon as possible after the same has been ascertained. For purposes hereof, "**Excusable Delay**" shall mean delay that directly affects, and is beyond the reasonable control of, the Party claiming the delay, including without limitation: (a) act of God; (b) civil commotion; (c) riot; (d) strike, picketing or

other labor dispute; (e) shortage of materials or supplies; (f) damage to work in progress by reason of fire, flood, earthquake or other casualty; (g) failure, delay or inability of City to provide adequate levels of public services, facilities or infrastructure to the Property including, by way of example only, the lack of water to serve any portion of the Property due to drought; (h) delay caused by a delay by other third party entities which are required to approve plans or documents for Developer to construct the Project, or restrictions imposed or mandated by such other third party entities or governmental entities other than City; or (i) litigation brought by a third party attacking the validity of this Agreement, a Project Approval, or any other action necessary for development of the Project. Except for an Excusable Delay under clause (i) above, the payment of fees or monies by Developer under this Agreement shall not be excused or delayed during any period of Excusable Delay.

32. Administration of Agreement. Any decision by City staff concerning the interpretation and administration of this Agreement and development of the Property in accordance herewith may be appealed by the Developer to the Planning Commission, provided that any such appeal shall be filed with the City Clerk of City within ten (10) days after the affected Developer receives written notice of the staff decision. The Planning Commission shall render its decision to affirm, reverse or modify the staff decision within thirty (30) days after the appeal was filed. Thereafter the Developer may appeal the decision of the Planning Commission to the City Council pursuant to the same deadlines. The Developer shall not seek judicial review of any staff decision without first having exhausted its remedies pursuant to this section.

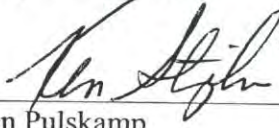
SIGNATURES ON THE FOLLOWING PAGES

IN WITNESS WHEREOF, the parties hereto have duly executed this Agreement.

“City”

CITY OF SANTA CLARITA  
a municipal corporation

Dated: January 23, 2009


By:  Assistant City Manager  
for Ken Pulskamp  
City Manager

ATTEST:

  
Sharon Dawson  
City Clerk



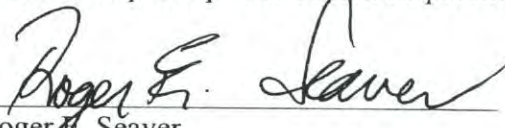
APPROVED AS TO FORM:

  
City Attorney

“HMNMH”

Henry Mayo Newhall Memorial Hospital,  
a California non-profit public benefit corporation

Dated: January 12, 2009

By:   
Roger E. Seaver  
President/CEO

“G&L”

G&L Valencia, LLC,  
a California limited liability company

By: G&L Realty Partnership, L.P.,  
a Delaware limited partnership  
Its: Sole Member

By: G&L Realty Properties, LLC  
a Nevada limited liability company  
Its: General Partner

Dated: January 13, 2009

By:   
Steven D. Lebowitz  
Its: Member



ACKNOWLEDGEMENT

State of California )  
County of Los Angeles )

On January 23, 2009, before me, Sharon A. Dawson, City Clerk, personally appeared KEN BULSKAMP, who proved to me on the basis of satisfactory evidence to be the person(s) whose name(s) is/are subscribed to the within instrument and acknowledged to me ~~that~~ <sup>that</sup> he/~~she/they~~ executed the same in his/~~her/their~~ authorized capacity(ies), and that by his/~~her/their~~ signature(s) on the instrument the person(s) or the entity upon behalf of which the person(s) acted, executed the instrument.

STRIPLIN

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing paragraph is true and correct.

WITNESS my hand and official seal.

Sharon A. Dawson  
Signature of Notary CITY CLERK

(seal)





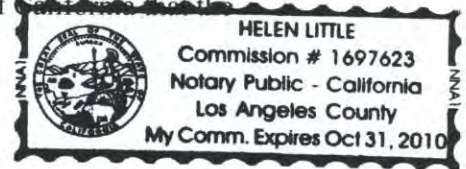
**ACKNOWLEDGEMENT**

State of California )  
 )  
County of Los Angeles )

On January 13, 2009, before me, Helen Little, Notary Public, personally appeared STEVEN D. LEBOWITZ, who proved to me on the basis of satisfactory evidence to be the person(s) whose name(s) is/are subscribed to the within instrument and acknowledged to me the he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their signature(s) on the instrument the person(s) or the entity upon behalf of which the person(s) acted, executed the instrument.

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing paragraph is true and correct.

WITNESS my hand and official seal.



Helen Little  
Signature of Notary

(seal)

**EXHIBIT "A"**  
**Legal Description of HMNMH Property**

PARCELS 1 AND 2 OF PARCEL MAP NO. 3083, IN THE CITY OF SANTA CLARITA, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP FILED IN BOOK 45, PAGE 91 OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY, EXCEPTING THEREFROM THE FOLLOWING DESCRIBED PARCEL:

BEGINNING AT THE MOST SOUTHERLY CORNER OF SAID PARCEL 1, SAID CORNER LYING ON THE NORTHWESTERLY LINE OF MC BEAN PARKWAY, 100.00 FEET WIDE, SAID NORTHWESTERLY LINE ALSO BEING A CURVE, CONCAVE NORTHWESTERLY AND HAVING A RADIUS OF 1950.00 FEET; THENCE NORTHWESTERLY ALONG THE SOUTHWESTERLY LINE OF SAID PARCEL 1, NORTH 19 DEGREES 37 MINUTES 08 SECONDS WEST, 551.90 FEET, SAID SOUTHWESTERLY LINE ALSO BEING A RADIAL LINE TO SAID CURVE; THENCE NORTH 42 DEGREES 30 MINUTES 21 SECONDS EAST, 510.04 FEET; THENCE SOUTH 47 DEGREES 43 MINUTES 44 SECONDS EAST, 103.96 FEET; THENCE SOUTH 42 DEGREES 24 MINUTES 24 SECONDS EAST, 37.17 FEET; THENCE SOUTH 38 DEGREES 57 MINUTES 00 SECONDS EAST, 24.83 FEET; THENCE SOUTH 51 DEGREES 19 MINUTES 23 SECONDS WEST, 20.00 FEET; THENCE SOUTH 38 DEGREES 35 MINUTES 36 SECONDS EAST, 108.15; THENCE SOUTH 33 DEGREES 01 MINUTES 43 SECONDS EAST, 45.04 FEET; THENCE NORTH 51 DEGREES 32 MINUTES 59 SECONDS EAST, 18.85 FEET; THENCE SOUTH 38 DEGREES 36 MINUTES 22 SECONDS EAST, 118.74 FEET; THENCE NORTH 51 DEGREES 40 MINUTES 23 SECONDS EAST, 24.00 FEET; THENCE SOUTH 41 DEGREES 24 MINUTES 03 SECONDS EAST, 250.71 FEET; THENCE SOUTH 03 DEGREES 50 MINUTES 00 SECONDS EAST, 21.88 FEET TO A POINT ON SAID NORTHWESTERLY LINE OF MC BEAN PARKWAY, 100.00 FEET WIDE AND SAID CURVE, A RADIAL LINE TO SAID CURVE BEARS NORTH 41 DEGREES 08 MINUTES 36 SECONDS WEST; THENCE SOUTHWESTERLY ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 21 DEGREES 31 MINUTES 28 SECONDS FOR AN ARC LENGTH OF 732.56 FEET TO THE POINT OF BEGINNING FOR THIS DESCRIPTION.

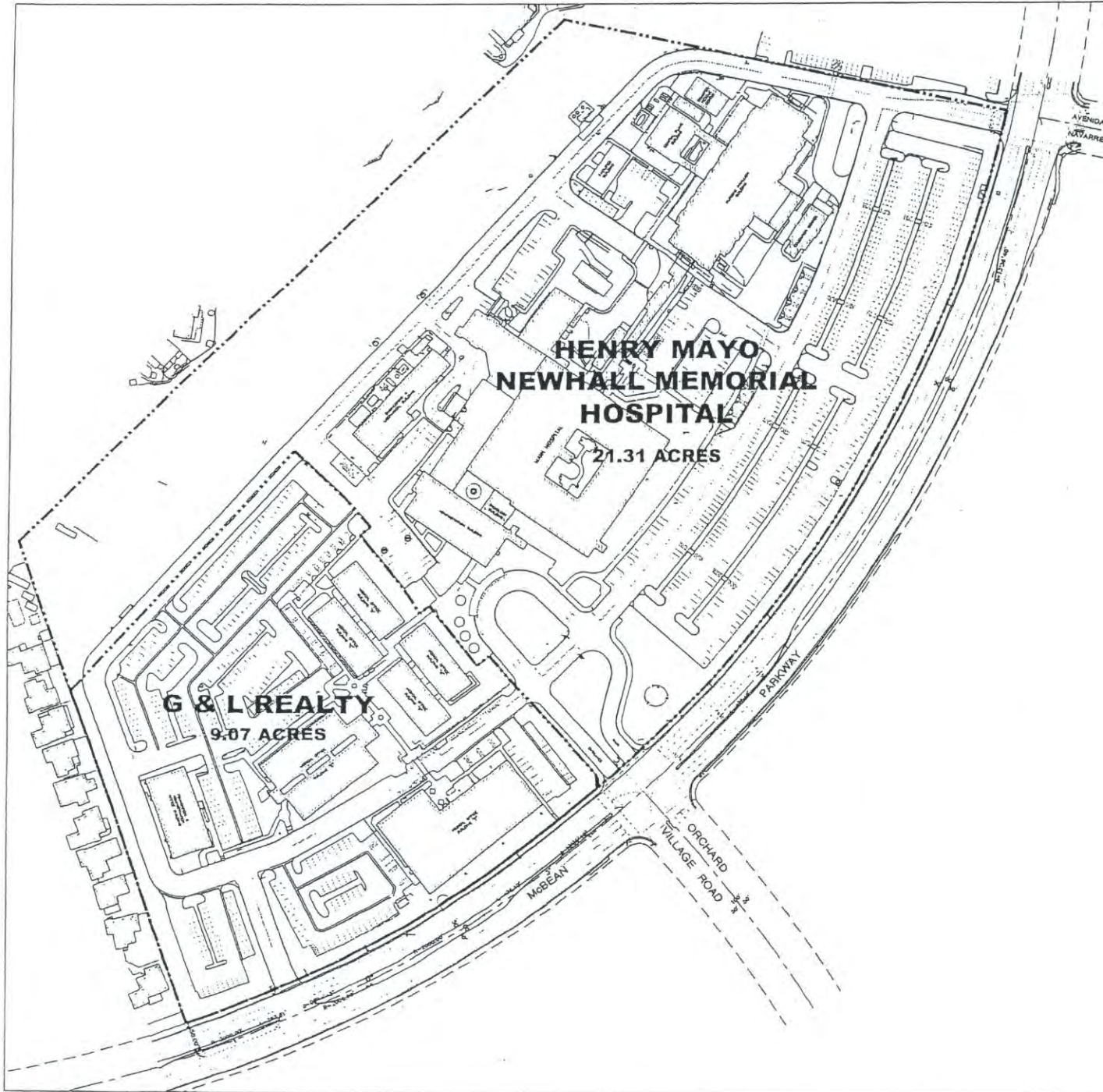
**EXHIBIT "B"**  
**Legal Description of G&L Property**

THOSE PORTIONS OF PARCELS 1 AND 2 OF PARCEL MAP NO. 3083, IN THE CITY OF SANTA CLARITA, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP FILED IN BOOK 45, PAGE 91 OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY, BEING DESCRIBED AS FOLLOWS:

BEGINNING AT THE MOST SOUTHERLY CORNER OF SAID PARCEL 1, SAID CORNER LYING ON THE NORTHWESTERLY LINE OF MC BEAN PARKWAY, 100.00 FEET WIDE, SAID NORTHWESTERLY LINE ALSO BEING A CURVE, CONCAVE NORTHWESTERLY AND HAVING A RADIUS OF 1950.00 FEET; THENCE NORTHWESTERLY ALONG THE SOUTHWESTERLY LINE OF SAID PARCEL 1, NORTH 19 DEGREES 37 MINUTES 08 SECONDS WEST, 551.90 FEET, SAID SOUTHWESTERLY LINE ALSO BEING A RADIAL LINE TO SAID CURVE; THENCE NORTH 42 DEGREES 30 MINUTES 21 SECONDS EAST, 510.04 FEET; THENCE SOUTH 47 DEGREES 43 MINUTES 44 SECONDS EAST, 103.96 FEET; THENCE SOUTH 42 DEGREES 24 MINUTES 24 SECONDS EAST, 37.17 FEET; THENCE SOUTH 38 DEGREES 57 MINUTES 00 SECONDS EAST, 24.83 FEET; THENCE SOUTH 51 DEGREES 19 MINUTES 23 SECONDS WEST, 20.00 FEET; THENCE SOUTH 38 DEGREES 35 MINUTES 36 SECONDS EAST, 108.15; THENCE SOUTH 33 DEGREES 01 MINUTES 43 SECONDS EAST, 45.04 FEET; THENCE NORTH 51 DEGREES 32 MINUTES 59 SECONDS EAST, 18.85 FEET; THENCE SOUTH 38 DEGREES 36 MINUTES 22 SECONDS EAST, 118.74 FEET; THENCE NORTH 51 DEGREES 40 MINUTES 23 SECONDS EAST, 24.00 FEET; THENCE SOUTH 41 DEGREES 24 MINUTES 03 SECONDS EAST, 250.71 FEET; THENCE SOUTH 03 DEGREES 50 MINUTES 00 SECONDS EAST, 21.88 FEET TO A POINT ON SAID NORTHWESTERLY LINE OF MC BEAN PARKWAY, 100.00 FEET WIDE AND SAID CURVE, A RADIAL LINE TO SAID CURVE BEARS NORTH 41 DEGREES 08 MINUTES 36 SECONDS WEST; THENCE SOUTHWESTERLY ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 21 DEGREES 31 MINUTES 28 SECONDS FOR AN ARC LENGTH OF 732.56 FEET TO THE POINT OF BEGINNING FOR THIS DESCRIPTION.

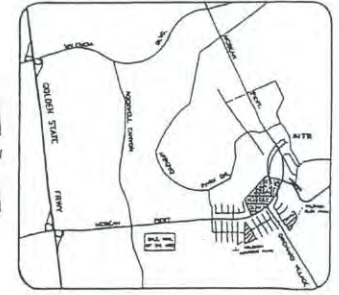
**EXHIBIT "C"**  
**Map of Campus Property**

The Map of the Campus Property is on the following page

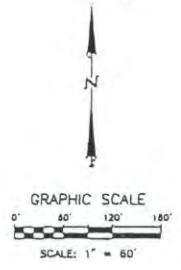


**HENRY MAYO  
NEWHALL MEMORIAL  
HOSPITAL**  
21.31 ACRES

**G & L REALTY**  
9.07 ACRES



VICINITY MAP  
NOT TO SCALE



**EXHIBIT "D"**  
**Existing Improvements**

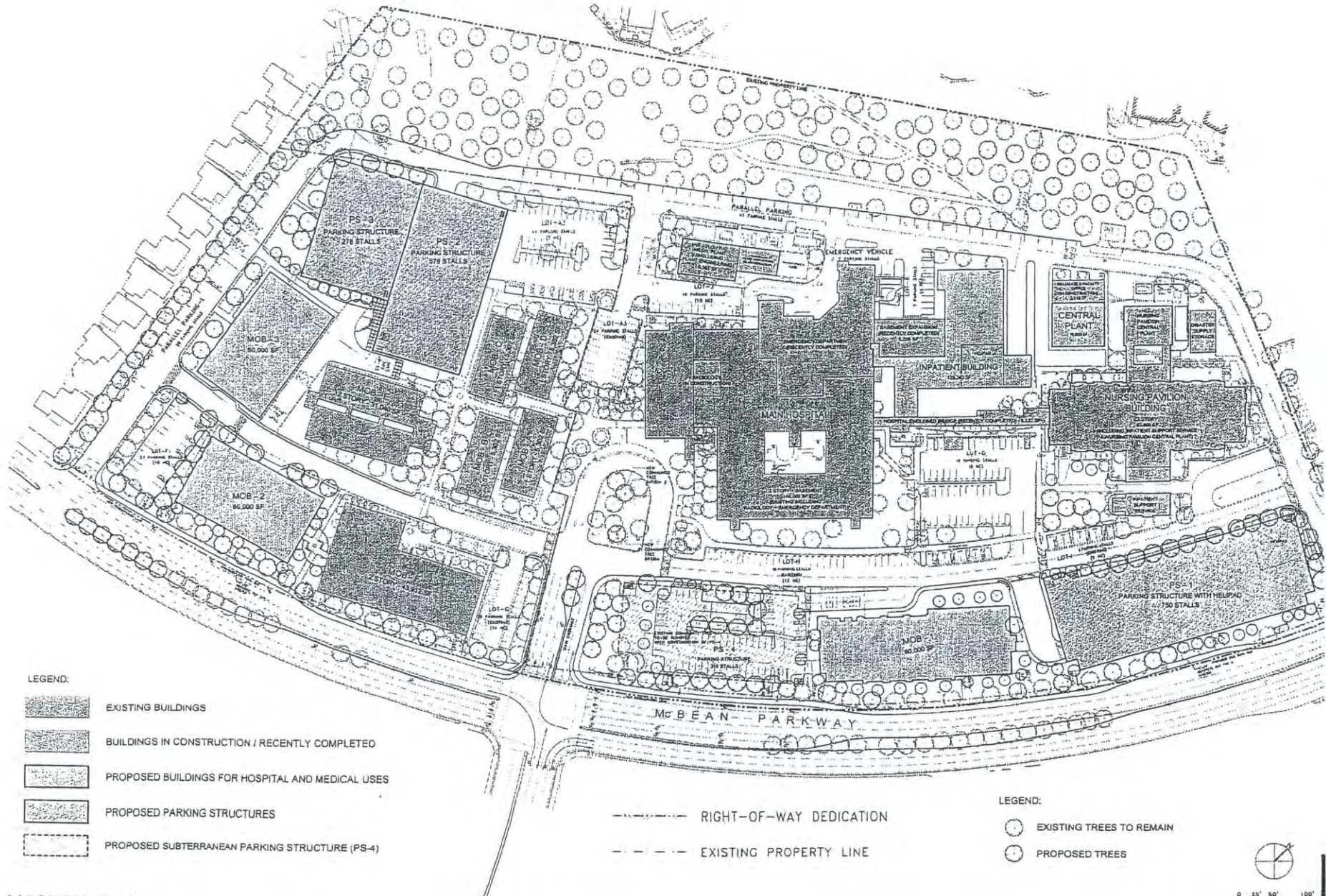
A Site Plan of the Campus Property depicting the Existing Improvements  
is on the following page.










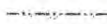

**EXHIBIT "E"**  
**Master Plan**

The Master Plan for the Campus Property depicting the Project Improvements is on the following page.





LEGEND:

-  EXISTING BUILDINGS
-  BUILDINGS IN CONSTRUCTION / RECENTLY COMPLETED
-  PROPOSED BUILDINGS FOR HOSPITAL AND MEDICAL USES
-  PROPOSED PARKING STRUCTURES
-  PROPOSED SUBTERRANEAN PARKING STRUCTURE (PS-4)

-  RIGHT-OF-WAY DEDICATION
-  EXISTING PROPERTY LINE

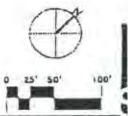
LEGEND:

-  EXISTING TREES TO REMAIN
-  PROPOSED TREES

MASTER PLAN

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
 23845 McBean Parkway, Santa Clara, CA 95055

SWA #: 060307 NOVEMBER 19, 2008



**EXHIBIT "F"**  
**EIR Traffic Mitigation Improvements**

1. **Prior to Issuance of the Certificate of Occupancy for MOB1.** The following traffic mitigations set forth in the EIR must be completed by Developer prior to the issuance by the City of a Certificate of Occupancy for MOB1:

- (a) **McBean Parkway at Magic Mountain (Intersection #45):** Add a third through lane to the westbound direction (by re-striping the lanes) and add right-turn overlap phasing for the westbound right-turn movement (by signal modification).
- (b) **Orchard Village Road at Wiley Canyon Road (Intersection #54):** Add a separate northbound right-turn lane with right-turn overlap phasing (within existing right-of-way between Wiley Canyon Road and the Santa Clara River South Fork Bridge).
- (c) **Orchard Village Road at McBean Parkway (Intersection #55):** Widen the southbound approach at the main driveway into the Campus Property to allow for a left-turn lane and a second through lane.

2. **Prior to Issuance of the Certificate of Occupancy for either the Inpatient Building or MOB2.** The following traffic mitigations set forth in the EIR must be completed by Developer prior to the issuance by the City of a Certificate of Occupancy for either the Inpatient Building or MOB2:

- (a) **McBean Parkway at Magic Mountain Parkway (Intersection #45):** Add a third through lane for eastbound direction (by re-striping the lanes).
- (b) **Orchard Village Road at McBean Parkway (Intersection #55):** Add a separate westbound right-turn lane for access to the Campus Property and a separate southbound right-turn lane at the main driveway to the Campus Property.
- (c) **Valencia Boulevard at Magic Mountain Parkway (Intersection #57):** Add a second westbound left-turn lane by removing the existing right-turn lane (by re-striping the westbound approach as a mirror image of the existing eastbound approach).

3. **Prior to Issuance of Building Permit for MOB3.** Satisfy the requirements of EIR mitigation measures TR7 and TR8.

4. **Prior to Issuance of Certificate of Occupancy for MOB3.** The following mitigations set forth in the EIR must be completed prior to issuance by the City of a Certificate of Occupancy for MOB3:

- (a) **Orchard Village Road at McBean Parkway (Intersection #55):** Restripe the hospital driveway to reconfigure the first through lane to be a shared left turn/through lane.

**EXHIBIT "G"**  
**City Required Parking**

The chart of the City Required Parking is on the following page.

MASTER PLAN PARKING SUMMARY

SWA ARCHITECTS

HENRY MAYO NEWHALL MEMORIAL HOSPITAL MASTER PLAN  
DATE: NOVEMBER 19, 2008

48 E. Holly Street  
Pasadena, CA 91103

REQUIRED PARKING				PARKING PER DESIGN	
BUILDING NAME	BUILDING AREA	BED COUNT	REQ'D PARKING STALLS	PARKING NAME/TYPE	PROVIDED PARKING STALLS
<b>MEDICAL BUILDINGS</b>					
MOB A (EXISTING)	5,302 / 4,567 SF	N/A	23	PS1 with 1 SUBTERRANEAN LEVEL	750
MOB B (EXISTING)	5,302 / 4,560 SF	N/A	23	PS2 with 1 SUBTERRANEAN LEVEL	579
MOB C (EXISTING)	5,302 / 4,561 SF	N/A	23	PS3 with 1 SUBTERRANEAN LEVEL	278
MOB D (EXISTING)	5,302 / 4,560 SF	N/A	23	PS4 with 2 SUBTERRANEAN LEVELS	316
MOB E (EXISTING)	31,040 / 25,508 SF	N/A	128	SURFACE (GENERAL)	253
MOB F (EXISTING)	43,912 / 43,912 SF	N/A	220	SURFACE (FOR PHYSICIANS-CURRENTLY GATED)	48
FOUNDATION BLDG. (EXISTING)	8,000 SF	N/A	32	SURFACE (FOR EMERGENCY)	7
MOB 1 (NEW)	80,000 / 78,400 SF	N/A	392	TOTAL PARKING PROVIDED	<b>2,237</b>
MOB 2 (NEW)	60,000 / 58,800 SF	N/A	294	<b>HANDICAP PARKING</b>	
MOB 3 (NEW)	60,000 / 58,800 SF	N/A	294		
FOUNDATION BLDG. (REMOVED)	(8,000 SF)	N/A	-32	PARKING NAME/TYPE	PROVIDED HANDICAP PARKING
<b>SUBTOTAL</b>			1,420		
<b>HOSPITAL BUILDINGS</b>					
MAIN HOSPITAL (EXISTING)	N/A	EXISTING: 121	242	PS1	15
		FUTURE ICU: 18	36	PS2	12
EMERGENCY DEPT, URGENT CARE (EXISTING)	5,518 SF	N/A	18	PS3	6
RADIOLOGY OUTPATIENT (EXISTING)	2,952 SF	N/A	8	PS4	7
RADIOLOGY (OUTPATIENT) (IN CONSTRUCTION)	2,905 SF	N/A	7	SURFACE	70
				<b>TOTAL HANDICAP PARKING PROVIDED</b>	<b>110</b>
NURSING PAVILION (EXISTING)	N/A	EXISTING: 100	200	Note: SF in bold reflects gross square footage per City of Santa Clarita Municipal Code, Chapter 17.07.010. "Floor area (gross)" shall mean the sum of the gross horizontal areas of the several floors of the building measured from the exterior faces of the exterior walls excluding exterior balconies and porches. Floor area shall not include interior parking spaces, loading spaces for motor vehicles, any space where the floor-to-ceiling height is less than six (6) feet, vertical shafts and attics and mechanical penthouses provided there are no usable rooms, no windows and the mechanical penthouse area is used exclusively for mechanical equipment. Floor area shall include any area used for storage and, for auto repair uses, any work area, or related facility, where vehicles are serviced and repaired.	
		FUTURE SUB ACUTE UPGRADE: 9	18		
HOSPITAL ENCLOSED BRIDGE (RECENTLY COMPLETED)	9,122 SF	N/A	N/A		
FACILITY BUILDING (IN CONSTRUCTION)	OFFICE AREA 734 SF WAREHOUSING 2,384 SF	N/A	5		
INPATIENT BLDG (NEW)	N/A	120	240		
	OUTPATIENT SERVICES 4,000 SF	N/A	10		
<b>SUBTOTAL</b>			784		
<b>TOTAL PARKING REQUIRED</b>			<b>2,204</b>		

MASTER PLAN PARKING SUMMARY

HENRY MAYO NEWHALL MEMORIAL HOSPITAL - MASTER PLAN  
23845 McBean Parkway, Santa Clara, CA 91355

SWA # 060307 NOVEMBER 19, 2008 ARCHITECT



38 east holly st  
pasadena, ca 91103  
913 793-1111

**EXHIBIT "H"**  
**Right of First Offer – Lease of Vacant Space**

For purposes of Section 5.3 of the Agreement, HMNMH shall have the right of first offer to lease any space in any Existing Improvement or Medical Building on the G&L Property that from time to time becomes vacant (the **"First-Offer Space"**). G&L shall provide HMNMH with written notice (the **"First-Offer Notice"**) when any First-Offer Space becomes available for lease (the **"Specific First-Offer Space"**), which notice will state the basic economic terms and conditions of such lease, including the rent. HMNMH shall have twenty (20) days after receipt of the First-Offer Notice to accept or decline to lease the Specific First-Offer Space on the same terms as contained in the First-Offer Notice. If HMNMH declines to lease the Specific First-Offer Space or fails to agree to lease the Specific First-Offer Space in writing within twenty (20) days following receipt of the First-Offer Notice, HMNMH's right of first offer with respect to such Specific First-Offer Space shall terminate until such right arises again pursuant to this Exhibit "H". Upon such termination, G&L may then lease the Specific First-Offer Space to any tenant, subject to the provisions of Section 5.2 of the Agreement; provided, however, that if G&L desires to lease the Specific First-Offer Space to another tenant at a rent that is less than ninety percent (90%) of the rental amount set forth in the First-Offer Notice, G&L shall provide HMNMH with a right of first offer on the revised terms (the **"Revised Lease Offer"**) and HMNMH may, within five (5) business days after receipt of the Revised Lease Offer, elect to lease the Specific First-Offer Space on the same terms as contained in the Revised Lease Offer. If G&L has not entered into a lease to lease the Specific First-Offer Space to a tenant other than HMNMH, within one hundred eighty (180) days after HMNMH's receipt of the First-Offer Notice, or if such Specific First-Offer is leased to a tenant but later becomes vacant, then G&L shall again offer such Specific First-Offer to HMNMH pursuant to the procedures set forth above in this Exhibit "H".

**EXHIBIT "I"**  
**Right to First of Offer – Sale of G&L Property**

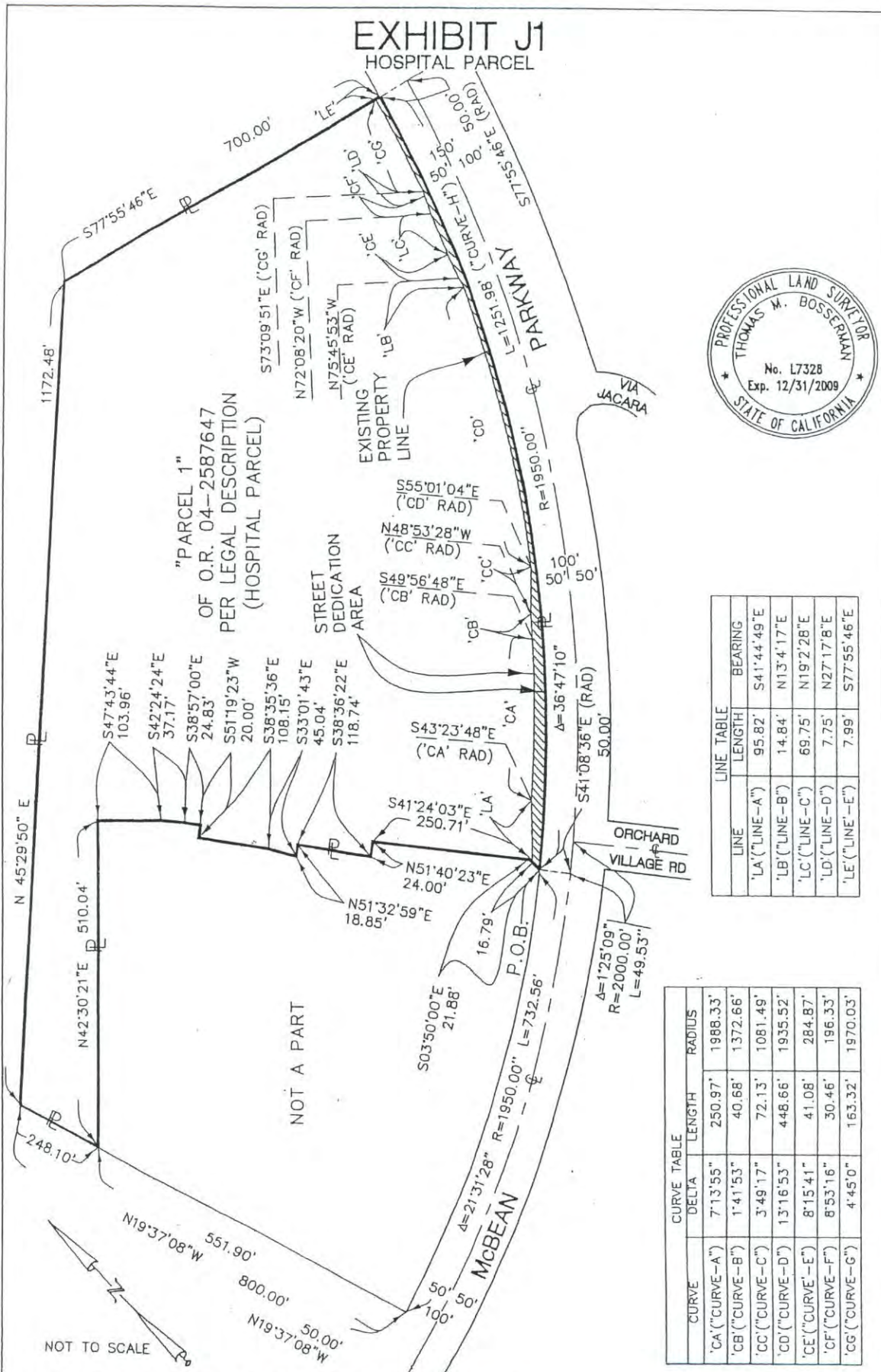
For purposes of Section 5.4 of the Agreement, HMNMH shall have the right of first offer to purchase all or part of the G&L Property in accordance with the procedures in this Exhibit "I". If G&L decides to sell all or part of the G&L Property (the **Specific First-Offer Property**"), then G&L shall provide to HMNMH the specific terms upon which G&L is willing to sell the Specific First-Offer Property (the **"Offer Terms"**). HMNMH shall have forty-five (45) days after receipt of the Offer Terms to accept or decline to purchase the Specific First-Offer Property on the Offer Terms. If HMNMH declines to purchase the Specific First-Offer Property or fails to agree to purchase the Specific First-Offer Property in writing within forty-five (45) days following receipt of the Offer Terms, HMNMH's right of first refusal with respect to the Specific First-Offer Property shall expire and be null and void. G&L may then sell the Specific First-Offer Property to any purchaser; provided, however, that if G&L desires to sell the Specific First-Offer Property to another purchaser at a purchase price that is less than ninety percent (90%) of the purchase price set forth in the Offer Terms, G&L will provide HMNMH with a right of first offer on the revised terms (the **"Revised Offer Terms"**) and HMNMH may, within ten (10) days after receipt of the Revised Offer Terms elect to purchase the Specific First-Offer Property on the Revised Offer Terms. If G&L has not entered into a contract to sell the Specific First-Offer Property within one hundred eighty (180) days after HMNMH's election not to purchase as set forth above and G&L still desires to sell the Specific First-Offer Property, then G&L shall again offer such Specific First-Offer Property to HMNMH pursuant to the procedures set forth above, except that HMNMH shall only have fifteen (15) days, instead of forty-five (45) days, after receipt of the Offer Terms to elect to accept or decline to purchase the Specific First-Offer Property on the Offer Terms.



**EXHIBIT "J"**  
**Depiction and Description of Dedicated Area**

Maps depicting the Dedicated Area and legal descriptions of the Dedicated Area are on the following pages

# EXHIBIT J1 HOSPITAL PARCEL



LINE	LENGTH	BEARING
'LA' ("LINE-A")	95.82'	S41°44'49"E
'LB' ("LINE-B")	14.84'	N13°4'17"E
'LC' ("LINE-C")	69.75'	N19°2'28"E
'LD' ("LINE-D")	7.75'	N27°17'8"E
'LE' ("LINE-E")	7.99'	S77°55'46"E

CURVE	DELTA	LENGTH	RADIUS
'CA' ("CURVE-A")	7°13'55"	250.97'	1988.33'
'CB' ("CURVE-B")	1°41'53"	40.68'	1372.66'
'CC' ("CURVE-C")	3°49'17"	72.13'	1081.49'
'CD' ("CURVE-D")	13°16'53"	448.66'	1935.52'
'CE' ("CURVE-E")	8°15'41"	41.08'	284.87'
'CF' ("CURVE-F")	8°53'16"	30.46'	196.33'
'CG' ("CURVE-G")	4°45'0"	163.32'	1970.03'

EXHIBIT J1  
LEGAL DESCRIPTION FOR STREET DEDICATION  
HOSPITAL PARCEL

THAT PORTION OF "PROPOSED PARCEL 1", HEREINAFTER KNOWN AS "PARCEL 1", OF THAT CERTAIN "CERTIFICATE OF COMPLIANCE", IN THE CITY OF SANTA CLARITA, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS RECORDED OCTOBER 7, 2004 AS INSTRUMENT NO. 04-2587647, OF OFFICIAL RECORDS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY, DESCRIBED AS FOLLOWS:

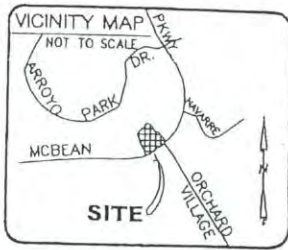
BEGINNING AT THE MOST SOUTHERLY SOUTHWEST CORNER OF SAID PARCEL 1, SAID SOUTHWEST CORNER BEING THE SOUTHERLY TERMINUS OF THE WESTERLY LINE OF SAID PARCEL 1 SHOWN AS HAVING A BEARING AND DISTANCE OF SOUTH 03E50'00" EAST 21.88 FEET ON EXHIBIT "B" OF SAID CERTIFICATE OF COMPLIANCE; THENCE ALONG SAID WESTERLY LINE OF PARCEL 1, NORTH 03E50'00" WEST, 16.79 FEET; THENCE LEAVING SAID WESTERLY LINE ON A LINE, HEREINAFTER KNOWN AS "LINE- A", BEARING NORTH 41E44'49"EAST, A DISTANCE OF 95.82 FEET TO A POINT ON A NON-TANGENT CURVE, HEREAFTER KNOWN AS "CURVE-A", CONCAVE NORTHWESTERLY, HAVING A RADIUS OF 1988.33 FEET, A RADIAL LINE TO SAID POINT BEARS SOUTH 43E22'48" EAST; THENCE NORTHEASTERLY ALONG SAID "CURVE-A" AN ARC DISTANCE OF 250.97 FEET THROUGH A CENTRAL ANGLE OF 07E13'55" TO A POINT ON A NON-TANGENT CURVE, HEREAFTER KNOWN AS "CURVE-B", CONCAVE NORTHWESTERLY, HAVING A RADIUS OF 1372.66 FEET, A RADIAL LINE TO SAID POINT BEARS SOUTH 49E56'48" EAST, THENCE NORTHEASTERLY ALONG SAID "CURVE-B" AN ARC DISTANCE OF 40.68 FEET THROUGH A CENTRAL ANGLE OF 01E41'53" TO A POINT ON A NON-TANGENT CURVE, HEREINAFTER KNOWN AS "CURVE-C", CONCAVE SOUTHEASTERLY, HAVING A RADIUS OF 1081.49 FEET, A RADIAL LINE TO SAID POINT BEARS NORTH 48E53'28" WEST; THENCE NORTHEASTERLY ALONG SAID "CURVE-C" AN ARC DISTANCE OF 72.13 FEET THROUGH A CENTRAL ANGLE OF 03E49'17" TO A POINT ON A NON-TANGENT CURVE, HEREINAFTER KNOWN AS "CURVE-D", CONCAVE NORTHWESTERLY, HAVING A RADIUS OF 1935.52 FEET, A RADIAL LINE TO SAID POINT BEARS SOUTH 55E01'04" EAST; THENCE NORTHEASTERLY ALONG SAID "CURVE-D" AN ARC DISTANCE OF 448.66 FEET THROUGH A CENTRAL ANGLE OF 13E16'53" TO A POINT ON A NON-TANGENT LINE, HEREINAFTER KNOWN AS "LINE-B", HAVING A

EXHIBIT J1  
LEGAL DESCRIPTION FOR STREET DEDICATION  
HOSPITAL PARCEL

BEARING NORTH 13E04'17" EAST; THENCE ALONG SAID "LINE-B", NORTH 13E04'17" EAST, 14.84 FEET

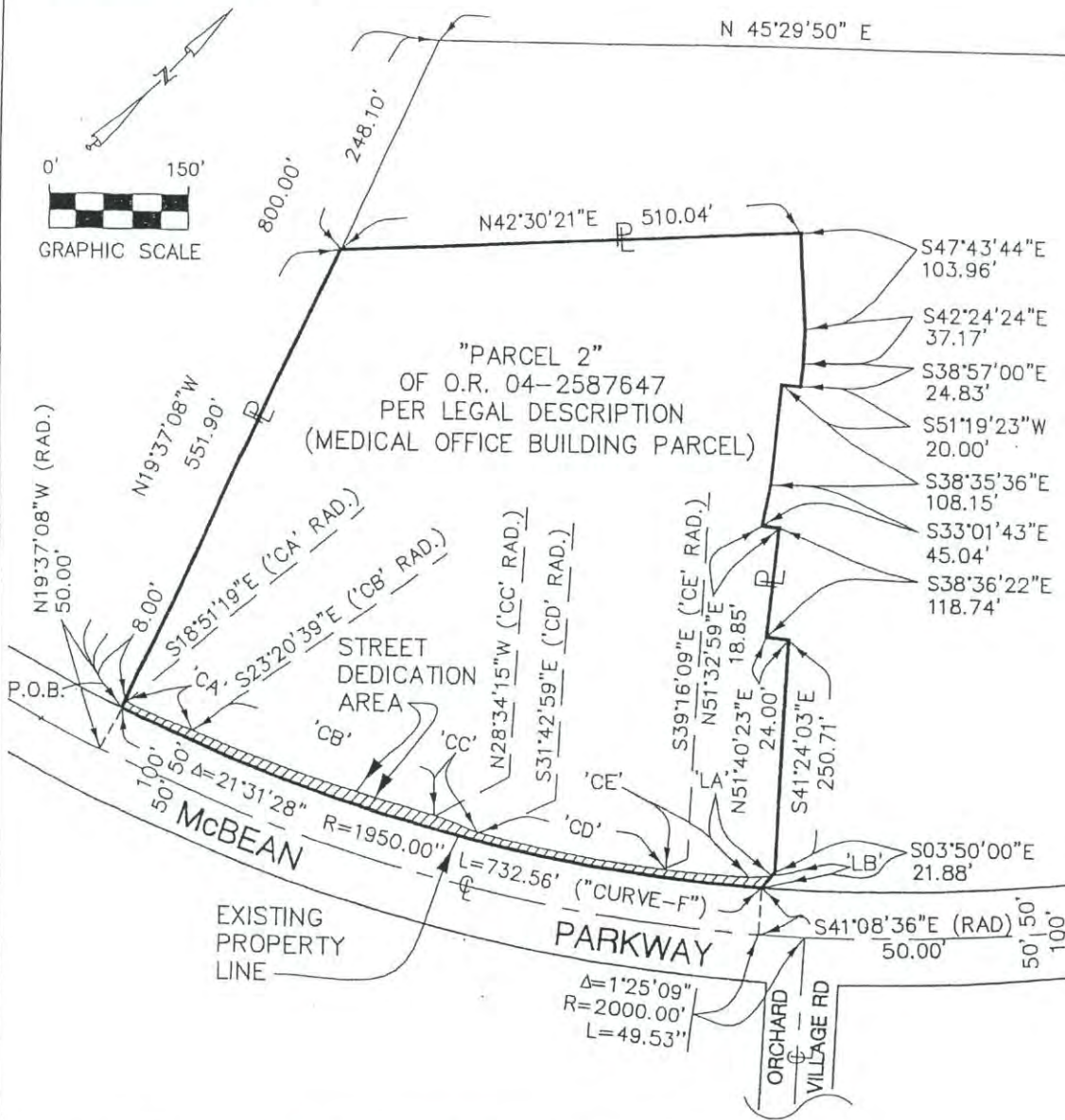
TO A POINT ON A NON-TANGENT CURVE, HEREINAFTER KNOWN AS "CURVE-E", CONCAVE SOUTHEASTERLY, HAVING A RADIUS OF 284.87 FEET, A RADIAL LINE TO SAID POINT BEARS NORTH 75E45'53" WEST; THENCE NORTHEASTERLY ALONG SAID "CURVE-E" AN ARC DISTANCE OF 41.08 FEET THROUGH A CENTRAL ANGLE OF 08E15'41" TO A POINT ON A NON-TANGENT LINE, HEREINAFTER KNOWN AS "LINE-C", HAVING A BEARING NORTH 19E02'28" EAST; THENCE ALONG SAID "LINE-C", NORTH 19E02'28" EAST, 69.75 FEET TO A POINT ON A NON-TANGENT CURVE, HEREINAFTER KNOWN AS "CURVE-F", CONCAVE SOUTHEASTERLY, HAVING A RADIUS OF 196.33 FEET, A RADIAL LINE TO SAID POINT BEARS NORTH 72E08'20" WEST; THENCE NORTHEASTERLY ALONG SAID "CURVE-F" AN ARC DISTANCE OF 30.46 FEET THROUGH A CENTRAL ANGLE OF 08E53'16" TO A POINT ON A NON-TANGENT LINE, HEREINAFTER KNOWN AS "LINE-D", HAVING A BEARING NORTH 27E17'08" EAST; THENCE ALONG SAID "LINE-D", NORTH 27E17'08" EAST, 7.75 FEET TO A POINT ON A NON-TANGENT CURVE, HEREINAFTER KNOWN AS "CURVE-G", CONCAVE NORTHWESTERLY, HAVING A RADIUS OF 1970.03 FEET, A RADIAL LINE TO SAID POINT BEARS SOUTH 73E09'51" EAST; THENCE NORTHEASTERLY ALONG SAID "CURVE-G" AN ARC DISTANCE OF 163.32 FEET THROUGH A CENTRAL ANGLE OF 04E45'00" TO A POINT ON A NON-TANGENT LINE, HEREINAFTER KNOWN AS "LINE-E", HAVING A BEARING SOUTH 77E55'46" EAST, SAID "LINE-E" BEING THE NORTHEAST LINE OF SAID PARCEL 1, THENCE ALONG SAID "LINE-E", SOUTH 77E55'46" EAST, 7.99 FEET TO A POINT ON A NON-TANGENT CURVE, HEREINAFTER KNOWN AS "CURVE-H", CONCAVE NORTHWESTERLY HAVING A RADIUS OF 1950.00 FEET, SAID "CURVE-H" BEING A SOUTHEAST LINE OF SAID PARCEL 1, A RADIAL LINE TO SAID POINT BEARS SOUTH 77E55'46" EAST; THENCE SOUTHWESTERLY ALONG SAID "CURVE-H" AN ARC DISTANCE OF 1251.98 FEET THROUGH A CENTRAL ANGLE OF 36E47'10" TO THE POINT OF BEGINNING.

END OF LEGAL DESCRIPTION



# EXHIBIT J2

## MEDICAL OFFICE BUILDING PARCEL



CURVE TABLE			
CURVE	DELTA	LENGTH	RADIUS
'CA' ("CURVE-A")	06°59'20"	79.21'	649.37'
'CB' ("CURVE-B")	07°11'31"	278.29'	2217.05'
'CC' ("CURVE-C")	09°17'48"	52.32'	322.46'
'CD' ("CURVE-D")	06°14'54"	211.65'	1940.76'
'CE' ("CURVE-E")	02°56'45"	93.80'	1824.29'

LINE TABLE		
LINE	LENGTH	BEARING
'LA' ("LINE-A")	24.37'	N41°44'49"E
'LB' ("LINE-B")	16.79'	S03°50'00"E

P.O.B. = SW'LY CORNER, "PARCEL 2" PER LEGAL DESCRIPTION

EXHIBIT J2

LEGAL DESCRIPTION FOR STREET DEDICATION  
MEDICAL OFFICE BUILDING PARCEL

THAT PORTION OF "PROPOSED PARCEL 2", HEREINAFTER KNOWN AS "PARCEL 2", OF THAT CERTAIN "CERTIFICATE OF COMPLIANCE", IN THE CITY OF SANTA CLARITA, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS RECORDED OCTOBER 7, 2004 AS INSTRUMENT NO. 04-2587647, OF OFFICIAL RECORDS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY, DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHWESTERLY CORNER OF SAID PARCEL 2; THENCE ALONG THE SOUTHWEST LINE OF SAID PARCEL 2, NORTH 19E 37' 08" WEST 8.00 FEET TO A POINT ON A NON-TANGENT CURVE, HEREAFTER KNOWN AS "CURVE-A", CONCAVE NORTHWESTERLY, HAVING A RADIUS OF 649.37 FEET, A RADIAL LINE TO SAID POINT BEARS SOUTH 18E51'19" EAST; THENCE NORTHEASTERLY ALONG SAID "CURVE-A" AN ARC DISTANCE OF 79.21 FEET THROUGH A CENTRAL ANGLE OF 06E59'20" TO A POINT ON A NON-TANGENT CURVE, HEREAFTER KNOWN AS "CURVE-B", CONCAVE NORTHWESTERLY, HAVING A RADIUS OF 2217.05 FEET, A RADIAL LINE TO SAID POINT BEARS SOUTH 23E20'39" EAST, THENCE NORTHEASTERLY ALONG SAID "CURVE-B" AN ARC DISTANCE OF 278.29 FEET THROUGH A CENTRAL ANGLE OF 07E11'31" TO A POINT ON A NON-TANGENT CURVE, HEREAFTER KNOWN AS "CURVE-C", CONCAVE SOUTHEASTERLY, HAVING A RADIUS OF 322.46 FEET, A RADIAL LINE TO SAID POINT BEARS NORTH 28E34'15" WEST; THENCE NORTHEASTERLY ALONG SAID "CURVE-C" AN ARC DISTANCE OF 52.32 FEET THROUGH A CENTRAL ANGLE OF 09E17'48" TO A POINT ON A NON-TANGENT CURVE, HEREAFTER KNOWN AS "CURVE-D", CONCAVE NORTHWESTERLY, HAVING A RADIUS OF 1940.76 FEET, A RADIAL LINE TO SAID POINT BEARS SOUTH 31E42'59" EAST; THENCE NORTHEASTERLY ALONG SAID "CURVE-D" AN ARC DISTANCE OF 211.65 FEET THROUGH A CENTRAL ANGLE OF 06E14'54" TO A POINT ON A NON-TANGENT CURVE, HEREAFTER KNOWN AS "CURVE-E", CONCAVE NORTHWESTERLY, HAVING A RADIUS OF 1824.29 FEET, A RADIAL LINE TO SAID POINT BEARS SOUTH 39E16'09" EAST; THENCE NORTHEASTERLY ALONG SAID "CURVE-E" AN ARC DISTANCE OF 93.80 FEET THROUGH A CENTRAL ANGLE OF 02E56'45" TO A NON-TANGENT LINE, HEREAFTER KNOWN AS "LINE-A", SAID "LINE-A" HAVING A BEARING OF

EXHIBIT J2

LEGAL DESCRIPTION FOR STREET DEDICATION  
MEDICAL OFFICE BUILDING PARCEL

NORTH 41E44'49" EAST; THENCE ALONG SAID "LINE-A", NORTH 41E44'49" EAST, 24.37 FEET TO POINT ON THE EASTERLY LINE OF SAID PARCEL 2, HEREINAFTER KNOWN AS "LINE-B", SAID "LINE-B" BEARS SOUTH 03E50'00" EAST; THENCE ALONG SAID "LINE-B", SOUTH 03E50'00" EAST, 16.79 FEET TO POINT ON A NON-TANGENT CURVE, HEREINAFTER KNOWN A "CURVE-F", CONCAVE NORTHWESTERLY AND HAVING A RADIUS OF 1950.00 FEET, SAID "CURVE-F" BEING ON THE SOUTHEASTERLY LINE OF SAID PARCEL 2, A RADIAL LINE TO SAID POINT BEARS SOUTH 41E08'36" EAST; THENCE SOUTHWESTERLY ALONG SAID "CURVE-F" AN ARC DISTANCE OF 732.56 FEET THROUGH A CENTRAL ANGLE OF 21E31'28" TO THE POINT OF BEGINNING.

END OF LEGAL DESCRIPTION

**EXHIBIT "K"**  
**Realignment Improvements**

In addition to the EIR required Traffic Mitigation Improvements set forth in Exhibit "F" to the Agreement, Developer shall construct the following Realignment Improvements to McBean Parkway prior to the issuance of the applicable Certificates of Occupancy for the Project set forth below:

1. **Prior to Issuance of the Certificate of Occupancy for MOB1.** The following Realignment Improvements must be completed by Developer prior to the issuance by the City of a Certificate of Occupancy for MOB1:

- (a) Construct a turn-out lane for buses along westbound McBean Parkway, west of Avenue Navarre, including transition, at the location required for the Designated Configuration in accordance with the Frontage Design Plan.
- (b) In performing the Traffic Mitigation Improvements to the intersection of McBean Parkway and Orchard Village Road (Intersection #55) that are set forth in Paragraph 1(c) of Exhibit "F", the grades for the intersection shall be in accordance with the Designated Configuration for the intersection on the Frontage Design Plan.
- (c) Modify the eastbound left-turn pocket on McBean Parkway at Avenida Navarre to provide for a left-turn pocket with 300 lineal feet of storage plus 120 lineal feet of additional taper at the location required for the Designated Configuration in accordance with the Frontage Design Plan.
- (d) Modify the northbound left-turn pocket on McBean Parkway at Orchard Village Road (Intersection #55) to provide for a left-turn pocket with 300 lineal feet of storage plus 90 lineal feet of additional taper at the location required for the Designated Configuration in accordance with the Frontage Design Plan.

2. **Prior to Issuance of the Certificate of Occupancy for Inpatient Building or MOB2.** The following Realignment Improvements must be completed by Developer prior to the issuance by the City of a Certificate of Occupancy for the Inpatient Building or MOB2:

- (a) Move the existing traffic signals on the northern side of McBean Parkway at the intersection of McBean Parkway and Orchard Village Road (Intersection #55) to the location required for the Designated Configuration in accordance with the Frontage Design Plan.
- (b) In performing the Traffic Mitigation Improvements to the intersection of McBean Parkway and Orchard Village Road (Intersection #55) that are set forth in Paragraph 2(b) of Exhibit "F", construct the westbound right turn lane to provide 300 lineal feet of storage plus 120 lineal feet of additional taper at the location required for the Designated Configuration in accordance with the Frontage Design



Plan.

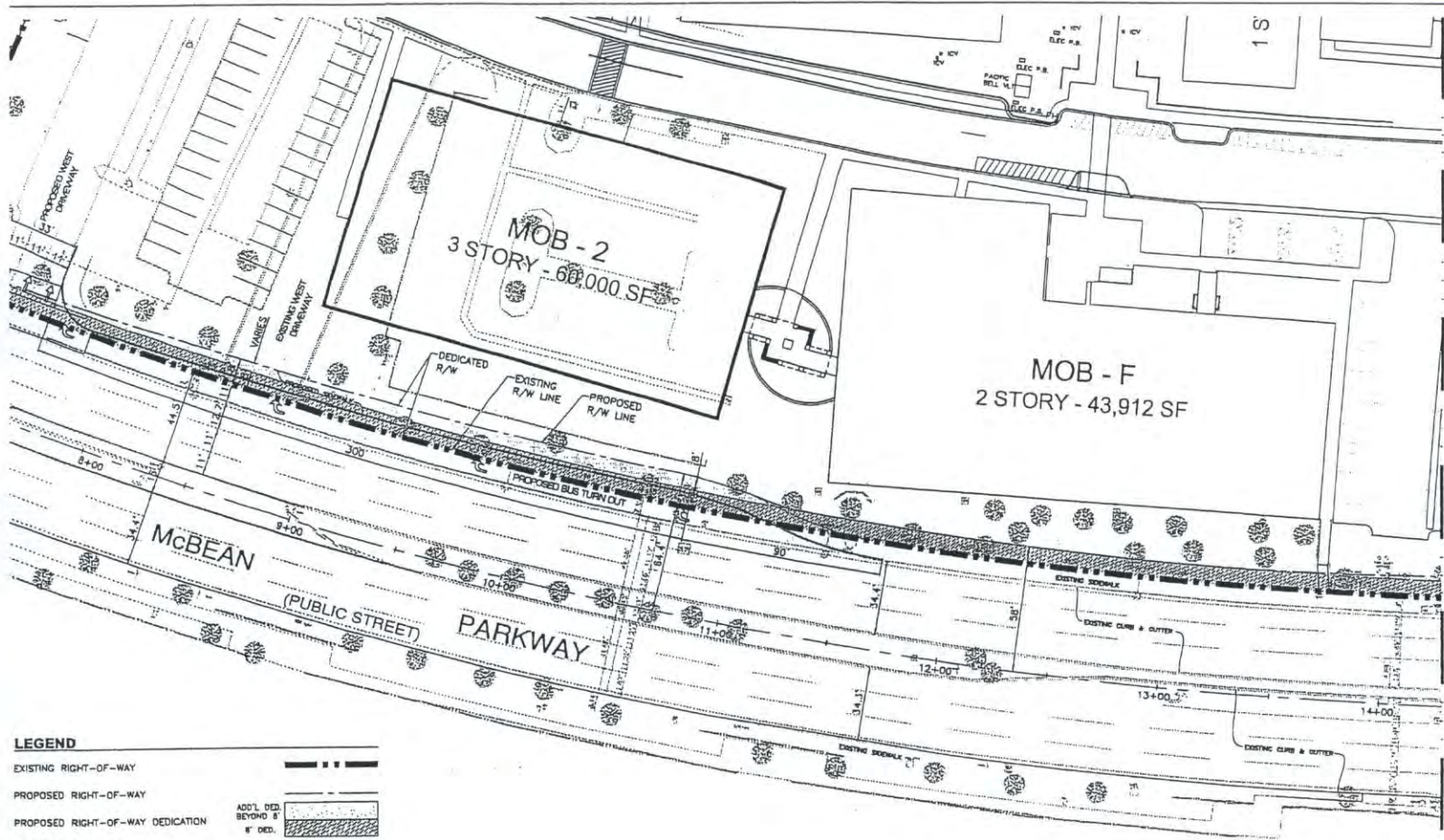
3. **Prior to Issuance of the Certificate of Occupancy for MOB 2 Only.**

- (a) Prior to the issuance by the City of a Certificate of Occupancy for MOB2, Developer shall complete the construction of a westerly driveway for the Campus Property and modify the existing median to include an eastbound left turn pocket on McBean Parkway with 300 lineal feet of storage plus 120 lineal feet of additional taper at the location required for the Designated Configuration in accordance with the Frontage Design Plan.
- (b) Construct a bus turn-out lane and the right-turn lane at the westerly driveway for the Campus Property to provide for a right-turn pocket with 300 lineal feet of storage plus 90 lineal feet of additional taper at the location required for the Designated Configuration in accordance with the Frontage Design Plan.

**EXHIBIT "L"**  
Frontage Design Plan

The Frontage Design Plan is on the following pages

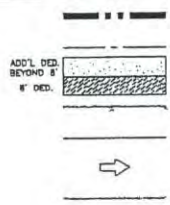




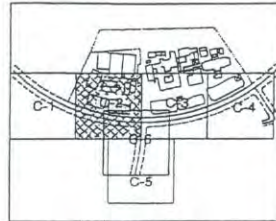
MATCHLINE-SEE SHEET C-3

**LEGEND**

- EXISTING RIGHT-OF-WAY
- PROPOSED RIGHT-OF-WAY
- PROPOSED RIGHT-OF-WAY DEDICATION
- NEW TRAFFIC SIGNALS
- NEW CURB
- NEW DIRECTIONAL ARROWS
- NEW STRIPING



**EXHIBIT "L"**



KEYMAP

REVISION	
DATE	
BY	
APP'D	
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BY	
APP'D	
DATE	
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APP'D	
DATE	

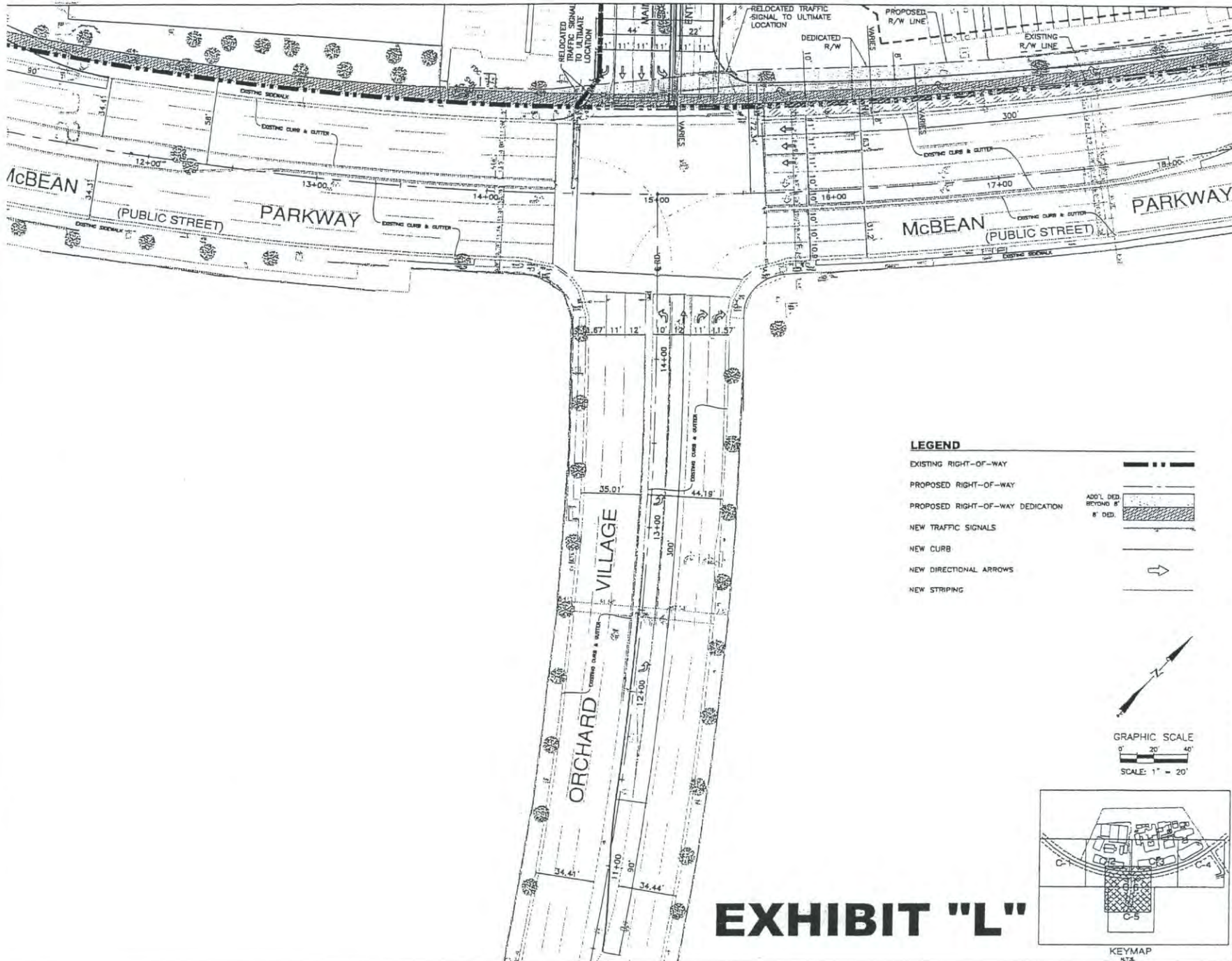
**HENRY MAYO NEWHALL MEMORIAL HOSPITAL**  
 23845 McBEAN PARKWAY  
 VALENCIA, CALIFORNIA 91355  
 CONCEPTUAL McBEAN PARKWAY PROPOSED IMPROVEMENT  
 REALIGNMENT(ULTIMATE) EXHIBIT, 08-08-08 REVISION

08/08/2008  
 AS SHOWN  
 C-2  
 SHEET 2 OF 5  
 07-1773-1864-REV-002.DWG







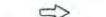


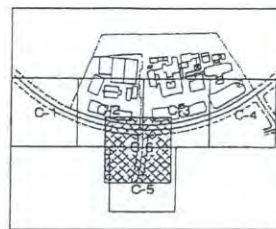
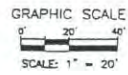
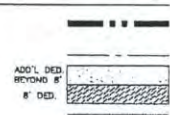








**LEGEND**

- EXISTING RIGHT-OF-WAY 
- PROPOSED RIGHT-OF-WAY 
- PROPOSED RIGHT-OF-WAY DEDICATION 
- NEW TRAFFIC SIGNALS 
- NEW CURB 
- NEW DIRECTIONAL ARROWS 
- NEW STRIPING 



KEYMAP  
N.T.S.

DATE	BY	CHECKED	APPROVED	SCALE
				
<b>DCA CIVIL ENGINEERING GROUP</b> <small>• CIVIL ENGINEERING • LAND PLANNING • SURVEYING • UTILITY • ALTA SURVEYS</small>				
<b>HENRY MAYO NEWHALL MEMORIAL HOSPITAL</b> 23845 McBEAN PARKWAY VALERIA, CALIFORNIA 91355 CONCEPTUAL McBEAN PARKWAY PROPOSED IMPROVEMENT REALIGNMENT(ULTIMATE) EXHIBIT, 08-06-08 REVISION				
	08/06/2008 25 SDCM C-6 SHEET 6 OF 8 <small>07-1272-186-000-003-07</small>			

**EXHIBIT "L"**



**APPENDIX C**  
*General Plan Consistency*



## APPENDIX C

### General Plan Consistency

**Table C-1**  
**Project Consistency with Applicable General Plan Goals and Policies**

City of Santa Clarita General Plan Implementing Goals and Policies	Consistency of Proposed Project
<i>Land Use Element</i>	
<p><b>Policy LU 1.1.4:</b> Preserve community character by maintaining natural features that act as natural boundaries between developed areas, including significant ridgelines, canyons, rivers and drainage courses, riparian areas, topographical features, habitat preserves, or other similar features, where appropriate.</p>	<p><b>Consistent.</b> The Specific Plan does not allow for any development within the setback areas, which provide natural buffers between the Specific Plan area and the adjacent residential uses.</p>
<p><b>Policy LU 1.1.5:</b> Increase infill development and re-use of underutilized sites within and adjacent to developed urban areas to achieve maximum benefit from existing infrastructure and minimize loss of open space, through redesignation of vacant sites for higher density and mixed use, where appropriate.</p>	<p><b>Consistent.</b> The HMNH Specific Plan allows for the expansion of hospital and medical office uses within the existing hospital campus thereby maximizing the land's utilization for these types of functions and includes structured parking to accommodate additional development within surface parking lots, thereby maximizing benefit from existing infrastructure.</p>
<p><b>Goal LU 6:</b> A scenic and beautiful urban environment that builds on the community's history and natural setting.</p>	<p><b>Consistent.</b> The HMNH Specific Plan includes development regulations and design guidelines for new development that focus on consistency and compatibility with the existing development on the site and in the surrounding area. All future development will be subject to approval by the City to ensure consistency with the Specific Plan.</p>
<p><b>Policy LU 6.3.1:</b> Promote planting of street trees throughout urban areas in the Santa Clarita Valley.</p>	<p><b>Consistent.</b> The HMNH Specific Plan provides for the preservation of existing street trees, as well as the planting of new and replacement street trees. The Specific Plan will continue to implement the approved Landscape Plan from the 2008 Master Plan, which provides a conceptual overview of future landscaping and street trees within the Specific Plan area.</p>
<p><b>Policy LU 6.5.1:</b> Require use of high quality, durable, and natural-appearing building materials pursuant to applicable ordinances.</p> <p><b>Policy LU 6.5.2:</b> Encourage the use of designs and architectural styles that incorporate classic and timeless architectural features.</p> <p><b>Policy LU 6.5.3:</b> Require architectural enhancement and articulation on all sides of buildings (360 degree architecture), with special consideration at building entrances and corners, and along facades adjacent to major arterial streets.</p> <p><b>Policy LU 6.5.4:</b> Evaluate new development in consideration of its context, to ensure that buildings create a coherent living environment, a cohesive urban fabric, and contribute to a sense of place consistent with the surrounding neighborhoods.</p>	<p><b>Consistent.</b> The HMNH Specific Plan is characterized by a consistent design theme and building materials that complement on- and off-site development. The Specific Plan includes required building setbacks and step-backs from adjacent residential neighborhoods, architectural features on the building facades, landscaping design guidelines, and building height zones that soften the appearance of the campus from surrounding areas.</p>

## APPENDIX C (Continued)

**Table C-1  
Project Consistency with Applicable General Plan Goals and Policies**

City of Santa Clarita General Plan Implementing Goals and Policies	Consistency of Proposed Project
<b>Goal LU 7:</b> Environmentally responsible development through site planning, building design, waste reduction, and responsible stewardship of resources.	<b>Consistent.</b> The Specific Plan provides for environmentally responsible development that is sensitive to the environment, maximizes use of existing infrastructure, and meets City standards for site planning, architectural design, parking, and pedestrian access. Expanding healthcare services at this location increases the significance of this site to the region.
<b>Policy LU 7.1.1:</b> Require shade trees within parking lots and adjacent to buildings to reduce the heat island effect, in consideration of Fire Department fuel modification restrictions.	<b>Consistent.</b> The HMNH Specific Plan provides for the preservation of existing trees, as well as the planting of new and replacement trees. The Specific Plan will continue to implement the approved 2008 Master Plan Landscape Plan, which provides a conceptual overview of future landscaping and trees within the Specific Plan area, including retention of existing trees within the remaining parking lots and adjacent to buildings.
<b>Policy LU 7.1.3:</b> Encourage development of energy-efficient buildings, and discourage construction of new buildings for which energy efficiency cannot be demonstrated.	<b>Consistent.</b> The HMNH Specific Plan requires all buildings to be compliant with the California Office of Statewide Health Planning and Development (OSHPD) and California Building Standards Code energy efficiency standards. Southern California Edison is granting HMNH tax credits for successfully participating in their Savings by Design program by reducing its electrical demands in the new Inpatient Building.
<b>Policy LU 7.2.3:</b> Require that all new development proposals demonstrate a sufficient and sustainable water supply prior to approval.	<b>Consistent.</b> Section 5.17, Water Supply, of the HMNH Master Plan EIR and Addendum determined that adequate water supplies are available to serve buildout of the Specific Plan.
<b>Policy LU 7.3.2:</b> Maintain stormwater runoff onsite by directing drainage into rain gardens, natural landscaped swales, rain barrels, permeable areas, and use of drainage areas as design elements, where feasible and reasonable.	<b>Consistent.</b> Implementation of the Specific Plan would not result in increased local runoff. As part of the Inpatient Building approved under the 2008 Master Plan, a new catch basin will be installed for stormwater runoff per Standard Urban Stormwater Mitigation Plan (SUSMP) requirements.
<b>Policy LU 7.4.1:</b> Require the use of drought tolerant landscaping, native California plant materials, and evapotranspiration (smart) irrigation systems.	<b>Consistent.</b> The Specific Plan Landscape Plan focuses primarily on the use of native and drought-tolerant tree and plant species. Plants listed that are non-native or not drought-tolerant will be used sparingly and only in areas that require their unique properties. The use of grass turf will be limited. New irrigation systems will be designed, installed, operated, and maintained in conformance with the State Water Efficient Landscape Ordinance.
<b>Policy LU 7.4.2:</b> Require the use of low-flow fixtures in all non-residential development and residential development with five or more dwelling units, which may include but are not limited to water conserving shower heads, toilets, waterless urinals and motion-sensor faucets, and encourage use of such fixtures in building retrofits as appropriate.	<b>Consistent.</b> New development within the Specific Plan will be required to comply with OSHPD and the State Building Standards Code, which requires the use of low-flow fixtures and other water-conserving features.
<b>Policy LU 7.5.1:</b> Ensure that all new development provides adequate space for recycling receptacles and bins on site.	<b>Consistent.</b> New development within the Specific Plan will be required to comply with City standards for recycling

## APPENDIX C (Continued)

**Table C-1**  
**Project Consistency with Applicable General Plan Goals and Policies**

City of Santa Clarita General Plan Implementing Goals and Policies	Consistency of Proposed Project
	receptacles and bins on site.
<b>Policy LU 7.6.1:</b> Limit outdoor lighting levels to the minimum needed for safety and security, and encourage lower lighting levels when businesses are closed.	<b>Consistent.</b> All lighting must comply with Chapter 17.51.050, Outdoor Lighting Standards, of the City’s Unified Development Code, as well as State and local safety and illumination requirements. Although the campus is a 24-hour operation, the Specific Plan’s outdoor lighting will be designed to ensure that minimum lighting levels are provided for safety and security needs. Light pollution will be minimized as much as programmatically feasible.
<b>Policy LU 7.6.4:</b> Encourage site designs that protect oak trees, hillsides, and biological resources through creative solutions.	<b>Consistent.</b> The HMNH Specific Plan preserves the hillside between the hospital campus and the adjacent residential uses to serve as a natural buffer between the uses. The Specific Plan does not allow for any development within the buffer around the hospital campus, which is identified as Zone 1 in Figure 4-1, Building Height Zones, in the Specific Plan.
<b>Goal LU 8:</b> Equitable and convenient access to social, cultural, educational, civic, medical, and recreational facilities and opportunities for all residents.  <b>Policy LU 8.1.7:</b> Work with medical service providers to facilitate preservation and enhancement of health services, including the Santa Clarita Valley’s trauma center, provided applications are in conformance with applicable General Plan policies and environmental requirements.	<b>Consistent.</b> The HMNH Specific Plan provides for the preservation and expansion of the existing hospital and medical services to help meet the healthcare needs of all segments of the population. The HMNH medical campus currently includes the Santa Clarita Valley’s only trauma center.
<b>Goal LU 9:</b> Adequate public facilities and services, provided in a timely manner and in appropriate locations to serve existing and future residents and businesses.	<b>Consistent.</b> The Specific Plan seeks to provide adequate medical facilities and services to the public in a timely manner. The Specific Plan area is an appropriate location because the Specific Plan facilitates infill development on an existing campus to enhance medical services.
<b>Policy LU 9.1.1:</b> Ensure construction of adequate infrastructure to meet the needs of new development prior to occupancy.	<b>Consistent.</b> Sufficient existing infrastructure supports the campus and surrounding area. The Specific Plan does not require the extension of infrastructure systems to meet project-related demands and will contribute to future public infrastructure improvements through payment of any applicable development fees.
<b>Policy LU 9.2.1:</b> Ensure that the cost of extending new sewer infrastructure is fully borne by the new development that is served, and is not passed on to the existing community.  <b>Policy LU 9.2.2:</b> Require that all new development mitigates its impact on existing sewer capacity by upgrading facilities when warranted or payment of a fee to allow construction of new facilities when needed.  <b>Policy LU 9.2.5:</b> Cooperate with the development community to allow reimbursement for the cost of constructed sewer	<b>Consistent.</b> Existing sewer infrastructure is sufficient to support buildout of the Specific Plan. The Specific Plan does not require the expansion of the existing sewer system to meet project-related demands and will contribute to future public infrastructure improvements through payment of any applicable development fees.

## APPENDIX C (Continued)

**Table C-1  
Project Consistency with Applicable General Plan Goals and Policies**

City of Santa Clarita General Plan Implementing Goals and Policies	Consistency of Proposed Project
facilities with a capacity that exceeds what would be required to mitigate a project's own sewer impact.	
<i>Circulation Element</i>	
<b>Goal C 1:</b> An inter-connected network of circulation facilities that integrates all travel modes, provides viable alternatives to automobile use, and conforms with regional plans.	<b>Consistent.</b> Multiple modes of transit that meet the requirements of the American Disabilities Act (ADA) are currently provided on-site, and will continue as such.
<b>Policy C 1.1.1:</b> Reduce dependence on the automobile, particularly single-occupancy vehicle use, by providing safe and convenient access to transit, bikeways, and walkways.	<b>Consistent.</b> The Specific Plan includes a system of pedestrian linkages to promote non-vehicular mobility and access to and throughout the project site. Access to the Specific Plan is available via various alternative transportation facilities, including sidewalks and public transit facilities. There are two proposed bus stops to be located along the McBean Parkway frontage of the hospital campus.
<b>Policy C 2.5.2:</b> Ensure that new development is provided with adequate emergency and/or secondary access for purposes of evacuation and emergency response; require two points of ingress and egress for every subdivision or phase thereof, except as otherwise approved for small subdivisions where physical constraints preclude a second access point.	<b>Consistent.</b> The Specific Plan provides three access points to ensure adequate ingress and egress including ambulances and other emergency vehicles.
<i>Noise Element</i>	
<b>Policy N 1.1.4:</b> Control noise sources adjacent to residential, recreational, and community facilities, and those land uses classified as noise sensitive.	<b>Consistent.</b> The Specific Plan would not cause an increase in noise sources adjacent to noise sensitive land uses.
<i>Open Space and Conservation Element</i>	
<b>Goal CO.1:</b> A balance between the social and economic needs of Santa Clarita Valley residents and protection of the natural environment, so that these needs can be met in the present and in the future.	<b>Consistent.</b> The Specific Plan requires incorporation of conservation measures and technologies into the design of the buildings that meet State standards for building energy efficiency and water usage.
<b>Policy CO 1.5.5:</b> Promote concentration of urban uses within the center of the Santa Clarita Valley through incentives for infill development and rebuilding, in order to limit impacts to open space, habitats, watersheds, hillsides, and other components of the Valley's natural ecosystems.	<b>Consistent.</b> The Specific Plan facilitates infill development within the currently developed Specific Plan area and will not impact the undeveloped hillside.
<b>Policy CO 3.1.5:</b> Promote the use of site-appropriate native or adapted plant materials, and prohibit use of invasive or noxious plant species in landscape designs.	<b>Consistent.</b> The Specific Plan Landscape Plan builds upon the existing landscaping within the Specific Plan area and focuses primarily on the use of native and drought-tolerant tree and plant species. Plants listed that are non-native or not drought-tolerant will be used sparingly and only in areas that require their unique properties. The use of grass turf will be limited. New irrigation systems will be designed, installed, operated, and maintained in conformance with the State Water Efficient Landscape Ordinance.
<b>Policy CO 3.1.7:</b> Limit the use of turf-grass on development sites and promote the use of native or adapted plantings to promote biodiversity and natural habitat.	
<b>Policy CO 3.1.8:</b> On development sites, require tree planting to provide habitat and shade to reduce the heat island effect caused by pavement and buildings.	

## APPENDIX C (Continued)

**Table C-1  
Project Consistency with Applicable General Plan Goals and Policies**

City of Santa Clarita General Plan Implementing Goals and Policies	Consistency of Proposed Project
<b>Policy CO 3.6.1:</b> Minimize light trespass, sky-glow, glare, and other adverse impacts on the nocturnal ecosystem by limiting exterior lighting to the level needed for safety and comfort; reduce unnecessary lighting for landscaping and architectural purposes, and encourage reduction of lighting levels during non-business nighttime hours.	<b>Consistent.</b> All lighting must comply with Chapter 17.51.050, Outdoor Lighting Standards, of the City's Unified Development Code, as well as State and local safety and illumination requirements. The Specific Plan's outdoor lighting will be designed to ensure that lighting does not interfere with on-site or surrounding uses.
<b>Policy CO 4.1.3:</b> Require low water use landscaping in new residential subdivisions and other private development projects, including a reduction in the amount of turf-grass.	<b>Consistent.</b> The Specific Plan Landscape Plan builds upon the existing landscaping within the Specific Plan area and focuses primarily on the use of native and drought-tolerant tree and plant species. Plants listed that are non-native or not drought-tolerant will be used sparingly and only in areas that require their unique properties. The use of grass turf will be limited. New irrigation systems will be designed, installed, operated, and maintained in conformance with the State Water Efficient Landscape Ordinance.
<b>Policy CO 4.1.5:</b> Promote the use of low-flow and/or waterless plumbing fixtures and appliances in all new non-residential development and residential development of five or more dwelling units.	<b>Consistent.</b> The Specific Plan requires incorporation of conservation measures and technologies into the design of the buildings that meet State standards for building water usage.
<b>Policy CO 4.3.2:</b> On previously developed sites proposed for major alteration, provide stormwater management improvements to restore natural infiltration, as required by the reviewing authority.	<b>Consistent.</b> Implementation of the Specific Plan would not result in increased local runoff. As part of the Inpatient Building approved under the 2008 Master Plan, a new catch basin will be installed for stormwater runoff per Standard Urban Stormwater Mitigation Plan (SUSMP) requirements.
<b>Policy CO 4.3.7:</b> Reduce the amount of pollutants entering the Santa Clara River and its tributaries by capturing and treating stormwater runoff at the source, to the extent possible.	
<i>Safety Element</i>	
<b>Goal S 1:</b> Protection of public safety and property from hazardous geological conditions, including seismic rupture and ground shaking, soil instability, and related hazards.	<b>Consistent.</b> Development under the Specific Plan is required to meet or exceed all applicable state and local seismic safety design standards including OSHPD requirements.
<b>Policy S 3.1.3:</b> Require adequate fire flow as a condition of approval for all new development, which may include installation of additional reservoir capacity and/or distribution facilities.	<b>Consistent.</b> The Specific Plan's water system is designed to meet the fire flow requirements of OSHPD and LACFD.
<b>Goal S 4:</b> Protection of public safety and property from hazardous materials.	<b>Consistent.</b> Procedures for handling hospital and medical office hazardous waste; and the transport, use, storage, and disposal of all Specific Plan-related hazardous wastes are subject to all local, state, and federal regulations.

## APPENDIX C (Continued)

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# **APPENDIX D**

*2020 Supplemental Environmental Impact Report  
(EIR) to the Original  
Master Plan EIR and 2016 Addendum*



**ADDENDUM TO THE HENRY MAYO NEWHALL  
MEMORIAL HOSPITAL MASTER  
PLAN CERTIFIED ENVIRONMENTAL IMPACT REPORT  
(SCH#2004111149)**

*Prepared for:*

**City of Santa Clarita**  
23920 Valencia Blvd., Suite 300  
Santa Clarita, California, 91355

*Prepared by:*

**DUDEK**  
38 N. Marengo Avenue  
Pasadena, California 91101  
*Contact: Ruta K. Thomas, REPA*

**MARCH 2016**



# Addendum to the Henry Mayo Newhall Memorial Hospital Master Plan Certified EIR

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# **Addendum to the Henry Mayo Newhall Memorial Hospital Master Plan Certified EIR**

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## **1 INTRODUCTION**

### **1.1 Purpose**

This is an Addendum to the Final Environmental Impact Report SCH#2004111149 (Master Plan EIR) for the Henry Mayo Newhall Memorial Hospital (Hospital) Master Plan (Master Plan), which was approved by the City Council of the City of Santa Clarita (City) on November 19, 2008. The City Council also approved the Master Plan, including Resolution No. 08-101 certifying the Master Plan EIR, Resolution No. 08-102, adopting the Master Plan, and Ordinance No. 08-17, adopting Development Agreement No. 06-001 (Development Agreement).

The purpose of the Master Plan and Development Agreement was to ensure that future expansion of the Hospital campus would not be considered in piecemeal fashion, but rather as part of a comprehensive plan that looked ahead to the ultimate build-out of the Hospital campus. As described in greater detail below, the regulatory framework for the Master Plan has changed since adoption in 2008, necessitating modifications to the Master Plan. The City determined that a Specific Plan was needed to address these modifications and future development of the Hospital campus. The proposed Specific Plan has been prepared in conjunction with applications for a General Plan Amendment to designate the project site as Specific Plan (SP), a zone change to Specific Plan (SP), and amendments to the Master Plan and Development Agreement. The Specific Plan would not replace the Master Plan and Development Agreement; rather the Master Plan and Development Agreement, as amended, would continue to exist as independent entitlements separate from the Specific Plan. Together, these actions are referred to as the “proposed project” and are analyzed collectively throughout this addendum.

### **1.2 Background**

In 2011, the City adopted a General Plan Update, which resulted in a change in the General Plan land use designation and zoning for the Hospital campus from Residential Low (RL) to Public Institutional (PI). In 2013, the City updated its Zoning Code, rezoning the Hospital campus from RL to PI. Subsequent to approval of the Master Plan, the California Building Standards Code was amended to impose mandatory new requirements related to the design and construction of hospitals. Compliance with the new regulations requires an increase in the square footage of the approved Inpatient Building to comply with enlarged 8-foot-wide minimum corridors in all public and patient areas (CBC Section 1224.4.7.1), provide two separate elevators for clean items delivered and soiled items hauled (CBC Section 1224.20.2.7), and a dedicated corridor between food storage areas and the kitchen (CBC Section 1224.20.2.1). The Hospital also seeks to enlarge its Obstetric Services Unit in order to better contract services from insurance providers and serve an increased demand for obstetric services. These changes would require an increase in

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square footage in order to allow construction of two new Caesarean Section rooms in the area between the existing Hospital and the Inpatient Building. In order for the design of the Inpatient Building to comply with the Federal Aviation Administration (FAA) and State required airspace obstruction clearance criteria, the Helipad must be relocated from its conceptual location at the southeast corner of the roof of the Inpatient Building to the north side of the roof. The relocation is necessary so that the elevator leading from the roof directly to the emergency room would not obstruct the flight path. In order to modify the Master Plan to implement the mandatory new California Building Standards Code requirements and permit the expanded obstetric services, it was determined that a Specific Plan was needed to outline existing uses, future uses approved under the Master Plan, proposed modifications to the Master Plan, and provide a cohesive set of standards and guidelines for how future development would occur on the campus.

The Master Plan and Development Agreement provide for a buildout of 667,434 square feet in combined hospital uses and medical office buildings (refer to Table 2, the Proposed Buildout Development Program for detailed square footages). Under the proposed project, at buildout, the amount of hospital and medical office space under the Master Plan (not including parking structures) would increase by 30,566 net square feet, resulting from a 36,966 square foot increase to the approved Inpatient Building and a 6,400-square-foot reduction in the approved Central Plant, which is currently under construction, for a total buildout of 698,000 square feet. The proposed project would not result in increases in the number of programs, employees, overall campus inpatient beds or square footage allocated to administration uses from the approved Master Plan. Building heights approved under the Master Plan would not be increased.

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## **2 SUMMARY OF THE PROPOSED PROJECT**

As described previously, the proposed project includes the proposed Specific Plan, a General Plan Amendment to designate the project site as Specific Plan (SP), a zone change to Specific Plan (SP), and amendments to the Master Plan and Development Agreement. The Specific Plan would not replace the Master Plan and Development Agreement; rather the Master Plan and Development Agreement, as amended, would continue to exist as independent entitlements separate from the Specific Plan.

The approved Master Plan buildings include an “Inpatient Building”, projected to have 120 beds. The Master Plan provides that the Inpatient Building would have 125,363 square feet, and a building height of 85 feet to the top of the parapet and 100 feet to the top of the wind sock and elevator shaft. The Master Plan also includes a new “Central Plant”. The Master Plan provides that the Central Plant would have 10,000 square feet and a building height of 26 feet. However, the Hospital has reduced the size of the Central Plant to 3,600 square feet and its height to 19 feet, 4 inches and, as of the date of this Addendum, it is under construction. As of the date of this Addendum, the 8,872 square foot Loading Dock is also under construction.

The proposed project would:

1. Increase the square footage of the Inpatient Building from 125,363 square feet to 162,329 square feet (an increase of 36,966 square feet), which includes 153,457 square feet for the Inpatient Building, and 8,872 square feet for the Loading Dock.
2. Increase the aggregate square footage of the Master Plan from 327,363 square feet to 357,929 square feet (an increase of 30,566 square feet) to reflect the net increase in square footage in the Master Plan from the increase in square footage of the Inpatient Building and the decrease in square footage of the Central Plant.
3. Relocate the approved Helipad on the roof of the Inpatient Building from the southeast corner to the north side of the roof in order to allow direct elevator access from the roof to the emergency room.
4. Modify the existing Site Plan for the Master Plan to reflect: (i) changes in the footprints of the Inpatient Building and the Central Plant; and (ii) the relocation of the helipad to the rear of the roof of the Inpatient Building.

Under the proposed project, there would be no increases in permitted building heights, number of overall campus inpatient beds, programs or employees or square footage allocated to administration uses from the approved Master Plan.

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The majority of the proposed additional square footage would be located within the basement, first, and second floors of the Inpatient Building. From McBean Parkway, these three levels would be located behind the existing two-story enclosed bridge between the Main Hospital and the Nursing Pavilion Building. Construction of two new Caesarean Section rooms would occur in the “infill area” between the Hospital and the Inpatient Building (the “Infill Area”), which the Master Plan does not now allocate to any particular use. The proposed additional square footage would be mostly interior to the Inpatient Building, and would remain south of the ring road. The ring road establishes the north edge of allowable building area for the campus under the Master Plan.

The net increase in square footage is sought because:

1. The Master Plan identified a conceptual footprint and general program for the Inpatient Building because it was premature to create a detailed design at that time.
2. Since the Master Plan approval, there have been amendments to the California Building Standards Code related to the design and construction of new hospitals, relating primarily to circulation. In order to comply with these mandatory state law requirements an increase in the square footage of the Master Plan is required because:
  - a. There are more exiting corridors required between departments. Each corridor is required to be eight feet wide.
  - b. Corridors connecting food storage areas and the kitchen must be dedicated.
  - c. Two separate elevators for clean items delivered and soiled items hauled are required.
3. In order for the design of the Inpatient Building to comply with the FAA and State required airspace obstruction clearance criteria, the helipad (“Helipad”) must be relocated from its conceptual location at the southeast corner of the roof of the Inpatient Building to the north side of the roof, so that the elevator leading from the roof directly to the Emergency Room would not be in the way of the flight path.
4. The Hospital has determined it would be prudent to replace the existing Obstetric Services Unit so it is better suited to the expanding needs of the community and the contracting of services from insurance providers. As part of the design of the new Obstetric Services Unit, two new Caesarean Section rooms would be constructed in the Infill Area.

A square footage analysis showing the differences between the allocation of the approved square footage in the Master Plan for the Inpatient Building and under the proposed project is provided in Table 1.

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**Table 1  
Campus And Inpatient Building Additional Square Footage**

Floor	Master Plan		Proposed		Difference
Basement	20,894	SF	32,300	SF	11,406
1st Floor	20,894	SF	34,500	SF	13,606
2nd Floor	20,894	SF	31,029	SF	10,135
3rd Floor	20,894	SF	21,500	SF	606
4th Floor	20,894	SF	21,500	SF	606
5th Floor	20,894	SF	21,500	SF	606
<b>TOTAL</b>	<b>125,363</b>	<b>SF</b>	<b>162,329</b>	<b>SF</b>	<b>36,966</b>
<b>INPATIENT BUILDING ADDITIONAL SQUARE FOOTAGE</b>					<b>36,966</b>
Central Plant (under construction)	10,000	SF	3,600	SF	(6,400)
<b>CAMPUS ADDITIONAL SQUARE FOOTAGE</b>					<b>30,566</b>

## 2.1 Project Location

The project site is coterminous with the Master Plan site, encompassing approximately 29.77 acres of land generally located north of the intersection of McBean Parkway and Orchard Village Road, east of Interstate 5 (I-5) in the City; see Figure 1, Regional Map. The Hospital campus was a total of 30.4 acres at the time of approval of the 2008 Master Plan; however, the 2008 Master Plan required the dedication of 0.63 acre to the City for the McBean Parkway traffic improvements, thereby reducing the total project site under the proposed project to 29.77 acres. The project site is located on McBean Parkway at the northern terminus of Orchard Village Road; see Figure 2, Vicinity Map.

## 2.2 Surrounding Uses

The areas adjacent to the project site include the following uses:

- **North:** Land uses consist of single- and multiple-family residences zoned Urban Residential (UR4) and (UR1), open space zoned Open Space (OS), medical office buildings zoned Public Institutional (PI), and the Sunrise at Sterling Canyon facility, a senior living facility that provides independent living, assisted living, and hospice care.
- **South and East:** Land uses consist primarily of single-family residences zoned Urban Residential (UR2), and the United Methodist Church is located on the opposite side of McBean Parkway. Residential uses immediately east and southeast of the project site were developed in 1969.

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- **West:** Land uses consist primarily of single-family residences zoned Urban Residential (UR1) and an open space area zoned Open Space (OS). The residential uses immediately west of the project site were developed in 1978.

### **2.3 Proposed Site Plan**

The Site Plan (Figure 3) and the Proposed Buildout Development Program (Table 2) provide the framework for development of the project site under the proposed project. Figure 3 illustrates the location of both existing and approved facilities anticipated under the proposed project. As shown on the Site Plan, medical office building (MOB) 1, parking structure (PS) 1, and PS 4, all approved under the Master Plan, have been constructed and are part of the existing conditions. Additionally, the 8,872-square-foot Loading Dock and 3,600-square-foot Central Plant are under construction, per the Master Plan. The 3,600-square-foot Central Plant represents a 6,400-square-foot reduction from that approved in the Master Plan.

All of the remaining buildings shown on the Site Plan were approved as part of the Master Plan. Minor adjustments to the approved square footage of the Master Plan for the Inpatient Building only are being facilitated through the proposed project. As earlier noted, these adjustments in square footage are due to two factors: (1) the implementation of requirements imposed by amendments to the California Building Standards Code subsequent to the approval of the Master Plan; and (2) expansion of obstetric services with construction of two new Caesarean Section rooms in the area between the existing Hospital and the Inpatient Building. Specifically, as heretofore noted the proposed project would increase the approved square footage of the Inpatient Building from 125,363 square feet to 162,329 square feet (an increase of 36,966 square feet).

The total square footage that remains to be built, under the proposed project, include the Inpatient Building (excluding the Loading Dock), MOB 2, MOB 3, PS 2, and PS 3. At buildout, the campus would include a total of 698,000 square feet of building space, including 377,415 square feet of hospital and hospital-related uses; 24,425 square feet of support facilities; and 296,160 square feet of medical office space, plus parking. The floor area ratio (FAR) approved under the 2008 Master Plan and Development Agreement, including the existing campus at that time, was 0.52, based on a lot size of 29.77 acres, or 1,296,781 square feet. As described previously, the Hospital campus was a total of 30.4 acres at the time of approval of the 2008 Master Plan; however, the 2008 Master Plan required the dedication of 0.63 acre to the City for the McBean Parkway traffic improvements and right-of-way, thereby reducing the total area to 29.77 acres. The Hospital has since dedicated the 0.63 acre as required. The additional FAR based on the square footage facilitated by the proposed project is 0.02 or a site FAR of 0.54.

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**Table 2  
Proposed Buildout Development Program**

Use	Area (square feet)				
	Existing Facilities (Pre-2008 Master Plan)	2008 Approved Master Plan Facilities	Proposed Specific Plan Development	Total Buildout Facilities	Hospital Beds <sup>2</sup>
<i>Hospital and Related Uses</i>					
Main Hospital	146,000	—	—	146,000	116 existing (20 existing beds to be de-licensed)
Main Hospital Basement	5,286	—	—	5,286	—
Nursing Pavilion Building	63,800	—	—	63,800	100 existing 10 new
Inpatient Building ( <b>Unbuilt</b> )	0	116,491	+36,966	153,457	142 new
Loading Dock ( <b>Under Construction</b> )	—	8,872	—	8,872	—
<i>Subtotal Hospital and Related Uses</i>	215,086	125,363	+36,966	377,415	368
<i>Support Facilities Uses</i>					
Hospital Bridge (covered walkway)	9,122	—	—	9,122	—
Mechanical Plant	8,585	—	—	8,585	—
Facilities Building (warehouse)	2,384	—	—	2,384	—
Facilities Building (office)	734	—	—	734	—
Central Plant ( <b>Under Construction</b> )	—	10,000	-6,400	3,600	—
<i>Subtotal Support Facilities Uses</i>	20,825	10,000	—	24,425	—
<i>Medical Office Buildings (MOB)</i>					
MOB A	5,302	—	—	5,302	—
MOB B	5,302	—	—	5,302	—
MOB C	5,302	—	—	5,302	—
MOB D	5,302	—	—	5,302	—
MOB E	31,040	—	—	31,040	—
MOB F Sheila R. Veloz Breast Imaging Center	43,912	—	—	43,912	—
MOB 1 ( <b>Built</b> )	—	80,000	—	80,000	—
MOB 2 ( <b>Unbuilt</b> )	0	60,000	—	60,000	—
MOB 3 ( <b>Unbuilt</b> )	0	60,000	—	60,000	—

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**Table 2  
Proposed Buildout Development Program**

Use	Area (square feet)				
	Existing Facilities (Pre-2008 Master Plan)	2008 Approved Master Plan Facilities	Proposed Specific Plan Development	Total Buildout Facilities	Hospital Beds <sup>2</sup>
Foundation & Administrative Office Building (To Be Demolished)	8,000	-8,000	—	0	—
<i>Subtotal Medical Office Buildings</i>	104,160	192,000	—	296,160	—
<b>TOTAL</b>	<b>340,071</b>	<b>327,363</b>	<b>30,566</b>	<b>698,000</b>	<b>368</b>
<b>SITE FLOOR AREA RATIO<sup>1</sup></b>	<b>0.52</b>		<b>0.02</b>	<b>0.54</b>	—

**Note:**

- <sup>1</sup> Floor area ratio (FAR) is the size of a building divided by the size of its parcel. In this instance, FAR is based on 29.77 acres, or 1,296,781 square feet.
- <sup>2</sup> The total number of hospital beds (368) remains the same as approved in the 2008 Master Plan and Development Agreement. The 2008 Master Plan and Development Agreement allow for the movement of beds between buildings within the overall campus. The beds proposed under the proposed project would shift from the approved Master Plan, but would not require or cause an increase in building square footage.

## 2.4 Site Coverage and Building Heights

The Main Hospital building is located in the center of the campus and is connected to the Nursing Pavilion Building, located in the northeastern portion of the site, by an enclosed bridge. Adjacent to the Nursing Pavilion Building are several smaller buildings, including the Central Plant, Facility Office, and Disaster Supply Storage Building. MOB 1, PS 1, and PS 4 are located across the internal driveway from the Main Hospital and Nursing Pavilion buildings, and front on McBean Parkway. MOB A, MOB B, MOB C, MOB D, MOB E, MOB F, and the Office Building (formerly the Foundation and Administration Office Building) are located in the southwestern portion of the site, along with several surface parking lots. All of the existing buildings on the campus, excluding parking structures, rise up to three stories in height; MOB 1 is the tallest existing building on the campus, besides parking structures, at a height of 45.5 feet.

## 2.5 Site Access and Circulation

### 2.5.1 Regional Access

Interstate 5 (I-5) provides regional access for vehicles traveling to and from the project site. I-5 is located west of the project site and can be accessed via a full interchange at McBean Parkway. Major arterial streets near the project site consist of McBean Parkway, Orchard Village Road, and Rockwell Canyon Road/Tournament Road.



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## **2.5.2 Site Access**

Access to the project site is provided by three driveways along McBean Parkway. The main entrance is provided at a four-way signalized intersection with Orchard Village Road (referred to as the “Center Driveway” in the 2008 Master Plan EIR). Orchard Village Road is a four-lane roadway that terminates at the project entrance. A second four-way signalized access point is located at the McBean Parkway and Avenida Navarre (referred to as the “East Driveway” in the 2008 Master Plan EIR). Avenida Navarre is a two-lane local street serving the residential uses south of the project site. The third access point is an unsignalized T-intersection along McBean Parkway, west of the Orchard Village Road main entrance, which allows for unprotected right-in, right-out, left-in, and left-out movements (referred to as the “West Driveway” in the 2008 Master Plan EIR).

## **2.5.3 Internal Circulation**

Internal vehicular circulation is provided throughout the project site. From the main entrance, an internal road connects vehicles to all buildings and a perimeter road that runs along the outer edge of the campus. The existing and future parking structures and surface lots are all accessible from the perimeter road and internal streets.

## **2.6 Parking**

Parking supply at buildout of the proposed project would total approximately 2,231 spaces distributed throughout the hospital campus, consisting of approximately 1,959 spaces in parking structures, 241 spaces in surface lots, and 31 parallel parking spaces along the internal circulation roadways of the site (see Figure 3). Two parking structures approved under the Master Plan (PS 2 and PS 3) remain to be built and would provide approximately 857 parking spaces. A total of 110 spaces would accommodate handicapped access. The proposed project would not affect the amount of parking required under the Master Plan.

## **2.7 Helipad**

The Master Plan conceptually located the Helipad at the southeast corner of the roof of the Inpatient Building. The proposed project would relocate the Helipad from the southeast corner to the north side of the roof. The Helipad location would still remain approximately 240 feet from the nearest residence. The relocation of the Helipad is necessary to comply with FAA and State required airspace obstruction clearance criteria in order to allow for the direct elevator transport from the Helipad to the emergency room since in its original location the elevator would be in the flight path.

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There is an existing helipad on PS 1, which under the Master Plan would be allowed to remain for emergency or temporary uses after the Helipad is constructed. The locations of the helipad on PS 1 and the proposed Helipad are illustrated in Figure 3.

### **2.8 Landscaping**

Landscaping is focused around each of the existing buildings, surface parking lots, and the hillside in the northwest portion of the campus. As future buildings are constructed, additional landscaping would be provided in accordance with the Master Plan. The proposed project would not change the landscaping requirements of the Master Plan.

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### **3 EXISTING GENERAL PLAN AND ZONING DESIGNATIONS**

The project site is currently designated PI (Public/Institutional) on both the City's General Plan Map and Zoning Map. The PI designation is intended for various types of public or/and community-serving facilities owned and operated by public agencies, special districts, nonprofit organizations, and other entities. The proposed project would change the General Plan and Zoning designation of the project site to SP (Specific Plan).

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### **4 ANTICIPATED PHASING**

The proposed project would generally be anticipated to be built as part of the following phases:

1. Inpatient Building: July 2016 – May 2018 (note the Loading Dock, which is considered part of the Inpatient Building is currently under construction, as approved under the 2008 Master Plan and is not considered part of the proposed project analyzed in this addendum).
2. Inpatient Building Site Work and Landscaping: December 2017 – March 2018

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## 5 LEGAL STANDARDS

In accordance with CEQA, the City's Community Development Department, Planning Division and City Attorney's office, have analyzed the potential environmental effects of the proposed project, including the proposed Specific Plan, amendments to the Master Plan and Development Agreement, and the General Plan and Zone Change from PI to SP, and concluded that this Addendum is the proper vehicle to inform the decision makers and the public of any potential environmental effects of the proposed project.

This Addendum to the Master Plan EIR has been prepared in accordance with the California Environmental Quality Act, Public Resources Code §21000, *et seq.* ("CEQA") and the State CEQA Guidelines, 14 Cal. Code Regs. §15000, *et seq.* ("CEQA Guidelines").

Section 15164(a) of the CEQA Guidelines governs the preparation of an Addendum. Section 15164(a) of the CEQA Guidelines provides:

- “(a) The lead agency or responsible agency shall prepare an addendum to a previously certified EIR if some changes or additions are necessary but none of the conditions described in Section 15162 calling for preparation of a subsequent EIR have occurred.”

Section 15162 of the CEQA Guidelines describes those conditions as follows:

- “(1) Substantial changes are proposed in the project which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;
- (2) Substantial changes occur with respect to the circumstances under which the project is undertaken which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; or
- (3) New information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the previous EIR was certified as complete or the negative declaration was adopted, shows any of the following:
  - (A) The project will have one or more significant effects not discussed in the previous EIR or negative declaration;

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- (B) Significant effects previously examined will be substantially more severe than shown in the previous EIR;
- (C) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible, and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measure or alternative; or
- (D) Mitigation measures or alternatives which are considerably different from those analyzed in the previous EIR would substantially reduce one or more significant effects on the environment, but the project proponents decline to adopt the mitigation measure or alternative.”

The City has analyzed the potential environmental effects of the proposed project in accordance with CEQA Guidelines §§ 15162 and 15164(a). The potential environmental effects of the Master Plan and Development Agreement were addressed in the certified Master Plan EIR. However, the proposed project now includes the proposed General Plan and Zone Change from PI to SP, and proposed amendments to the 2008 Master Plan and Development Agreement. The City has determined that an Addendum is appropriate because no new significant environmental effects would occur nor would the severity of effects previously identified as significant substantially increase nor would any other condition set forth in Section 15162 occur as a result of the implementation of the proposed project.



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## **6 ENVIRONMENTAL ANALYSIS**

The proposed project as discussed above does not change the conclusions of the impact analysis of the Master Plan EIR. The modifications described in Section 2 would have no new significant adverse environmental impacts nor substantially increase the severity of previously identified significant adverse effects. To ensure that no new or substantially more severe significant environmental impacts would occur, the proposed project would adhere to applicable mitigation measures in the certified Master Plan EIR and approved by the City Council. Only the mitigation measures that apply to the proposed project are restated in the applicable sections below. Refer to Appendix A of this Addendum, the Mitigation Monitoring and Reporting Program, which contains all of the approved mitigation measures.

### **6.1 Aesthetics, Light, and Glare**

#### **6.1.1 Short-Term Construction**

The Master Plan EIR found the Master Plan's short term construction effects would be less than significant with implementation of Mitigation Measures AES1 and AES2, provided in Appendix A. Mitigation Measure AES1 provides for appropriate screening to buffer views from adjacent residential uses, existing campus hospital operations and McBean Parkway. Mitigation Measure AES2 provides that construction-related security lighting shall be directed away from adjacent residential areas and shall consist of the minimum wattage necessary to provide safety at the construction site. Construction associated with the proposed project would not alter the duration or intensity of the construction activities anticipated in the Master Plan EIR, which would remain short-term and occur at various locations and at different times as the Master Plan is built out. The additional square footage requested for the Inpatient Building would not affect the nature of the Master Plan's short term construction effects because there are no additional trades required and sequencing would remain unchanged. Construction in the Infill Area would not be at a greater height nor closer to any residences than analyzed in the certified EIR. Any short-term construction impacts would be mitigated by Mitigation Measures AES1 and AES2. No additional mitigation measures are required. No new significant effect would occur.

#### **6.1.2 Long-Term Aesthetic/Visual Character**

The Master Plan EIR found that Long-Term Aesthetic and Visual Character effects would be less than significant with implementation of Mitigation Measure AES3. Mitigation Measure AES3 provides that prior to issuance of building permits, each structure must undergo Development Review in conformance with the Master Plan and conditions of approval.

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The Aesthetic/Visual Character effects of the Inpatient Building considered in the Master Plan EIR are the (1) effects on the view from the Summit Residential Neighborhood; and (2) visibility of the Inpatient Building looking north from the intersection of McBean Parkway and Orchard Village Drive.

With implementation of the proposed project, there would be no added obstruction of the view from the Summit Residential Neighborhood because there would be no increase in the height of the Inpatient Building nor increased visibility of the Helipad. The approved Master Plan Inpatient Building is five stories in height. Although the proposed project Infill Area conceptually fills in more of the southwest corner between the Main Hospital and the Enclosed Bridge as shown in the Master Plan, it is only two stories tall compared to the five stories approved under the Master Plan. See Figure 4 for an overlay of the approved Master Plan with the proposed project. The view of the Infill Area from the Summit Residential Neighborhood would still be of built rather than green space. However, the additional built space would be consistent with the existing built space, with little alteration or degradation of the view analyzed in the Master Plan EIR. Construction in the Infill Area would remain approximately 85 feet below the Summit Residential Neighborhood. The Inpatient Building's top of parapet would remain approximately the same elevation as the closest residential property line elevation. The two Caesarean Section rooms would be on the second floor of the five-floor structure and would cantilever partially over the existing Basement Expansion. The only other view of the built out Infill Area would be from the on-site road known as the ring road, which would be limited to the project site. The visibility of the Inpatient Building looking north from McBean Parkway and Orchard Village Drive as analyzed in the certified EIR would not change. The proposed project would otherwise affect only interior space. No new significant Aesthetic/Visual Character effects would occur.

### **6.1.3 Landscaping**

The Master Plan EIR found that the effects of Landscaping would be less than significant with implementation of Mitigation Measure AES4. Mitigation Measure AES4 provides that landscaping shall be installed in conformance with the approved conceptual landscaping plans, in compliance with the conditions of approval, prior to issuance of a Certificate of Occupancy for each building and parking structure. The landscaping for the proposed project would comply with the approved conceptual landscaping plans and conditions of approval and would include only minimal, if any, additional landscaping because virtually all of the proposed modifications would be interior. No additional mitigation measures are necessary. No new significant effect would occur.

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### **6.1.4 Long-Term Light and Glare**

The Master Plan EIR found that the effects of Long-Term Light and Glare would be less than significant with implementation of Mitigation Measures AES3 and AES4. The only proposed project that might potentially cause a Long-Term Light and Glare effect not analyzed in the Master Plan EIR would be the two new Caesarean Section rooms constructed in the Infill Area. However, even with the proposed construction in the Infill Area, the height of the Inpatient Building and Central Plant and thus the visual elevation from McBean Parkway, would remain the same as analyzed in the Master Plan EIR and would not be exceeded by the heights of the new Caesarean Section rooms. All exterior lighting that could potentially be added in the Infill Area would be below the height of the existing Enclosed Bridge between the Inpatient Building and the Main Hospital and would not affect any residences. No additional mitigation measures are required. No new significant effect would occur.

### **6.1.5 Cumulative**

The Master Plan EIR found that cumulative Aesthetic/Visual effects would be less than significant with implementation of Mitigation Measures AES1 through AES4. The majority of the additional square footage resulting from the proposed project would be interior and not visible to the public. The construction in the Infill Area would be below the height of the existing Enclosed Bridge between the Inpatient Building and the Main Hospital. See Sections 6.1.2 through 6.1.4, above. Cumulative impacts would remain less than significant.

## **6.2 Air Quality**

For ease of reference, the Mitigation Monitoring Program imposed on the Master Plan is attached as Appendix A.

### **6.2.1 Construction**

The Master Plan EIR found that Short-Term Construction Air Quality effects would be significant and unavoidable even with implementation of Mitigation Measures AQ1 through 4. The proposed project would not cause a substantial increase in the severity of Construction Air Quality effects because the length of time over which construction would occur would remain the same, the approximate 240-foot distance to neighboring residences would remain the same, and the nature of the construction equipment used would remain the same. There are no additional feasible mitigation measures available beyond those imposed on the Master Plan EIR.

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### **6.2.2 Operation**

The Master Plan EIR found that Operational Air Quality effects would be less than significant with implementation of Mitigation Measure AQ5. The proposed project would not change the nature or intensity of uses approved under the Master Plan and would result in only a minimal increase in campus square footage. There would be no increase the number of programs, employees, overall campus inpatient beds or square footage allocated to administration uses. Minimal, if any, additional traffic would be generated beyond that anticipated in the Master Plan, see the Traffic Study Addendum (TSA) dated February 9, 2016, prepared by Linscott, Law & Greenspan, which is attached as Appendix B to this Addendum. No operations would be closer to sensitive receptors than under the approved Master Plan. Operational emissions would not be increased from those projected in the Master Plan EIR. No additional mitigation measures are required. No new significant effect would occur.

### **6.2.3 AQMP Consistency**

The Master Plan EIR found that the Master Plan would have a less than significant impact on consistency with the Air Quality Management Plan (AQMP) with implementation of Mitigation Measures AQ2 and AQ5. The proposed project would not require amendment of the AQMP nor would they cause the Master Plan build out to exceed growth projections in the Regional Transportation Plan, as the proposed project would be consistent with the City's General Plan, as described in Section 6.3, Land Use. The proposed project would not cause any exceedance of regional or local thresholds for criteria pollutants beyond that analyzed in the Master Plan EIR, so would not result in an increase in the frequency or severity of existing air quality violations or contribute to new violations. The proposed project would not cause an increase in the nature or intensity of uses, programs, employees, number of patient beds campus-wide nor square footage allocated to administration uses. The proposed project would not cause formation of CO hotspots, as they would not cause an increase in LOS at the relevant study intersections, see TSA, Appendix B. Therefore, no additional mitigation measures are required. No new significant effect would occur.

### **6.2.4 Long-Term Operational Cumulative**

The Master Plan EIR found that Long-Term Operational cumulative effects were less than significant. The proposed project would not cause a new significant operational effect and thus would not cause or make a considerable contribution to any cumulative impact. Therefore, there would be no new significant Long-Term Operational cumulative effects for the reasons set forth in Sections 6.2.2 and 6.2.3 above.

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### **6.2.5 Global Climate Change**

The Master Plan EIR found that the Master Plan's on-site emissions and energy emissions effects would be less than significant with implementation of Mitigation Measures AQ6 through AQ8. It found however, that the Master Plan's mobile source emissions would remain significant and unavoidable after mitigation. The City adopted a Climate Action Plan (CAP) in August 2012. The purpose of the CAP is to measure the amount of greenhouse gas emissions generated within the City and to develop strategies to reduce the emissions in the future. Using the goals, objectives and policies of the General Plan as a starting point, the CAP identifies those measures that can be quantified and translated into significant reductions in the greenhouse gas emissions by the year 2020. The proposed project would not conflict with the applicable greenhouse gas (GHG) reduction measures included in the CAP. The proposed project would be consistent with the CAP overall land use transportation measure to reduce total vehicle miles travelled (VMT) because buildout of the site would continue to increase the overall density of the site and location efficiency by increasing the Inpatient Building square footage. With implementation of the proposed project the site would also continue to be transit accessible. The proposed project would also be consistent with the CAP water measures since future development would continue to use low-flow water fixtures and new irrigation systems would be designed, installed, operated, and maintained in conformance with the State Water Efficient Landscape Ordinance, as required in the Master Plan. The project would allow for minimal increases in square footage beyond what was approved under the Master Plan. There would be no increase in the nature or intensity of uses, number of programs, employees or square footage devoted to administration.

Implementation of the project would not cause the effects of on-site GHG emissions or energy emissions to become significant nor would the significant effects of mobile source emissions be substantially more severe because there would be little or no increase in traffic generation beyond that analyzed in the Master Plan EIR, based on existing conditions. See TSA, Appendix B. The Master Plan addresses energy, water, water quality, and landscaping components to ensure overall efficiency and conservation. The proposed project would not cause the effects of on-site emissions or energy emissions to become significant for the reasons set forth in Sections 6.2.2 and 6.2.3, above. As noted, the effects of mobile source emissions would not be substantially more severe because the proposed project would cause little or no increase in traffic generation based on existing conditions, see the TSA, Appendix B. No additional feasible mitigation measures are available for mobile source emissions. No new, or substantially more severe, significant effects would occur.

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### **6.3 Land Use**

The Master Plan EIR found the Master Plan would not conflict with any applicable land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect, including the City's General Plan and Unified Development Code. Since the Master Plan EIR, the project site has been re-designated PI (Public/Institutional) on both the City's General Plan Map and Zoning Map. The proposed project would change the General Plan and zoning designation of the site to Specific Plan (SP). The SP zone/designation is intended to achieve the following purposes:

1. To promote and protect the public health, safety and welfare;
2. To implement the objectives and policies of the General Plan;
3. To safeguard and enhance environmental amenities, such as oak trees and ridgelines, and enhance the quality of development;
4. To attain the physical, social, and economic advantage resulting from comprehensive and orderly planned use of land resources;
5. To lessen congestion and assure convenience of access: to secure safety from fire, flood and other dangers; to provide for adequate light, air, sunlight and open space; to promote and encourage conservation of scarce resources; to facilitate the creation of a convenient, attractive and harmonious community; to attain a desirable balance of residential and employment opportunities; and to expedite the provision of adequate and essential public services;
6. To facilitate development within the City in accordance with the General Plan by permitting greater flexibility and, consequently, more creative and imaginative designs for large-scale development projects than generally are possible under conventional zoning regulations;
7. To promote more economical and efficient use of the land while providing a harmonious variety of housing choices and commercial and industrial activities, a high level of urban and public amenities and preservation of natural and scenic qualities of open space; and
8. In the case of a specific plan, to provide a process for initiation, review and regulation of large-scale comprehensively planned communities that afford the maximum flexibility to the developer within the context of an overall development program and specific, phased development plans coordinated with the provision of necessary public services and facilities.

The proposed project, which includes the proposed Specific Plan, would achieve the purposes of the City's SP zone, and would serve as the zoning code for the project site. The proposed project

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would eliminate the maximum floor area ratio (FAR) for the site, which is 0.50 under the PI zone. Although the Specific Plan would not include a maximum FAR, it would include a cap on square footage, which allows for a total FAR of 0.54. Since the approved Master Plan allows for an FAR of 0.52, the proposed project would only represent a 0.02 increase in FAR, which is not considered a substantial increase and would not cause any additional impacts.

The proposed project would implement the goals and policies of the City's General Plan, as documented in Appendix C, which contains an analysis of the proposed project's consistency with the City's General Plan. This analysis confirms that adoption of the proposed project would not conflict with the applicable General Plan goals and policies. With adoption of the proposed project, there would be no significant impacts related to land use.

### **6.4 Geology, Soils and Seismicity**

#### **6.4.1 Surface Fault Rupture, Ground Failure, Landslides and Slope Stability, Corrosive Soils, Soil Erosion, Cumulative Effects**

The Master Plan EIR found the effects of surface fault rupture, ground failure, landslides and slope stability, corrosive soils, soil erosion, both project and cumulative, to be less than significant. The project site characteristics and nature and timing of construction analyzed in the Master Plan EIR would remain the same if the proposed project is implemented. No mitigation measures are required. No new significant effect would occur.

#### **6.4.2 Seismic Ground Shaking, Expansive Soils**

The Master Plan EIR found the effects of seismic ground shaking to be less than significant with implementation of Mitigation Measure GEO1, which requires a registered geologist to prepare a Probabilistic Seismic Hazard Analysis (PSHA) prior to issuance of grading permits for the Inpatient Building. The effects of expansive soils were found to be less than significant with implementation of Mitigation Measure GEO2, which requires special foundation designs and reinforcements, if expansive soils are encountered during construction. The site characteristics and nature and timing of construction analyzed in the Master Plan EIR would remain the same with implementation of the proposed project. No additional mitigation measures are required. No new significant effect would occur.

#### **6.4.3 Site Grading**

Implementation of the proposed project would not increase site grading beyond the amount forecast in the Master Plan for the Inpatient Building.. For the Inpatient Building, the amount of export anticipated in the Master Plan EIR was 13,100 cubic yards. Including the proposed

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project, under existing conditions, the amount of soil export in conjunction with construction of the Inpatient Building is anticipated to be approximately 12,600 cubic yards. The Master Plan EIR found that the effects of site grading would be less than significant with implementation of Mitigation Measures AQ1 and HWQ3, which specify requirements during construction to minimize effects associated with grading, and compliance with state and local regulations. Implementation of the proposed project, including construction in the Infill Area, would not increase site grading beyond the amount approved in the Master Plan. No new mitigation measures are required. No new significant effect would occur.

### **6.5 Hazards and Hazardous Materials**

The Master Plan EIR found that the effects of Use, Storage and Handling, Generation, Exposure and Hazards and Hazardous Materials both project and cumulative were less than significant. Implementation of the proposed project would cause no effects beyond those forecast in the Master Plan EIR because there would be no changes in the nature or intensity of uses, number of programs, employees, overall campus inpatient beds, or square footage allocated to administration uses. Therefore, implementation of the proposed project would not cause an increase in the generation, use, storage, handling or exposure to hazardous materials. No mitigation measures are required. No new significant effect would occur.

### **6.6 Hydrology and Water Quality**

Implementation of the proposed project would cause little or no change in Drainage, Hydrology, Water Quality or Cumulative effects because there would be no changes in the natures, or intensity of uses, and no increase in the number of programs, employees, overall campus inpatient beds or square footage allocated to administration uses. Required site grading would not increase. Construction timing and mix of equipment would not change. The Master Plan EIR found all of the foregoing effects to be less than significant with implementation of mitigation. For ease of reference, list of Hydrology and Water Quality Mitigation Measures is attached in Appendix A. No additional mitigation measures are required. No new significant effect would occur.

### **6.7 Noise**

For ease of reference, a list of Noise Mitigation Measures is attached in Appendix A.

#### **6.7.1 Construction**

The Master Plan EIR found that even with implementation of Mitigation Measures N1 and N2, the effects of construction noise would remain significant and unavoidable. Implementation of the proposed project would not cause this effect to be substantially more severe because the



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distance to sensitive receptors would not change nor would the intensity and length of time of construction or mix of construction equipment used on site. Only minimal, if any, additional construction related traffic would be generated. See the TSA at Appendix B. There are no additional feasible mitigation measures available. This significant effect would not be substantially more severe.

### **6.7.2 Operational Traffic Noise**

The Master Plan EIR found that the effects of Interim Year Traffic, Off-Site and On-Site Traffic Noise effects would be less than significant with implementation of Mitigation Measure N3. Implementation of the proposed project would not cause these effects to become significant because they would not generate any increased traffic, see the TSA at Appendix B. No additional mitigation measures are required. No new significant effect would occur.

### **6.7.3 Operational Stationary Source Noise**

The Master Plan EIR found that the effects of Operational Stationary Source Noise would be less than significant. Implementation of the proposed project would not cause this effect to become significant because there would be no increase in loading and unloading, truck movements or other parking lot activities. There would be no expansion of the Hospital's emergency medical services. There would be no increase in the nature or intensity of uses, number of programs, employees, overall campus inpatient beds or square footage allocated to administration. No mitigation measures are required. No new significant effect would occur.

### **6.7.4 Cumulative Operational**

The Master Plan EIR found that Cumulative Operational Noise effects would be less than significant. Implementation of the proposed project would not cause this effect to become significant because any noise effects from on-site stationary sources would continue to be limited to the project site and vicinity and would not increase because there would be no changes in the nature, or intensity of uses, no increase in truck movements or parking lot activities nor expansion of emergency services. Most of the proposed modifications to the Inpatient Building would be within the interior of the building. No mitigation measures are required. No new significant effect would occur.

### **6.7.5 Long-Term Cumulative Traffic**

The Master Plan EIR found that Long-Range Cumulative Off-Site and On-Site Traffic Noise effects would be less than significant with implementation of Mitigation Measure N3. Implementation of the proposed project would not cause this effect to become significant

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because there would be little or no increase in traffic generation, under existing conditions, beyond that analyzed in the Master Plan EIR. See the TSA Appendix B. No additional mitigation measures are required. No new significant effect would occur.

### **6.7.6 The Helipad**

The Master Plan approved construction of the Helipad on the roof of the Inpatient Building. The approved Inpatient Building is to be approximately 85 feet high and 240 feet from the nearest residence, which is within the Valencia Summit Residential Neighborhood. The proposed project would relocate the Helipad from the southeast to the north side of the roof of the Inpatient Building. The relocation of the Helipad is necessary to comply with FAA and State required airspace obstruction clearance criteria in order to allow for the direct elevator transport from the Helipad to the emergency room since in its original location the elevator would be in the flight path.

The BridgeNet Report attached as Appendix J, to the Master Plan EIR, found the Hospital's helipad operations averaged 10 to 12 arrivals or departures per month. As explained in the BridgeNet Report, and as is Hospital's standard practice, a helicopter arriving with a patient counts as one "flight" or "landing"; likewise, a helicopter departing with a patient counts as one "flight" or one "landing." The Hospital does not own a helicopter nor do any helicopters reside at the Hospital. The Master Plan EIR anticipated helicopter activity of 15 to 17 flights (arrivals or departures) per month, increasing the existing noise exposure level of approximately 1.5 dBA (in terms of the 24-hour weighted average scale of CNEL), which is not large enough to be perceptible. Relocation of the Helipad to the north side of the roof of the Inpatient Building would not decrease the 240-foot distance to the nearest residence nor cause an increase in size of the Helipad nor any change in the number of flights. The flight paths to and from the Helipad would remain the same. As shown in Table 3, there were a total of 34 helicopter flights between January and December 2015. These flights include incoming flights for trauma and other transfers to the Hospital and departing flights for specialty care, such as for an advanced Neonatal Intensive Care Unit or seriously ill pediatric patients, at the expected maximum number of 2 – 3 specialty flights per month. The maximum number of incoming flights in any month was 5 in June 2015. The minimum was 1 in May and December 2015. An incoming flight does not mean there is an equivalent departing flight or "round trip." Nevertheless, assuming a worst case of 5 incoming flights, 5 outgoing flights and 3 specialty flights per month, the number of anticipated monthly flights would be 13. This number of flights would not exceed the maximum of 15-17 flights per month anticipated in the Master Plan EIR. No new significant effect would occur.

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**Table 3  
2015 Helicopter Flights**

Month	Helicopter Flights
January	4
February	4
March	4
April	2
May	1
June	5
July	2
August	2
September	4
October	3
November	2
December	1
<b>TOTAL</b>	<b>34</b>

Source: HMNH, January 29, 2016.

### **6.8 Parking**

The Master Plan EIR found that Short-Term Construction Parking effects would be less than significant with implementation of Mitigation Measure PRK1 and Long-Term Operational Parking effects would be less than significant with implementation of Mitigation Measure PRK2. Mitigation Measure PRK1 requires that a Construction Parking Management Plan (PRK1) be reviewed and approved by the City before construction begins. This Plan would be required to include any additional parking that might incidentally be required by implementation of the proposed project. Mitigation PRK2 requires the provision of the City’s Code-required parking. Implementation of the proposed project would not change the nature or intensity of uses nor would it increase the number of programs, employees, overall campus inpatient beds, or square footage allocated to administration uses. Construction of the two new Caesarean Section rooms in the Infill Area would not cause the number of required parking spaces to increase beyond the number analyzed in the Master Plan EIR. No additional mitigation measures are required. No new significant effect would occur.

### **6.9 Public Services**

#### **6.9.1 Fire Protection Services**

The Master Plan EIR found that effects on Fire Protection Services would be less than significant with implementation of Mitigation Measures FS1 through 9, which for ease of reference, are

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attached in Appendix A. Implementation of the proposed project would result in increased interior circulation in the Inpatient Building, including more exiting corridors between departments, potentially improving the ingress and egress approved under the Master Plan. The nature and intensity of uses, number of programs, employees, overall campus inpatient beds and square feet allocated to administration uses would not change. No additional mitigation measures are required. No new significant effect would occur.

### **6.9.2 Sheriff, Highway Patrol, Emergency Response/Evacuation Plans**

The Master Plan EIR found that effects on Sheriff Services, California Highway Patrol and Emergency Response/Evacuation Plans would be less than significant.

Implementation of the proposed project would not increase any of these effects because neither the nature or intensity of uses would change, there would be no increase in the number of programs, employees, overall campus inpatient beds or square feet allocated to administration uses, no changes in site access or interior circulation, and traffic generation would be increased only minimally, if at all. Ingress and egress in the Inpatient Building might be improved over the Master Plan. No mitigation measures are required. No new significant effect would occur.

### **6.9.3 Schools/Education**

The Master Plan EIR found that effects on Schools/Education would be less than significant with implementation of Mitigation Measures SE1 and SE2. The increased square footage that would result from implementation of the proposed project might result in payment of additional fees in accordance with Mitigation Measures SE1 and SE2. Payment of these fees would constitute full CEQA mitigation as a matter of law.

## **6.10 Public Utilities**

### **6.10.1 Electricity Supplies and Services**

Implementation of the proposed project would not result in any significant increase in energy consumption because the nature and intensity of uses would not change. It has been confirmed in an email from Stephan LeBlanc, Service Planner with the Valencia District of Southern California Edison, dated January 12, 2016, that any increase caused by implementation of the proposed project would be within the existing service capacities of provider Southern California Edison (see Appendix D).

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### **6.10.2 Natural Gas**

No increase in natural gas consumption is anticipated. It has been confirmed in an email from Kurtis Foster, Account Executive with the Southern California Gas Company, dated January 15, 2016, that provider Southern California Gas Company has sufficient capacity to service any increased natural gas demands that might be caused by implementation of the proposed project (see Appendix D). Southern California Gas Company will upsize the existing, on-campus 2” diameter natural gas line with a new 4” diameter line, and upsize their meter on McBean Parkway accordingly to serve the Inpatient Building. Southern California Gas Company’s existing 6” medium-pressure line on McBean Parkway can supply this load without any required mitigation. No additional demands for natural gas consumption are required by the proposed project since the number of total licensed beds on campus, as approved in the 2008 Master Plan and Development Agreement, does not increase.

### **6.11 Solid Waste**

The Master Plan EIR found that Project level and Cumulative Solid Waste effects would be significant and unavoidable. Implementation of the proposed project would be anticipated to generate little, if any, additional solid waste over that analyzed in the Master Plan EIR because there would be no change in the nature of or intensity of uses, no increase in the number of programs, employees, overall campus inpatient beds or square footage allocated to administration uses. There are no new feasible mitigation measures available. This previously identified significant effect would not be substantially more severe.

### **6.12 Traffic**

#### **6.12.1 Background Facts**

The Henry Mayo Newhall Memorial Hospital Master Plan Traffic Impact Analysis was prepared by Austin-Foust Associates, Inc. in May 2008, and was incorporated into the Master Plan EIR (the “2008 Traffic Study”).

A Traffic Study Addendum (TSA) dated February 9, 2016 was prepared by the firm of Linscott, Law & Greenspan (LLG), and is attached as Appendix B, to evaluate the effects of the increase in square footage proposed by the project. The Vicinity Map for the Hospital is provided in Figure 1 of the TSA. The site plan for the approved Master Plan is illustrated in Figure 2 of the TSA. Vehicular access to the Hospital site is provided via driveways on its west end along McBean Parkway, at Orchard Village Road and at Avenida Navarre. No changes to site access and internal circulation are proposed in conjunction with the proposed project.

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The following intersections were evaluated in the 2008 Traffic Study for potential traffic effects:

1. I-5 SB On- and Off-Ramps / McBean Parkway
2. I-5 NB Off- and On-Ramps / McBean Parkway
3. Rockwell Canyon Road-Tournament Road / McBean Parkway
4. McBean Parkway / Valencia Boulevard
5. McBean Parkway / Magic Mountain
6. Wiley Canyon Road / Lyons Avenue
7. Tournament Road / Wiley Canyon Road
8. Orchard Village Road / Lyons Avenue
9. Orchard Village Road / Wiley Canyon Road
10. Orchard Village Road / McBean Parkway
11. Newhall Avenue / Lyons Avenue
12. Valencia Boulevard / Magic Mountain Parkway
13. Avenida Navarre / McBean Parkway

The study intersections are shown on Figure 1 of the TSA. The existing travel lane configurations and traffic control devices at the study intersections are shown in Figure 3 of the TSA.

Construction to implement the approved Master Plan has commenced. The building identified as MOB 1 has been developed and is fully occupied. The parking structures identified as PS1 and PS4 have been developed and are open for use.

The Master Plan EIR identifies Traffic Mitigation Measures applicable to MOB 1 (specifically, TR1, TR2, TR3 as listed on pages 3.4-34 and 3.4-35 of the Master Plan EIR). These traffic mitigation measures required for MOB 1 have been completed.

### **6.12.2 Interim Year Conditions**

The Master Plan EIR addressed effects under Interim Year conditions (with and without the Master Plan). It concluded based on the City's impact criteria that significant effects would occur at the following four locations:

- McBean Parkway at Magic Mountain Parkway
- Orchard Village Road at Wiley Canyon Road

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- Orchard Village Road at McBean Parkway
- Valencia Boulevard at Magic Mountain Parkway

The EIR found that with implementation of Mitigation Measures TR1 through TR5, these effects would be reduced to a less than significant level.

### **6.12.3 Congestion Management Plan**

The Master Plan EIR found that the Master Plan would have a less than significant effect on the Los Angeles County Congestion Management Plan.

### **6.12.4 Site Access**

The Master Plan EIR found that the Master Plan's effect on Site Access would be mitigated to a less than significant level by Mitigation Measure TR 3, which was implemented in conjunction with the construction of MOB 1. The remaining components of Mitigation Measures TR1 and TR3, are required to be implemented in conjunction with construction of MOB 2 and the Inpatient Building. Implementation of the proposed project would not affect the existing on-site access approved in the Master Plan nor require any changes in the approved access.

### **6.12.5 On-Site Circulation**

The Master Plan EIR found a less than significant effect on On-Site Circulation. Implementation of the proposed project would not affect the On-Site Circulation approved in the Master Plan nor require any change therein.

### **6.12.6 Long Range Cumulative**

The Master Plan EIR found that the following two roadways would exceed target levels of service for long-range cumulative (2030) build out conditions:

- McBean Parkway between I-5 NB Ramps and Rockwell Canyon Road
- Valencia Boulevard between McBean Parkway and Citrus Drive

It found that under the long-range conditions, the following two locations would be unavoidably significantly affected:

- McBean Parkway at Valencia Boulevard
- Orchard Village Road at McBean Parkway

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The Master Plan EIR found that Mitigation Measures TR6 through TR8 would mitigate long-range cumulative effects, except that significant unavoidable effects would remain at the latter two locations.

### **6.12.7 Conclusions**

The TSA concludes that under existing conditions, implementation of the proposed project would not create new, or substantially more severe, significant effects than those identified in the Master Plan EIR and 2008 Traffic Study.

The TSA finds that implementation of the proposed project under existing conditions would not cause any new, or substantially more severe, significant traffic effects at any of the 13 study intersections, see Section 6.12.1, above.

The TSA also finds that, utilizing Interim Year traffic volumes, operations at the study intersections would be better than what would be forecast using the parameters of the 2008 Traffic Study, and that the potential traffic effects of the Master Plan would be less than forecast in the Master Plan EIR, because the existing baseline conditions are better than anticipated, for the reasons set forth in the TSA.

No additional mitigation measures are required. No new feasible mitigation measures are available at the two intersections that would be unavoidably significantly affected under long term conditions. Implementation of the proposed project would not cause any new, or substantially more severe, significant traffic effects.

### **6.13 Wastewater**

The Master Plan EIR found that the Master Plan would have a less than significant effect with respect to Wastewater. Implementation of the proposed project would cause little or no increase in the generation of wastewater beyond that forecast in the Master Plan EIR. Implementation of the proposed project would not change or intensify Master Plan uses, cause an increase in employees or number of overall campus inpatient beds nor add additional square feet for administration use. No mitigation measures are required. No new significant effect would occur.

### **6.14 Water Supply**

The Master Plan EIR found a less than significant impact on Water Supply. Implementation of the proposed project would cause little, if any, increase in the use of water beyond that analyzed in the Master Plan EIR. It has been confirmed in an email from Cris Pérez, Vice President of Operations for the Valencia Water Company, dated January 15, 2016, that retail purveyor



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Valencia Water Company would have sufficient capacity to service any increase that might incidentally be caused by implementation of the proposed project (see Appendix D). No mitigation measures are required. No new significant effect would occur.

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### **7 FINDINGS**

None of the conditions identified in Section 15162 have occurred and, therefore, pursuant to Section 15164(a) of the CEQA Guidelines, an Addendum has been prepared.

The proposed project would require only minor, technical changes in the Master Plan EIR.

The proposed project would cause no substantial changes in the Master Plan which would require major revisions of the Master Plan EIR due to the involvement of new significant environmental effects or substantial increases in the severity of previously identified significant effects.

There have been no substantial changes in the circumstances under which the proposed project would be undertaken which would require major revisions of the EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects.

There is no new information of substantial importance, as defined in CEQA Guidelines §15162(a)(3), which has become available. None of the four situations described in CEQA Guidelines §15162(3)(A) through (D) apply.

Therefore, pursuant to CEQA and the CEQA Guidelines, there is no need for the preparation of a subsequent or supplemental EIR and this Addendum, which describes the proposed minor technical changes that would result from implementation of the proposed project, has been prepared in compliance with CEQA.

DATE: \_\_\_\_\_, 2016      By: \_\_\_\_\_

**Addendum to the Henry Mayo Newhall Memorial Hospital  
Master Plan Certified EIR**

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# Addendum to the Henry Mayo Newhall Memorial Hospital Master Plan Certified EIR

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## **8 REPORT PREPARERS**

### **City of Santa Clarita**

Fred Follstad, Associate Planner  
Patrick Leclair, Associate Planner  
Greg Murphy, City Attorney

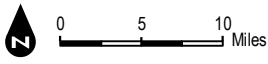
### **Dudek**

Ruta Thomas, REPA, Principal  
Shannon Kimball Wages, AICP, Project Manager  
Heather Ivey, AICP, Environmental Planner

**Addendum to the Henry Mayo Newhall Memorial Hospital  
Master Plan Certified EIR**

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SOURCE: Shaded Relief



**FIGURE 1**  
Regional Map

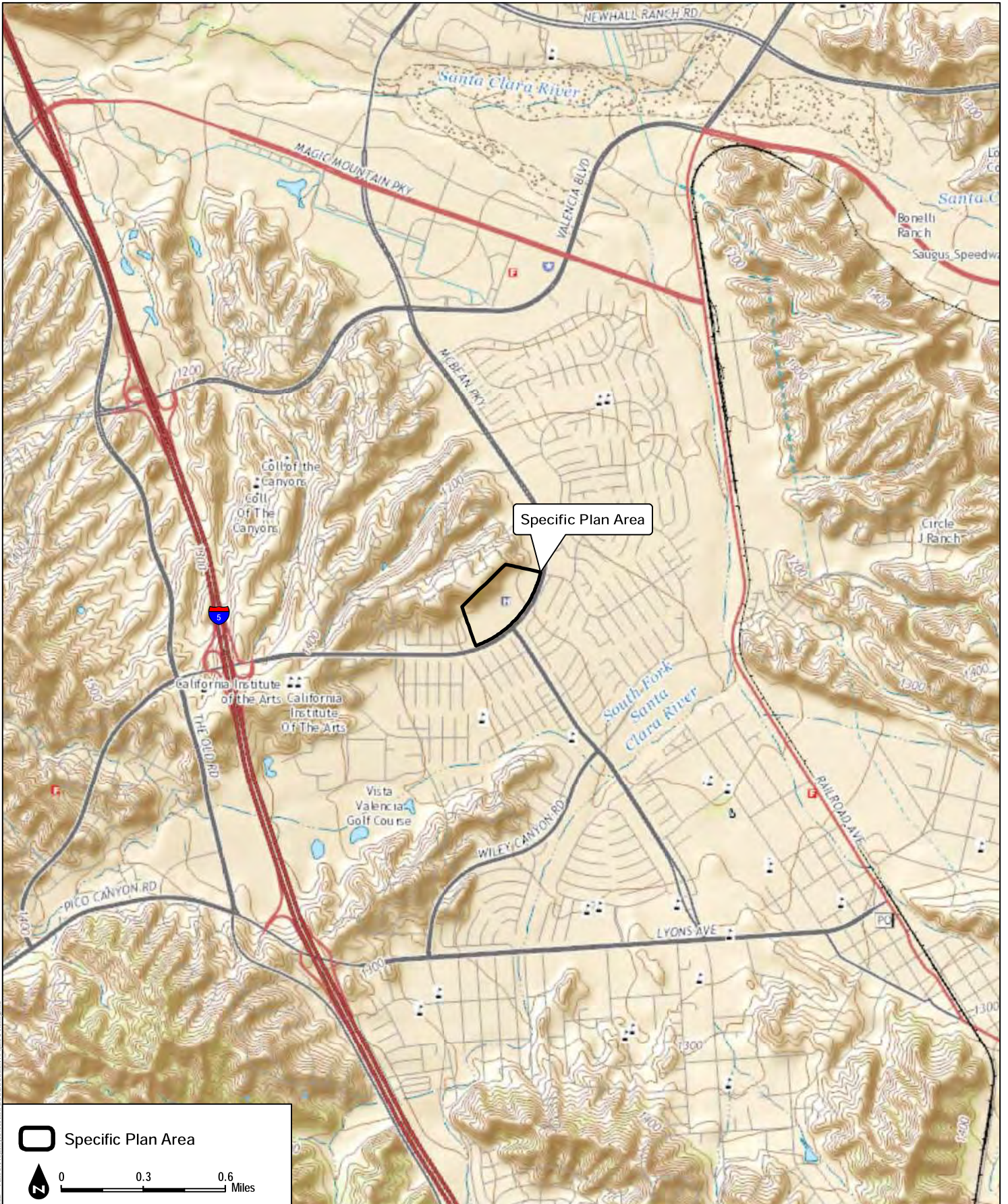
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**Addendum to the Henry Mayo Newhall Memorial Hospital  
Master Plan Certified EIR**

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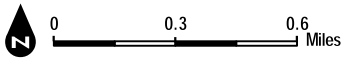
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Specific Plan Area

 Specific Plan Area



SOURCE: USGS National Map 7.5 Minute Topo Series Newhall Quadrangle

**DUDEK**

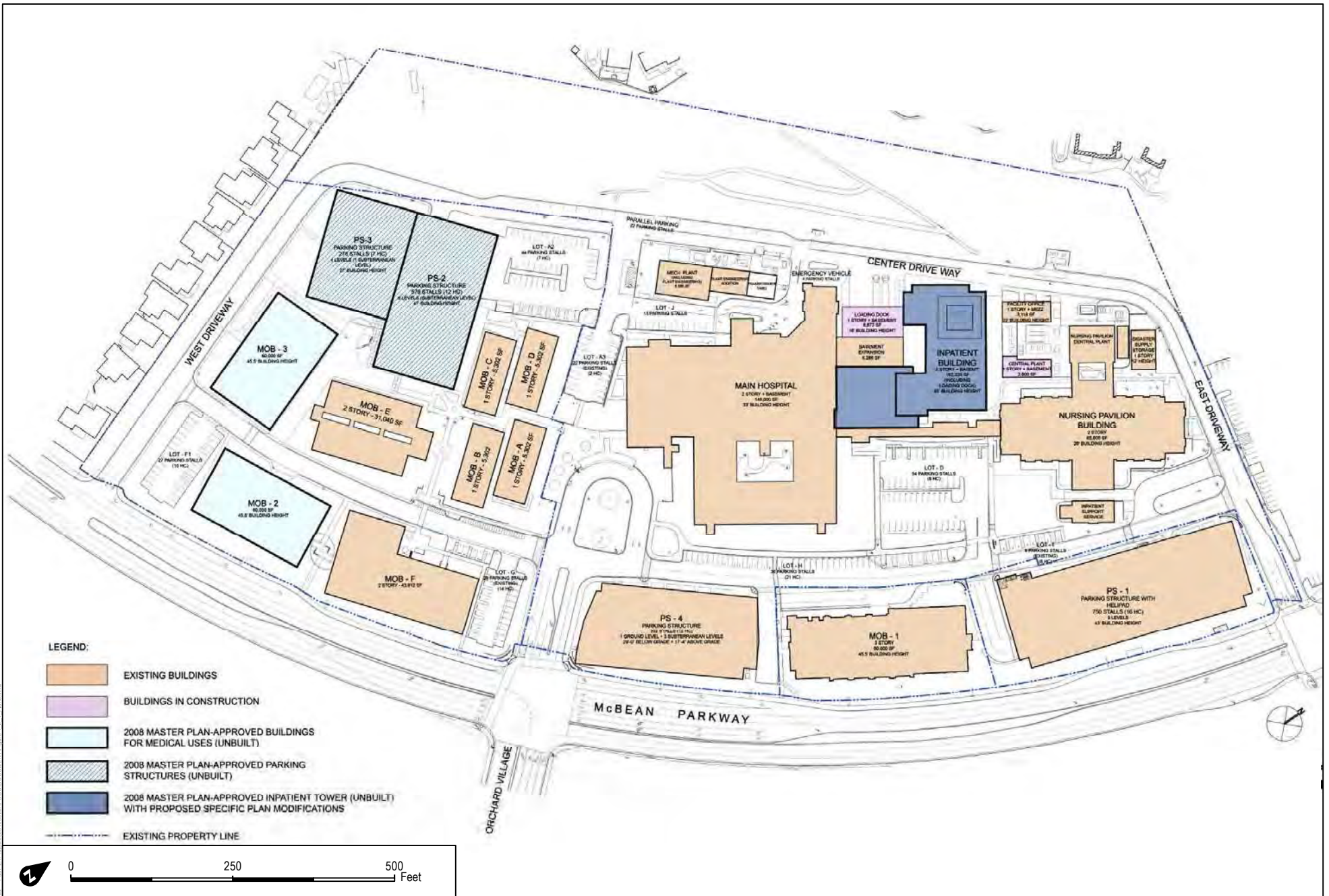
Henry Mayo Newhall Memorial Hospital Master Plan EIR Addendum

**FIGURE 2**  
Vicinity Map

**Addendum to the Henry Mayo Newhall Memorial Hospital  
Master Plan Certified EIR**

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SOURCE: SHP Project Development 2016



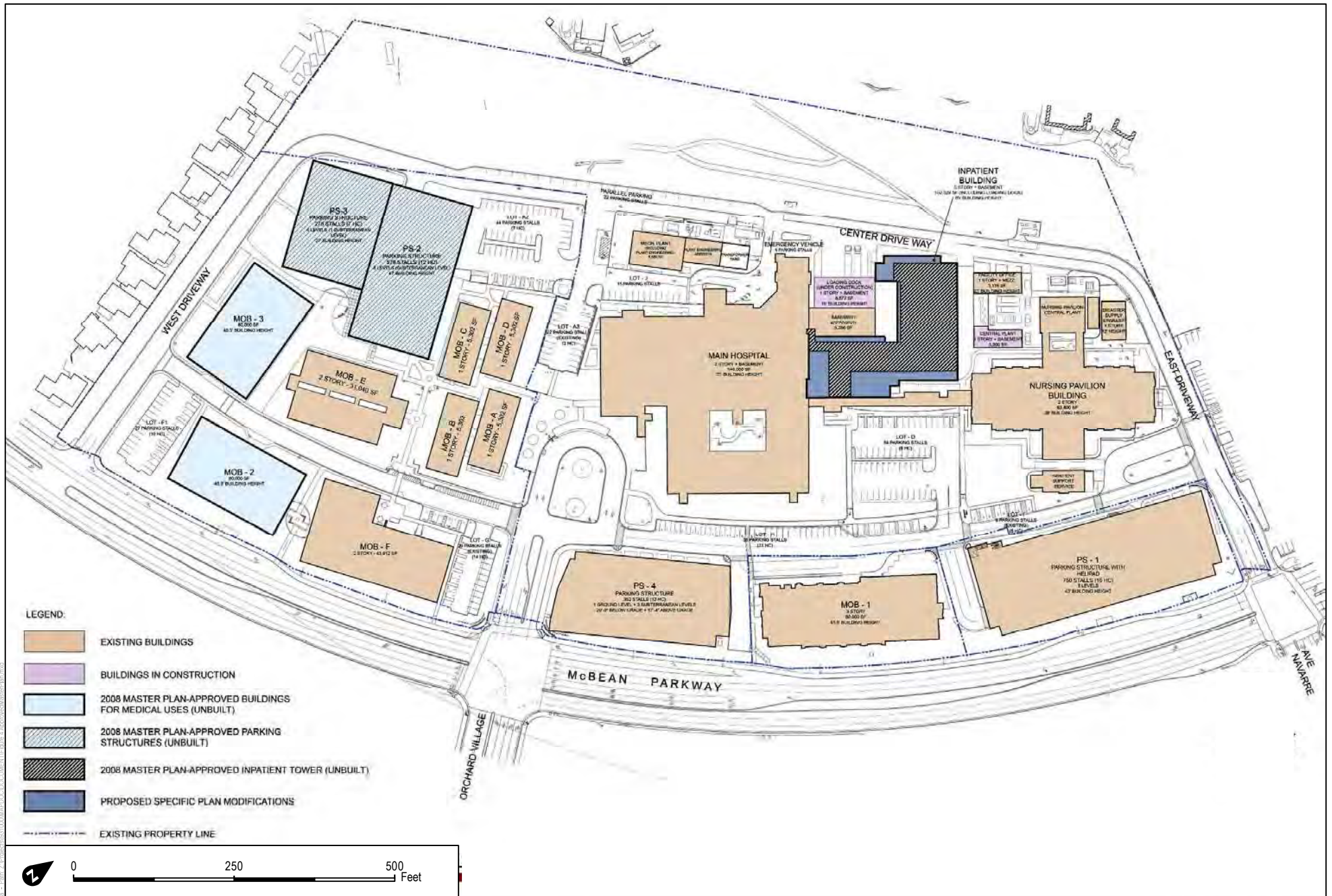
Henry Mayo Newhall Memorial Hospital Master Plan EIR Addendum

**FIGURE 3**  
Site Plan

**Addendum to the Henry Mayo Newhall Memorial Hospital  
Master Plan Certified EIR**

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SOURCE: SHP Project Development 2016



Henry Mayo Newhall Memorial Hospital Master Plan EIR Addendum

**FIGURE 4**  
Approved Master Plan with Specific Plan Overlay

**Addendum to the Henry Mayo Newhall Memorial Hospital  
Master Plan Certified EIR**

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# **APPENDIX A**

## *Master Plan EIR Mitigation Monitoring and Reporting Program*





Section 11.0  
MITIGATION MONITORING  
AND REPORTING PROGRAM

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## 11.0 MITIGATION MONITORING AND REPORTING PROGRAM

Sections 1.0 and 5.0 of this EIR identify the mitigation measures that will be implemented to reduce the impacts associated with the Henry Mayo Newhall Memorial Hospital Master Plan project. Section 21081.6 of the California Environmental Quality Act (CEQA) requires a public agency to adopt a monitoring and reporting program for assessing and ensuring compliance with any required mitigation measures applied to proposed development. As stated in Section 21081.6 of the Public Resources Code,

*. . . the public agency shall adopt a reporting or monitoring program for the changes to the project which it has adopted, or made a condition of project approval, in order to mitigate or avoid significant effects on the environment.*

Section 21081.6 provides general guidelines for implementing mitigation monitoring programs and indicates that specific reporting and/or monitoring requirements, to be enforced during project implementation, shall be defined prior to final certification of the EIR.

The mitigation monitoring table below lists those mitigation measures that may be included as conditions of approval for the project. These measures correspond to those outlined in Section 1.0 and discussed in Section 5.0. To ensure that the mitigation measures are properly implemented, a monitoring program has been devised which identifies the timing and responsibility for monitoring each measure. The project applicant will have the responsibility for implementing the measures, and the various City of Santa Clarita departments will have the primary responsibility for monitoring and reporting the implementation of the mitigation measures.



<b>HENRY MAYO NEWHALL MEMORIAL HOSPITAL MASTER PLAN ENVIRONMENTAL IMPACT REPORT</b>						
<b>MITIGATION MONITORING AND REPORTING PROGRAM</b>						
Mitigation Measure	Party Responsible for Mitigation	Monitoring Action/Timing	Enforcing, Monitoring Agency	Verification of Compliance		
				Initials	Date	Remarks
<b>AESTHETICS</b>						
AES1	Appropriate screening (i.e., temporary fencing with opaque material) shall be used to buffer views of construction activities, equipment and material from adjacent residential uses, existing hospital campus operations, and from McBean Parkway.	Project Applicant	Review plan(s) at plan check, and during construction.	City of Santa Clarita Planning Division		
AES2	Construction-related security lighting shall be directed away from adjacent residential areas and shall consist of the minimal wattage necessary to provide safety at the construction site.	Project Applicant	Review plan(s) at plan check, and during construction.	City of Santa Clarita Planning Division		
AES3	Prior to issuance of building permits, each structure shall undergo Development Review (DR) approval in conformance with the adopted Master Plan and conditions of approval for overall site design and architectural conformity.	Project Applicant	Process Development Review prior to issuance of building permits.	City of Santa Clarita Planning Division		
AES4	Landscaping shall be installed in conformance with the approved Master Plan conceptual landscaping plans and in compliance with the conditions of approval prior to issuance of a Certificate of Occupancy for each building and parking structure.	Project Applicant	Verify conformance during plan check.	City of Santa Clarita Planning Division		
<b>TRAFFIC</b>						
TR1	<p>In order to address impacts along McBean Parkway at the Magic Mountain Parkway intersection, the following improvements shall be required:</p> <ul style="list-style-type: none"> <li>◆ Add a third through lane for the westbound direction (re-striping). This improvement shall be implemented in conjunction with the construction of MOB1.</li> <li>◆ Add right-turn overlap phasing for the westbound</li> </ul>	Project Applicant	Review plan(s) at plan check.	City of Santa Clarita Department of Public Works		



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<p>right-turn movement (signal modification). This improvement shall be implemented in conjunction with the construction of MOB1.</p> <ul style="list-style-type: none"> <li>◆ Add a third through lane for the eastbound direction (re-striping). This improvement shall be implemented in conjunction with the construction of the Inpatient Building/MOB2.</li> </ul>							
<p>TR2</p> <p>In order to address impacts along Orchard Village Road at the Wiley Canyon Road intersection, the following improvement shall be required:</p> <ul style="list-style-type: none"> <li>◆ Add a separate northbound right-turn lane with right-turn overlap phasing (within existing right-of-way between Wiley Canyon Road and the Santa Clara River South Fork Bridge). This improvement shall be implemented in conjunction with the construction of MOB1.</li> </ul>	Project Applicant	Review plan(s) at plan check.	City of Santa Clarita Department of Public Works				
<p>TR3</p> <p>In order to address impacts along Orchard Village Road at the McBean Parkway intersection, the following improvements shall be required:</p> <ul style="list-style-type: none"> <li>◆ Widen the southbound approach (project driveway) to allow for a left-turn lane and a second through lane. This improvement shall be implemented in conjunction with the construction of MOB1.</li> <li>◆ Add a separate westbound right-turn lane (for project access). This improvement shall be implemented in conjunction with the construction of the Inpatient Building/MOB2.</li> <li>◆ Add a separate southbound right-turn lane (project</li> </ul>	Project Applicant	Review plan(s) at plan check.	City of Santa Clarita Department of Public Works				



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	driveway). This improvement shall be implemented in conjunction with the construction of the Inpatient Building /MOB2.						
TR4	<p>In order to address impacts along Valencia Boulevard at the Magic Mountain Parkway intersection, the following improvement shall be required:</p> <ul style="list-style-type: none"> <li>◆ Add a second westbound left-turn lane by removing the existing right-turn lane (re-striping the westbound approach as a mirror image of the existing eastbound approach). This improvement shall be implemented in conjunction with the construction of the Inpatient Building/MOB2.</li> </ul>	Project Applicant	Review plan(s) at plan check.	City of Santa Clarita Department of Public Works			
TR5	The project applicant shall pay fees to the established Valencia Bridge and Thoroughfare District, in accordance with City policy, in order to provide a fair-share contribution of funds for future traffic system improvements.	Project Applicant	Verify fee payment prior to issuance of building permits.	City of Santa Clarita Department of Public Works			
TR6	<p>In order to address impacts along McBean Parkway at the Orchard Village Road intersection, the following improvement shall be required:</p> <ul style="list-style-type: none"> <li>◆ Restripe the hospital driveway to reconfigure the first through lane to a shared left-turn/through lane. This improvement shall be implemented in conjunction with the construction of MOB3.</li> </ul>	Project Applicant	Review plan(s) at plan check.	City of Santa Clarita Department of Public Works			
TR7	<p>In order to address long-term (2030) impacts along McBean Parkway at the Valencia Boulevard intersection, the following improvement shall be required:</p> <ul style="list-style-type: none"> <li>◆ Add a fourth westbound through lane (requires the</li> </ul>	Project Applicant	Verify fee payment prior to issuance of building permits.	City of Santa Clarita Department of Public Works			



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<p>widening of Valencia Boulevard)</p> <p>The project's fair share equals 4.3 percent of the cost of this improvement. If a fair share program has been adopted or if these improvements have been added to a district, such as a Bridge &amp; Thoroughfare District, payment of fair share costs shall be made prior to the issuance of a building permit for MOB3. This fair share payment shall be considered this project's full compliance of Mitigation Measure TR7.</p>							
<p>TR8</p> <p>In order to address long-term (2030) impacts along McBean Parkway at the Orchard Village Road intersection, the following improvement shall be required:</p> <ul style="list-style-type: none"> <li>◆ Add a separate eastbound right-turn lane (requires the widening of McBean Parkway).</li> </ul> <p>The project's fair share equals 30.5 percent of the cost of this improvement. If a fair share program has been adopted or if these improvements have been added to a district, such as a Bridge &amp; Thoroughfare District, payment of fair share costs shall be made prior to the issuance of a building permit for MOB3. This fair share payment shall be considered this project's full compliance of Mitigation Measure TR8.</p>	Project Applicant	Verify fee payment prior to issuance of building permits.	City of Santa Clarita Department of Public Works				
<b>PARKING</b>							
<p>PRK1</p> <p>To maximize the on-site parking for non-construction uses, the project applicant shall prepare and implement a Parking Management Plan during the construction phases of the project. The Plan may include provisions for: 1) no construction worker parking on-site, and 2) off-site parking at an existing facility or facilities with a parking surplus, with a shuttle system, or other similar transportation method to and</p>	Project Applicant	Review and approve parking management plan prior to issuance of building permits.	City of Santa Clarita Planning Division and Department of Public Works				



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	from the hospital campus. The Plan shall be approved by the Director of Community Development prior to the issuance of any building permit included in the HMNMH Master Plan, which is identified in Section 3.0, Project Description, of this EIR.						
PRK2	As part of the plan review process for each phase of Master Plan buildout, the City of Santa Clarita shall ensure that the project applicant accompanies each development phase with adequate parking, in compliance with the City's <i>Municipal Code</i> .	Project Applicant	Review and approve as part of Development Review process	City of Santa Clarita Planning Division			
<b>AIR QUALITY</b>							
AQ1	<p>During construction, project applicant shall require the contractor to be responsible for ensuring that all measures listed in Table 5.6-9, Standard Measures for Construction-Related Emissions, are implemented. To achieve the particulate control efficiencies shown, finished surfaces are to be stabilized with water and/or dust palliatives and isolated from traffic flows to prevent emissions of fugitive dust from these areas. In addition, the following water application rates are required:</p> <ul style="list-style-type: none"> <li>◆ Roads traveled by autos, rock trucks, water trucks, fuel trucks, and maintenance trucks: up to twice per hour;</li> <li>◆ Roads traveled by scrapers and loaders in active excavation areas: up to three times per hour;</li> <li>◆ Finish grading areas: up to once every two hours.</li> </ul>	Project Applicant	Prior to and during construction activities.	City of Santa Clarita Department of Public Works			



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AQ2	The project applicant shall require the construction contractor to ensure that all construction equipment shall be maintained in good operating condition so as to reduce operational emissions. The contractor shall ensure that all construction equipment is properly serviced and maintained.	Project Applicant	Throughout construction activities.	City of Santa Clarita Department of Public Works			
AQ3	The project applicant shall require the construction contractor to utilize, as much as possible, pre-coated/natural colored building materials, water-based or low-VOC coating, and coating transfer or spray equipment with high transfer efficiency, such as HVLP spray method, or manual coatings application such as a paintbrush, hand roller, trowel, spatula, dauber, rag, or sponge.	Project Applicant	Throughout construction activities.	City of Santa Clarita Department of Public Works			
AQ4	All trucks that are to haul excavated or graded material on-site shall comply with <i>State Vehicle Code</i> Section 23114 (Spilling Loads on Highways), with special attention to Sections 23114(b)(2)(F), (e)(4) as amended, regarding the prevention of such material spilling onto public streets and roads. Prior to the issuance of grading permits, the project applicant shall demonstrate to the City of Santa Clarita how the project operations subject to that specification during hauling activities shall comply with the provisions set forth in Sections 23114(b)(2)(F), (e)(4).	Project Applicant	Prior to issuance of grading permits, and throughout construction activities.	City of Santa Clarita Department of Public Works			
AQ5	Proposed uses shall be designed to use low-emitting paints and solvents throughout. In addition, this shall be specified on the building plans.	Project Architect	Prior to issuance of building permits.	City of Santa Clarita Department of Public Works and/or Division of State Architect			





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AQ6	Install light-colored paving and cool roofs where feasible. The paving and roofs shall be specified on the building plans.	Project Architect	Prior to issuance of building permits.	City of Santa Clarita Department of Public Works and/or Division of State Architect			
AQ7	Plant shade trees pursuant to City requirements and standards, and shall be specified on the landscape plans.	Project Architect	Prior to issuance of building permits.	City of Santa Clarita Department of Public Works and/or Division of State Architect			
AQ8	Utilize light emitting diodes (LEDs) for outdoor lighting and limit the hours of outdoor lighting operation to hours of darkness. The location of outdoor lighting shall be specified on the building plans.	Project Architect	Prior to issuance of building permits.	City of Santa Clarita Department of Public Works and/or Division of State Architect			
<b>NOISE</b>							
N1	During all site excavation and grading, the project applicant shall require the project contractor(s) to equip all construction equipment, fixed or mobile, with properly operating and maintained mufflers, consistent with manufacturers' standards.	Construction Contractor	Throughout construction activities.	City of Santa Clarita Planning Division			
N2	The project applicant shall require the project contractor(s) to locate equipment staging in areas that would create the greatest distance between construction-related noise sources and noise-sensitive receptors nearest the project site during all project construction, to the extent practicable.	Construction Contractor	Throughout construction activities.	City of Santa Clarita Planning Division			



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N3	To meet the 45-dBA CNEL interior noise standard for medical office uses, mechanical ventilation, such as an air-conditioning system, shall be required for medical office buildings along the southern portion of the project site along McBean Parkway, in order to ensure that windows can remain closed for prolonged periods of time.	Project Architect	Review plan(s) at plan check.	City of Santa Clarita Department of Public Works			
<b>GEOLOGY, SOILS, AND SEISMICITY</b>							
GEO1	The project applicant shall have a geologist registered by the State of California prepare a Probabilistic Seismic Hazard Analysis (PSHA) prior to issuance of grading permits for the Inpatient Building. Any recommendations in the study are applicable to the Inpatient Building, if required by OSHPD, and shall be implemented during site grading and construction.	Project Applicant	Prepare analysis prior to issuance of grading permits. Recommendations in analysis implemented during construction activities.	City of Santa Clarita Department of Public Works			
GEO2	If potentially expansive units (i.e., clay soils) are encountered during construction, they shall be evaluated by the Project Geotechnical Engineer. Special foundation designs and reinforcement shall be utilized to mitigate expansive material as specified by the Project Geotechnical Engineer and to the satisfaction of the City. Specifically, if clay soils are exposed at the deeper subgrade level, the Construction Contractor shall employ dewatering techniques, as the clay soils shall not be allowed to dry out.	Project Applicant	Inspect soils during excavation and grading activities.	City of Santa Clarita Department of Public Works			



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<b>HAZARDS AND HAZARDOUS MATERIALS</b>						
HAZ1	The project applicant shall retain a qualified environmental specialist (e.g., a Registered Environmental Assessor or similarly qualified individual) to perform pre-construction hazardous materials surveys to inspect existing building areas subject to demolition or renovation for the presence of as yet unidentified asbestos, PCBs, mercury, lead, or other hazardous materials. If found at levels that require special handling, the Project Applicant shall manage these materials as required by law and according to federal and state regulations and guidelines, including those of DTSC, SCAQMD, Cal/OSHA, and any other agency with jurisdiction over these hazardous materials.	Project Applicant	Perform surveys prior to any demolition, grading, or construction activities.	City of Santa Clarita Department of Public Works		
<b>HYDROLOGY AND WATER QUALITY</b>						
HWQ1	The design of the parking structures (PS1, PS2, PS3, PS4) shall include trench drains and catch basins or similar technology in each level of the structures where runoff would be directed into an on-site storm drain pipe system and conveyed to a retention basin.	Project Applicant	Review plans(s) at plan check.	City of Santa Clarita Department of Public Works		
HWQ2	Estimate the amount of runoff to be retained on-site for each structure prior to issuance of a grading permit that incorporate storm water retention facilities equivalent to the 1 inch 1 hour storm and incorporate sediment and oily water separator BMPs into the drainage design. The retention facilities shall be serviceable without replacement. The overflow pipe shall provide for sampling water flows before they enter the McBean Parkway stormdrain pipe.	Project Applicant	Verify compliance prior to issuance of grading permit.	City of Santa Clarita Department of Public Works		



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HWQ3	During the detailed engineering design phase and prior to the issuance of grading permits, the Project Applicant shall prepare an Urban Storm Water Management Plan (USMP) for each individual building. The USMP shall be prepared by a California registered Civil Engineer, Architect, Landscape Architect or any professional knowledgeable about storm water management issues and shall comply with post-construction and applicable BMPs, as detailed in the Los Angeles County Standard Urban Stormwater Mitigation Plan (SUSMP), to address each source of pollutants identified by the Project evaluation. Possible BMPs include: Bioretention basins, bioswales, catch basin filters, regular street and parking lot sweeping, porous pavement, roof runoff controls, efficient irrigation, alternative building materials, stormdrain signage, trash enclosures, preservation of existing vegetation, hydraulic mulch, hydroseeding, soil binders, straw mulch, geotextiles and mats, wood mulching, earth dikes and drainage swales, velocity dissipation devices, slope drains, polyacrylamide, and stockpile management.	Project Applicant	Verify compliance prior to issuance of grading permit.	City of Santa Clarita Department of Public Works			
<b>FIRE PROTECTION SERVICES</b>							
FS1	Concurrent with the issuance of building permits, the project applicant shall participate in the Developer Fee Program to the satisfaction of the County of Los Angeles Fire Department.	Project Applicant	Verify compliance prior to issuance of grading or building permits.	City of Santa Clarita Department of Public Works			
FS2	Adequate access to all buildings on the project site shall be provided and properly maintained for emergency vehicles during the building construction process to the satisfaction of the County of Los Angeles Fire Department.	Project Applicant	Review construction plans prior to issuance of building permits.	Los Angeles County Fire Department			



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FS3	Adequate water availability shall be provided to service construction activities to the satisfaction of the County of Los Angeles Fire Department.	Project Applicant	Review construction plans prior to issuance of building permits.	Los Angeles County Fire Department			
FS4	Every building constructed shall be accessible to Fire Department apparatus by way of access roadways, with an all-weather surface of not less than the prescribed width, unobstructed, clear-to-sky. The edge of the roadway shall be within 150 feet of all portions of the exterior walls when measured by an unobstructed route around the exterior of the building.	Project Applicant	Review plan(s) at plan check.	City of Santa Clarita Planning Division and Department of Public Works, and Los Angeles County Fire Department			
FS5	Commercial development shall require fire flows up to 5,000 gallons per minute at 20 pounds per square inch residual pressure for up to a five-hour duration, unless otherwise deemed appropriate by the Fire Department. Final fire flows shall be based on the size of the buildings, their relationship to other structures, property lines, and types of construction used. Fire hydrant spacing shall be 300 feet and shall meet the following requirements: <ul style="list-style-type: none"> <li>◆ No portion of lot frontage shall be more than 200 feet via vehicular access from a public fire hydrant.</li> <li>◆ No portion of a building shall exceed 400 feet via vehicular access from a properly spaced public fire hydrant.</li> <li>◆ Additional hydrants will be required if hydrant spacing exceeds specified distances.</li> </ul>	Project Applicant	Review plan(s) at plan check.	City of Santa Clarita Planning Division and Department of Public Works, and Los Angeles County Fire Department			
FS6	Turning radii shall not be less than 32 feet. This measurement shall be determined at the centerline of the road. A Fire Department approved turning area shall be provided for all driveways exceeding 150 feet in length. All on-site driveways	Project Applicant	Review plan(s) at plan check.	City of Santa Clarita Planning Division and Department of Public Works, and Los			



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	shall provide a minimum unobstructed width of 28 feet. The on-site driveway is to be within 150 feet of all portions of the exterior walls of the first story of any building. The centerline of the access driveway shall be located parallel to, and within 30 feet of an exterior wall on one side of the proposed structure.			Angeles County Fire Department			
FS7	Any access way less than 34 feet in width shall be labeled "Fire Lane" on the final building plans.	Project Applicant	Review plan(s) at plan check.	City of Santa Clarita Planning Division and Department of Public Works, and Los Angeles County Fire Department			
FS8	The entrance to the street/driveway and intermittent spacing distances of 150 feet shall be posted with Fire Department approved signs stating "NO PARKING – FIRE LANE" in three-inch high letters. Driveway labeling is necessary to ensure access for Fire Department use.	Project Applicant	Review plan(s) at plan check.	City of Santa Clarita Planning Division and Department of Public Works, and Los Angeles County Fire Department			
FS9	All proposals for traffic calming measures (speed humps/bumps/cushions, traffic circles, roundabouts, etc.) shall be submitted to the Fire Department for review and approval, prior to issuance of building permit.	Project Applicant	Verify Fire Department review prior to issuance of building permits.	City of Santa Clarita Planning Division and Department of Public Works, and Los Angeles County Fire Department			



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<b>SHERIFF SERVICES</b>						
SS1	During construction, private security patrols shall be utilized to protect the project site.	Project Architect	Ongoing during construction	City of Santa Clarita Planning Division		
SS2	Construction-related traffic, including all off-site earthmoving operations, shall be limited to between the hours of 9:00 AM and 2:00 PM in order to avoid weekday peak traffic conditions.	Project Architect	Ongoing during Construction.	City of Santa Clarita Planning Division		
SS3	As final site and building plans are submitted to the City for approval in the future, Sheriff's Department design requirements which reduce demands for service and ensure adequate public safety (such as those pertaining to site access, site security lighting), shall be incorporated into building designs.	Project Architect	Review as part of Development Review process and require during plan check.	City of Santa Clarita Planning Division		
SS4	Project design shall landscape the project site with low-growing groundcover and shade trees, rather than a predominance of shrubs that could conceal potential criminal activity around buildings and parking areas.	Project Architect	Review as part of Development Review process and require during plan check.	City of Santa Clarita Planning Division		
SS5	Project design shall provide lighting around and throughout the development to enhance crime prevention and enforcement efforts.	Project Architect	Review as part of Development Review process and require during plan check	City of Santa Clarita Planning Division		
SS6	Project design shall provide clearly visible (during the day and night) address signs and/or building numbers for easy identification during emergencies.	Project Architect	Review as part of Development Review process and require during plan check.	City of Santa Clarita Planning Division		



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					Initials	Date	Remarks
SS7	Project design shall provide visibility of doors and windows from the street and between buildings.	Project Architect	Review plan(s) at plan check.	City of Santa Clarita Planning Division			
SS8	Concurrent with the issuance of building permits, the project applicant shall participate in the Police Facility Fee Program to the satisfaction of the City of Santa Clarita.	Project Applicant	Verify fee payment prior to issuance of building permits.	City of Santa Clarita Public Works Dept.			
<b>SCHOOLS/EDUCATION</b>							
SE1	The project applicant shall pay the required mitigation fees in place at time of payment to the Newhall District, prior to issuance of building permit as full mitigation of project impacts on this district.	Project Applicant	Verify fee payment prior to issuance of building permits.	City of Santa Clarita Department of Public Works			
SE2	The project applicant shall pay the required mitigation fees in place at time of payment to the Hart District, prior to issuance of building permit as full mitigation of project impacts on this district.	Project Applicant	Verify fee payment prior to issuance of building permits.	City of Santa Clarita Department of Public Works			
<b>SOLID WASTE</b>							
SW1	The location of recycling/separation areas shall be in proximity to dumpsters for non-recyclables, elevators, loading docks, and primary internal and external access points.	Project Applicant	Review plan(s) at plan check.	City of Santa Clarita Environmental Services Division			
SW2	The location of recycling/separation areas shall be convenient for those persons who deposit, collect, and load the recyclable materials.	Project Applicant	Review plan(s) at plan check.	City of Santa Clarita Environmental Services Division			
SW3	Recycling containers/bins shall be located so that they do not block access to each other.	Project Applicant	Review plan(s) at plan check.	City of Santa Clarita Environmental Services Division			





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**APPENDIX B**  
*Traffic Study Addendum*



# MEMORANDUM

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To: City of Santa Clarita Date: February 9, 2016

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From: David S. Shender, P.E. LLG Ref: 5-15-0159-1  
Tin T. Nguyen  
Linscott, Law & Greenspan, Engineers

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Subject: **Henry Mayo Newhall Memorial Hospital – Updated Traffic Study Addendum**

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This memorandum has been prepared by Linscott, Law & Greenspan, Engineers (LLG) to summarize an updated traffic study addendum prepared for the approved Henry Mayo Newhall Memorial Hospital Master Plan project located along the west side of McBean Parkway at the intersection with Orchard Village Road in the City of Santa Clarita.

The approved Master Plan project for the Hospital comprises 200,000 square feet of additional medical office floor area, 8,000 square feet of hospital demolition, and 135,363 square feet of new hospital floor area space (which includes a 10,000 square-foot central plant). A traffic study<sup>1</sup> was previously prepared in 2008 for the Master Plan project (the “2008 Traffic Study”). The findings of the 2008 Traffic Study report were incorporated into the City’s Environmental Impact Report (EIR) prepared for the Master Plan project. The EIR was certified by the City of Santa Clarita.

The Hospital proposes to modify its approved Master Plan so as to provide additional hospital floor area related to a new patient tower. Briefly, the amount of new hospital floor area proposed for construction would increase by 30,566 square feet (from 135,363 square feet of approved space to 162,329 square feet of floor area, inclusive of the proposed central plant).

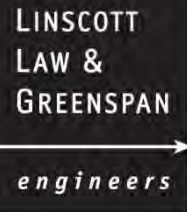
It is noted that LLG prepared a prior addendum document<sup>2</sup> evaluating a change in the Master Plan. The prior addendum evaluated an increase in floor area by approximately 25,700 square feet to a total of 159,100 square feet of hospital floor area. This updated addendum has been prepared based on the modest change in the proposed total hospital floor area (i.e., an increase of 30,566 square feet instead of 25,700 square feet evaluated in the prior addendum).

Based on the relatively modest change in the approved Master Plan project, we understand the City of Santa Clarita has requested this traffic study addendum to determine if conditions related to the project and local traffic conditions have changed such that new traffic impacts would be identified based on the increase in hospital floor area. Accordingly, this traffic study addendum provides:

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<sup>1</sup> *Henry Mayo Newhall Memorial Hospital Master Plan Traffic Impact Analysis*, Austin-Foust Associates, Inc., May 2008.

<sup>2</sup> *Henry Mayo Newhall Memorial Hospital – Traffic Study Addendum*, LLG, March 5, 2015.



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- Updated peak hour traffic counts at the intersections evaluated in the 2008 Traffic Study;
- A revised forecast in the site trip generation based on the proposed modified Master Plan project with the additional hospital floor area; and
- An updated traffic impact assessment for the Master Plan project.

Based on the impact analysis contained herein, it is concluded that the proposed modification to the Hospital's Master Plan project is not expected to create significant traffic impacts that were not previously identified in the 2008 Traffic Study, and by extension the previously certified EIR for the Master Plan project.

### **Current Existing Setting**

The Vicinity Map for the Hospital is provided in *Figure 1*. The site plan for the approved Master Plan is illustrated in *Figure 2*. Vehicular access for the Hospital site is provided via driveways on the west end along McBean Parkway, at Orchard Village Road, and at Avenida Navarre. No changes to site access and internal circulation are proposed in conjunction with the proposed revisions to the Master Plan.

The following intersections were evaluated in the 2008 Traffic Study for potential traffic impacts due to the Master Plan project:

1. I-5 SB On- and Off- Ramps / McBean Parkway
2. I-5 NB Off- and On- Ramps / McBean Parkway
3. Rockwell Canyon Road-Tournament Road / McBean Parkway
4. McBean Parkway / Valencia Boulevard
5. McBean Parkway / Magic Mountain
6. Wiley Canyon Road / Lyons Avenue
7. Tournament Road / Wiley Canyon Road
8. Orchard Village Road / Lyons Avenue
9. Orchard Village Road / Wiley Canyon Road
10. Orchard Village Road / McBean Parkway
11. Newhall Avenue / Lyons Avenue
12. Valencia Boulevard / Magic Mountain Parkway
13. Avenida Navarre / McBean Parkway

The study intersections are shown by location on *Figure 1*. The existing travel lane configurations and traffic control devices at the study intersections are provided on *Figure 3*.

It is noted that construction has started at the Hospital site to implement elements of the approved Master Plan. Specifically, MOB 1 (80,000 square feet) has been constructed at the site, as well as additional parking. MOB 2 and MOB 3, as well as the inpatient tower have not been completed.

Additionally, the EIR for the Master Plan project<sup>3</sup> identifies traffic Mitigation Measures applicable to MOB 1 (specifically, TR 1, TR 2, TR 3 as listed on pages 3.4-34 and 3.4-35). These measures are:

- TR 1 (McBean Parkway/Magic Mountain Parkway):
  - Add a third through lane in the westbound direction (completed)
  - Add a right-turn overlap in the westbound direction (completed)
- TR 2 (Orchard Village Road/Wiley Canyon Road)
  - Add a northbound right-turn lane (completed)
  - Add a right-turn overlap in the northbound direction (completed)
- TR 3 (Site Driveway-Orchard Village Road/McBean Parkway)
  - Widen the project driveway to provide a left-turn lane and second through lane (completed)

Based on a field review, the traffic mitigation measures associated with MOB 1 have been completed as recommended in the EIR.

## Traffic Volumes

The study intersections evaluated in the 2008 Traffic Study prepared for the Hospital Master Plan are based on traffic counts conducted in 2003 and 2005. Additionally, the 2008 Traffic Study (as incorporated into the certified EIR) assesses the potential traffic impacts of the Master Plan against a forecast “Interim Year” condition. As described in the 2008 Traffic Study: “The Interim Year transportation system consists of roadway improvements and future infrastructure consistent with the cumulative projects included within the horizon year. Generally this horizon year corresponds to a level of development approximately 10 to 15 year in the future. While this horizon does not coincide specifically with the buildout of the project, it represents the best timeframe for planning purposes since it includes a comprehensive set of cumulative

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<sup>3</sup> 2008 Draft Environmental Impact Report for the Henry Mayo Newhall Memorial Hospital Master Plan, City of Santa Clarita, September 2008.

development project that have been incorporated into the SCVCTM [Santa Clarita Valley Computer Traffic Model].”

Further, the 2008 Traffic Study states that its Interim Year conditions include the effects of three important roadway projects:

- Extension of Newhall Ranch Road east to Golden Valley Road/Soledad Canyon Road;
- Connection of Via Princessa between its current terminus near San Fernando Road to its eastern terminus near Rainbow Glenn Drive; and
- Extension of Magic Mountain Parkway to Via Princessa (coinciding with the initial development of the Whitaker-Bermite site).

Of these roadway projects, only the extension of Newhall Ranch Road, and the partial extension of Via Princessa (westerly to Wiley Canyon Road) have been completed. However, many of the related development projects (with the exception of the Whitaker-Bermite project) have been completed. Therefore, it is reasonable to:

- Utilize current weekday morning (AM) and afternoon (PM) peak period traffic counts at the intersections evaluated in the 2008 Traffic Study; and
- Compare the relative differences in traffic volumes between the 2015 traffic counts and the forecast Interim Year traffic volumes from the 2008 traffic study as these the basis for assessing the relative traffic impacts of the Master Plan.

Manual traffic counts of vehicular turning movements were conducted in February 2015 at each of the study intersections during the weekday AM and PM commuter periods to determine the peak hour traffic volumes. The manual traffic counts at the study intersections were conducted from 7:00 AM to 9:00 AM and from 4:00 PM to 6:00 PM to determine the peak commuter hours. Summary data worksheets of the manual traffic counts at the study intersections are contained in *Appendix A*.

Additionally, the traffic counts were increased using a growth rate of 3% to reflect existing (2016) conditions. The growth factor was based on general traffic growth factors provided in the 2010 Congestion Management Program for Los Angeles County (the “CMP manual”). It is noted that based on review of the general traffic growth factors provided in the CMP manual for the Santa Clarita area, it is anticipated that traffic volumes are expected to increase at an annual rate of approximately 2.9% per year between the years 2015 and 2020. Thus, the 3% growth factor was applied to the 2015 traffic counts.



As previously noted, MOB 1 at the Hospital has been completed and is occupied. Therefore, to provide an “apples to apples” evaluation, the estimated traffic volumes associated with MOB 1 were subtracted from the existing traffic counts for purposes of comparing these intersection volumes to those provided in the 2008 Traffic Study for the Interim Year (without Project) condition.

The weekday AM and PM peak hour traffic counts at the study intersections – modified to eliminate the traffic associated with existing MOB 1 – are summarized in **Table 1**. The prior forecast Interim Year traffic volumes are also provided in **Table 1**. As shown, when the traffic volumes for the 13 intersections are totaled, there is an overall 24% decrease in the counted AM peak hour volumes and a 22% decrease in the counted PM peak hour volumes when compared to the Interim Year (without Project) traffic volumes provided in the 2008 Traffic Study. The existing traffic volumes at the study intersections during the weekday AM and PM peak hours (modified to remove MOB 1 traffic) are shown in **Figures 4** and **5**, respectively.

In general, it can be concluded that the forecast increase in traffic volumes provided in the 2008 Traffic Study have not occurred. Further, the baseline (no project) traffic conditions for purposes of assessing the impacts of the Master Plan are at better levels in 2016 as compared to the forecast traffic conditions provided in the 2008 Traffic Impacts. Thus, in general, the relative traffic impacts of the Master Plan will be less since overall traffic operations at the study intersections are better than the conditions forecast in the 2008 Traffic Study.

### **Project Trip Generation**

The 2008 Traffic Study forecast vehicle trips for the components of the Master Plan based on trip generation rates provided in the 7<sup>th</sup> Edition of the *Trip Generation* manual published by the Institute of Transportation Engineers (ITE). As shown in Table 3-2 of the 2008 Traffic Study, the approved Master Plan project was forecast to generate 519 AM peak hour trips, 715 PM peak hour trips, and 7,571 daily trips.

For the proposed revision to the Master Plan (i.e., consisting of the additional hospital floor area), an updated trip generation forecast was prepared for the project utilizing the most current version of the ITE *Trip Generation* manual (i.e., the 9<sup>th</sup> Edition). The 9<sup>th</sup> Edition is an update to the 7<sup>th</sup> Edition as it considers the most current trip generation data submitted to ITE, and is reflected in the recommended trip generation rates for land uses provided therein. It is noted that for the hospital and medical office land use categories, the trip generation rates recommended by ITE have been reduced slightly when comparing the 9<sup>th</sup> Edition to the 7<sup>th</sup> Edition.

**Table 2** attached to this memorandum provides the trip generation forecast for the Master Plan. As shown in *Table 2*, the Master Plan is forecast to generate 493 net new AM peak hour trips (404 inbound trips and 89 outbound trips) and 680 net new PM peak hour trips (165 inbound trips and 515 outbound trips) on a typical weekday, as well as 7,374 daily trips (3,687 inbound trips and 3,687 outbound trips) on a typical weekend.

When compared to the forecast provided in the 2008 Traffic Study for the approved project, *Table 1* show that there is an overall decrease in number of trips generated by the Master Plan, even when accounting for the proposed 30,566 square feet of hospital floor area. This reduction is due primarily to the reduction in the ITE-recommended trip generation rate for the hospital and medical office land uses as provided in the more current 9<sup>th</sup> Edition of the *Trip Generation* manual.

### **Project Traffic Distribution and Assignment**

The forecast peak hour trips for the revised Master Plan were assigned to the study intersections. The assignment utilized a similar distribution analyzed in the 2008 Traffic Study. The directional traffic distribution patterns for the Master Plan project are presented in **Figures 6** and **7** for the AM and PM peak hours, respectively. The forecast net new weekday AM and PM peak hour project traffic volumes at the study intersections associated with the revised Master Plan project are presented in **Figures 8** and **9**, respectively. Note that the traffic volumes shown on *Figures 8* and *9* include the estimated traffic associated with the recently completed MOB 1 project, as well as the remaining elements of the Master Plan. The traffic volume assignments presented in *Figure 8* and *9* reflects the traffic distribution characteristics shown in *Figure 6* and *7* and the project traffic generation forecast presented in *Table 2*.

### **Traffic Impact Analysis Methodology**

LLG has prepared updated intersection Level of Service calculations to evaluate the potential traffic impacts due to the revised Master Plan project. The relative impact of the added traffic volumes to be generated by the Master Plan project during the AM and PM peak hours was evaluated based on analysis of existing operating conditions at the study intersections, without and with the project. The previously discussed capacity analysis procedures were utilized to evaluate the future *v/c* relationships and service level characteristics at the study intersections.

The traffic impact analysis scenarios and significance of the potential impacts of project generated traffic was identified using the traffic impact criteria set forth in City of Santa Clarita's *Preliminary Traffic Impact Guidelines* and *General Plan Circulation Element*.

## Traffic Analysis

The traffic impact analysis was prepared for the 13 study intersections using the ICU methodology and application of the City of Santa Clarita significant traffic impact criteria is summarized in *Table 3*. The ICU data worksheets for the analyzed intersection are contained in *Appendix B*.

Column [1] presents the existing (2016) volume-to-capacity ratios (v/c) and Levels of Service (LOS) at the study intersections during the AM and PM peak hours. Column [2] removes the estimated traffic associated with the MOB 1 project so as create a new baseline for traffic assessment purposes, similar to the Interim Year conditions provided in the 2008 Traffic Study.

Column [3] of *Table 3* applies the updated forecast traffic volumes associated with the proposed revised Master Plan project. As shown in *Table 3*, application of the City's threshold criteria indicates that the revised Master Plan project is not expected to create significant impacts at the study intersections. Incremental, but not significant, impacts are noted at the study intersections.

Column [4] on *Table 3* is provided for informational purposes. It utilizes the Interim Year traffic volumes on the existing roadway network. It indicates that except for a few isolated locations, traffic operations at the study intersections are substantially better at the study intersections than what would be forecast using the Interim Year traffic volumes provided in the 2008 Traffic Study. It further points out the potential traffic impacts of the Master Plan will be less than original forecast because the baseline conditions are substantially better.

Based on the above, it is concluded that the revised Master Plan will not create new traffic impacts that were not previously identified in the 2008 Traffic Study, and by extension in the previously certified EIR. This analysis is provided in consideration of recent available information such as new traffic counts and use of the current edition of the ITE *Trip Generation* manual. No additional analysis of traffic impacts related to the Master Plan is required or recommended.

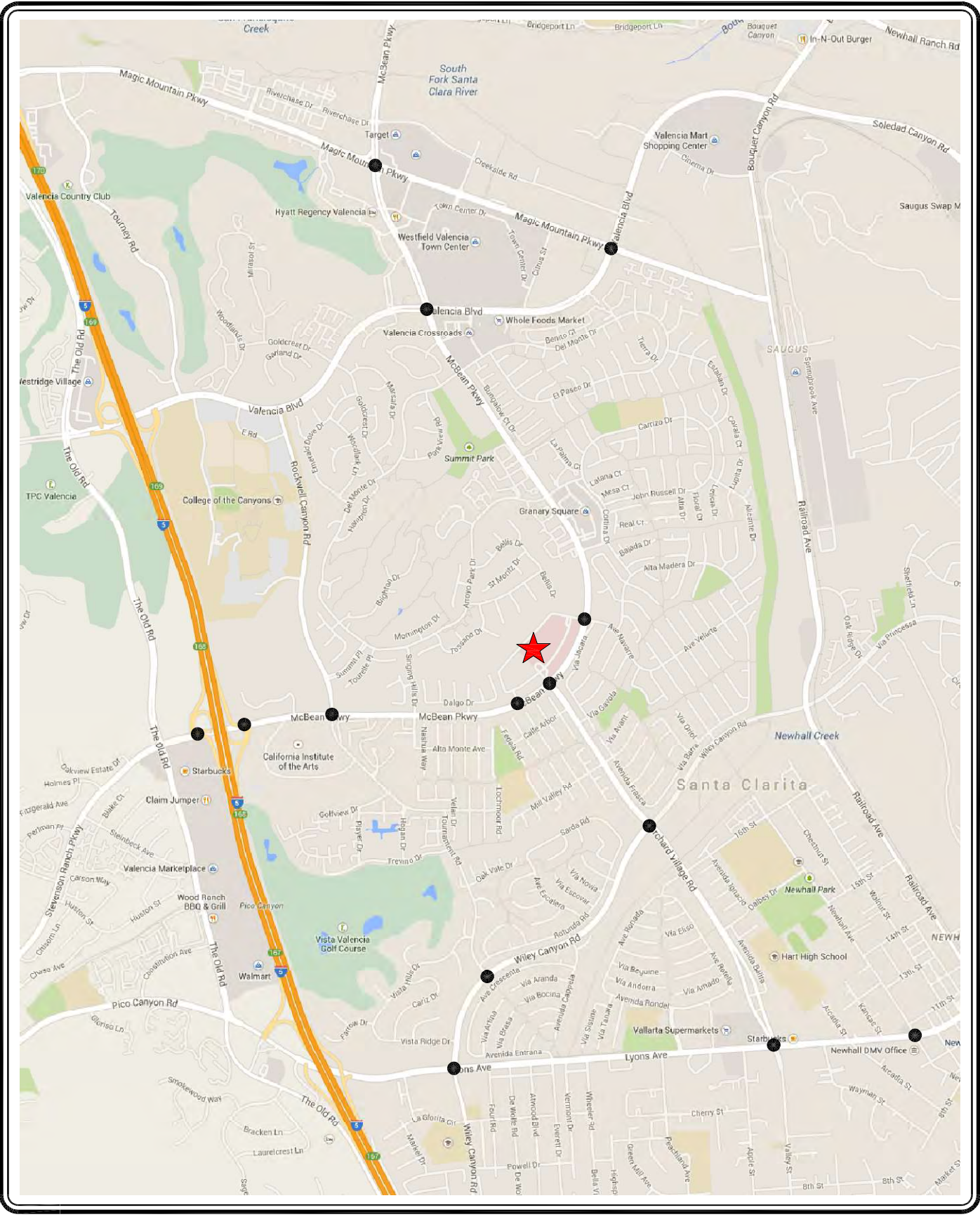
## Conclusion

In summary, the Henry Mayo Newhall Memorial Hospital project with the revised site plan – 162,329 square feet of hospital space – and existing conditions would not result in significant traffic impacts at any of the 13 study intersections. These findings are improved over the May 2008 traffic study which concluded that the prior site plan would result in significant traffic impacts at three of the 13 study intersections. Accordingly, no additional analysis of traffic impacts is required or recommended as a result of the updated project description.

## Attachments

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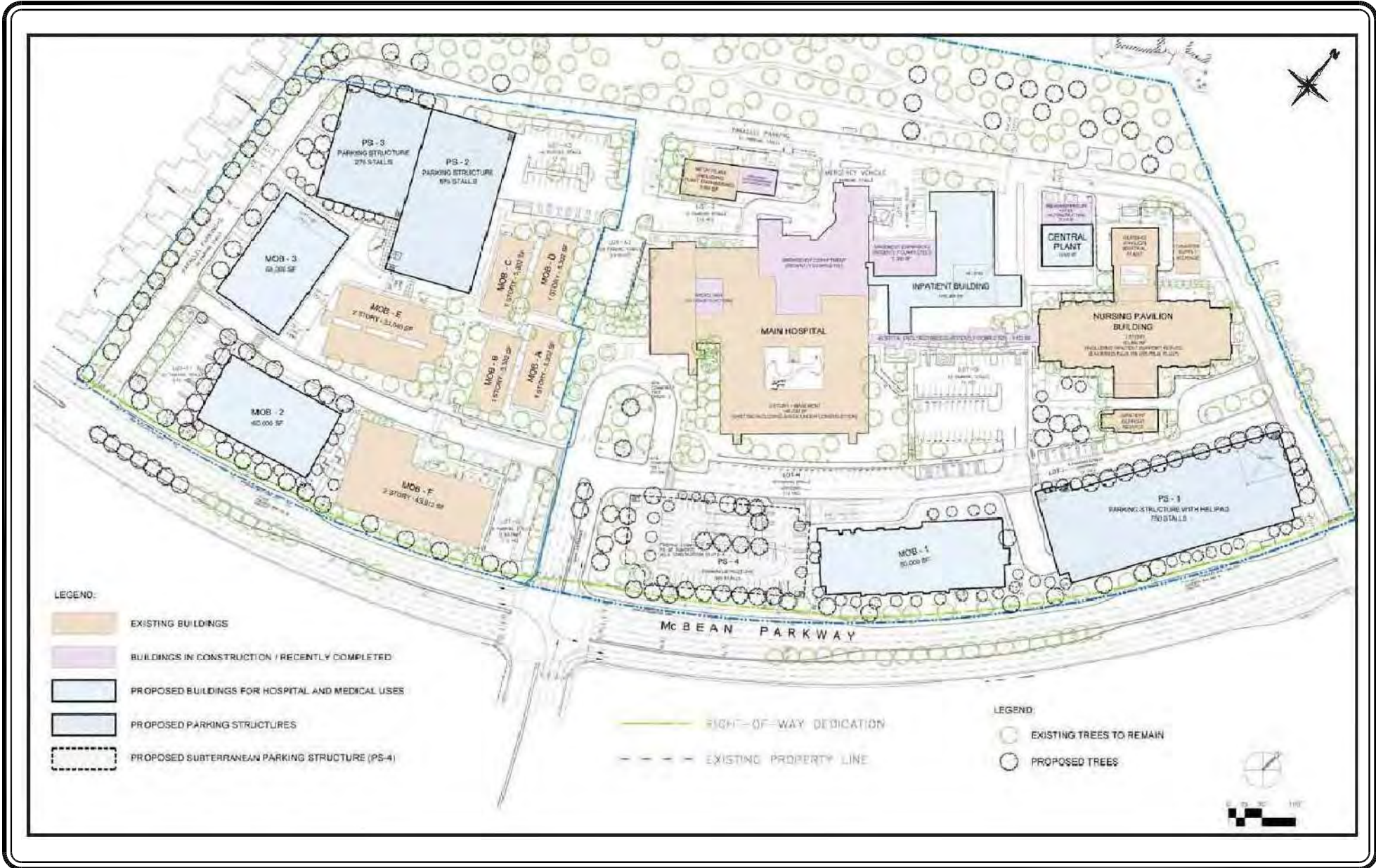
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- MAP SOURCE: GOOGLE MAPS
- ★ PROJECT SITE
- STUDY INTERSECTION

# FIGURE 1 VICINITY MAP

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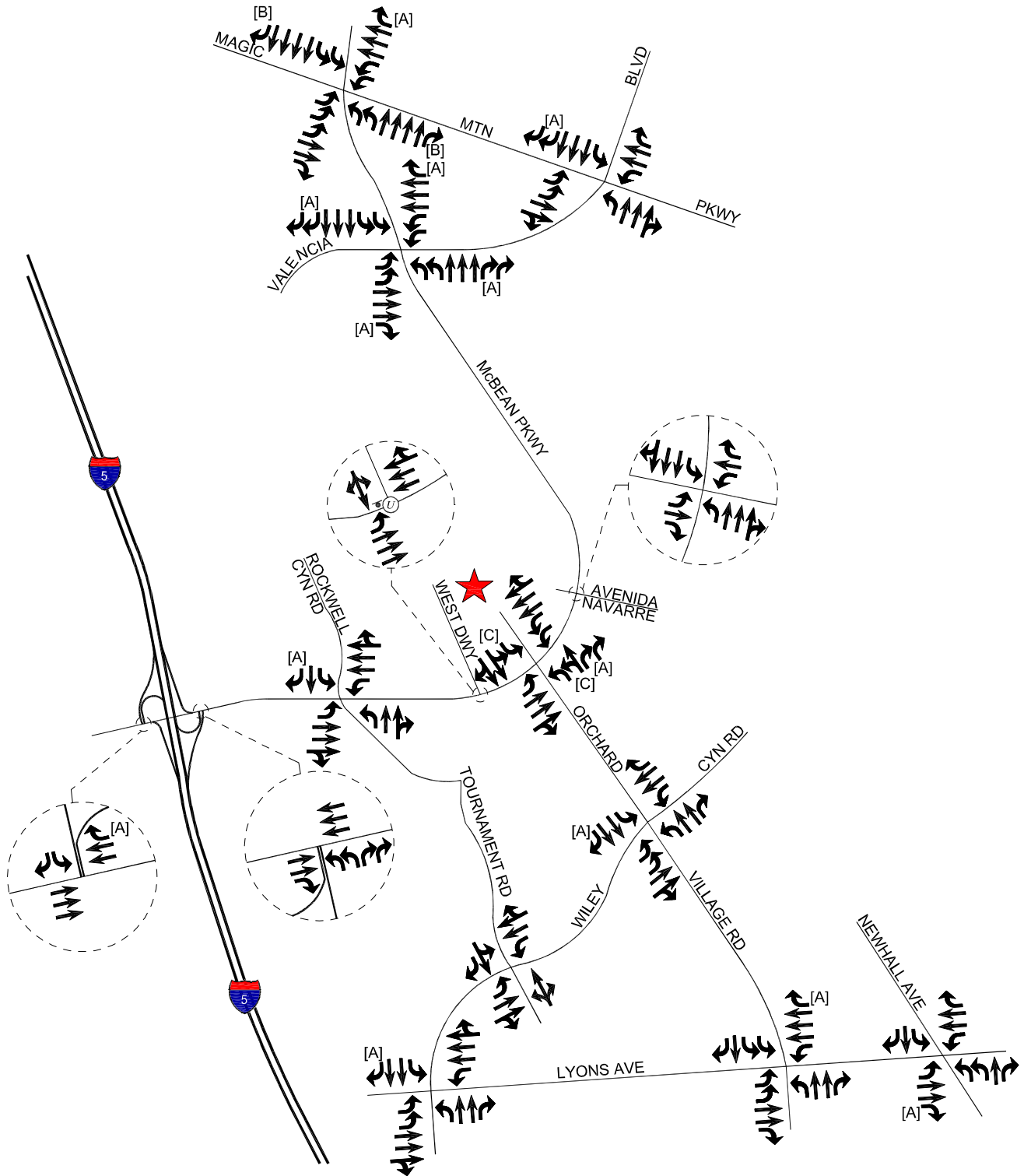
SOURCE: AUSTIN-FOUST ASSOCIATES, INC.

FIGURE 2  
PROJECT SITE PLAN

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HENRY MAYO UPDATE

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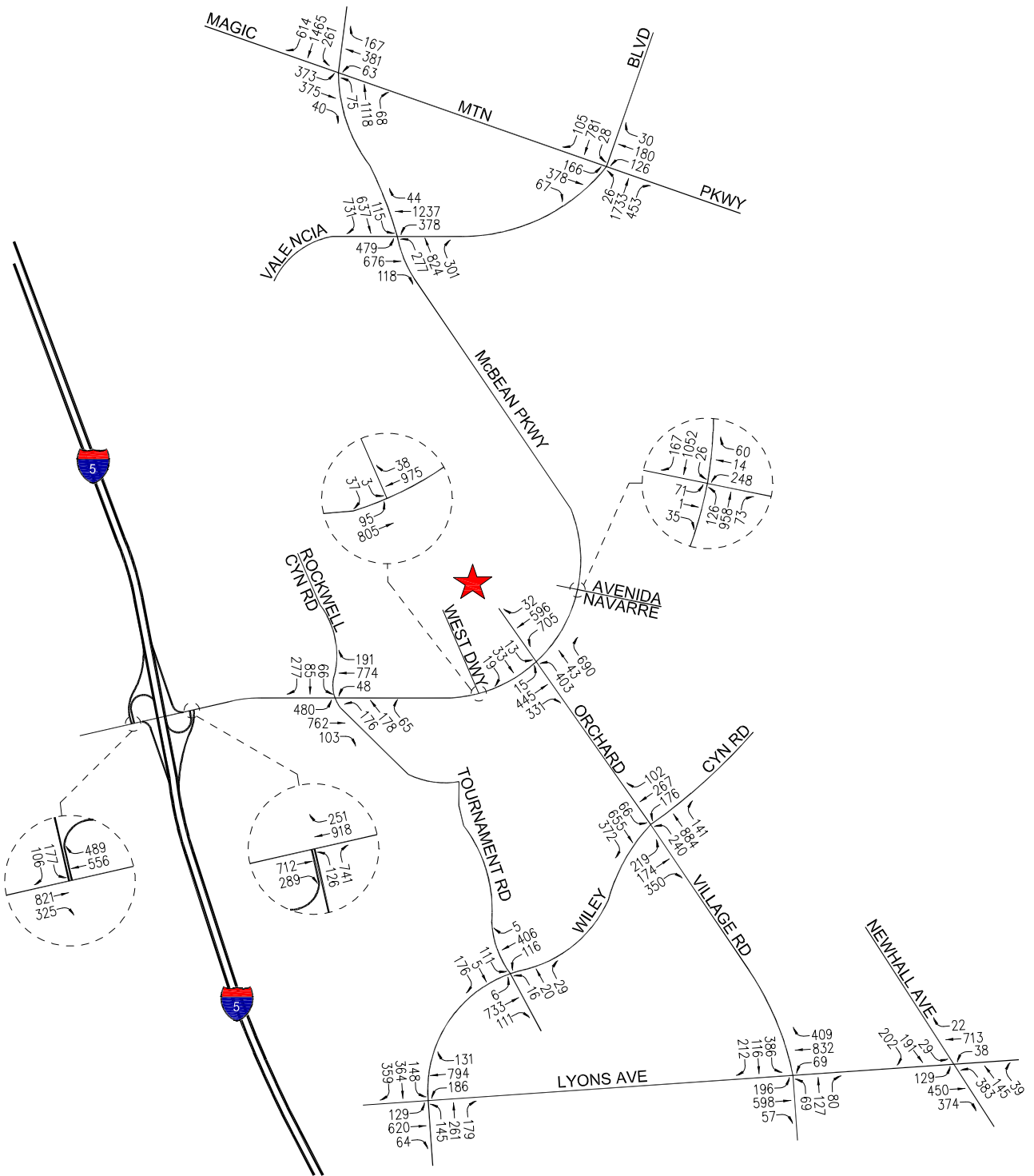
★ PROJECT SITE  
 NOTE: ALL INTERSECTIONS ARE SIGNALIZED UNLESS OTHERWISE INDICATED  
 U UNSIGNALIZED  
 [A] RIGHT-TURN OVERLAP  
 [B] FREE FLOW RIGHT-TURN  
 [C] SPLIT-PHASE

# FIGURE 3 EXISTING LANE CONFIGURATIONS

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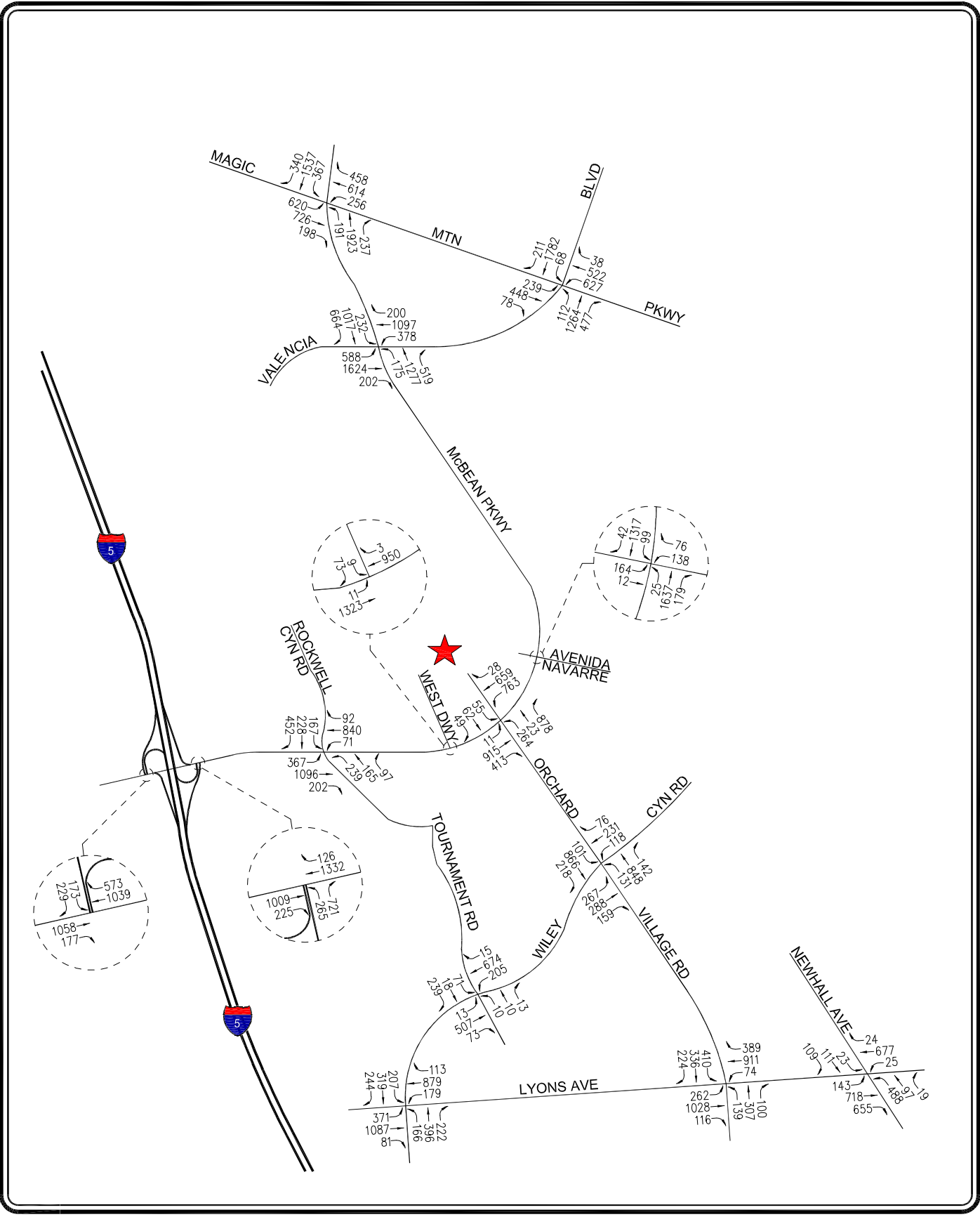
**FIGURE 4**  
**EXISTING TRAFFIC VOLUMES**  
WEEKDAY AM PEAK HOUR

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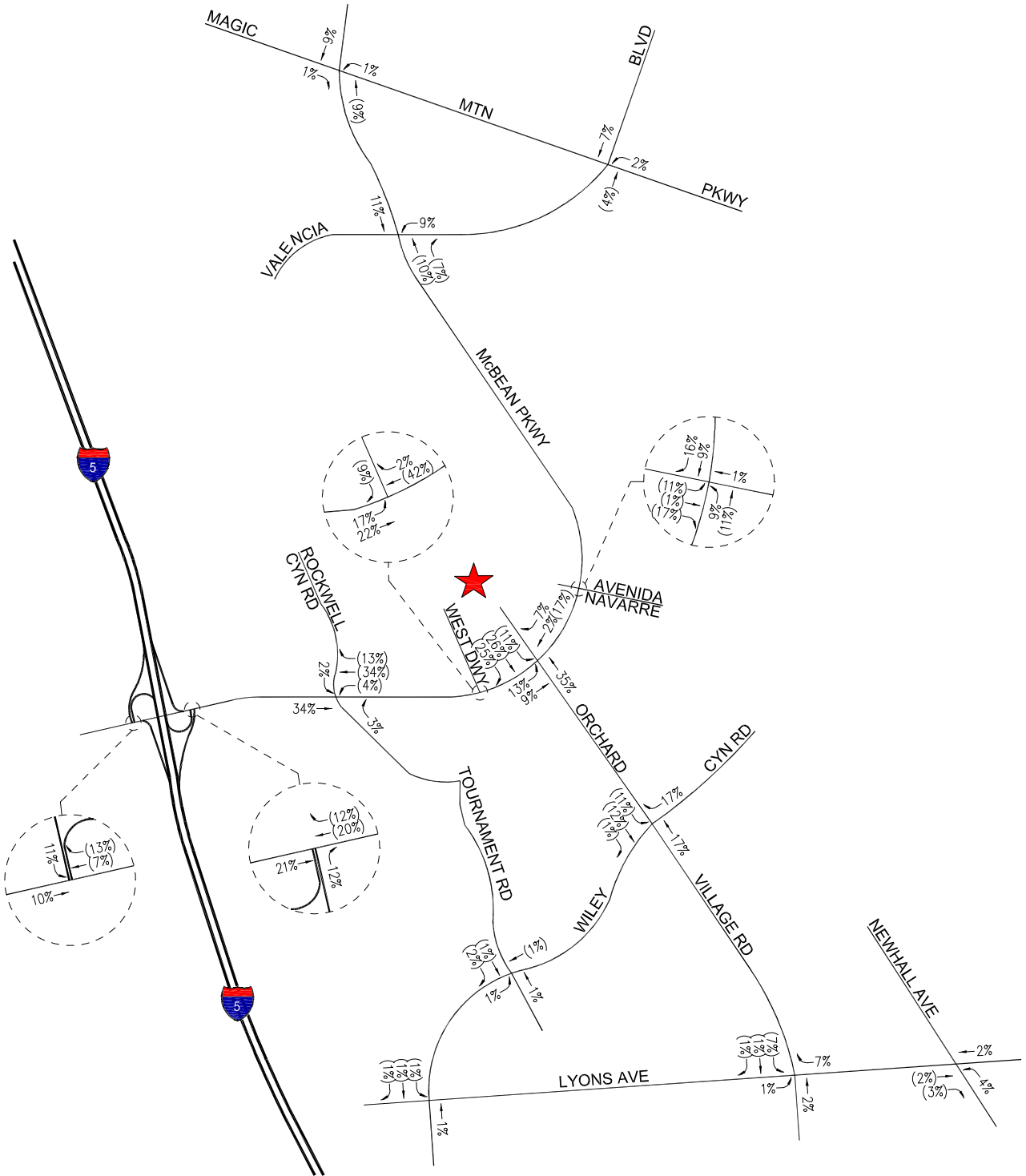
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**FIGURE 5**  
**EXISTING TRAFFIC VOLUMES**  
 WEEKDAY PM PEAK HOUR

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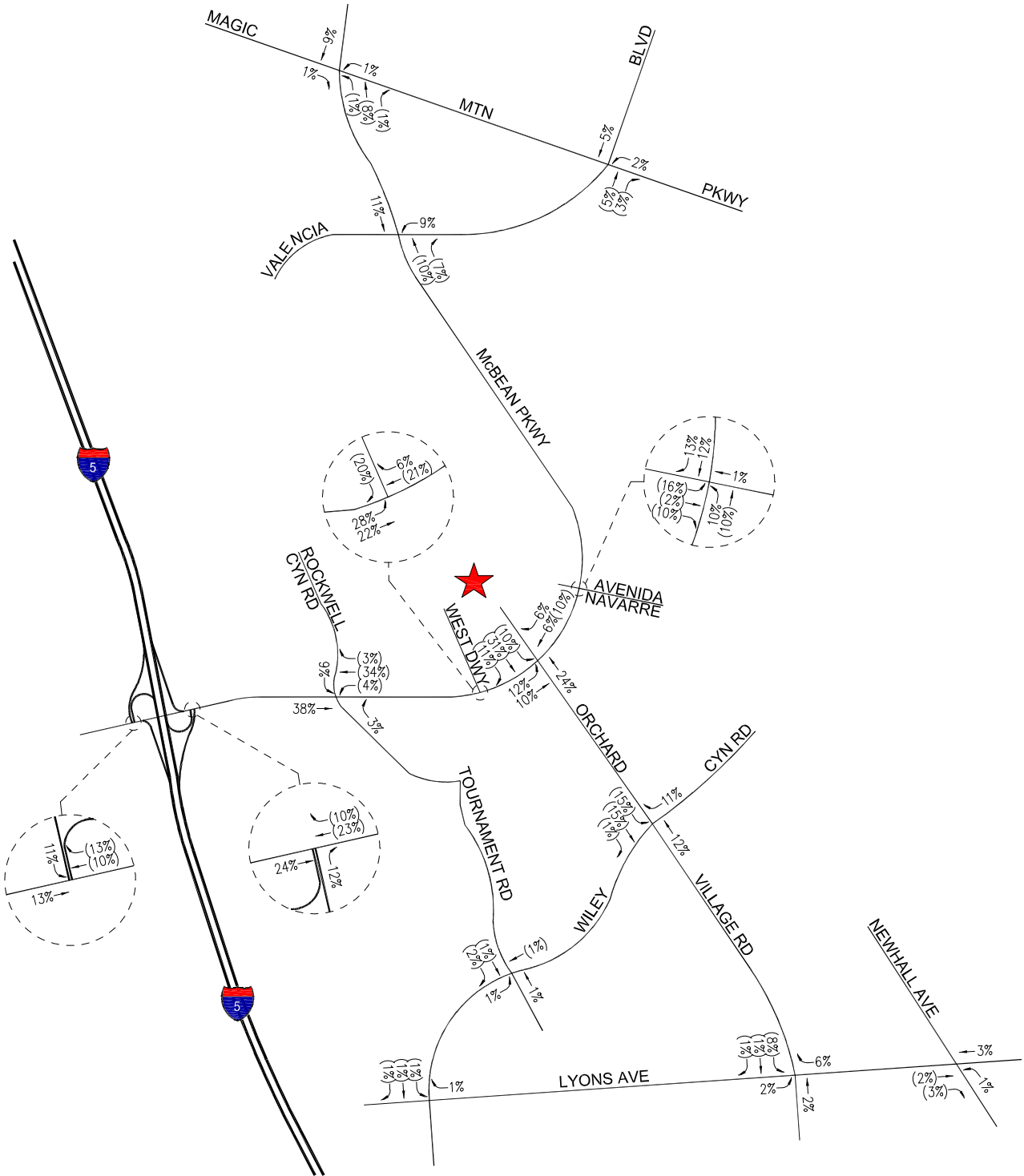
★ PROJECT SITE  
 ## = INBOUND PERCENTAGES  
 (##) = OUTBOUND PERCENTAGES

**FIGURE 6**  
**PROJECT TRIP DISTRIBUTION**  
 WEEKDAY AM PEAK HOUR

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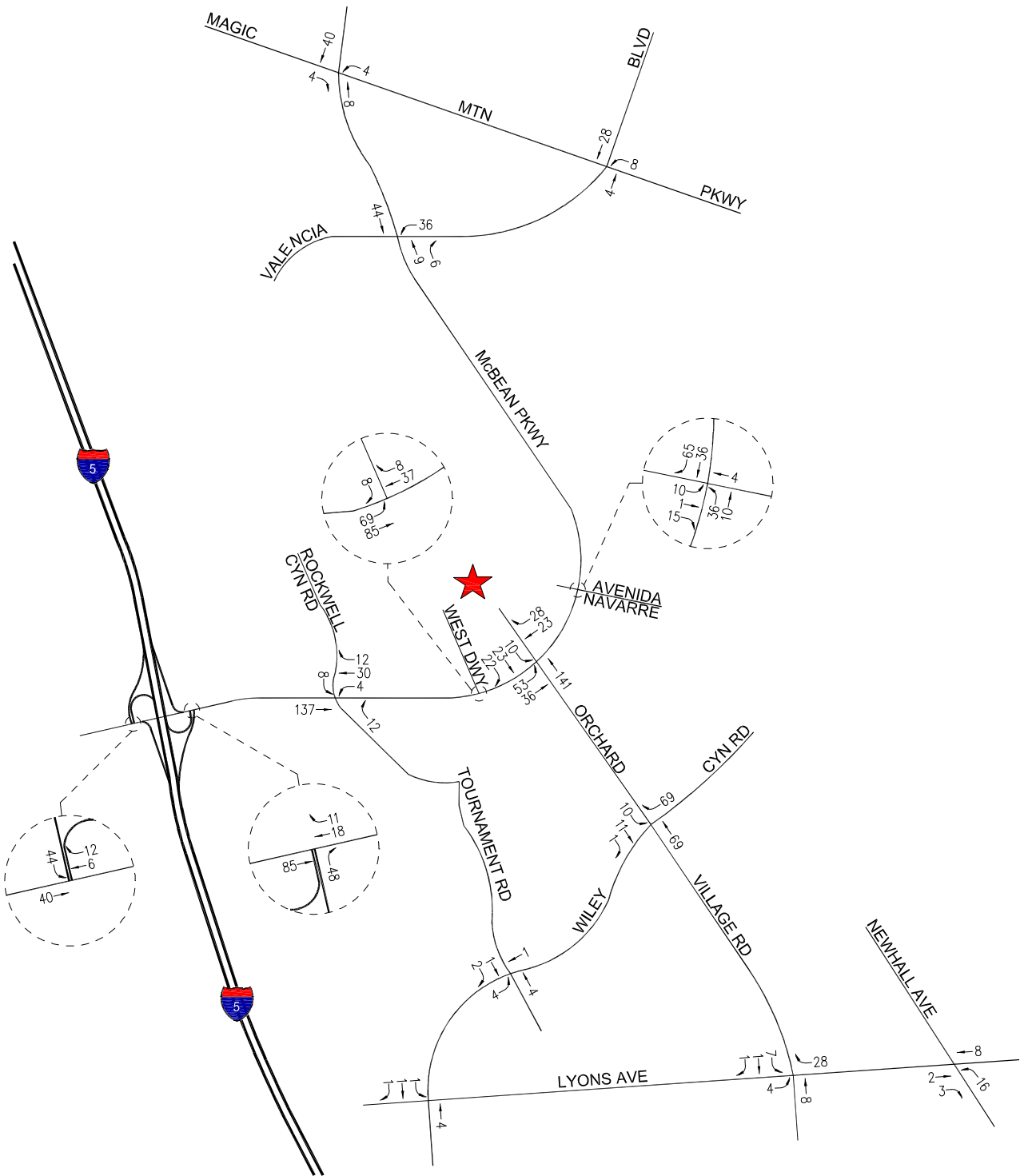
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**FIGURE 7**  
**PROJECT TRIP DISTRIBUTION**  
 WEEKDAY PM PEAK HOUR

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# FIGURE 8

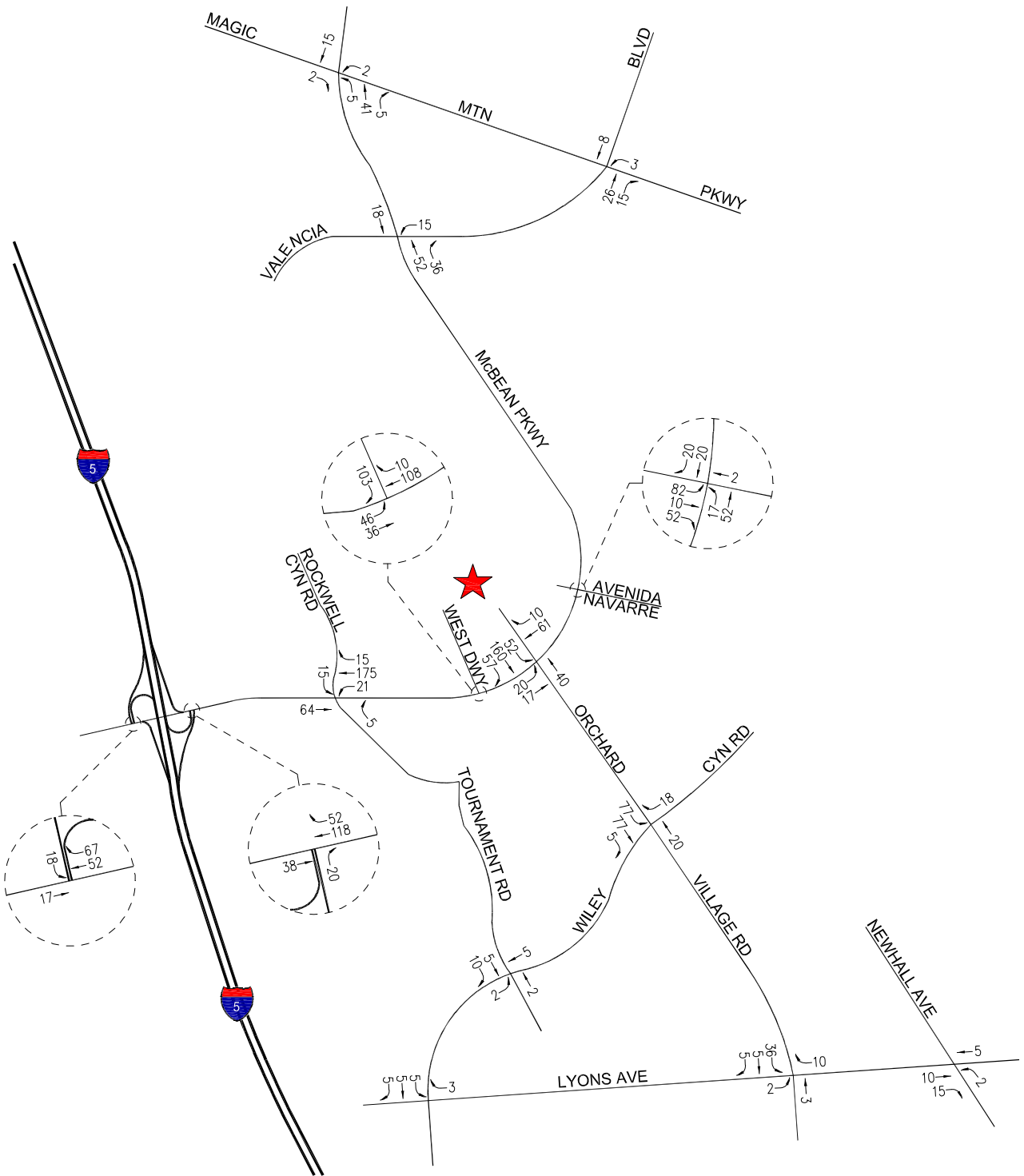
## NET REVISED PROJECT TRAFFIC VOLUMES

### WEEKDAY AM PEAK HOUR

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# NET REVISED PROJECT TRAFFIC VOLUMES

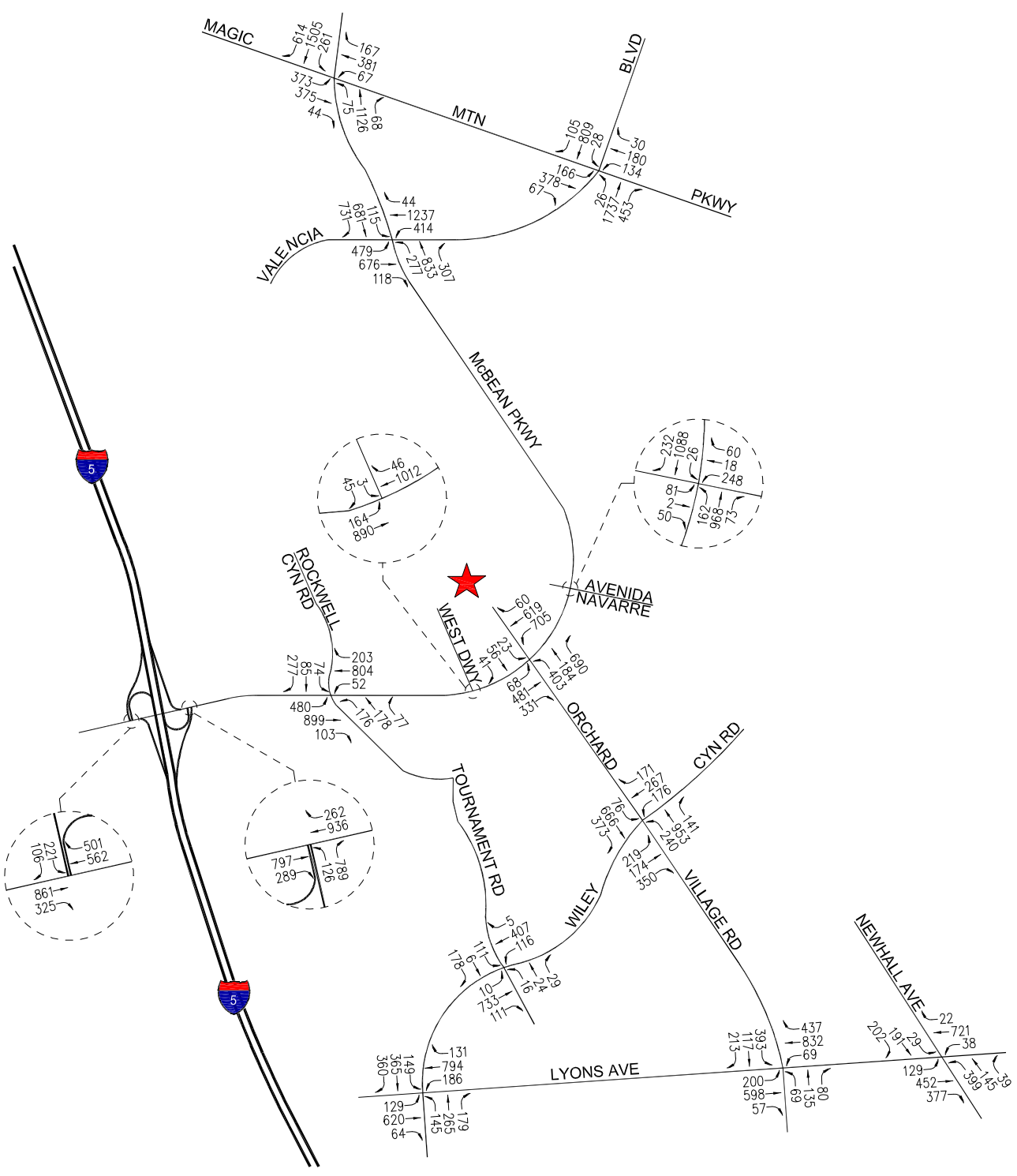
## WEEKDAY PM PEAK HOUR

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HENRY MAYO UPDATE

FIGURE 9

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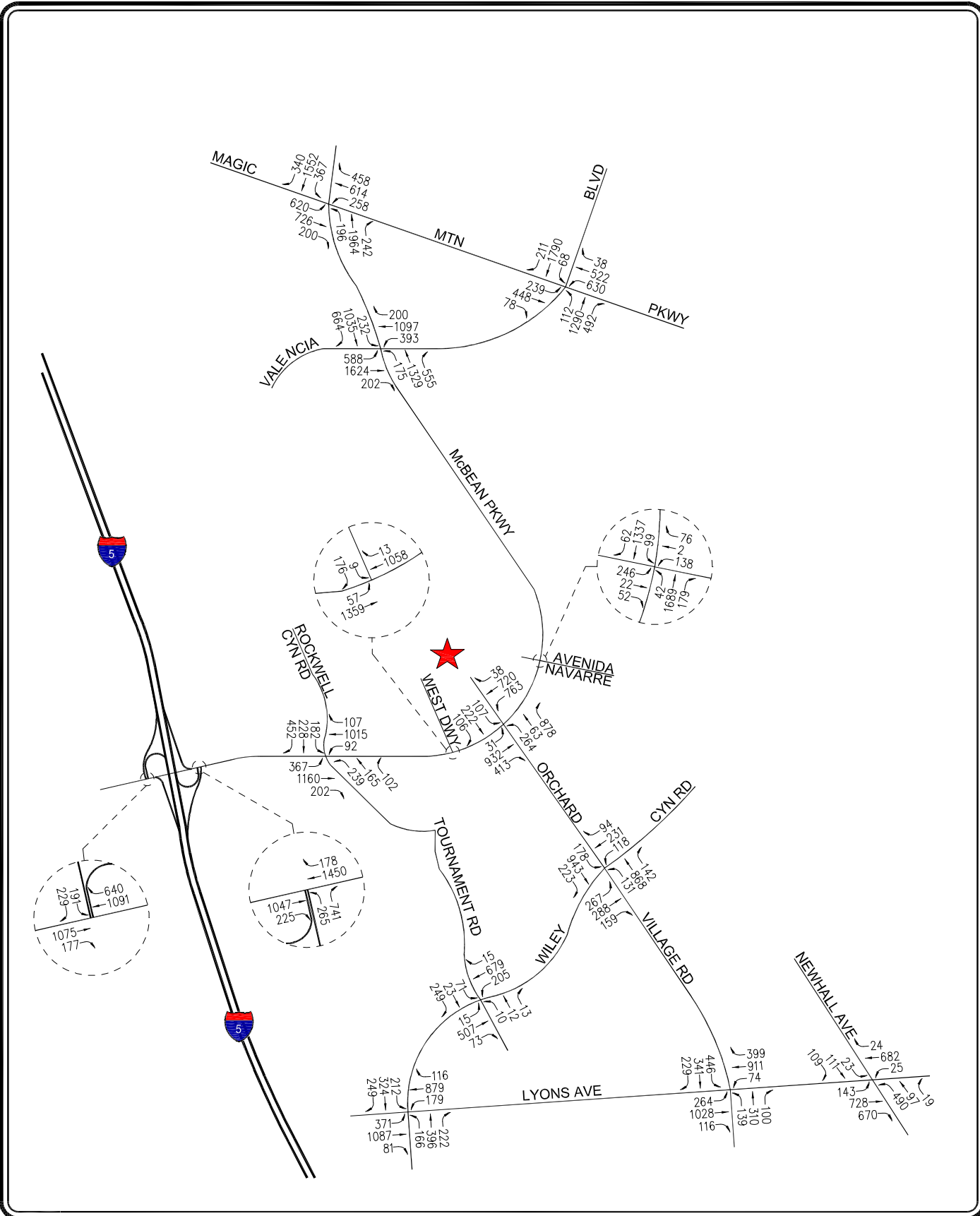


★ PROJECT SITE

**FIGURE 10**  
**EXISTING WITH REVISED**  
**PROJECT TRAFFIC VOLUMES**  
 WEEKDAY AM PEAK HOUR  
 HENRY MAYO UPDATE

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★ PROJECT SITE

**FIGURE 11**  
**EXISTING WITH REVISED**  
**PROJECT TRAFFIC VOLUMES**  
 WEEKDAY PM PEAK HOUR  
 HENRY MAYO UPDATE

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**Table 1  
TRAFFIC VOLUME COMPARISON**

09-Feb-16

NO.	INTERSECTION	EXISTING [1]				INTERIM W/O PROJECT [2]			AM PK. HR. COMPARISON		PM PK. HR. COMPARISON		
		DIR	AM PEAK HOUR		PM PEAK HOUR		DIR	AM PK. HR. VOLUME	PM PK. HR. VOLUME	Difference	Percent	Difference	Percent
			BEGAN	VOLUME	BEGAN	VOLUME							
1	I-5 SB Ramps / McBean Parkway	NB	7:30 AM	0	5:00 PM	0	NB	0	0	0	---	0	---
		SB		269		396	SB	503	993	(234)	-87%	(597)	-151%
		EB		1,134		1,230	EB	1,407	1,339	(273)	-24%	(109)	-9%
		WB		1,041		1,571	WB	1,562	1,562	(521)	-50%	9	1%
2	I-5 NB Ramps / McBean Parkway	NB	7:30 AM	850	5:00 PM	980	NB	737	1,331	113	13%	(351)	-36%
		SB		0		0	SB	0	0	0	---	0	---
		EB		973		1,222	EB	1,330	1,703	(357)	-37%	(481)	-39%
		WB		1,163		1,399	WB	1,431	1,208	(268)	-23%	191	14%
3	Tournament Road-Rockwell Canyon Road / McBean Parkway	NB	7:15 AM	415	4:45 PM	499	NB	778	416	(363)	-87%	83	17%
		SB		425		842	SB	213	846	212	50%	(4)	0%
		EB		1,300		1,644	EB	1,353	2,248	(53)	-4%	(604)	-37%
		WB		1,001		931	WB	1,353	981	(352)	-35%	(50)	-5%
4	McBean Parkway / Valencia Boulevard	NB	7:30 AM	1,399	4:45 PM	1,941	NB	1,815	2,104	(416)	-30%	(163)	-8%
		SB		1,469		1,907	SB	2,183	2,672	(714)	-49%	(765)	-40%
		EB		1,273		2,414	EB	1,260	2,660	13	1%	(246)	-10%
		WB		1,647		1,670	WB	2,250	1,786	(603)	-37%	(116)	-7%
5	McBean Parkway / Magic Mountain Parkway	NB	7:30 AM	1,259	5:00 PM	2,332	NB	1,842	2,711	(583)	-46%	(379)	-16%
		SB		2,326		2,238	SB	3,248	3,365	(922)	-40%	(1127)	-50%
		EB		787		1,543	EB	1,748	2,309	(961)	-122%	(766)	-50%
		WB		610		1,328	WB	1,269	1,329	(659)	-108%	(1)	0%
6	Wiley Canyon Road / Lyons Avenue	NB	7:30 AM	584	4:45 PM	784	NB	284	800	300	51%	(16)	-2%
		SB		871		764	SB	837	936	34	4%	(172)	-23%
		EB		813		1,539	EB	980	2,020	(167)	-21%	(481)	-31%
		WB		1,111		1,170	WB	1,300	1,407	(189)	-17%	(237)	-20%
7	Tournament Road / Wiley Canyon Road	NB	7:15 AM	64	5:00 PM	33	NB	28	99	36	56%	(66)	-200%
		SB		293		322	SB	237	564	56	19%	(242)	-75%
		EB		850		592	EB	343	927	507	60%	(335)	-57%
		WB		527		892	WB	810	689	(283)	-54%	203	23%



**Table 1 (Cont.)  
TRAFFIC VOLUME COMPARISON**

09-Feb-16

NO.	INTERSECTION	EXISTING [1]				INTERIM W/O PROJECT [2]			AM PK. HR. COMPARISON		PM PK. HR. COMPARISON		
		DIR	AM PEAK HOUR		PM PEAK HOUR		DIR	AM PK. HR. VOLUME	PM PK. HR. VOLUME	Difference	Percent	Difference	Percent
			BEGAN	VOLUME	BEGAN	VOLUME							
8	Orchard Village Road / Lyons Avenue	NB	7:30 AM	273	5:00 PM	545	NB	273	386	0	0%	159	29%
		SB		713		953	SB	591	881	122	17%	72	8%
		EB		850		1,405	EB	979	1,659	(129)	-15%	(254)	-18%
		WB		1,301		1,371	WB	1,858	1,830	(557)	-43%	(459)	-33%
9	Orchard Village Road / Wiley Canyon Road	NB	7:15 AM	1,243	5:00 PM	1,115	NB	978	1,361	265	21%	(246)	-22%
		SB		1,089		1,132	SB	1,109	1,264	(20)	-2%	(132)	-12%
		EB		744		714	EB	430	1,120	314	42%	(406)	-57%
		WB		523		419	WB	828	962	(305)	-58%	(543)	-129%
10	Orchard Village Road / McBean Parkway	NB	7:15 AM	1,090	5:00 PM	1,152	NB	1,400	1,422	(310)	-28%	(270)	-23%
		SB		53		74	SB	40	160	13	25%	(86)	-116%
		EB		762		1,328	EB	1,197	1,774	(435)	-57%	(446)	-34%
		WB		1,318		1,426	WB	1,222	1,560	96	7%	(134)	-9%
11	Newhall Avenue / Lyons Avenue	NB	7:30 AM	563	4:45 PM	603	NB	613	638	(50)	-9%	(35)	-6%
		SB		421		243	SB	270	280	151	36%	(37)	-15%
		EB		952		1,507	EB	1,085	1,706	(133)	-14%	(199)	-13%
		WB		770		723	WB	1,310	1,295	(541)	-70%	(572)	-79%
12	Valencia Boulevard / Magic Mountain Parkway	NB	7:30 AM	2,211	5:00 PM	1,839	NB	936	2,266	1275	58%	(427)	-23%
		SB		905		2,058	SB	2,730	1,632	(1825)	-202%	426	21%
		EB		611		765	EB	1,020	1,980	(409)	-67%	(1215)	-159%
		WB		333		1,187	WB	1,622	1,457	(1289)	-387%	(270)	-23%
13	Avenida Navarre / McBean Parkway	NB	7:15 AM	321	5:00 PM	213	NB	257	273	64	20%	(60)	-28%
		SB		102		126	SB	63	175	39	38%	(49)	-39%
		EB		1,143		1,818	EB	1,055	1,779	88	8%	39	2%
		WB		1,211		1,446	WB	1,273	1,595	(62)	-5%	(149)	-10%
<b>TOTAL</b>				42,746		54,899		52,967	66,865	(10221)	-24%	(11966)	-22%

[1] Traffic counts were conducted by National Data and Surveying Services on February 12, 2015.

Traffic counts were increased using a growth rate of 3.0% to derive Existing (i.e., 2016) traffic volume conditions.

Traffic counts removed estimated traffic associated with MOB 1 at Henry Mayo Hospital.

[2] Interim w/o Project traffic volumes from Henry Mayo Newhall Memorial Hospital Master Plan Traffic Impact Analysis, Austin-Foust Associates Inc. May 2008.

**Table 2**  
**PROJECT TRIP GENERATION [1,4]**

05-Feb-16

LAND USE	SIZE	DAILY TRIP ENDS [2] VOLUMES	AM PEAK HOUR VOLUMES [2]			PM PEAK HOUR VOLUMES [2]		
			IN	OUT	TOTAL	IN	OUT	TOTAL
<i>Revised Project</i>								
Medical Office Building 1 [3]	80,000 GSF	2,890	151	40	191	80	206	286
On-Site Volume [4]		(578)	(20)	(20)	(40)	(30)	(30)	(60)
Medical Office Building 2 [3]	60,000 GSF	2,168	113	30	143	60	154	214
On-Site Volume [4]		(433)	(15)	(15)	(30)	(22)	(22)	(44)
Hospital [5]	162,329 GSF	2,146	97	57	154	57	94	151
On-Site Volume [4]		(476)	(16)	(16)	(32)	(16)	(16)	(32)
Medical Office Building 3 [3]	60,000 GSF	2,168	113	30	143	60	154	214
Hospital Demolition [5]	(8,000) GSF	(106)	(5)	(3)	(8)	(3)	(4)	(7)
On-Site Volume [4]		(405)	(14)	(14)	(28)	(21)	(21)	(42)
<b>NET REVISED PROJECT TRIPS</b>		<b>7,374</b>	<b>404</b>	<b>89</b>	<b>493</b>	<b>165</b>	<b>515</b>	<b>680</b>
<i>Approved Project</i>								
Master Plan Buildout [4]		7,571	429	90	519	161	554	715
<b>DIFFERENCE BETWEEN REVISED AND APPROVED PROJECT</b>		<b>(197)</b>	<b>(25)</b>	<b>(1)</b>	<b>(26)</b>	<b>4</b>	<b>(39)</b>	<b>(35)</b>

[1] Source: ITE "Trip Generation", 9th Edition, 2012.

[2] Trips are one-way traffic movements, entering or leaving.

[3] ITE Land Use Code 720 (Medical-Dental Office Building) trip generation average rates.

- Daily Trip Rate: 36.13 trips/1,000 SF of floor area; 50% inbound/50% outbound

- AM Peak Hour Trip Rate: 2.39 trips/1,000 SF of floor area; assume 79% inbound/21% outbound

- PM Peak Hour Trip Rate: 3.57 trips/1,000 SF of floor area; 28% inbound/72% outbound

[4] Henry Mayo Newhall Memorial Hospital Master Plan Traffic Impact Analysis, Austin-Foust Associates Inc. May 2008. Table 3-2

[5] ITE Land Use Code 610 (Hospital) trip generation average rates.

- Daily Trip Rate: 13.22 trips/1,000 SF of floor area; 50% inbound/50% outbound

- AM Peak Hour Trip Rate: 0.95 trips/1,000 SF of floor area; assume 63% inbound/37% outbound

- PM Peak Hour Trip Rate: 0.93 trips/1,000 SF of floor area; 38% inbound/62% outbound

**Table 3  
SUMMARY AND COMPARISON OF VOLUME TO CAPACITY RATIOS  
AND LEVELS OF SERVICE  
WEEKDAY AM AND PM PEAK HOURS**

NO.	INTERSECTION	PEAK HOUR	[1]		[2]		[3]			[4]				
			EXISTING		EXISTING W/O MOB 1		CONDITION [2] W/ REVISED PROJECT		CHANGE V/C [(3)-(2)]	SIGNIF. IMPACT [b]	INTERIM YEAR W/O APPROVED PROJECT [c]		COMPARE DIFFERENCE IN V/C	
			V/C	LOS	V/C	LOS	V/C	LOS			V/C	LOS	[(2)-(4)]	%
1	I-5 SB Ramps / McBean Parkway	AM	0.436	A	0.424	A	0.473	A	0.049	NO	0.610	B	-0.186	-44%
		PM	0.533	A	0.531	A	0.542	A	0.011	NO	0.740	C	-0.209	-39%
2	I-5 NB Ramps / McBean Parkway	AM	0.534	A	0.529	A	0.554	A	0.025	NO	0.480	A	0.049	9%
		PM	0.594	A	0.589	A	0.622	B	0.033	NO	0.750	C	-0.161	-27%
3	Tournament Road-Rockwell Canyon Road / McBean Parkway	AM	0.707	C	0.706	C	0.715	C	0.009	NO	0.740	C	-0.034	-5%
		PM	0.754	C	0.741	C	0.790	C	0.049	NO	0.750	C	-0.009	-1%
4	McBean Parkway / Valencia Boulevard	AM	0.673	B	0.670	B	0.681	B	0.011	NO	0.870	D	-0.200	-30%
		PM	0.827	D	0.822	D	0.841	D	0.019	NO	0.860	D	-0.038	-5%
5	McBean Parkway / Magic Mountain Parkway	AM	0.478	A	0.477	A	0.480	A	0.003	NO	0.980	E	-0.503	-105%
		PM	0.760	C	0.758	C	0.767	C	0.009	NO	1.080	F	-0.322	-42%
6	Wiley Canyon Road / Lyons Avenue	AM	0.588	A	0.588	A	0.589	A	0.001	NO	0.670	B	-0.082	-14%
		PM	0.656	B	0.655	B	0.659	B	0.004	NO	0.830	D	-0.175	-27%
7	Tournament Road / Wiley Canyon Road	AM	0.516	A	0.516	A	0.516	A	0.000	NO	0.440	A	0.076	15%
		PM	0.521	A	0.519	A	0.527	A	0.008	NO	0.720	C	-0.201	-39%

**Table 3 (Cont.)  
SUMMARY AND COMPARISON OF VOLUME TO CAPACITY RATIOS  
AND LEVELS OF SERVICE  
WEEKDAY AM AND PM PEAK HOURS**

NO.	INTERSECTION	PEAK HOUR	[1]		[2]		[3]			[4]				
			EXISTING		EXISTING W/O MOB 1		CONDITION [2] W/ REVISED PROJECT		CHANGE V/C [(3)-(2)]	SIGNIF. IMPACT [b]	INTERIM YEAR W/O APPROVED PROJECT [c]		COMPARE DIFFERENCE IN V/C	
			V/C	LOS	V/C	LOS	V/C	LOS			V/C	LOS	V/C	LOS
8	Orchard Village Road / Lyons Avenue	AM	0.461	A	0.459	A	0.466	A	0.007	NO	0.570	A	-0.111	-24%
		PM	0.707	C	0.706	C	0.710	C	0.004	NO	0.700	B	0.006	1%
9	Orchard Village Road / Wiley Canyon Road	AM	0.675	B	0.674	B	0.678	B	0.004	NO	0.710	C	-0.036	-5%
		PM	0.618	B	0.610	B	0.645	B	0.035	NO	1.050	F	-0.440	-72%
10	Orchard Village Road / McBean Parkway	AM	0.595	A	0.578	A	0.656	B	0.078	NO	0.700	B	-0.122	-21%
		PM	0.688	B	0.669	B	0.794	C	0.125	NO	0.870	D	-0.201	-30%
11	Newhall Avenue / Lyons Avenue	AM	0.596	A	0.593	A	0.602	B	0.009	NO	0.690	B	-0.097	-16%
		PM	0.578	A	0.577	A	0.580	A	0.003	NO	0.690	B	-0.113	-20%
12	Valencia Boulevard / Magic Mountain Parkway	AM	0.731	C	0.729	C	0.737	C	0.008	NO	1.130	F	-0.401	-55%
		PM	1.012	F	1.011	F	1.016	F	0.005	NO	1.150	F	-0.139	-14%
13	Avenida Navarre / McBean Parkway	AM	0.546	A	0.533	A	0.587	A	0.054	NO	0.490	A	0.043	8%
		PM	0.596	A	0.583	A	0.654	B	0.071	NO	0.570	A	0.013	2%

[a] Existing traffic counts modified to removed estimated traffic associated with MOB 1 at Henry Mayo Hospital.

[b] City of Santa Clarita signalized intersection impact threshold criteria is as follows:

Final v/c	LOS	Project Related Increase in v/c
> 0.81 - 0.90	D	greater than or equal to 0.02 or degrades from LOS E or F
> 0.91	E or F	greater than or equal to 0.01

[c] Interim w/o Project traffic volumes (Year 2014) from Henry Mayo Newhall Memorial Hospital Master Plan Traffic Impact Analysis, Austin-Foust Associates Inc. May 2008.

**APPENDIX A**  
**MANUAL TRAFFIC COUNT DATA**

# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-001

Day: Thursday

City: Valencia

Date: 2/12/2015

AM													
NS/EW Streets:	I-5 SB Ramps			I-5 SB Ramps			McBean Pkwy			McBean Pkwy			TOTAL
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
6:30 AM	0	0	0	14	0	10	0	58	81	0	37	136	336
6:45 AM	0	0	0	39	0	18	0	98	83	0	67	128	433
7:00 AM	0	0	0	23	0	21	0	84	60	0	109	153	450
7:15 AM	0	0	0	59	0	18	0	144	30	0	94	118	463
7:30 AM	0	0	0	40	0	21	1	208	95	0	127	135	627
7:45 AM	0	0	0	35	0	32	0	232	52	0	143	131	625
8:00 AM	0	0	0	45	0	25	0	190	76	0	167	114	617
8:15 AM	0	0	0	52	0	25	0	167	93	0	103	95	535
<b>TOTAL VOLUMES :</b>	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
<b>APPROACH %'s :</b>	#DIV/0!	#DIV/0!	#DIV/0!	64.36%	0.00%	35.64%	0.06%	67.41%	32.53%	0.00%	45.61%	54.39%	4086
<b>PEAK HR START TIME :</b>	730 AM												<b>TOTAL</b>
<b>PEAK HR VOL :</b>	0	0	0	172	0	103	1	797	316	0	540	475	2404
<b>PEAK HR FACTOR :</b>	0.000			0.893			0.916			0.903			0.959

UTURNS			
NB	SB	EB	WB
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	1	0
0	0	0	0
0	0	0	0
0	1	0	0
NB	SB	EB	WB
0	1	1	0

CONTROL : Signalized



# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-002

Day: Thursday

City: Valencia

Date: 2/12/2015

AM													
NS/EW Streets:	I-5 NB Ramps			I-5 NB Ramps			McBean Pkwy			McBean Pkwy			TOTAL
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
6:30 AM	7	0	71	0	0	0	0	59	15	0	161	31	344
6:45 AM	8	0	80	0	0	0	0	102	31	0	193	37	451
7:00 AM	24	0	86	0	0	0	0	79	32	0	233	31	485
7:15 AM	18	0	133	0	0	0	0	157	41	0	202	40	591
7:30 AM	24	0	214	0	0	0	0	191	64	0	230	48	771
7:45 AM	33	0	219	0	0	0	0	194	65	0	247	85	843
8:00 AM	31	0	145	0	0	0	0	142	103	0	245	67	733
8:15 AM	34	0	141	0	0	0	0	164	49	0	169	44	601
<b>TOTAL VOLUMES :</b>	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
<b>APPROACH %'s :</b>	179	0	1089	0	0	0	0	1088	400	0	1680	383	4819
	14.12%	0.00%	85.88%	#DIV/0!	#DIV/0!	#DIV/0!	0.00%	73.12%	26.88%	0.00%	81.43%	18.57%	
<b>PEAK HR START TIME :</b>	730 AM												<b>TOTAL</b>
<b>PEAK HR VOL :</b>	122	0	719	0	0	0	0	691	281	0	891	244	2948
<b>PEAK HR FACTOR :</b>	0.834			0.000			0.938			0.855			0.874

UTURNS			
NB	SB	EB	WB
0	0	0	0

NB	SB	EB	WB
0	0	0	0

CONTROL : Signalized



# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-002

Day: Thursday

City: Valencia

Date: 2/12/2015

PM

NS/EW Streets:	I-5 NB Ramps		I-5 NB Ramps			McBean Pkwy			McBean Pkwy			TOTAL	
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
	2	0	2	0	0	0	0	2	1	0	3	0	
4:00 PM	41	0	161	0	0	0	0	249	43	0	261	33	788
4:15 PM	35	0	152	0	0	0	0	225	50	0	259	32	753
4:30 PM	52	0	170	0	0	0	0	224	48	0	245	28	767
4:45 PM	63	0	195	0	0	0	0	209	59	0	302	32	860
5:00 PM	62	0	195	0	0	0	0	240	58	0	369	44	968
5:15 PM	72	0	169	0	0	0	0	240	49	0	328	25	883
5:30 PM	48	0	168	0	0	0	0	249	47	0	303	33	848
5:45 PM	75	0	168	0	0	0	0	251	64	0	293	20	871
<b>TOTAL VOLUMES :</b>	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
<b>APPROACH %'s :</b>	448	0	1378	0	0	0	0	1887	418	0	2360	247	6738
	24.53%	0.00%	75.47%	#DIV/0!	#DIV/0!	#DIV/0!	0.00%	81.87%	18.13%	0.00%	90.53%	9.47%	
<b>PEAK HR START TIME :</b>	500 PM												<b>TOTAL</b>
<b>PEAK HR VOL :</b>	257	0	700	0	0	0	0	980	218	0	1293	122	3570
<b>PEAK HR FACTOR :</b>	0.931			0.000			0.951			0.857			0.922

UTURNS			
NB	SB	EB	WB

NB	SB	EB	WB
0	0	0	0

CONTROL : Signalized

# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-003

Day: Thursday

City: Valencia

Date: 2/12/2015

AM

NS/EW Streets:	Rockwell Canyon Rd			Rockwell Canyon Rd			McBean Pkwy			McBean Pkwy			TOTAL
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL 1	NT 2	NR 0	SL 1	ST 2	SR 1	EL 1	ET 3	ER 0	WL 1	WT 3	WR 0	
6:30 AM	21	11	8	2	6	44	31	93	11	4	129	10	370
6:45 AM	25	13	7	5	9	80	35	135	7	8	121	8	453
7:00 AM	35	21	11	5	6	55	36	115	9	8	181	11	493
7:15 AM	42	25	14	7	10	50	85	163	15	8	158	28	605
7:30 AM	36	52	24	22	19	62	167	184	24	19	183	63	855
7:45 AM	54	65	17	25	29	79	146	207	39	14	199	66	940
8:00 AM	39	31	8	10	25	78	68	186	22	6	211	28	712
8:15 AM	42	23	9	9	15	56	64	183	30	7	134	21	593
<b>TOTAL VOLUMES :</b>	NL 294	NT 241	NR 98	SL 85	ST 119	SR 504	EL 632	ET 1266	ER 157	WL 74	WT 1316	WR 235	TOTAL 5021
<b>APPROACH %'s :</b>	46.45%	38.07%	15.48%	12.01%	16.81%	71.19%	30.75%	61.61%	7.64%	4.55%	80.98%	14.46%	
<b>PEAK HR START TIME :</b>	7:15 AM												<b>TOTAL</b>
<b>PEAK HR VOL :</b>	171	173	63	64	83	269	466	740	100	47	751	185	3112
<b>PEAK HR FACTOR :</b>	0.748			0.782			0.833			0.881			0.828

UTURNS			
NB	SB	EB	WB

NB 0	SB 0	EB 0	WB 0
---------	---------	---------	---------

CONTROL : Signalized

# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-003

Day: Thursday

City: Valencia

Date: 2/12/2015

PM

NS/EW Streets:	Rockwell Canyon Rd			Rockwell Canyon Rd			McBean Pkwy			McBean Pkwy			TOTAL
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL 1	NT 2	NR 0	SL 1	ST 2	SR 1	EL 1	ET 3	ER 0	WL 1	WT 3	WR 0	
4:00 PM	70	41	18	22	31	64	77	269	43	13	169	24	841
4:15 PM	54	34	26	20	24	68	81	240	46	25	173	18	809
4:30 PM	50	53	21	22	37	91	83	228	49	19	158	42	853
4:45 PM	49	49	16	38	55	121	99	264	37	19	183	28	958
5:00 PM	79	42	25	57	68	134	96	250	49	10	212	21	1043
5:15 PM	57	31	17	38	49	93	73	266	47	17	216	17	921
5:30 PM	47	38	36	29	49	91	88	284	63	23	205	23	976
5:45 PM	48	38	27	19	41	54	74	253	50	22	210	26	862
<b>TOTAL VOLUMES :</b>	NL 454	NT 326	NR 186	SL 245	ST 354	SR 716	EL 671	ET 2054	ER 384	WL 148	WT 1526	WR 199	TOTAL 7263
<b>APPROACH %'s :</b>	47.00%	33.75%	19.25%	18.63%	26.92%	54.45%	21.58%	66.07%	12.35%	7.90%	81.47%	10.62%	
<b>PEAK HR START TIME :</b>	445 PM												<b>TOTAL</b>
<b>PEAK HR VOL :</b>	232	160	94	162	221	439	356	1064	196	69	816	89	3898
<b>PEAK HR FACTOR :</b>	0.832			0.793			0.929			0.970			0.934

UTURNS			
NB	SB	EB	WB
0	0	0	0

NB 0	SB 0	EB 0	WB 0
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CONTROL : Signalized

# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-004

Day: Thursday

City: Valencia

Date: 2/12/2015

AM

NS/EW Streets:	McBean Pkwy		McBean Pkwy			Valencia Blvd			Valencia Blvd			TOTAL	
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL 2	NT 3	NR 2	SL 2	ST 3	SR 2	EL 2	ET 3	ER 1	WL 2	WT 3	WR 1	
6:30 AM	41	71	33	5	61	90	55	55	15	52	162	3	643
6:45 AM	60	106	27	7	90	126	76	78	20	89	222	7	908
7:00 AM	43	107	45	9	113	97	69	97	19	78	175	8	860
7:15 AM	46	160	28	7	169	158	98	117	21	85	324	9	1222
7:30 AM	60	187	49	15	187	220	96	158	24	81	366	7	1450
7:45 AM	75	214	66	34	137	189	124	165	42	99	344	11	1500
8:00 AM	75	231	94	33	153	169	127	183	30	87	251	16	1449
8:15 AM	59	168	83	30	141	132	118	150	19	100	240	9	1249
<b>TOTAL VOLUMES :</b>	NL 459	NT 1244	NR 425	SL 140	ST 1051	SR 1181	EL 763	ET 1003	ER 190	WL 671	WT 2084	WR 70	TOTAL 9281
<b>APPROACH %'s :</b>	21.57%	58.46%	19.97%	5.90%	44.31%	49.79%	39.01%	51.28%	9.71%	23.75%	73.77%	2.48%	
<b>PEAK HR START TIME :</b>	730 AM												
<b>PEAK HR VOL :</b>	269	800	292	112	618	710	465	656	115	367	1201	43	5648
<b>PEAK HR FACTOR :</b>	0.851		0.853			0.909			0.887			0.941	

UTURNS			
NB	SB	EB	WB
0	0	0	0
0	0	0	0
0	0	0	0
1	0	0	0
0	0	0	0
0	0	0	0
1	0	0	0
0	0	0	0
0	0	0	0
2	0	0	0

CONTROL : Signalized

# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

**Project ID:** 15-5082-004

**Day:** Thursday

**City:** Valencia

**Date:** 2/12/2015

**PM**

NS/EW Streets:	McBean Pkwy		McBean Pkwy			Valencia Blvd			Valencia Blvd			TOTAL	
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT		WR
LANES:	2	3	2	2	3	2	2	3	1	2	3	1	
4:00 PM	54	222	134	49	196	93	110	296	39	117	190	43	1543
4:15 PM	33	268	129	45	239	142	135	267	27	92	230	42	1649
4:30 PM	52	262	97	50	225	156	137	264	38	89	241	46	1657
4:45 PM	57	302	84	59	269	184	152	351	53	93	244	44	1892
5:00 PM	43	343	159	49	232	154	140	404	21	95	276	48	1964
5:15 PM	43	317	104	59	255	178	138	428	58	88	266	49	1983
5:30 PM	27	278	157	58	231	129	141	394	64	91	279	53	1902
5:45 PM	45	260	117	63	239	149	137	316	50	99	245	55	1775
<b>TOTAL VOLUMES :</b>	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
<b>APPROACH %'s :</b>	354	2252	981	432	1886	1185	1090	2720	350	764	1971	380	14365
	9.87%	62.78%	27.35%	12.33%	53.84%	33.83%	26.20%	65.38%	8.41%	24.53%	63.27%	12.20%	
<b>PEAK HR START TIME :</b>	445 PM												<b>TOTAL</b>
<b>PEAK HR VOL :</b>	170	1240	504	225	987	645	571	1577	196	367	1065	194	7741
<b>PEAK HR FACTOR :</b>	0.878			0.907			0.939			0.961			0.976

UTURNS			
NB	SB	EB	WB
0	0	1	0
0	0	0	0
0	0	0	0
0	0	2	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
NB	SB	EB	WB
0	0	3	0

**CONTROL :** Signalized

# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-005

Day: Thursday

City: Valencia

Date: 2/12/2015

AM													
NS/EW Streets:	McBean Pkwy			McBean Pkwy			Magic Mountain			Magic Mountain			
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
6:30 AM	14	99	7	17	154	117	57	46	7	5	59	31	613
6:45 AM	8	154	10	25	237	150	53	67	4	8	44	29	789
7:00 AM	7	146	7	24	216	119	54	62	7	7	84	18	751
7:15 AM	11	195	11	40	316	137	72	74	10	4	97	27	994
7:30 AM	11	274	15	41	407	117	79	93	10	17	90	30	1184
7:45 AM	20	327	15	78	390	174	91	96	10	16	100	46	1363
8:00 AM	22	271	18	72	333	170	96	98	12	12	85	46	1235
8:15 AM	20	213	18	62	292	135	96	77	7	16	95	40	1071
<b>TOTAL VOLUMES :</b>	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
<b>APPROACH %'s :</b>	5.97%	88.70%	5.34%	9.39%	61.34%	29.27%	46.79%	47.97%	5.24%	8.45%	65.01%	26.54%	8000
<b>PEAK HR START TIME :</b>	730 AM												<b>TOTAL</b>
<b>PEAK HR VOL :</b>	73	1085	66	253	1422	596	362	364	39	61	370	162	4853
<b>PEAK HR FACTOR :</b>	0.845			0.884			0.928			0.915			0.890

UTURNS			
NB	SB	EB	WB
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	1
0	0	0	1
0	0	0	2
NB	SB	EB	WB
0	0	0	4

CONTROL : Signalized

# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-005

Day: Thursday

City: Valencia

Date: 2/12/2015

PM

NS/EW Streets:	McBean Pkwy		McBean Pkwy			Magic Mountain			Magic Mountain			TOTAL	
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL 2	NT 4	NR 1	SL 2	ST 4	SR 1	EL 2	ET 2	ER 1	WL 2	WT 3	WR 1	
4:00 PM	38	390	50	86	306	78	130	140	23	53	127	95	1516
4:15 PM	45	369	53	76	320	80	141	157	37	53	116	72	1519
4:30 PM	27	395	48	79	401	92	158	177	40	58	118	79	1672
4:45 PM	40	393	42	75	327	82	177	205	42	69	124	79	1655
5:00 PM	40	411	76	85	362	77	176	202	38	70	153	95	1785
5:15 PM	32	516	58	85	379	75	122	202	57	68	141	123	1858
5:30 PM	49	475	51	86	379	81	148	157	55	62	142	111	1796
5:45 PM	64	465	45	100	372	97	156	144	42	49	160	116	1810
<b>TOTAL VOLUMES :</b>	NL 335	NT 3414	NR 423	SL 672	ST 2846	SR 662	EL 1208	ET 1384	ER 334	WL 482	WT 1081	WR 770	TOTAL 13611
<b>APPROACH %'s :</b>	8.03%	81.83%	10.14%	16.08%	68.09%	15.84%	41.29%	47.30%	11.41%	20.66%	46.34%	33.00%	
<b>PEAK HR START TIME :</b>	500 PM												
<b>PEAK HR VOL :</b>	185	1867	230	356	1492	330	602	705	192	249	596	445	7249
<b>PEAK HR FACTOR :</b>	0.941			0.957			0.901			0.971			0.975

UTURNS			
NB	SB	EB	WB
0	0	0	2
0	0	0	5
0	0	0	6
0	0	0	1
1	0	0	1
0	0	1	1
0	0	0	2
0	0	0	3
NB 1	SB 0	EB 1	WB 21

CONTROL : Signalized

# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-006

Day: Thursday

City: Valencia

Date: 2/12/2015

AM

NS/EW Streets:	Wiley Canyon Rd		Wiley Canyon Rd			Lyons Ave			Lyons Ave			TOTAL	
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL 1	NT 2	NR 1	SL 1	ST 2	SR 1	EL 2	ET 3	ER 0	WL 1	WT 3	WR 0	
6:30 AM	16	15	16	17	62	67	17	55	13	13	122	6	419
6:45 AM	19	20	32	19	83	62	14	85	13	22	135	11	515
7:00 AM	13	19	14	22	81	75	24	86	15	32	120	9	510
7:15 AM	30	39	28	25	89	89	37	84	19	32	124	13	609
7:30 AM	34	72	59	25	103	97	25	171	11	34	173	28	832
7:45 AM	28	79	43	52	102	113	41	181	25	48	214	31	957
8:00 AM	44	47	39	33	84	84	34	118	15	63	217	35	813
8:15 AM	35	55	33	34	64	55	25	132	11	36	167	33	680
<b>TOTAL VOLUMES :</b>	NL 219	NT 346	NR 264	SL 227	ST 668	SR 642	EL 217	ET 912	ER 122	WL 280	WT 1272	WR 166	TOTAL 5335
<b>APPROACH %'s :</b>	26.42%	41.74%	31.85%	14.77%	43.46%	41.77%	17.35%	72.90%	9.75%	16.30%	74.04%	9.66%	
<b>PEAK HR START TIME :</b>	730 AM												<b>TOTAL</b>
<b>PEAK HR VOL :</b>	141	253	174	144	353	349	125	602	62	181	771	127	3282
<b>PEAK HR FACTOR :</b>	0.861			0.792			0.799			0.856			0.857

UTURNS			
NB	SB	EB	WB
0	3	0	0
0	2	0	0
0	5	0	0
0	7	0	1
0	2	0	0
0	5	0	0
0	3	1	0
0	8	0	1
NB 0	SB 35	EB 1	WB 2

CONTROL : Signalized



# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-006

Day: Thursday

City: Valencia

Date: 2/12/2015

PM

NS/EW Streets:	Wiley Canyon Rd		Wiley Canyon Rd			Lyons Ave			Lyons Ave			TOTAL	
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL 1	NT 2	NR 1	SL 1	ST 2	SR 1	EL 2	ET 3	ER 0	WL 1	WT 3	WR 0	
4:00 PM	33	84	48	43	74	68	73	218	19	40	176	22	898
4:15 PM	35	77	50	25	61	69	73	206	19	38	148	33	834
4:30 PM	38	69	46	38	67	57	73	248	26	45	191	34	932
4:45 PM	36	72	47	48	65	60	80	262	17	37	229	38	991
5:00 PM	38	95	60	55	70	46	101	250	17	48	219	27	1026
5:15 PM	42	103	50	50	72	72	84	289	22	46	212	26	1068
5:30 PM	45	114	59	48	103	59	95	254	23	43	193	19	1055
5:45 PM	40	89	63	45	80	63	74	245	23	36	204	26	988
<b>TOTAL VOLUMES :</b>	NL 307	NT 703	NR 423	SL 352	ST 592	SR 494	EL 653	ET 1972	ER 166	WL 333	WT 1572	WR 225	TOTAL 7792
<b>APPROACH %'s :</b>	21.42%	49.06%	29.52%	24.48%	41.17%	34.35%	23.40%	70.66%	5.95%	15.63%	73.80%	10.56%	
<b>PEAK HR START TIME :</b>	445 PM												<b>TOTAL</b>
<b>PEAK HR VOL :</b>	161	384	216	201	310	237	360	1055	79	174	853	110	4140
<b>PEAK HR FACTOR :</b>	0.873			0.890			0.946			0.935			0.969

UTURNS			
NB	SB	EB	WB
0	2	0	1
0	5	0	0
0	3	0	0
0	5	0	0
0	3	0	1
0	4	0	0
0	6	0	0
0	5	0	0
NB 0	SB 33	EB 0	WB 2

CONTROL : Signalized

# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-007

Day: Thursday

City: Valencia

Date: 2/12/2015

**AM**

NS/EW Streets:	Wiley Canyon Rd		Wiley Canyon Rd			Tournament Rd			Tournament Rd			TOTAL	
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
6:30 AM	8	33	1	1	95	7	10	0	27	5	3	3	193
6:45 AM	15	27	1	0	127	9	14	1	36	4	1	3	238
7:00 AM	14	44	0	4	126	6	9	1	34	2	2	0	242
7:15 AM	13	81	1	1	179	19	12	1	25	3	1	3	339
7:30 AM	38	100	1	2	174	32	38	2	50	8	7	10	462
7:45 AM	35	131	0	2	209	30	42	1	54	4	7	10	525
8:00 AM	27	82	3	1	150	27	16	1	42	1	4	5	359
8:15 AM	28	94	3	1	96	4	9	1	29	5	2	3	275
<b>TOTAL VOLUMES :</b>	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
<b>APPROACH %'s :</b>	178	592	10	12	1156	134	150	8	297	32	27	37	2633
	22.82%	75.90%	1.28%	0.92%	88.79%	10.29%	32.97%	1.76%	65.27%	33.33%	28.13%	38.54%	
<b>PEAK HR START TIME :</b>	7:15 AM												<b>TOTAL</b>
<b>PEAK HR VOL :</b>	113	394	5	6	712	108	108	5	171	16	19	28	1685
<b>PEAK HR FACTOR :</b>	0.771			0.857			0.732			0.630			0.802

UTURNS			
NB	SB	EB	WB
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	1	0	0
0	0	0	0
0	0	0	0
0	0	0	0
NB	SB	EB	WB
0	1	0	0

CONTROL : Signalized



# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-008

Day: Thursday

City: Valencia

Date: 2/12/2015

AM

NS/EW Streets:	Orchard Village Rd		Orchard Village Rd			Lyons Ave			Lyons Ave			TOTAL	
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL 1	NT 2	NR 1	SL 2	ST 1	SR 1	EL 2	ET 2	ER 1	WL 1	WT 3	WR 1	
6:30 AM	13	12	6	37	3	15	23	79	6	2	103	17	316
6:45 AM	9	14	18	118	6	18	26	150	5	8	147	49	568
7:00 AM	12	14	11	47	8	24	19	88	6	4	124	33	390
7:15 AM	16	17	12	62	11	31	30	126	11	9	134	77	536
7:30 AM	10	39	19	117	27	52	62	176	21	13	173	107	816
7:45 AM	24	38	27	92	25	62	57	156	17	15	213	141	867
8:00 AM	14	20	18	98	40	60	48	136	11	22	258	93	818
8:15 AM	19	26	14	68	21	32	23	113	6	17	164	56	559
<b>TOTAL VOLUMES :</b>	NL 117	NT 180	NR 125	SL 639	ST 141	SR 294	EL 288	ET 1024	ER 83	WL 90	WT 1316	WR 573	TOTAL 4870
<b>APPROACH %'s :</b>	27.73%	42.65%	29.62%	59.50%	13.13%	27.37%	20.65%	73.41%	5.95%	4.55%	66.50%	28.95%	
<b>PEAK HR START TIME :</b>	730 AM												<b>TOTAL</b>
<b>PEAK HR VOL :</b>	67	123	78	375	113	206	190	581	55	67	808	397	3060
<b>PEAK HR FACTOR :</b>	0.753			0.876			0.797			0.853			0.882

UTURNS			
NB	SB	EB	WB
1	0	0	0
0	0	0	0
0	0	0	1
1	0	0	3
0	0	1	1
0	0	1	3
0	0	0	1
0	0	0	1
2	0	2	10

CONTROL : Signalized

# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-008

Day: Thursday

City: Valencia

Date: 2/12/2015

PM

NS/EW Streets:	Orchard Village Rd		Orchard Village Rd			Lyons Ave			Lyons Ave			TOTAL	
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL 1	NT 2	NR 1	SL 2	ST 1	SR 1	EL 2	ET 2	ER 1	WL 1	WT 3	WR 1	
4:00 PM	25	70	25	75	79	44	76	219	21	27	193	71	925
4:15 PM	23	73	17	70	102	40	68	189	24	19	163	57	845
4:30 PM	36	73	19	101	63	34	87	210	38	35	218	73	987
4:45 PM	23	88	20	107	57	54	60	197	24	29	236	88	983
5:00 PM	37	82	19	102	75	48	61	248	27	21	236	94	1050
5:15 PM	39	80	21	93	89	48	59	232	26	17	267	71	1042
5:30 PM	35	67	32	85	97	58	62	279	19	14	185	109	1042
5:45 PM	24	69	25	118	65	63	72	239	41	20	196	104	1036
<b>TOTAL VOLUMES :</b>	NL 242	NT 602	NR 178	SL 751	ST 627	SR 389	EL 545	ET 1813	ER 220	WL 182	WT 1694	WR 667	TOTAL 7910
<b>APPROACH %'s :</b>	23.68%	58.90%	17.42%	42.50%	35.48%	22.01%	21.14%	70.33%	8.53%	7.16%	66.61%	26.23%	
<b>PEAK HR START TIME :</b>	500 PM												
<b>PEAK HR VOL :</b>	135	298	97	398	326	217	254	998	113	72	884	378	4170
<b>PEAK HR FACTOR :</b>	0.946			0.956			0.948			0.939			0.993

UTURNS			
NB	SB	EB	WB
0	1	1	2
1	0	0	2
0	0	0	0
0	0	0	1
0	0	0	0
2	0	0	1
0	0	0	1
1	0	0	0
4	1	1	7

CONTROL : Signalized

# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-009

Day: Thursday

City: Valencia

Date: 2/12/2015

AM

NS/EW Streets:	Orchard Village Rd		Orchard Village Rd			Wiley Canyon Rd			Wiley Canyon Rd			TOTAL	
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL 1	NT 2	NR 1	SL 1	ST 2	SR 1	EL 2	ET 2	ER 0	WL 1	WT 2	WR 1	
6:30 AM	13	69	8	3	77	30	15	16	16	18	49	10	324
6:45 AM	25	137	17	9	152	21	14	16	46	25	48	13	523
7:00 AM	23	88	15	3	83	38	14	15	14	16	63	13	385
7:15 AM	37	141	14	18	169	96	39	42	32	20	80	16	704
7:30 AM	55	194	31	11	214	106	51	54	104	61	79	23	983
7:45 AM	61	275	54	19	126	106	78	44	136	56	60	39	1054
8:00 AM	80	248	38	16	127	53	45	29	68	34	40	21	799
8:15 AM	22	138	27	14	114	44	19	32	36	14	48	18	526
<b>TOTAL VOLUMES :</b>	NL 316	NT 1290	NR 204	SL 93	ST 1062	SR 494	EL 275	ET 248	ER 452	WL 244	WT 467	WR 153	TOTAL 5298
<b>APPROACH %'s :</b>	17.46%	71.27%	11.27%	5.64%	64.40%	29.96%	28.21%	25.44%	46.36%	28.24%	54.05%	17.71%	
<b>PEAK HR START TIME :</b>	7:15 AM												<b>TOTAL</b>
<b>PEAK HR VOL :</b>	233	858	137	64	636	361	213	169	340	171	259	99	3540
<b>PEAK HR FACTOR :</b>	0.787			0.801			0.700			0.811			0.840

UTURNS			
NB	SB	EB	WB
0	0	1	0
1	0	0	0
3	2	0	0
0	6	0	0
0	2	0	2
3	1	0	3
0	1	0	2
6	0	0	0
NB 13	SB 12	EB 1	WB 7

CONTROL : Signalized

# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-009

Day: Thursday

City: Valencia

Date: 2/12/2015

PM

NS/EW Streets:	Orchard Village Rd		Orchard Village Rd			Wiley Canyon Rd			Wiley Canyon Rd			TOTAL	
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL 1	NT 2	NR 1	SL 1	ST 2	SR 1	EL 2	ET 2	ER 0	WL 1	WT 2	WR 1	
4:00 PM	32	203	45	20	187	46	49	68	19	31	46	24	770
4:15 PM	27	160	32	39	196	66	61	60	33	24	55	21	774
4:30 PM	28	189	40	18	196	41	52	53	30	39	38	20	744
4:45 PM	38	243	32	23	208	59	37	71	25	19	65	23	843
5:00 PM	30	215	28	26	196	36	82	68	25	24	54	13	797
5:15 PM	28	182	39	27	191	59	63	72	51	29	51	19	811
5:30 PM	24	220	33	19	227	59	60	64	36	23	69	20	854
5:45 PM	45	206	38	26	227	58	54	76	42	39	50	22	883
<b>TOTAL VOLUMES :</b>	NL 252	NT 1618	NR 287	SL 198	ST 1628	SR 424	EL 458	ET 532	ER 261	WL 228	WT 428	WR 162	TOTAL 6476
<b>APPROACH %'s :</b>	11.68%	75.01%	13.31%	8.80%	72.36%	18.84%	36.61%	42.53%	20.86%	27.87%	52.32%	19.80%	
<b>PEAK HR START TIME :</b>	500 PM												<b>TOTAL</b>
<b>PEAK HR VOL :</b>	127	823	138	98	841	212	259	280	154	115	224	74	3345
<b>PEAK HR FACTOR :</b>	0.941			0.925			0.931			0.922			0.947

UTURNS			
NB	SB	EB	WB
0	3	0	0
0	14	0	1
0	2	0	2
2	5	0	0
0	3	0	0
0	2	0	0
0	0	0	2
1	2	0	1
NB 3	SB 31	EB 0	WB 6

CONTROL : Signalized

# Intersection Turning Movement

Prepared by:

National Data & Surveying Services

Project ID: 15-5082-010

Day: Thursday

City: Valencia

Date: 2/12/2015

AM

NS/EW Streets:	Orchard Village Rd			Orchard Village Rd			McBean Pkwy			McBean Pkwy			TOTAL
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
	1.5	0.5	2	1.5	1	0.5	1	3	0	2	3	0	
6:30 AM	39	4	52	1	0	1	4	53	30	56	91	1	332
6:45 AM	51	7	95	0	1	2	2	58	51	84	97	4	452
7:00 AM	31	3	69	0	1	3	1	60	49	83	153	3	456
7:15 AM	67	4	103	3	6	2	3	69	79	187	134	6	663
7:30 AM	101	4	167	1	16	10	6	115	115	215	138	10	898
7:45 AM	109	18	213	5	3	4	1	132	78	169	179	11	922
8:00 AM	114	16	187	4	7	2	5	116	49	113	128	4	745
8:15 AM	54	4	111	4	3	3	3	120	55	102	124	11	594
<b>TOTAL VOLUMES :</b>	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
<b>APPROACH %'s :</b>	566	60	997	18	37	27	25	723	506	1009	1044	50	5062
	34.87%	3.70%	61.43%	21.95%	45.12%	32.93%	1.99%	57.66%	40.35%	47.98%	49.64%	2.38%	
<b>PEAK HR START TIME :</b>	7:15 AM												<b>TOTAL</b>
<b>PEAK HR VOL :</b>	391	42	670	13	32	18	15	432	321	684	579	31	3228
<b>PEAK HR FACTOR :</b>	0.811			0.583			0.814			0.891			0.875

UTURNS			
NB	SB	EB	WB
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0

CONTROL : Signalized



# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-010

Day: Thursday

City: Valencia

Date: 2/12/2015

PM

NS/EW Streets:	Orchard Village Rd			Orchard Village Rd			McBean Pkwy			McBean Pkwy			TOTAL
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
	1.5	0.5	2	1.5	1	0.5	1	3	0	2	3	0	
4:00 PM	68	6	187	20	11	9	7	203	82	164	125	11	893
4:15 PM	55	2	165	18	8	14	5	202	92	149	145	10	865
4:30 PM	56	8	190	13	11	10	4	169	73	196	112	6	848
4:45 PM	53	3	208	9	11	9	4	216	107	172	139	5	936
5:00 PM	73	4	238	21	19	15	4	225	90	178	168	8	1043
5:15 PM	42	5	199	14	19	11	4	219	107	186	151	8	965
5:30 PM	75	7	223	11	15	15	3	210	109	181	158	7	1014
5:45 PM	66	6	192	7	7	7	0	234	95	196	163	4	977
<b>TOTAL VOLUMES :</b>	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
<b>APPROACH %'s :</b>	488	41	1602	113	101	90	31	1678	755	1422	1161	59	7541
	22.90%	1.92%	75.18%	37.17%	33.22%	29.61%	1.26%	68.10%	30.64%	53.82%	43.94%	2.23%	
<b>PEAK HR START TIME :</b>	500 PM												<b>TOTAL</b>
<b>PEAK HR VOL :</b>	256	22	852	53	60	48	11	888	401	741	640	27	3999
<b>PEAK HR FACTOR :</b>	0.897			0.732			0.985			0.970			0.959

UTURNS			
NB	SB	EB	WB
0	0	0	0
0	0	1	0
0	0	0	0
0	0	0	0
0	0	0	0
0	0	0	0
1	0	0	0
0	0	0	0
1	0	1	0

CONTROL : Signalized

# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-011

Day: Thursday

City: Valencia

Date: 2/12/2015

AM

NS/EW Streets:	Newhall Ave		Newhall Ave			Lyons Ave			Lyons Ave			TOTAL	
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
6:30 AM	40	12	2	2	18	20	13	29	42	3	53	4	238
6:45 AM	48	24	3	4	42	64	20	135	51	2	66	3	462
7:00 AM	54	16	2	1	22	25	18	50	74	2	87	3	354
7:15 AM	90	24	2	2	15	35	30	103	73	6	86	9	475
7:30 AM	112	61	5	3	42	48	48	155	86	4	130	5	699
7:45 AM	105	53	7	15	54	72	42	110	114	5	192	9	778
8:00 AM	70	16	6	9	62	57	21	93	71	13	236	5	659
8:15 AM	85	11	20	1	27	19	14	79	92	15	134	2	499
<b>TOTAL VOLUMES :</b>	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
<b>APPROACH %'s :</b>	604	217	47	37	282	340	206	754	603	50	984	40	4164
	69.59%	25.00%	5.41%	5.61%	42.79%	51.59%	13.18%	48.24%	38.58%	4.66%	91.62%	3.72%	
<b>PEAK HR START TIME :</b>	730 AM												<b>TOTAL</b>
<b>PEAK HR VOL :</b>	372	141	38	28	185	196	125	437	363	37	692	21	2635
<b>PEAK HR FACTOR :</b>	0.774			0.725			0.800			0.738			0.847

UTURNS			
NB	SB	EB	WB
0	0	1	0
0	0	1	0
0	0	0	0
0	0	3	0
0	0	4	0
0	0	2	0
0	0	4	0
0	0	3	0
NB	SB	EB	WB
0	0	18	0

CONTROL : Signalized

# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-011

Day: Thursday

City: Valencia

Date: 2/12/2015

PM

NS/EW Streets:	Newhall Ave		Newhall Ave			Lyons Ave			Lyons Ave			TOTAL	
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL 2	NT 1	NR 0	SL 1	ST 1	SR 0	EL 1	ET 3	ER 0	WL 1	WT 2	WR 1	
4:00 PM	108	48	9	7	21	11	27	154	153	8	145	8	699
4:15 PM	94	23	11	5	25	24	23	134	127	9	121	4	600
4:30 PM	109	23	12	3	28	23	33	133	142	5	163	4	678
4:45 PM	126	20	7	5	28	26	38	137	156	8	161	7	719
5:00 PM	120	30	3	1	23	24	30	183	163	6	157	7	747
5:15 PM	103	28	7	6	31	28	42	169	152	6	181	7	760
5:30 PM	125	16	1	10	26	28	29	208	165	4	158	2	772
5:45 PM	117	26	7	2	24	25	32	153	143	3	154	12	698
<b>TOTAL VOLUMES :</b>	NL 902	NT 214	NR 57	SL 39	ST 206	SR 189	EL 254	ET 1271	ER 1201	WL 49	WT 1240	WR 51	TOTAL 5673
<b>APPROACH %'s :</b>	76.90%	18.24%	4.86%	8.99%	47.47%	43.55%	9.32%	46.63%	44.06%	3.66%	92.54%	3.81%	
<b>PEAK HR START TIME :</b>	445 PM												<b>TOTAL</b>
<b>PEAK HR VOL :</b>	474	94	18	22	108	106	139	697	636	24	657	23	2998
<b>PEAK HR FACTOR :</b>	0.958		0.908			0.915			0.907			0.971	

UTURNS			
NB	SB	EB	WB
0	0	8	1
0	0	5	0
0	0	6	0
0	0	8	0
0	0	4	0
0	0	10	0
0	0	6	0
0	0	9	0
NB 0	SB 0	EB 56	WB 1

CONTROL : Signalized

# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-012

Day: Thursday

City: Valencia

Date: 2/12/2015

AM

NS/EW Streets:	Valencia Blvd		Valencia Blvd			Magic Mountain			Magic Mountain			TOTAL	
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
6:30 AM	3	236	52	3	73	10	19	47	1	20	19	2	485
6:45 AM	7	336	73	0	78	13	18	71	7	21	34	2	660
7:00 AM	7	283	79	9	105	16	13	46	5	24	27	2	616
7:15 AM	4	435	110	1	128	15	32	68	10	21	17	1	842
7:30 AM	4	422	112	3	192	31	48	83	9	30	47	7	988
7:45 AM	7	491	114	8	213	30	41	93	18	40	32	3	1090
8:00 AM	4	383	105	10	170	22	29	98	19	22	53	12	927
8:15 AM	10	387	109	6	183	19	43	93	19	30	43	7	949
<b>TOTAL VOLUMES :</b>	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
<b>APPROACH %'s :</b>	46	2973	754	40	1142	156	243	599	88	208	272	36	6557
	1.22%	78.80%	19.98%	2.99%	85.35%	11.66%	26.13%	64.41%	9.46%	40.31%	52.71%	6.98%	
<b>PEAK HR START TIME :</b>	730 AM												<b>TOTAL</b>
<b>PEAK HR VOL :</b>	25	1683	440	27	758	102	161	367	65	122	175	29	3954
<b>PEAK HR FACTOR :</b>	0.877			0.883			0.956			0.937			0.907

UTURNS			
NB	SB	EB	WB
0	2	0	0
0	0	0	0
0	2	0	0
0	0	0	0
0	1	1	0
0	2	2	0
0	2	0	0
1	3	0	0
NB	SB	EB	WB
1	12	3	0

CONTROL : Signalized

# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-012

Day: Thursday

City: Valencia

Date: 2/12/2015

PM

NS/EW Streets:	Valencia Blvd			Valencia Blvd			Magic Mountain			Magic Mountain			TOTAL
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL 1	NT 3	NR 0	SL 1	ST 3	SR 2	EL 2	ET 2	ER 0	WL 1	WT 2	WR 1	
4:00 PM	18	252	93	22	375	43	46	78	23	132	95	7	1184
4:15 PM	14	278	88	22	417	47	41	91	17	123	114	6	1258
4:30 PM	13	297	84	26	376	39	50	79	15	156	123	5	1263
4:45 PM	21	272	101	24	391	45	60	100	24	152	108	10	1308
5:00 PM	23	320	90	22	406	45	52	95	21	159	135	10	1378
5:15 PM	34	313	125	16	427	54	61	129	21	135	133	8	1456
5:30 PM	24	304	121	16	458	48	65	102	16	144	111	13	1422
5:45 PM	28	290	127	12	439	58	54	109	18	171	128	6	1440
<b>TOTAL VOLUMES :</b>	NL 175	NT 2326	NR 829	SL 160	ST 3289	SR 379	EL 429	ET 783	ER 155	WL 1172	WT 947	WR 65	TOTAL 10709
<b>APPROACH %'s :</b>	5.26%	69.85%	24.89%	4.18%	85.92%	9.90%	31.38%	57.28%	11.34%	53.66%	43.36%	2.98%	
<b>PEAK HR START TIME :</b>	500 PM												<b>TOTAL</b>
<b>PEAK HR VOL :</b>	109	1227	463	66	1730	205	232	435	76	609	507	37	5696
<b>PEAK HR FACTOR :</b>	0.953			0.958			0.880			0.945			0.978

UTURNS			
NB	SB	EB	WB
0	4	0	0
2	8	1	0
0	8	1	0
1	5	5	0
2	3	4	0
0	2	3	0
1	3	5	0
0	2	4	0
NB 6	SB 35	EB 23	WB 0

CONTROL : Signalized

# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-013

Day: Thursday

City: Valencia

Date: 2/12/2015

AM													
NS/EW Streets:	Avenida Navarre			Avenida Navarre			McBean Pkwy			McBean Pkwy			
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL 1	NT 3	NR 0	SL 1	ST 3	SR 0	EL 1	ET 1	ER 1	WL 1	WT 1	WR 1	TOTAL
6:30 AM	41	0	10	2	0	2	19	91	4	5	100	41	315
6:45 AM	40	3	11	6	0	4	32	111	7	11	143	80	448
7:00 AM	51	1	12	8	0	3	8	116	9	5	177	29	419
7:15 AM	57	1	18	10	0	4	19	144	9	10	274	28	574
7:30 AM	90	7	16	37	0	17	23	257	12	5	251	34	749
7:45 AM	66	5	12	11	1	12	42	273	27	5	286	61	801
8:00 AM	28	1	12	11	0	1	38	256	23	5	210	39	624
8:15 AM	27	0	15	16	0	6	24	184	26	10	211	38	557
<b>TOTAL VOLUMES :</b>	NL 400	NT 18	NR 106	SL 101	ST 1	SR 49	EL 205	ET 1432	ER 117	WL 56	WT 1652	WR 350	TOTAL 4487
<b>APPROACH %'s :</b>	76.34%	3.44%	20.23%	66.89%	0.66%	32.45%	11.69%	81.64%	6.67%	2.72%	80.27%	17.01%	
<b>PEAK HR START TIME :</b>	7:15 AM												<b>TOTAL</b>
<b>PEAK HR VOL :</b>	241	14	58	69	1	34	122	930	71	25	1021	162	2748
<b>PEAK HR FACTOR :</b>	0.692			0.481			0.821			0.858			0.858

UTURNS			
NB	SB	EB	WB
0	0	0	1
0	0	1	1
0	0	0	0
0	0	1	1
0	0	0	0
0	0	1	1
0	0	0	1
0	0	0	2
NB 0	SB 0	EB 3	WB 7

CONTROL : Signalized

# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-013

Day: Thursday

City: Valencia

Date: 2/12/2015

PM													
NS/EW Streets:	Avenida Navarre			Avenida Navarre			McBean Pkwy			McBean Pkwy			
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL 1	NT 3	NR 0	SL 1	ST 3	SR 0	EL 1	ET 1	ER 1	WL 1	WT 1	WR 1	TOTAL
4:00 PM	15	1	18	48	1	0	7	366	38	18	281	13	806
4:15 PM	26	0	19	41	1	0	12	356	26	18	288	16	803
4:30 PM	33	3	27	53	4	0	8	326	34	20	269	12	789
4:45 PM	25	2	21	43	1	0	8	386	50	25	297	15	873
5:00 PM	35	0	16	62	5	0	5	421	49	19	317	14	943
5:15 PM	34	0	19	39	5	0	7	388	40	24	318	10	884
5:30 PM	36	0	16	39	2	0	6	382	40	26	303	7	857
5:45 PM	29	0	23	19	0	0	6	398	45	27	341	10	898
<b>TOTAL VOLUMES :</b>	NL 233	NT 6	NR 159	SL 344	ST 19	SR 0	EL 59	ET 3023	ER 322	WL 177	WT 2414	WR 97	TOTAL 6853
<b>APPROACH %'s :</b>	58.54%	1.51%	39.95%	94.77%	5.23%	0.00%	1.73%	88.81%	9.46%	6.58%	89.81%	3.61%	
<b>PEAK HR START TIME :</b>	500 PM												TOTAL
<b>PEAK HR VOL :</b>	134	0	74	159	12	0	24	1589	174	96	1279	41	3582
<b>PEAK HR FACTOR :</b>	0.981			0.638			0.941			0.937			0.950

UTURNS			
NB	SB	EB	WB
0	0	1	4
0	0	0	3
0	0	0	3
0	0	0	5
0	0	0	2
0	0	0	0
0	0	0	8
0	0	0	2
NB 0	SB 0	EB 1	WB 27

CONTROL : Signalized

# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-014

Day: Thursday

City: Valencia

Date: 2/12/2015

AM													
NS/EW Streets:	Medical Center Dwy ( west Dwy)			Medical Center Dwy ( west Dwy)			McBean Pkwy			McBean Pkwy			TOTAL
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
6:30 AM	0	0	0	0	0	2	12	96	0	0	139	1	250
6:45 AM	0	0	0	0	0	4	42	102	0	0	137	4	289
7:00 AM	0	0	0	2	0	6	25	109	0	0	189	3	334
7:15 AM	0	0	0	2	0	7	15	160	0	0	192	4	380
7:30 AM	0	0	0	0	0	14	20	225	0	0	246	7	512
7:45 AM	0	0	0	0	0	11	32	211	0	0	273	13	540
8:00 AM	0	0	0	1	0	4	25	186	0	0	236	13	465
8:15 AM	0	0	0	2	0	6	34	160	0	0	165	11	378
<b>TOTAL VOLUMES :</b>	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
<b>APPROACH %'s :</b>	#DIV/0!	#DIV/0!	#DIV/0!	11.48%	0.00%	88.52%	14.10%	85.90%	0.00%	0.00%	96.57%	3.43%	3148
<b>PEAK HR START TIME :</b>	7:15 AM												<b>TOTAL</b>
<b>PEAK HR VOL :</b>	0	0	0	3	0	36	92	782	0	0	947	37	1897
<b>PEAK HR FACTOR :</b>	0.000			0.696			0.892			0.860			0.878

CONTROL : 1 Way Stop (SB)

UTURNS			
NB	SB	EB	WB
0	0	0	0

NB	SB	EB	WB
0	0	0	0



# Intersection Turning Movement

Prepared by:

**National Data & Surveying Services**

Project ID: 15-5082-014

Day: Thursday

City: Valencia

Date: 2/12/2015

PM

NS/EW Streets:	Medical Center Dwy ( west Dwy)		Medical Center Dwy ( west Dwy)			McBean Pkwy			McBean Pkwy			TOTAL	
	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			
LANES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
4:00 PM	0	0	0	1	0	16	10	282	0	0	214	4	527
4:15 PM	0	0	0	2	0	6	8	289	0	0	213	3	521
4:30 PM	0	0	0	2	0	21	9	265	0	0	185	0	482
4:45 PM	0	0	0	6	0	15	6	304	0	0	215	2	548
5:00 PM	0	0	0	3	0	28	3	351	0	0	249	0	634
5:15 PM	0	0	0	4	0	16	3	296	0	0	205	2	526
5:30 PM	0	0	0	1	0	14	5	320	0	0	234	0	574
5:45 PM	0	0	0	1	0	13	0	317	0	0	234	1	566
<b>TOTAL VOLUMES :</b>	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
<b>APPROACH %'s :</b>	#DIV/0!	#DIV/0!	#DIV/0!	13.42%	0.00%	86.58%	1.78%	98.22%	0.00%	0.00%	99.32%	0.68%	4378
<b>PEAK HR START TIME :</b>	500 PM												<b>TOTAL</b>
<b>PEAK HR VOL :</b>	0	0	0	9	0	71	11	1284	0	0	922	3	2300
<b>PEAK HR FACTOR :</b>	0.000			0.645			0.915			0.929			0.907

CONTROL : 1 Way Stop (SB)

UTURNS			
NB	SB	EB	WB

NB	SB	EB	WB
0	0	0	0

## **APPENDIX B**

### **ICU AND LEVELS OF SERVICE EXPLANATION ICU DATA WORKSHEETS – WEEKDAY AM AND PM PEAK HOURS**

## INTERSECTION CAPACITY UTILIZATION (ICU) DESCRIPTION

Level of Service is a term used to describe prevailing conditions and their effect on traffic. Broadly interpreted, the Levels of Service concept denotes any one of a number of differing combinations of operating conditions which may occur as a roadway is accommodating various traffic volumes. Level of Service is a qualitative measure of the effect of such factors as travel speed, travel time, traffic interruptions, freedom to maneuver, safety, driving comfort and convenience.

Six Levels of Service, A through F, have been defined in the 1965 *Highway Capacity Manual*, published by the Transportation Research Board. Level of Service A describes a condition of free flow, with low traffic volumes and relatively high speeds, while Level of Service F describes forced traffic flow at low speeds with jammed conditions and queues which cannot clear during the green phases.

The Intersection Capacity Utilization (ICU) method of intersection capacity analysis has been used in our studies. It directly relates traffic demand and available capacity for key intersection movements, regardless of present signal timing. The capacity per hour of green time for each approach is calculated based on the methods of the *Highway Capacity Manual*. The proportion of total signal time needed by each key movement is determined and compared to the total time available (100 percent of the hour). The result of summing the requirements of the conflicting key movements plus an allowance for clearance times is expressed as a decimal fraction. Conflicting key traffic movements are those opposing movements whose combined green time requirements are greatest.

The resulting ICU represents the proportion of the total hour required to accommodate intersection demand volumes if the key conflicting traffic movements are operating at capacity. Other movements may be operating near capacity, or may be operating at significantly better levels. The ICU may be translated to a Level of Service as tabulated below.

The Levels of Service (abbreviated from the *Highway Capacity Manual*) are listed here with their corresponding ICU and Load Factor equivalents. Load Factor is that proportion of the signal cycles during the peak hour which are fully loaded; i.e. when all of the vehicles waiting at the beginning of green are not able to clear on that green phase.

Intersection Capacity Utilization Characteristics		
Level of Service	Load Factor	Equivalent ICU
A	0.0	0.00 - 0.60
B	0.0 - 0.1	0.61 - 0.70
C	0.1 - 0.3	0.71 - 0.80
D	0.3 - 0.7	0.81 - 0.90
E	0.7 - 1.0	0.91 - 1.00
F	Not Applicable	Not Applicable

### SERVICE LEVEL A

There are no loaded cycles and few are even close to loaded at this service level. No approach phase is fully utilized by traffic and no vehicle waits longer than one red indication.

### SERVICE LEVEL B

This level represents stable operation where an occasional approach phase is fully utilized and a substantial number are approaching full use. Many drivers begin to feel restricted within platoons of vehicles.

### SERVICE LEVEL C

At this level stable operation continues. Loading is still intermittent but more frequent than at Level B. Occasionally drivers may have to wait through more than one red signal indication and backups may develop behind turning vehicles. Most drivers feel somewhat restricted, but not objectionably so.

### SERVICE LEVEL D

This level encompasses a zone of increasing restriction approaching instability at the intersection. Delays to approaching vehicles may be substantial during short peaks within the peak hour, but enough cycles with lower demand occur to permit periodic clearance of queues, thus preventing excessive backups. Drivers frequently have to wait through more than one red signal. This level is the lower limit of acceptable operation to most drivers.

### SERVICE LEVEL E

This represents near capacity and capacity operation. At capacity (ICU = 1.0) it represents the most vehicles that the particular intersection can accommodate. However, full utilization of every signal cycle is seldom attained no matter how great the demand. At this level all drivers wait through more than one red signal, and frequently through several.

### SERVICE LEVEL F

Jammed conditions. Traffic backed up from a downstream location on one of the street restricts or prevents movement of traffic through the intersection under consideration.

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**INTERSECTION CAPACITY UTILIZATION**

N-S St: I-5 SB On/Off Ramps  
 E-W St: McBean Parkway  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU1

I-5 SB On/Off Ramps @ McBean Parkway  
 Peak hr: AM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION			
	1 Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio
Nb Left	0	0	0.000	0	0	0	0.000	0	0	0	0.000	0	0	0	0.000
Nb Thru	0	0	0.000	0	0	0	0.000	0	0	0	0.000	0	0	0	0.000
Nb Right	0	0	-	0	0	0	-	0	0	0	-	0	0	0	-
Sb Left	177	1750	0.101	-14	163	1750	0.093	44	221	1750	0.126	0	221	1750	0.126
Sb Thru	0	0	0.000	0	0	0	0.000	0	0	0	0.000	0	0	0	0.000
Sb Right	106	1750	0.061	0	106	1750	0.061	0	106	1750	0.061	0	106	1750	0.061
Eb Left	1	0	0.000	0	1	0	0.000	0	1	0	0.000	0	1	0	0.000
Eb Thru	821	3500	0.235 *	-13	808	3500	0.231 *	40	861	3500	0.246 *	0	861	3500	0.246 *
Eb Right	325	1750	0.186	0	325	1750	0.186	0	325	1750	0.186	0	325	1750	0.186
Wb Left	0	0	0.000 *	0	0	0	0.000 *	0	0	0	0.000 *	0	0	0	0.000 *
Wb Thru	556	3500	0.159	-1	555	3500	0.159	6	562	3500	0.161	0	562	3500	0.161
Wb Right [3]	489	1750	0.178	-3	486	1750	0.185	12	501	1750	0.160	0	501	1750	0.160
Yellow Allowance:			0.100 *				0.100 *				0.100 *				0.100 *
ICU LOS			0.436 A				0.424 A				0.473 A				0.473 A

\*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green  
 3 The westbound right-turn lane has an overlapping phase with the southbound left-turn phase.

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**INTERSECTION CAPACITY UTILIZATION**

N-S St: I-5 SB On/Off Ramps  
 E-W St: McBean Parkway  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU1

I-5 SB On/Off Ramps @ McBean Parkway  
 Peak hr: PM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION						
	Volume	Capacity	V/C	Added Volume	Total Volume	Capacity	V/C	Added Volume	Total Volume	Capacity	V/C	Added Volume	Total Volume	Capacity	V/C			
Nb Left	0	0	0.000 *	0	0	0	0.000 *	0	0	0	0.000 *	0	0	0	0.000 *			
Nb Thru	0	0	0.000	0	0	0	0.000	0	0	0	0.000	0	0	0	0.000			
Nb Right	0	0	-	0	0	0	-	0	0	0	-	0	0	0	-			
Sb Left	173	1750	0.099	-6	167	1750	0.095	18	191	1750	0.109	0	191	1750	0.109			
Sb Thru	0	0	0.000	0	0	0	0.000	0	0	0	0.000	0	0	0	0.000			
Sb Right	229	1750	0.131 *	0	229	1750	0.131 *	0	229	1750	0.131 *	0	229	1750	0.131 *			
Eb Left	0	0	0.000	0	0	0	0.000	0	0	0	0.000 *	0	0	0	0.000 *			
Eb Thru	1058	3500	0.302 *	-5	1053	3500	0.301 *	17	1075	3500	0.307	0	1075	3500	0.307			
Eb Right	177	1750	0.101	0	177	1750	0.101	0	177	1750	0.101	0	177	1750	0.101			
Wb Left	0	0	0.000 *	0	0	0	0.000 *	0	0	0	0.000	0	0	0	0.000			
Wb Thru	1039	3500	0.297	-18	1021	3500	0.292	52	1091	3500	0.312 *	0	1091	3500	0.312 *			
Wb Right [3]	573	1750	0.228	-23	550	1750	0.219	67	640	1750	0.256	0	640	1750	0.256			
Yellow Allowance:			0.100 *					0.100 *					0.100 *					0.100 *
ICU	0.533			0.531				0.542				0.542						
LOS	A			A				A				A						

\*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green  
 3 The westbound right-turn lane has an overlapping phase with the southbound left-turn phase.

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**INTERSECTION CAPACITY UTILIZATION**

N-S St: I-5 NB On/Off Ramps  
 E-W St: McBean Parkway  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU2

I-5 NB On/Off Ramps @ McBean Parkway  
 Peak hr: AM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION						
	1 Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio			
Nb Left	126	3500	0.036	0	126	3500	0.036	0	126	3500	0.036	0	126	3500	0.036			
Nb Thru	0	0	0.000	0	0	0	0.000	0	0	0	0.000	0	0	0	0.000			
Nb Right	741	3500	0.212 *	-16	725	3500	0.207 *	48	789	3500	0.225 *	0	789	3500	0.225 *			
Sb Left	0	0	0.000 *	0	0	0	0.000 *	0	0	0	0.000 *	0	0	0	0.000 *			
Sb Thru	0	0	0.000	0	0	0	0.000	0	0	0	0.000	0	0	0	0.000			
Sb Right	0	0	-	0	0	0	-	0	0	0	-	0	0	0	-			
Eb Left	0	0	0.000 *	0	0	0	0.000 *	0	0	0	0.000 *	0	0	0	0.000 *			
Eb Thru	712	3500	0.203	-28	684	3500	0.195	85	797	3500	0.228	0	797	3500	0.228			
Eb Right	289	1750	0.165	0	289	1750	0.165	0	289	1750	0.165	0	289	1750	0.165			
Wb Left	0	0	0.000	0	0	0	0.000	0	0	0	0.000	0	0	0	0.000			
Wb Thru	918	5250	0.223 *	-4	914	5250	0.222 *	18	936	5250	0.228 *	0	936	5250	0.228 *			
Wb Right	251	0	-	-2	249	0	-	11	262	0	-	0	262	0	-			
Yellow Allowance:			0.100 *					0.100 *					0.100 *					
ICU LOS			0.534 A					0.529 A					0.554 A					0.554 A

\*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green

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**INTERSECTION CAPACITY UTILIZATION**

N-S St: I-5 NB On/Off Ramps  
 E-W St: McBean Parkway  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU2

I-5 NB On/Off Ramps @ McBean Parkway  
 Peak hr: PM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION						
	1 Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio			
Nb Left	265	3500	0.076	0	265	3500	0.076	0	265	3500	0.076	0	265	3500	0.076			
Nb Thru	0	0	0.000	0	0	0	0.000	0	0	0	0.000	0	0	0	0.000			
Nb Right	721	3500	0.206 *	-6	715	3500	0.204 *	20	741	3500	0.212 *	0	741	3500	0.212 *			
Sb Left	0	0	0.000 *	0	0	0	0.000 *	0	0	0	0.000 *	0	0	0	0.000 *			
Sb Thru	0	0	0.000	0	0	0	0.000	0	0	0	0.000	0	0	0	0.000			
Sb Right	0	0	-	0	0	0	-	0	0	0	-	0	0	0	-			
Eb Left	0	0	0.000	0	0	0	0.000	0	0	0	0.000 *	0	0	0	0.000 *			
Eb Thru	1009	3500	0.288 *	-12	997	3500	0.285 *	38	1047	3500	0.299	0	1047	3500	0.299			
Eb Right	225	1750	0.128	0	225	1750	0.128	0	225	1750	0.128	0	225	1750	0.128			
Wb Left	0	0	0.000 *	0	0	0	0.000 *	0	0	0	0.000	0	0	0	0.000			
Wb Thru	1332	5250	0.278	-40	1292	5250	0.267	118	1450	5250	0.310 *	0	1450	5250	0.310 *			
Wb Right	126	0	-	-18	108	0	-	52	178	0	-	0	178	0	-			
Yellow Allowance:			0.100 *					0.100 *					0.100 *					0.100 *
ICU	0.594			0.589				0.622				0.622						
LOS	A			A				B				B						

\*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green

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**INTERSECTION CAPACITY UTILIZATION**

N-S St: Rockwell Canyon Rd/Tournament Rd  
 E-W St: McBean Parkway  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU3

Rockwell Canyon Rd/Tournament Rd @ McBean Parkway  
 Peak hr: AM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION			
	1 Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio
Nb Left	176	1750	0.101 *	0	176	1750	0.101 *	0	176	1750	0.101 *	0	176	1750	0.101 *
Nb Thru	178	3500	0.069	0	178	3500	0.068	0	178	3500	0.073	0	178	3500	0.073
Nb Right	65	0	-	-4	61	0	-	12	77	0	-	0	77	0	-
Sb Left	66	1750	0.038	-3	63	1750	0.036	8	74	1750	0.042	0	74	1750	0.042
Sb Thru	85	1750	0.049 *	0	85	1750	0.049 *	0	85	1750	0.049 *	0	85	1750	0.049 *
Sb Right [3]	277	1750	0.000	0	277	1750	0.000	0	277	1750	0.000	0	277	1750	0.000
Eb Left	480	1750	0.274 *	0	480	1750	0.274 *	0	480	1750	0.274 *	0	480	1750	0.274 *
Eb Thru	762	5250	0.165	-45	717	5250	0.156	137	899	5250	0.191	0	899	5250	0.191
Eb Right	103	0	-	0	103	0	-	0	103	0	-	0	103	0	-
Wb Left	48	1750	0.028	-1	47	1750	0.027	4	52	1750	0.030	0	52	1750	0.030
Wb Thru	774	5250	0.184 *	-7	767	5250	0.182 *	30	804	5250	0.192 *	0	804	5250	0.192 *
Wb Right	191	0	-	-3	188	0	-	12	203	0	-	0	203	0	-
Yellow Allowance:			0.100 *				0.100 *				0.100 *				0.100 *
ICU			0.707				0.706				0.715				0.715
LOS			C				C				C				C

02:02 PM

\*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green  
 3 The southbound right-turn lane has an overlapping phase with the eastbound left-turn phase.



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**INTERSECTION CAPACITY UTILIZATION**

N-S St: Rockwell Canyon Rd/Tournament Rd  
 E-W St: McBean Parkway  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU3

Rockwell Canyon Rd/Tournament Rd @ McBean Parkway  
 Peak hr: PM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION						
	1 Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio			
Nb Left	239	1750	0.137 *	0	239	1750	0.137 *	0	239	1750	0.137 *	0	239	1750	0.137 *			
Nb Thru	165	3500	0.075	0	165	3500	0.074	0	165	3500	0.076	0	165	3500	0.076			
Nb Right	97	0	-	-2	95	0	-	5	102	0	-	0	102	0	-			
Sb Left	167	1750	0.095	-5	162	1750	0.092	15	182	1750	0.104	0	182	1750	0.104			
Sb Thru	228	1750	0.130 *	0	228	1750	0.130 *	0	228	1750	0.130 *	0	228	1750	0.130 *			
Sb Right [3]	452	1750	0.049	0	452	1750	0.049	0	452	1750	0.049	0	452	1750	0.049			
Eb Left	367	1750	0.210 *	0	367	1750	0.210 *	0	367	1750	0.210 *	0	367	1750	0.210 *			
Eb Thru	1096	5250	0.247	-20	1076	5250	0.243	64	1160	5250	0.259	0	1160	5250	0.259			
Eb Right	202	0	-	0	202	0	-	0	202	0	-	0	202	0	-			
Wb Left	71	1750	0.041	-7	64	1750	0.037	21	92	1750	0.053	0	92	1750	0.053			
Wb Thru	840	5250	0.178 *	-60	780	5250	0.165 *	175	1015	5250	0.214 *	0	1015	5250	0.214 *			
Wb Right	92	0	-	-5	87	0	-	15	107	0	-	0	107	0	-			
Yellow Allowance:			0.100 *					0.100 *					0.100 *					0.100 *
ICU	0.754							0.741					0.790					0.790
LOS	C							C					C					C

\*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green  
 3 The southbound right-turn lane has an overlapping phase with the eastbound left-turn phase.

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**INTERSECTION CAPACITY UTILIZATION**

N-S St: McBean Parkway  
 E-W St: Valencia Boulevard  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU4

McBean Parkway @ Valencia Boulevard  
 Peak hr: AM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION					
	1 Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio		
Nb Left	277	3500	0.079 *	0	277	3500	0.079 *	0	277	3500	0.079 *	0	277	3500	0.079 *		
Nb Thru	824	5250	0.157	-2	822	5250	0.157	9	833	5250	0.159	0	833	5250	0.159		
Nb Right [3]	301	3500	0.000	-1	300	3500	0.000	6	307	3500	0.000	0	307	3500	0.000		
Sb Left	115	3500	0.033	0	115	3500	0.033	0	115	3500	0.033	0	115	3500	0.033		
Sb Thru	637	5250	0.121 *	-14	623	5250	0.119 *	44	681	5250	0.130 *	0	681	5250	0.130 *		
Sb Right [4]	731	3500	0.072	0	731	3500	0.072	0	731	3500	0.072	0	731	3500	0.072		
Eb Left	479	3500	0.137 *	0	479	3500	0.137 *	0	479	3500	0.137 *	0	479	3500	0.137 *		
Eb Thru	676	5250	0.129	0	676	5250	0.129	0	676	5250	0.129	0	676	5250	0.129		
Eb Right [5]	118	1750	0.000	0	118	1750	0.000	0	118	1750	0.000	0	118	1750	0.000		
Wb Left	378	3500	0.108	-12	366	3500	0.105	36	414	3500	0.118	0	414	3500	0.118		
Wb Thru	1237	5250	0.236 *	0	1237	5250	0.236 *	0	1237	5250	0.236 *	0	1237	5250	0.236 *		
Wb Right [6]	44	1750	0.000	0	44	1750	0.000	0	44	1750	0.000	0	44	1750	0.000		
Yellow Allowance:			0.100 *					0.100 *					0.100 *				
ICU	0.673			0.670				0.681				0.681					
LOS	B			B				B				B					

11:43 AM

- \*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green  
 3 The northbound right-turn lane has an overlapping phase with the westbound left-turn phase.  
 4 The southbound right-turn lane has an overlapping phase with the eastbound left-turn phase.  
 5 The eastbound right-turn lane has an overlapping phase with the northbound left-turn phase.  
 6 The westbound right-turn lane has an overlapping phase with the southbound left-turn phase.

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**INTERSECTION CAPACITY UTILIZATION**

N-S St: McBean Parkway  
 E-W St: Valencia Boulevard  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU4

McBean Parkway @ Valencia Boulevard  
 Peak hr: PM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION					
	Volume	Capacity	V/C	Added Volume	Total Volume	Capacity	Ratio	Added Volume	Total Volume	Capacity	Ratio	Added Volume	Total Volume	Capacity	Ratio		
Nb Left	175	3500	0.050	0	175	3500	0.050	0	175	3500	0.050	0	175	3500	0.050		
Nb Thru	1277	5250	0.243 *	-18	1259	5250	0.240 *	52	1329	5250	0.253 *	0	1329	5250	0.253 *		
Nb Right [3]	519	3500	0.040	-12	507	3500	0.038	36	555	3500	0.046	0	555	3500	0.046		
Sb Left	232	3500	0.066 *	0	232	3500	0.066 *	0	232	3500	0.066 *	0	232	3500	0.066 *		
Sb Thru	1017	5250	0.194	-6	1011	5250	0.192	18	1035	5250	0.197	0	1035	5250	0.197		
Sb Right [4]	664	3500	0.022	0	664	3500	0.022	0	664	3500	0.022	0	664	3500	0.022		
Eb Left	588	3500	0.168	0	588	3500	0.168	0	588	3500	0.168	0	588	3500	0.168		
Eb Thru	1624	5250	0.309 *	0	1624	5250	0.309 *	0	1624	5250	0.309 *	0	1624	5250	0.309 *		
Eb Right [5]	202	1750	0.065	0	202	1750	0.065	0	202	1750	0.065	0	202	1750	0.065		
Wb Left	378	3500	0.108 *	-5	373	3500	0.107 *	15	393	3500	0.112 *	0	393	3500	0.112 *		
Wb Thru	1097	5250	0.209	0	1097	5250	0.209	0	1097	5250	0.209	0	1097	5250	0.209		
Wb Right [6]	200	1750	0.048	0	200	1750	0.048	0	200	1750	0.048	0	200	1750	0.048		
Yellow Allowance:			0.100 *					0.100 *					0.100 *				
ICU	0.827			0.822				0.841				0.841					
LOS	D			D				D				D					

\*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green  
 3 The northbound right-turn lane has an overlapping phase with the westbound left-turn phase.  
 4 The southbound right-turn lane has an overlapping phase with the eastbound left-turn phase.  
 5 The eastbound right-turn lane has an overlapping phase with the northbound left-turn phase.  
 6 The westbound right-turn lane has an overlapping phase with the southbound left-turn phase.

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**INTERSECTION CAPACITY UTILIZATION**

N-S St: McBean Parkway  
 E-W St: Magic Mountain Parkway  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU5

McBean Parkway @ Magic Mountain Parkway  
 Peak hr: AM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION						
	1 Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio			
Nb Left	75	3500	0.021	0	75	3500	0.021	0	75	3500	0.021 *	0	75	3500	0.021 *			
Nb Thru	1118	7000	0.160 *	-2	1116	7000	0.159 *	8	1126	7000	0.161	0	1126	7000	0.161			
Nb Right [3]	68	1000000	0.000	0	68	1000000	0.000	0	68	1000000	0.000	0	68	1000000	0.000			
Sb Left	261	3500	0.074 *	0	261	3500	0.074 *	0	261	3500	0.074	0	261	3500	0.074			
Sb Thru	1465	7000	0.209	-13	1452	7000	0.207	40	1505	7000	0.215 *	0	1505	7000	0.215 *			
Sb Right [3]	614	1000000	0.001	0	614	1000000	0.001	0	614	1000000	0.001	0	614	1000000	0.001			
Eb Left	373	5250	0.071 *	0	373	5250	0.071 *	0	373	5250	0.071 *	0	373	5250	0.071 *			
Eb Thru	375	3500	0.107	0	375	3500	0.107	0	375	3500	0.107	0	375	3500	0.107			
Eb Right [5]	40	1750	0.012	-1	39	1750	0.012	4	44	1750	0.014	0	44	1750	0.014			
Wb Left	63	3500	0.018	-1	62	3500	0.018	4	67	3500	0.019	0	67	3500	0.019			
Wb Thru	381	5250	0.073 *	0	381	5250	0.073 *	0	381	5250	0.073 *	0	381	5250	0.073 *			
Wb Right [4]	167	1750	0.021	0	167	1750	0.021	0	167	1750	0.021	0	167	1750	0.021			
Yellow Allowance:			0.100 *					0.100 *					0.100 *					0.100 *
ICU	0.478			0.477				0.480				0.480						
LOS	A			A				A				A						

11:43 AM

- \*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green  
 3 Freeflow right-turn lane.  
 4 The westbound right-turn lane has an overlapping phase with the southbound left-turn phase.  
 5 Right turns on red from exclusive lanes, 50% of overlapping left turn

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**INTERSECTION CAPACITY UTILIZATION**

N-S St: McBean Parkway  
 E-W St: Magic Mountain Parkway  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU5

McBean Parkway @ Magic Mountain Parkway  
 Peak hr: PM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION					
	1	2	V/C	Added	Total	2	V/C	Added	Total	2	V/C	Added	Total	2	V/C		
	Volume	Capacity	Ratio	Volume	Volume	Capacity	Ratio	Volume	Volume	Capacity	Ratio	Volume	Volume	Capacity	Ratio		
Nb Left	191	3500	0.054	-2	189	3500	0.054	5	196	3500	0.056	0	196	3500	0.056		
Nb Thru	1923	7000	0.275 *	-14	1909	7000	0.273 *	41	1964	7000	0.281 *	0	1964	7000	0.281 *		
Nb Right [3]	237	1000000	0.000	-2	235	1000000	0.000	5	242	1000000	0.000	0	242	1000000	0.000		
Sb Left	367	3500	0.105 *	0	367	3500	0.105 *	0	367	3500	0.105 *	0	367	3500	0.105 *		
Sb Thru	1537	7000	0.220	-5	1532	7000	0.219	15	1552	7000	0.222	0	1552	7000	0.222		
Sb Right [3]	340	1000000	0.000	0	340	1000000	0.000	0	340	1000000	0.000	0	340	1000000	0.000		
Eb Left	620	5250	0.118	0	620	5250	0.118	0	620	5250	0.118	0	620	5250	0.118		
Eb Thru	726	3500	0.207 *	0	726	3500	0.207 *	0	726	3500	0.207 *	0	726	3500	0.207 *		
Eb Right [5]	198	1750	0.086	-1	197	1750	0.085	2	200	1750	0.086	0	200	1750	0.086		
Wb Left	256	3500	0.073 *	-1	255	3500	0.073 *	2	258	3500	0.074 *	0	258	3500	0.074 *		
Wb Thru	614	5250	0.117	0	614	5250	0.117	0	614	5250	0.117	0	614	5250	0.117		
Wb Right [4]	458	1750	0.157	0	458	1750	0.157	0	458	1750	0.157	0	458	1750	0.157		
Yellow Allowance:			0.100 *					0.100 *					0.100 *				
ICU			0.760					0.758					0.767				
LOS			C					C					C				

\*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green  
 3 Freeflow right-turn lane.  
 4 The westbound right-turn lane has an overlapping phase with the southbound left-turn phase.  
 5 Right turns on red from exclusive lanes, 50% of overlapping left turn

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**INTERSECTION CAPACITY UTILIZATION**

N-S St: Wiley Canyon Road  
 E-W St: Lyons Avenue  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU6

Wiley Canyon Road @ Lyons Avenue  
 Peak hr: AM  
 Annual Growth: 0.00%  
 Date: 02/08/2016  
 Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION					
	1 Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio		
Nb Left	145	1750	0.083 *	0	145	1750	0.083 *	0	145	1750	0.083 *	0	145	1750	0.083 *		
Nb Thru	261	3500	0.074	-1	260	3500	0.074	4	265	3500	0.076	0	265	3500	0.076		
Nb Right [4]	179	1750	0.000	0	179	1750	0.000	0	179	1750	0.000	0	179	1750	0.000		
Sb Left	148	1750	0.085	0	148	1750	0.085	1	149	1750	0.085	0	149	1750	0.085		
Sb Thru	364	3500	0.104	0	364	3500	0.104	1	365	3500	0.104	0	365	3500	0.104		
Sb Right [3]	359	1750	0.169 *	0	359	1750	0.169 *	1	360	1750	0.169 *	0	360	1750	0.169 *		
Eb Left	129	3500	0.037	0	129	3500	0.037	0	129	3500	0.037	0	129	3500	0.037		
Eb Thru	620	5250	0.130 *	0	620	5250	0.130 *	0	620	5250	0.130 *	0	620	5250	0.130 *		
Eb Right	64	0	-	0	64	0	-	0	64	0	-	0	64	0	-		
Wb Left	186	1750	0.107 *	0	186	1750	0.107 *	0	186	1750	0.107 *	0	186	1750	0.107 *		
Wb Thru	794	5250	0.176	0	794	5250	0.176	0	794	5250	0.176	0	794	5250	0.176		
Wb Right	131	0	-	0	131	0	-	0	131	0	-	0	131	0	-		
Yellow Allowance:			0.100 *					0.100 *					0.100 *				
ICU	0.588			0.588				0.589				0.589					
LOS	A			A				A				A					

\*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green  
 3 The southbound right-turn lane has an overlapping phase with the eastbound left-turn phase.  
 4 Right turns on red from exclusive lanes, 50% of overlapping left turn

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**INTERSECTION CAPACITY UTILIZATION**

N-S St: Wiley Canyon Road  
 E-W St: Lyons Avenue  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU6

Wiley Canyon Road @ Lyons Avenue  
 Peak hr: PM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION						
	1 Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio			
Nb Left	166	1750	0.095	0	166	1750	0.095	0	166	1750	0.095	0	166	1750	0.095			
Nb Thru	396	3500	0.113 *	0	396	3500	0.113 *	0	396	3500	0.113 *	0	396	3500	0.113 *			
Nb Right [4]	222	1750	0.076	0	222	1750	0.076	0	222	1750	0.076	0	222	1750	0.076			
Sb Left	207	1750	0.118 *	-2	205	1750	0.117 *	5	212	1750	0.121 *	0	212	1750	0.121 *			
Sb Thru	319	3500	0.091	-2	317	3500	0.091	5	324	3500	0.093	0	324	3500	0.093			
Sb Right [3]	244	1750	0.034	-2	242	1750	0.032	5	249	1750	0.036	0	249	1750	0.036			
Eb Left	371	3500	0.106	0	371	3500	0.106	0	371	3500	0.106	0	371	3500	0.106			
Eb Thru	1087	5250	0.222 *	0	1087	5250	0.222 *	0	1087	5250	0.222 *	0	1087	5250	0.222 *			
Eb Right	81	0	-	0	81	0	-	0	81	0	-	0	81	0	-			
Wb Left	179	1750	0.102 *	0	179	1750	0.102 *	0	179	1750	0.102 *	0	179	1750	0.102 *			
Wb Thru	879	5250	0.189	0	879	5250	0.189	0	879	5250	0.190	0	879	5250	0.190			
Wb Right	113	0	-	-1	112	0	-	3	116	0	-	0	116	0	-			
Yellow Allowance:			0.100 *					0.100 *					0.100 *					0.100 *
ICU	0.656			0.655				0.659				0.659						
LOS	B			B				B				B						

\*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green  
 3 The southbound right-turn lane has an overlapping phase with the eastbound left-turn phase.  
 4 Right turns on red from exclusive lanes, 50% of overlapping left turn

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**INTERSECTION CAPACITY UTILIZATION**

N-S St: Tournament Road  
 E-W St: Wiley Canyon Road  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU7

Tournament Road @ Wiley Canyon Road  
 Peak hr: AM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION					
	1 Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio		
Nb Left	16	0	0.009 *	0	16	0	0.009 *	0	16	0	0.009 *	0	16	0	0.009 *		
Nb Thru	20	1750	0.037	-1	19	1750	0.037	4	24	1750	0.039	0	24	1750	0.039		
Nb Right	29	0	-	0	29	0	-	0	29	0	-	0	29	0	-		
Sb Left	111	0	0.064	0	111	0	0.064	0	111	0	0.064	0	111	0	0.064		
Sb Thru	5	1750	0.067	0	5	1750	0.067	1	6	1750	0.067	0	6	1750	0.067		
Sb Right [3]	176	1750	0.099 *	0	176	1750	0.099 *	2	178	1750	0.099 *	0	178	1750	0.099 *		
Eb Left	6	1750	0.004	-1	5	1750	0.003	4	10	1750	0.006	0	10	1750	0.006		
Eb Thru	733	3500	0.241 *	0	733	3500	0.241 *	0	733	3500	0.241 *	0	733	3500	0.241 *		
Eb Right	111	0	-	0	111	0	-	0	111	0	-	0	111	0	-		
Wb Left	116	1750	0.067 *	0	116	1750	0.067 *	0	116	1750	0.067 *	0	116	1750	0.067 *		
Wb Thru	406	3500	0.117	0	406	3500	0.117	1	407	3500	0.118	0	407	3500	0.118		
Wb Right	5	0	-	0	5	0	-	0	5	0	-	0	5	0	-		
Yellow Allowance:			0.100 *					0.100 *					0.100 *				
ICU			0.516					0.516					0.516				
LOS			A					A					A				

11:44 AM

\*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green  
 3 Right turns on red from exclusive lanes, 50% of overlapping left turn



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**INTERSECTION CAPACITY UTILIZATION**

N-S St: Tournament Road  
 E-W St: Wiley Canyon Road  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU7

Tournament Road @ Wiley Canyon Road  
 Peak hr: PM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION						
	Volume	Capacity	V/C	Added Volume	Total Volume	Capacity	V/C	Added Volume	Total Volume	Capacity	V/C	Added Volume	Total Volume	Capacity	V/C			
Nb Left	10	0	0.006 *	0	10	0	0.006 *	0	10	0	0.006 *	0	10	0	0.006 *			
Nb Thru	10	1750	0.019	-1	9	1750	0.019	2	12	1750	0.021	0	12	1750	0.021			
Nb Right	13	0	-	0	13	0	-	0	13	0	-	0	13	0	-			
Sb Left	71	0	0.041	0	71	0	0.041	0	71	0	0.041	0	71	0	0.041			
Sb Thru	18	1750	0.051	-2	16	1750	0.049	5	23	1750	0.053	0	23	1750	0.053			
Sb Right [3]	239	1750	0.133 *	-4	235	1750	0.131 *	10	249	1750	0.138 *	0	249	1750	0.138 *			
Eb Left	13	1750	0.008	-1	12	1750	0.007	2	15	1750	0.009	0	15	1750	0.009			
Eb Thru	507	3500	0.166 *	0	507	3500	0.166 *	0	507	3500	0.166 *	0	507	3500	0.166 *			
Eb Right	73	0	-	0	73	0	-	0	73	0	-	0	73	0	-			
Wb Left	205	1750	0.117 *	0	205	1750	0.117 *	0	205	1750	0.117 *	0	205	1750	0.117 *			
Wb Thru	674	3500	0.197	-2	672	3500	0.196	5	679	3500	0.198	0	679	3500	0.198			
Wb Right	15	0	-	0	15	0	-	0	15	0	-	0	15	0	-			
Yellow Allowance:			0.100 *					0.100 *					0.100 *					0.100 *
ICU			0.521					0.519					0.527					0.527
LOS			A					A					A					A

\*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green  
 3 Right turns on red from exclusive lanes, 50% of overlapping left turn

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**INTERSECTION CAPACITY UTILIZATION**

N-S St: Orchard Village Road  
 E-W St: Lyons Avenue  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU8

Orchard Village Road @ Lyons Avenue  
 Peak hr: AM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION					
	1 Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio		
Nb Left	69	1750	0.039	0	69	1750	0.039	0	69	1750	0.039	0	69	1750	0.039		
Nb Thru	127	3500	0.036 *	-3	124	3500	0.035 *	8	135	3500	0.038 *	0	135	3500	0.038 *		
Nb Right [4]	80	1750	0.026	0	80	1750	0.026	0	80	1750	0.026	0	80	1750	0.026		
Sb Left	386	3500	0.110 *	-2	384	3500	0.110 *	7	393	3500	0.112 *	0	393	3500	0.112 *		
Sb Thru	116	1750	0.067	0	116	1750	0.067	1	117	1750	0.067	0	117	1750	0.067		
Sb Right [4]	212	1750	0.093	0	212	1750	0.093	1	213	1750	0.093	0	213	1750	0.093		
Eb Left	196	3500	0.056 *	-1	195	3500	0.056 *	4	200	3500	0.057 *	0	200	3500	0.057 *		
Eb Thru	598	3500	0.171	0	598	3500	0.171	0	598	3500	0.171	0	598	3500	0.171		
Eb Right [4]	57	1750	0.000	0	57	1750	0.000	0	57	1750	0.000	0	57	1750	0.000		
Wb Left	69	1750	0.039	0	69	1750	0.039	0	69	1750	0.039	0	69	1750	0.039		
Wb Thru	832	5250	0.159 *	0	832	5250	0.159 *	0	832	5250	0.159 *	0	832	5250	0.159 *		
Wb Right [3]	409	1750	0.123	-9	400	1750	0.119	28	437	1750	0.137	0	437	1750	0.137		
Yellow Allowance:			0.100 *					0.100 *					0.100 *				
ICU	0.461			0.459				0.466				0.466					
LOS	A			A				A				A					

11:44 AM

- \*Key conflicting movement as a part of ICU
- 1 Counts conducted by: National Data and Surveying Services
- 2 Capacity expressed in veh/hour of green
- 3 The westbound right-turn lane has an overlapping phase with the southbound left-turn phase.
- 4 Right turns on red from exclusive lanes, 50% of overlapping left turn

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**INTERSECTION CAPACITY UTILIZATION**

N-S St: Orchard Village Road  
 E-W St: Lyons Avenue  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU8

Orchard Village Road @ Lyons Avenue  
 Peak hr: PM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION					
	Volume	Capacity	V/C	Added Volume	Total Volume	Capacity	V/C	Added Volume	Total Volume	Capacity	V/C	Added Volume	Total Volume	Capacity	V/C		
Nb Left	139	1750	0.079 *	0	139	1750	0.079 *	0	139	1750	0.079 *	0	139	1750	0.079 *		
Nb Thru	307	3500	0.088	-1	306	3500	0.087	3	310	3500	0.089	0	310	3500	0.089		
Nb Right [4]	100	1750	0.036	0	100	1750	0.036	0	100	1750	0.036	0	100	1750	0.036		
Sb Left	410	3500	0.117	-12	398	3500	0.114	36	446	3500	0.127	0	446	3500	0.127		
Sb Thru	336	1750	0.192 *	-2	334	1750	0.191 *	5	341	1750	0.195 *	0	341	1750	0.195 *		
Sb Right [4]	224	1750	0.090	-2	222	1750	0.089	5	229	1750	0.093	0	229	1750	0.093		
Eb Left	262	3500	0.075	-1	261	3500	0.074	2	264	3500	0.075	0	264	3500	0.075		
Eb Thru	1028	3500	0.294 *	0	1028	3500	0.294 *	0	1028	3500	0.294 *	0	1028	3500	0.294 *		
Eb Right [4]	116	1750	0.000	0	116	1750	0.000	0	116	1750	0.000	0	116	1750	0.000		
Wb Left	74	1750	0.042 *	0	74	1750	0.042 *	0	74	1750	0.042 *	0	74	1750	0.042 *		
Wb Thru	911	5250	0.173	0	911	5250	0.173	0	911	5250	0.173	0	911	5250	0.173		
Wb Right [3]	389	1750	0.105	-3	386	1750	0.107	10	399	1750	0.101	0	399	1750	0.101		
Yellow Allowance:			0.100 *					0.100 *					0.100 *				
ICU			0.707					0.706					0.710				
LOS			C					C					C				

\*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green  
 3 The westbound right-turn lane has an overlapping phase with the southbound left-turn phase.  
 4 Right turns on red from exclusive lanes, 50% of overlapping left turn

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**INTERSECTION CAPACITY UTILIZATION**

N-S St: Orchard Village Road  
 E-W St: Wiley Canyon Road  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU9

Orchard Village Road @ Wiley Canyon Road  
 Peak hr: AM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION			
	1 Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio
Nb Left	240	1750	0.137 *	0	240	1750	0.137 *	0	240	1750	0.137 *	0	240	1750	0.137 *
Nb Thru	884	3500	0.252	-22	862	3500	0.246	69	953	3500	0.272	0	953	3500	0.272
Nb Right [4]	141	1750	0.000	0	141	1750	0.000	0	141	1750	0.000	0	141	1750	0.000
Sb Left	66	1750	0.038	-2	64	1750	0.037	10	76	1750	0.043	0	76	1750	0.043
Sb Thru	655	3500	0.187 *	-2	653	3500	0.187 *	11	666	3500	0.190 *	0	666	3500	0.190 *
Sb Right [3]	372	1750	0.150	0	372	1750	0.150	1	373	1750	0.150	0	373	1750	0.150
Eb Left	219	3500	0.063	0	219	3500	0.063	0	219	3500	0.063	0	219	3500	0.063
Eb Thru	174	3500	0.150 *	0	174	3500	0.150 *	0	174	3500	0.150 *	0	174	3500	0.150 *
Eb Right	350	0	-	0	350	0	-	0	350	0	-	0	350	0	-
Wb Left	176	1750	0.101 *	0	176	1750	0.101 *	0	176	1750	0.101 *	0	176	1750	0.101 *
Wb Thru	267	3500	0.076	0	267	3500	0.076	0	267	3500	0.076	0	267	3500	0.076
Wb Right [4]	102	1750	0.039	-22	80	1750	0.027	69	171	1750	0.076	0	171	1750	0.076
Yellow Allowance:			0.100 *				0.100 *				0.100 *				0.100 *
ICU			0.675				0.674				0.678				0.678
LOS			B				B				B				B

11:44 AM

- \*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green  
 3 The southbound right-turn lane has an overlapping phase with the eastbound left-turn phase.  
 4 Right turns on red from exclusive lanes, 50% of overlapping left turn

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**INTERSECTION CAPACITY UTILIZATION**

N-S St: Orchard Village Road  
 E-W St: Wiley Canyon Road  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU9

Orchard Village Road @ Wiley Canyon Road  
 Peak hr: PM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION					
	Volume	Capacity	V/C	Added Volume	Total Volume	Capacity	V/C	Added Volume	Total Volume	Capacity	V/C	Added Volume	Total Volume	Capacity	V/C		
Nb Left	131	1750	0.075 *	0	131	1750	0.075 *	0	131	1750	0.075	0	131	1750	0.075		
Nb Thru	848	3500	0.242	-6	842	3500	0.240	20	868	3500	0.248 *	0	868	3500	0.248 *		
Nb Right [4]	142	1750	0.047	0	142	1750	0.047	0	142	1750	0.047	0	142	1750	0.047		
Sb Left	101	1750	0.058	-26	75	1750	0.043	77	178	1750	0.102 *	0	178	1750	0.102 *		
Sb Thru	866	3500	0.247 *	-26	840	3500	0.240 *	77	943	3500	0.269	0	943	3500	0.269		
Sb Right [3]	218	1750	0.049	-2	216	1750	0.047	5	223	1750	0.051	0	223	1750	0.051		
Eb Left	267	3500	0.076	0	267	3500	0.076	0	267	3500	0.076	0	267	3500	0.076		
Eb Thru	288	3500	0.128 *	0	288	3500	0.128 *	0	288	3500	0.128 *	0	288	3500	0.128 *		
Eb Right	159	0	-	0	159	0	-	0	159	0	-	0	159	0	-		
Wb Left	118	1750	0.068 *	0	118	1750	0.068 *	0	118	1750	0.068 *	0	118	1750	0.068 *		
Wb Thru	231	3500	0.066	0	231	3500	0.066	0	231	3500	0.066	0	231	3500	0.066		
Wb Right [4]	76	1750	0.000	-6	70	1750	0.000	18	94	1750	0.000	0	94	1750	0.000		
Yellow Allowance:			0.100 *					0.100 *					0.100 *				
ICU			0.618					0.610					0.645				
LOS			B					B					B				

\*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green  
 3 The southbound right-turn lane has an overlapping phase with the eastbound left-turn phase.  
 4 Right turns on red from exclusive lanes, 50% of overlapping left turn

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**INTERSECTION CAPACITY UTILIZATION**

N-S St: Orchard Village Road  
 E-W St: McBean Parkway  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU10

Orchard Village Road @ McBean Parkway  
 Peak hr: AM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION					
	1 Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio		
Nb Left	403	0	0.115	0	403	0	0.115 *	0	403	0	0.115	0	403	0	0.115		
Nb Thru	43	3500	0.127 *	-46	-3	3500	0.114	141	184	3500	0.168 *	0	184	3500	0.168 *		
Nb Right [4]	690	3500	0.000	0	690	3500	0.000	0	690	3500	0.000	0	690	3500	0.000		
Sb Left	13	1750	0.008	-2	11	1750	0.007	10	23	1750	0.013	0	23	1750	0.013		
Sb Thru	33	1750	0.019 *	-5	28	1750	0.016 *	23	56	1750	0.032 *	0	56	1750	0.032 *		
Sb Right	19	1750	0.011	-5	14	1750	0.008	22	41	1750	0.023	0	41	1750	0.023		
Eb Left	15	1750	0.009	-17	-2	1750	-0.001	53	68	1750	0.039	0	68	1750	0.039		
Eb Thru	445	5250	0.148 *	-12	433	5250	0.145 *	36	481	5250	0.155 *	0	481	5250	0.155 *		
Eb Right	331	0	-	0	331	0	-	0	331	0	-	0	331	0	-		
Wb Left	705	3500	0.201 *	0	705	3500	0.201 *	0	705	3500	0.201 *	0	705	3500	0.201 *		
Wb Thru	596	5250	0.120	-6	590	5250	0.117	23	619	5250	0.129	0	619	5250	0.129		
Wb Right	32	0	-	-9	23	0	-	28	60	0	-	0	60	0	-		
Yellow Allowance:			0.100 *					0.100 *					0.100 *				
ICU	0.595			0.578				0.656				0.656					
LOS	A			A				B				B					

\*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green  
 3 Northbound and southbound operate with split phasing.  
 4 The northbound right-turn lane has an overlapping phase with the westbound left-turn phase.

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**INTERSECTION CAPACITY UTILIZATION**

N-S St: Orchard Village Road  
 E-W St: McBean Parkway  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU10

Orchard Village Road @ McBean Parkway  
 Peak hr: PM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION						
	Volume	Capacity	V/C	Added Volume	Total Volume	Capacity	V/C	Added Volume	Total Volume	Capacity	V/C	Added Volume	Total Volume	Capacity	V/C			
Nb Left	264	0	0.075	0	264	0	0.075	0	264	0	0.075	0	264	0	0.075			
Nb Thru	23	3500	0.082 *	-12	11	3500	0.078 *	40	63	3500	0.093 *	0	63	3500	0.093 *			
Nb Right [4]	878	3500	0.033	0	878	3500	0.033	0	878	3500	0.033	0	878	3500	0.033			
Sb Left	55	1750	0.031	-18	37	1750	0.021 *	52	107	1750	0.061	0	107	1750	0.061			
Sb Thru	62	1750	0.035 *	-55	7	1750	0.004	160	222	1750	0.127 *	0	222	1750	0.127 *			
Sb Right	49	1750	0.028	-19	30	1750	0.017	57	106	1750	0.061	0	106	1750	0.061			
Eb Left	11	1750	0.006	-6	5	1750	0.003	20	31	1750	0.018	0	31	1750	0.018			
Eb Thru	915	5250	0.253 *	-5	910	5250	0.252 *	17	932	5250	0.256 *	0	932	5250	0.256 *			
Eb Right	413	0	-	0	413	0	-	0	413	0	-	0	413	0	-			
Wb Left	763	3500	0.218 *	0	763	3500	0.218 *	0	763	3500	0.218 *	0	763	3500	0.218 *			
Wb Thru	659	5250	0.131	-21	638	5250	0.126	61	720	5250	0.144	0	720	5250	0.144			
Wb Right	28	0	-	-3	25	0	-	10	38	0	-	0	38	0	-			
Yellow Allowance:			0.100 *					0.100 *					0.100 *					0.100 *
ICU	0.688			0.669				0.794				0.794						
LOS	B			B				C				C						

\*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green  
 3 Northbound and southbound operate with split phasing.  
 4 The northbound right-turn lane has an overlapping phase with the westbound left-turn phase.

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**INTERSECTION CAPACITY UTILIZATION**

N-S St: Newhall Avenue  
 E-W St: Lyons Avenue  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU11

Newhall Avenue @ Lyons Avenue  
 Peak hr: AM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION					
	1 Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio		
Nb Left	383	3500	0.109 *	-5	378	3500	0.108 *	16	399	3500	0.114 *	0	399	3500	0.114 *		
Nb Thru	145	1750	0.083	0	145	1750	0.083	0	145	1750	0.083	0	145	1750	0.083		
Nb Right [4]	39	1750	0.011	0	39	1750	0.011	0	39	1750	0.011	0	39	1750	0.011		
Sb Left	29	1750	0.016	0	29	1750	0.016	0	29	1750	0.016	0	29	1750	0.016		
Sb Thru	191	1750	0.109 *	0	191	1750	0.109 *	0	191	1750	0.109 *	0	191	1750	0.109 *		
Sb Right [4]	202	1750	0.079	0	202	1750	0.079	0	202	1750	0.079	0	202	1750	0.079		
Eb Left	129	1750	0.074 *	0	129	1750	0.074 *	0	129	1750	0.074 *	0	129	1750	0.074 *		
Eb Thru	450	3500	0.129	0	450	3500	0.129	2	452	3500	0.129	0	452	3500	0.129		
Eb Right [3]	374	1750	0.104	-1	373	1750	0.105	3	377	1750	0.101	0	377	1750	0.101		
Wb Left	38	1750	0.022	0	38	1750	0.022	0	38	1750	0.022	0	38	1750	0.022		
Wb Thru	713	3500	0.204 *	-3	710	3500	0.203 *	8	721	3500	0.206 *	0	721	3500	0.206 *		
Wb Right [4]	22	1750	0.000	0	22	1750	0.000	0	22	1750	0.000	0	22	1750	0.000		
Yellow Allowance:			0.100 *					0.100 *					0.100 *				
ICU	0.596			0.593				0.602				0.602					
LOS	A			A				B				B					

02:20 PM

- \*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green  
 3 The eastbound right-turn lane has an overlapping phase with the northbound left-turn phase.  
 4 Right turns on red from exclusive lanes, 50% of overlapping left turn



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**INTERSECTION CAPACITY UTILIZATION**

N-S St: Newhall Avenue  
 E-W St: Lyons Avenue  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU11

Newhall Avenue @ Lyons Avenue  
 Peak hr: PM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION					
	1 Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio		
Nb Left	488	3500	0.139 *	-1	487	3500	0.139 *	2	490	3500	0.140 *	0	490	3500	0.140 *		
Nb Thru	97	1750	0.055	0	97	1750	0.055	0	97	1750	0.055	0	97	1750	0.055		
Nb Right [4]	19	1750	0.000	0	19	1750	0.000	0	19	1750	0.000	0	19	1750	0.000		
Sb Left	23	1750	0.013	0	23	1750	0.013	0	23	1750	0.013	0	23	1750	0.013		
Sb Thru	111	1750	0.064 *	0	111	1750	0.064 *	0	111	1750	0.064 *	0	111	1750	0.064 *		
Sb Right [4]	109	1750	0.000	0	109	1750	0.000	0	109	1750	0.000	0	109	1750	0.000		
Eb Left	143	1750	0.082 *	0	143	1750	0.082 *	0	143	1750	0.082 *	0	143	1750	0.082 *		
Eb Thru	718	3500	0.205	-4	714	3500	0.204	10	728	3500	0.208	0	728	3500	0.208		
Eb Right [3]	655	1750	0.235	-5	650	1750	0.232	15	670	1750	0.243	0	670	1750	0.243		
Wb Left	25	1750	0.014	0	25	1750	0.014	0	25	1750	0.014	0	25	1750	0.014		
Wb Thru	677	3500	0.193 *	-2	675	3500	0.193 *	5	682	3500	0.195 *	0	682	3500	0.195 *		
Wb Right [4]	24	1750	0.007	0	24	1750	0.007	0	24	1750	0.007	0	24	1750	0.007		
Yellow Allowance:			0.100 *					0.100 *					0.100 *				
ICU			0.578					0.577					0.580				
LOS			A					A					A				

\*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green  
 3 The eastbound right-turn lane has an overlapping phase with the northbound left-turn phase.  
 4 Right turns on red from exclusive lanes, 50% of overlapping left turn

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**INTERSECTION CAPACITY UTILIZATION**

N-S St: Valencia Boulevard  
 E-W St: Magic Mountain Parkway  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU12

Valencia Boulevard @ Magic Mountain Parkway  
 Peak hr: AM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION			
	1 Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio
Nb Left	26	1750	0.015	0	26	1750	0.015	0	26	1750	0.015	0	26	1750	0.015
Nb Thru	1733	5250	0.417 *	-1	1732	5250	0.416 *	4	1737	5250	0.417 *	0	1737	5250	0.417 *
Nb Right	453	0	-	0	453	0	-	0	453	0	-	0	453	0	-
Sb Left	28	1750	0.016 *	0	28	1750	0.016 *	0	28	1750	0.016 *	0	28	1750	0.016 *
Sb Thru	781	5250	0.149	-9	772	5250	0.147	28	809	5250	0.154	0	809	5250	0.154
Sb Right [3]	105	3500	0.000	0	105	3500	0.000	0	105	3500	0.000	0	105	3500	0.000
Eb Left	166	3500	0.047	0	166	3500	0.047	0	166	3500	0.047	0	166	3500	0.047
Eb Thru	378	3500	0.127 *	0	378	3500	0.127 *	0	378	3500	0.127 *	0	378	3500	0.127 *
Eb Right	67	0	-	0	67	0	-	0	67	0	-	0	67	0	-
Wb Left	126	1750	0.072 *	-3	123	1750	0.070 *	8	134	1750	0.076 *	0	134	1750	0.076 *
Wb Thru	180	3500	0.052	0	180	3500	0.052	0	180	3500	0.052	0	180	3500	0.052
Wb Right [4]	30	1750	0.009	0	30	1750	0.009	0	30	1750	0.009	0	30	1750	0.009
Yellow Allowance:			0.100 *				0.100 *				0.100 *				0.100 *
ICU			0.731				0.729				0.737				0.737
LOS			C				C				C				C

11:44 AM

- \*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green  
 3 The southbound right-turn lane has an overlapping phase with the eastbound left-turn phase.  
 4 Right turns on red from exclusive lanes, 50% of overlapping left turn

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**INTERSECTION CAPACITY UTILIZATION**

N-S St: Valencia Boulevard  
 E-W St: Magic Mountain Parkway  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU12

Valencia Boulevard @ Magic Mountain Parkway  
 Peak hr: PM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION							
	1 Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio				
Nb Left	112	1750	0.064 *	0	112	1750	0.064 *	0	112	1750	0.064 *	0	112	1750	0.064 *				
Nb Thru	1264	5250	0.332	-9	1255	5250	0.329	26	1290	5250	0.339	0	1290	5250	0.339				
Nb Right	477	0	-	-5	472	0	-	15	492	0	-	0	492	0	-				
Sb Left	68	1750	0.039	0	68	1750	0.039	0	68	1750	0.039	0	68	1750	0.039				
Sb Thru	1782	5250	0.339 *	-3	1779	5250	0.339 *	8	1790	5250	0.341 *	0	1790	5250	0.341 *				
Sb Right [3]	211	3500	0.000	0	211	3500	0.000	0	211	3500	0.000	0	211	3500	0.000				
Eb Left	239	3500	0.068	0	239	3500	0.068	0	239	3500	0.068	0	239	3500	0.068				
Eb Thru	448	3500	0.150 *	0	448	3500	0.150 *	0	448	3500	0.150 *	0	448	3500	0.150 *				
Eb Right	78	0	-	0	78	0	-	0	78	0	-	0	78	0	-				
Wb Left	627	1750	0.358 *	-1	626	1750	0.358 *	3	630	1750	0.360 *	0	630	1750	0.360 *				
Wb Thru	522	3500	0.149	0	522	3500	0.149	0	522	3500	0.149	0	522	3500	0.149				
Wb Right [4]	38	1750	0.000	0	38	1750	0.000	0	38	1750	0.000	0	38	1750	0.000				
Yellow Allowance:			0.100 *					0.100 *					0.100 *					0.100 *	
ICU	1.012							1.011				1.016				1.016			
LOS	F							F				F				F			

\*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green  
 3 The southbound right-turn lane has an overlapping phase with the eastbound left-turn phase.  
 4 Right turns on red from exclusive lanes, 50% of overlapping left turn

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**INTERSECTION CAPACITY UTILIZATION**

N-S St: Avenida Navarre  
 E-W St: McBean Parkway  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU13

Avenida Navarre @ McBean Parkway  
 Peak hr: AM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION			
	1 Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio
Nb Left	248	1750	0.142 *	0	248	1750	0.142 *	0	248	1750	0.142 *	0	248	1750	0.142 *
Nb Thru	14	1750	0.008	-1	13	1750	0.008	4	18	1750	0.011	0	18	1750	0.011
Nb Right [3]	60	1750	0.027	0	60	1750	0.027	0	60	1750	0.027	0	60	1750	0.027
Sb Left	71	1750	0.041	-2	69	1750	0.039	10	81	1750	0.046	0	81	1750	0.046
Sb Thru	1	1750	0.001 *	0	1	1750	0.001 *	1	2	1750	0.001 *	0	2	1750	0.001 *
Sb Right [3]	35	1750	0.000	-3	32	1750	0.000	15	50	1750	0.000	0	50	1750	0.000
Eb Left	126	1750	0.072 *	-12	114	1750	0.065 *	36	162	1750	0.092 *	0	162	1750	0.092 *
Eb Thru	958	5250	0.196	-2	956	5250	0.196	10	968	5250	0.198	0	968	5250	0.198
Eb Right	73	0	-	0	73	0	-	0	73	0	-	0	73	0	-
Wb Left	26	1750	0.015	0	26	1750	0.015	0	26	1750	0.015	0	26	1750	0.015
Wb Thru	1052	5250	0.232 *	-12	1040	5250	0.226 *	36	1088	5250	0.251 *	0	1088	5250	0.251 *
Wb Right	167	0	-	-21	146	0	-	65	232	0	-	0	232	0	-
Yellow Allowance:			0.100 *				0.100 *				0.100 *				0.100 *
ICU			0.546				0.533				0.587				0.587
LOS			A				A				A				A

11:43 AM

\*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green  
 3 Right turns on red from exclusive lanes, 50% of overlapping left turn

LINSCOTT, LAW & GREENSPAN, ENGINEERS  
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**INTERSECTION CAPACITY UTILIZATION**

N-S St: Avenida Navarre  
 E-W St: McBean Parkway  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU13

Avenida Navarre @ McBean Parkway  
 Peak hr: PM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION					
	1 Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio		
Nb Left	138	1750	0.079	0	138	1750	0.079 *	0	138	1750	0.079	0	138	1750	0.079		
Nb Thru	0	1750	0.000	-1	-1	1750	-0.001	2	2	1750	0.001 *	0	2	1750	0.001 *		
Nb Right [3]	76	1750	0.000	0	76	1750	0.000	0	76	1750	0.000	0	76	1750	0.000		
Sb Left	164	1750	0.094	-28	136	1750	0.078	82	246	1750	0.140 *	0	246	1750	0.140 *		
Sb Thru	12	1750	0.007	-4	8	1750	0.005 *	10	22	1750	0.013	0	22	1750	0.013		
Sb Right [3]	0	1750	0.000	-18	-18	1750	0.000	52	52	1750	0.018	0	52	1750	0.018		
Eb Left	25	1750	0.014	-5	20	1750	0.011	17	42	1750	0.024	0	42	1750	0.024		
Eb Thru	1637	5250	0.346 *	-18	1619	5250	0.342 *	52	1689	5250	0.356 *	0	1689	5250	0.356 *		
Eb Right	179	0	-	0	179	0	-	0	179	0	-	0	179	0	-		
Wb Left	99	1750	0.057 *	0	99	1750	0.057 *	0	99	1750	0.057 *	0	99	1750	0.057 *		
Wb Thru	1317	5250	0.259	-6	1311	5250	0.257	20	1337	5250	0.267	0	1337	5250	0.267		
Wb Right	42	0	-	-6	36	0	-	20	62	0	-	0	62	0	-		
Yellow Allowance:			0.100 *					0.100 *					0.100 *				
ICU			0.596					0.583					0.654				
LOS			A					A					B				

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**INTERSECTION CAPACITY UTILIZATION**

N-S St: West Driveway  
 E-W St: McBean Parkway  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU14

West Driveway @ McBean Parkway  
 Peak hr: AM  
 Annual Growth: 0.00%  
 Date: 02/08/2016  
 Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION			
	1 Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio	Added Volume	Total Volume	2 Capacity	V/C Ratio
Nb Left	0	0	0.000 *	0	0	0	0.000 *	0	0	0	0.000 *	0	0	0	0.000 *
Nb Thru	0	0	0.000	0	0	0	0.000	0	0	0	0.000	0	0	0	0.000
Nb Right	0	0	-	0	0	0	-	0	0	0	-	0	0	0	-
Sb Left	3	0	0.002	0	3	0	0.002	0	3	0	0.002	0	3	0	0.002
Sb Thru	0	1750	0.023 *	0	0	1750	0.022 *	0	0	1750	0.028 *	0	0	1750	0.028 *
Sb Right	37	0	-	-2	35	0	-	8	45	0	-	0	45	0	-
Eb Left	95	1750	0.054 *	-22	73	1750	0.042 *	69	164	1750	0.094 *	0	164	1750	0.094 *
Eb Thru	805	5250	0.153	-28	777	5250	0.148	85	890	5250	0.170	0	890	5250	0.170
Eb Right	0	0	-	0	0	0	-	0	0	0	-	0	0	0	-
Wb Left	0	0	0.000	0	0	0	0.000	0	0	0	0.000	0	0	0	0.000
Wb Thru	975	5250	0.193 *	-8	967	5250	0.191 *	37	1012	5250	0.202 *	0	1012	5250	0.202 *
Wb Right	38	0	-	-3	35	0	-	8	46	0	-	0	46	0	-
Yellow Allowance:			0.100 *				0.100 *				0.100 *				0.100 *
ICU			0.370				0.354				0.423				0.423
LOS			A				A				A				A

\*Key conflicting movement as a part of ICU  
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**INTERSECTION CAPACITY UTILIZATION**

N-S St: West Driveway  
 E-W St: McBean Parkway  
 Project: 5-15-0159-1 Henry Mayo Update  
 File: ICU14

West Driveway @ McBean Parkway  
 Peak hr: PM Date: 02/08/2016  
 Annual Growth: 0.00% Date of Count: 2016  
 Projection Year: 2016

Movement	2016 EXIST. TRAFFIC			2016 EXIST. W/O BUILT PROJECT				2016 W/ FULL REVISED PROJECT				2016 W/ MITIGATION						
	Volume	Capacity	V/C Ratio	Added Volume	Total Volume	Capacity	V/C Ratio	Added Volume	Total Volume	Capacity	V/C Ratio	Added Volume	Total Volume	Capacity	V/C Ratio			
Nb Left	0	0	0.000 *	0	0	0	0.000 *	0	0	0	0.000 *	0	0	0	0.000 *			
Nb Thru	0	0	0.000	0	0	0	0.000	0	0	0	0.000	0	0	0	0.000			
Nb Right	0	0	-	0	0	0	-	0	0	0	-	0	0	0	-			
Sb Left	9	0	0.005	0	9	0	0.005	0	9	0	0.005	0	9	0	0.005			
Sb Thru	0	1750	0.047 *	0	0	1750	0.027 *	0	0	1750	0.106 *	0	0	1750	0.106 *			
Sb Right	73	0	-	-35	38	0	-	103	176	0	-	0	176	0	-			
Eb Left	11	1750	0.006	-14	-3	1750	-0.002	46	57	1750	0.033	0	57	1750	0.033			
Eb Thru	1323	5250	0.252 *	-11	1312	5250	0.250 *	36	1359	5250	0.259 *	0	1359	5250	0.259 *			
Eb Right	0	0	-	0	0	0	-	0	0	0	-	0	0	0	-			
Wb Left	0	0	0.000 *	0	0	0	0.000 *	0	0	0	0.000 *	0	0	0	0.000 *			
Wb Thru	950	5250	0.181	-37	913	5250	0.174	108	1058	5250	0.204	0	1058	5250	0.204			
Wb Right	3	0	-	-3	0	0	-	10	13	0	-	0	13	0	-			
Yellow Allowance:			0.100 *					0.100 *					0.100 *					0.100 *
ICU			0.399					0.377					0.465					0.465
LOS			A					A					A					A

\*Key conflicting movement as a part of ICU  
 1 Counts conducted by: National Data and Surveying Services  
 2 Capacity expressed in veh/hour of green





**APPENDIX C**  
*General Plan Consistency*



## APPENDIX C

### General Plan Consistency

**Table C-1**  
**Project Consistency with Applicable General Plan Goals and Policies**

City of Santa Clarita General Plan Implementing Goals and Policies	Consistency of Proposed Project
<i>Land Use Element</i>	
<p><b>Policy LU 1.1.4:</b> Preserve community character by maintaining natural features that act as natural boundaries between developed areas, including significant ridgelines, canyons, rivers and drainage courses, riparian areas, topographical features, habitat preserves, or other similar features, where appropriate.</p>	<p><b>Consistent.</b> The Specific Plan does not allow for any development within the setback areas, which provide natural buffers between the Specific Plan area and the adjacent residential uses.</p>
<p><b>Policy LU 1.1.5:</b> Increase infill development and re-use of underutilized sites within and adjacent to developed urban areas to achieve maximum benefit from existing infrastructure and minimize loss of open space, through redesignation of vacant sites for higher density and mixed use, where appropriate.</p>	<p><b>Consistent.</b> The HMNH Specific Plan allows for the expansion of hospital and medical office uses within the existing hospital campus thereby maximizing the land's utilization for these types of functions and includes structured parking to accommodate additional development within surface parking lots, thereby maximizing benefit from existing infrastructure.</p>
<p><b>Goal LU 6:</b> A scenic and beautiful urban environment that builds on the community's history and natural setting.</p>	<p><b>Consistent.</b> The HMNH Specific Plan includes development regulations and design guidelines for new development that focus on consistency and compatibility with the existing development on the site and in the surrounding area. All future development will be subject to approval by the City to ensure consistency with the Specific Plan.</p>
<p><b>Policy LU 6.3.1:</b> Promote planting of street trees throughout urban areas in the Santa Clarita Valley.</p>	<p><b>Consistent.</b> The HMNH Specific Plan provides for the preservation of existing street trees, as well as the planting of new and replacement street trees. The Specific Plan will continue to implement the approved Landscape Plan from the 2008 Master Plan, which provides a conceptual overview of future landscaping and street trees within the Specific Plan area.</p>
<p><b>Policy LU 6.5.1:</b> Require use of high quality, durable, and natural-appearing building materials pursuant to applicable ordinances.</p> <p><b>Policy LU 6.5.2:</b> Encourage the use of designs and architectural styles that incorporate classic and timeless architectural features.</p> <p><b>Policy LU 6.5.3:</b> Require architectural enhancement and articulation on all sides of buildings (360 degree architecture), with special consideration at building entrances and corners, and along facades adjacent to major arterial streets.</p> <p><b>Policy LU 6.5.4:</b> Evaluate new development in consideration of its context, to ensure that buildings create a coherent living environment, a cohesive urban fabric, and contribute to a sense of place consistent with the surrounding neighborhoods.</p>	<p><b>Consistent.</b> The HMNH Specific Plan is characterized by a consistent design theme and building materials that complement on- and off-site development. The Specific Plan includes required building setbacks and step-backs from adjacent residential neighborhoods, architectural features on the building facades, landscaping design guidelines, and building height zones that soften the appearance of the campus from surrounding areas.</p>

## APPENDIX C (Continued)

**Table C-1**  
**Project Consistency with Applicable General Plan Goals and Policies**

City of Santa Clarita General Plan Implementing Goals and Policies	Consistency of Proposed Project
<b>Goal LU 7:</b> Environmentally responsible development through site planning, building design, waste reduction, and responsible stewardship of resources.	<b>Consistent.</b> The Specific Plan provides for environmentally responsible development that is sensitive to the environment, maximizes use of existing infrastructure, and meets City standards for site planning, architectural design, parking, and pedestrian access. Expanding healthcare services at this location increases the significance of this site to the region.
<b>Policy LU 7.1.1:</b> Require shade trees within parking lots and adjacent to buildings to reduce the heat island effect, in consideration of Fire Department fuel modification restrictions.	<b>Consistent.</b> The HMNH Specific Plan provides for the preservation of existing trees, as well as the planting of new and replacement trees. The Specific Plan will continue to implement the approved 2008 Master Plan Landscape Plan, which provides a conceptual overview of future landscaping and trees within the Specific Plan area, including retention of existing trees within the remaining parking lots and adjacent to buildings.
<b>Policy LU 7.1.3:</b> Encourage development of energy-efficient buildings, and discourage construction of new buildings for which energy efficiency cannot be demonstrated.	<b>Consistent.</b> The HMNH Specific Plan requires all buildings to be compliant with the California Office of Statewide Health Planning and Development (OSHPD) and California Building Standards Code energy efficiency standards. Southern California Edison is granting HMNH tax credits for successfully participating in their Savings by Design program by reducing its electrical demands in the new Inpatient Building.
<b>Policy LU 7.2.3:</b> Require that all new development proposals demonstrate a sufficient and sustainable water supply prior to approval.	<b>Consistent.</b> Section 5.17, Water Supply, of the HMNH Master Plan EIR and Addendum determined that adequate water supplies are available to serve buildout of the Specific Plan.
<b>Policy LU 7.3.2:</b> Maintain stormwater runoff onsite by directing drainage into rain gardens, natural landscaped swales, rain barrels, permeable areas, and use of drainage areas as design elements, where feasible and reasonable.	<b>Consistent.</b> Implementation of the Specific Plan would not result in increased local runoff. As part of the Inpatient Building approved under the 2008 Master Plan, a new catch basin will be installed for stormwater runoff per Standard Urban Stormwater Mitigation Plan (SUSMP) requirements.
<b>Policy LU 7.4.1:</b> Require the use of drought tolerant landscaping, native California plant materials, and evapotranspiration (smart) irrigation systems.	<b>Consistent.</b> The Specific Plan Landscape Plan focuses primarily on the use of native and drought-tolerant tree and plant species. Plants listed that are non-native or not drought-tolerant will be used sparingly and only in areas that require their unique properties. The use of grass turf will be limited. New irrigation systems will be designed, installed, operated, and maintained in conformance with the State Water Efficient Landscape Ordinance.
<b>Policy LU 7.4.2:</b> Require the use of low-flow fixtures in all non-residential development and residential development with five or more dwelling units, which may include but are not limited to water conserving shower heads, toilets, waterless urinals and motion-sensor faucets, and encourage use of such fixtures in building retrofits as appropriate.	<b>Consistent.</b> New development within the Specific Plan will be required to comply with OSHPD and the State Building Standards Code, which requires the use of low-flow fixtures and other water-conserving features.
<b>Policy LU 7.5.1:</b> Ensure that all new development provides adequate space for recycling receptacles and bins on site.	<b>Consistent.</b> New development within the Specific Plan will be required to comply with City standards for recycling

## APPENDIX C (Continued)

**Table C-1  
Project Consistency with Applicable General Plan Goals and Policies**

City of Santa Clarita General Plan Implementing Goals and Policies	Consistency of Proposed Project
	receptacles and bins on site.
<b>Policy LU 7.6.1:</b> Limit outdoor lighting levels to the minimum needed for safety and security, and encourage lower lighting levels when businesses are closed.	<b>Consistent.</b> All lighting must comply with Chapter 17.51.050, Outdoor Lighting Standards, of the City’s Unified Development Code, as well as State and local safety and illumination requirements. Although the campus is a 24-hour operation, the Specific Plan’s outdoor lighting will be designed to ensure that minimum lighting levels are provided for safety and security needs. Light pollution will be minimized as much as programmatically feasible.
<b>Policy LU 7.6.4:</b> Encourage site designs that protect oak trees, hillsides, and biological resources through creative solutions.	<b>Consistent.</b> The HMNH Specific Plan preserves the hillside between the hospital campus and the adjacent residential uses to serve as a natural buffer between the uses. The Specific Plan does not allow for any development within the buffer around the hospital campus, which is identified as Zone 1 in Figure 4-1, Building Height Zones, in the Specific Plan.
<b>Goal LU 8:</b> Equitable and convenient access to social, cultural, educational, civic, medical, and recreational facilities and opportunities for all residents.  <b>Policy LU 8.1.7:</b> Work with medical service providers to facilitate preservation and enhancement of health services, including the Santa Clarita Valley’s trauma center, provided applications are in conformance with applicable General Plan policies and environmental requirements.	<b>Consistent.</b> The HMNH Specific Plan provides for the preservation and expansion of the existing hospital and medical services to help meet the healthcare needs of all segments of the population. The HMNH medical campus currently includes the Santa Clarita Valley’s only trauma center.
<b>Goal LU 9:</b> Adequate public facilities and services, provided in a timely manner and in appropriate locations to serve existing and future residents and businesses.	<b>Consistent.</b> The Specific Plan seeks to provide adequate medical facilities and services to the public in a timely manner. The Specific Plan area is an appropriate location because the Specific Plan facilitates infill development on an existing campus to enhance medical services.
<b>Policy LU 9.1.1:</b> Ensure construction of adequate infrastructure to meet the needs of new development prior to occupancy.	<b>Consistent.</b> Sufficient existing infrastructure supports the campus and surrounding area. The Specific Plan does not require the extension of infrastructure systems to meet project-related demands and will contribute to future public infrastructure improvements through payment of any applicable development fees.
<b>Policy LU 9.2.1:</b> Ensure that the cost of extending new sewer infrastructure is fully borne by the new development that is served, and is not passed on to the existing community.  <b>Policy LU 9.2.2:</b> Require that all new development mitigates its impact on existing sewer capacity by upgrading facilities when warranted or payment of a fee to allow construction of new facilities when needed.  <b>Policy LU 9.2.5:</b> Cooperate with the development community to allow reimbursement for the cost of constructed sewer	<b>Consistent.</b> Existing sewer infrastructure is sufficient to support buildout of the Specific Plan. The Specific Plan does not require the expansion of the existing sewer system to meet project-related demands and will contribute to future public infrastructure improvements through payment of any applicable development fees.

## APPENDIX C (Continued)

**Table C-1  
Project Consistency with Applicable General Plan Goals and Policies**

City of Santa Clarita General Plan Implementing Goals and Policies	Consistency of Proposed Project
facilities with a capacity that exceeds what would be required to mitigate a project's own sewer impact.	
<i>Circulation Element</i>	
<b>Goal C 1:</b> An inter-connected network of circulation facilities that integrates all travel modes, provides viable alternatives to automobile use, and conforms with regional plans.	<b>Consistent.</b> Multiple modes of transit that meet the requirements of the American Disabilities Act (ADA) are currently provided on-site, and will continue as such.
<b>Policy C 1.1.1:</b> Reduce dependence on the automobile, particularly single-occupancy vehicle use, by providing safe and convenient access to transit, bikeways, and walkways.	<b>Consistent.</b> The Specific Plan includes a system of pedestrian linkages to promote non-vehicular mobility and access to and throughout the project site. Access to the Specific Plan is available via various alternative transportation facilities, including sidewalks and public transit facilities. There are two proposed bus stops to be located along the McBean Parkway frontage of the hospital campus.
<b>Policy C 2.5.2:</b> Ensure that new development is provided with adequate emergency and/or secondary access for purposes of evacuation and emergency response; require two points of ingress and egress for every subdivision or phase thereof, except as otherwise approved for small subdivisions where physical constraints preclude a second access point.	<b>Consistent.</b> The Specific Plan provides three access points to ensure adequate ingress and egress including ambulances and other emergency vehicles.
<i>Noise Element</i>	
<b>Policy N 1.1.4:</b> Control noise sources adjacent to residential, recreational, and community facilities, and those land uses classified as noise sensitive.	<b>Consistent.</b> The Specific Plan would not cause an increase in noise sources adjacent to noise sensitive land uses.
<i>Open Space and Conservation Element</i>	
<b>Goal CO.1:</b> A balance between the social and economic needs of Santa Clarita Valley residents and protection of the natural environment, so that these needs can be met in the present and in the future.	<b>Consistent.</b> The Specific Plan requires incorporation of conservation measures and technologies into the design of the buildings that meet State standards for building energy efficiency and water usage.
<b>Policy CO 1.5.5:</b> Promote concentration of urban uses within the center of the Santa Clarita Valley through incentives for infill development and rebuilding, in order to limit impacts to open space, habitats, watersheds, hillsides, and other components of the Valley's natural ecosystems.	<b>Consistent.</b> The Specific Plan facilitates infill development within the currently developed Specific Plan area and will not impact the undeveloped hillside.
<b>Policy CO 3.1.5:</b> Promote the use of site-appropriate native or adapted plant materials, and prohibit use of invasive or noxious plant species in landscape designs.	<b>Consistent.</b> The Specific Plan Landscape Plan builds upon the existing landscaping within the Specific Plan area and focuses primarily on the use of native and drought-tolerant tree and plant species. Plants listed that are non-native or not drought-tolerant will be used sparingly and only in areas that require their unique properties. The use of grass turf will be limited. New irrigation systems will be designed, installed, operated, and maintained in conformance with the State Water Efficient Landscape Ordinance.
<b>Policy CO 3.1.7:</b> Limit the use of turf-grass on development sites and promote the use of native or adapted plantings to promote biodiversity and natural habitat.	
<b>Policy CO 3.1.8:</b> On development sites, require tree planting to provide habitat and shade to reduce the heat island effect caused by pavement and buildings.	

## APPENDIX C (Continued)

**Table C-1  
Project Consistency with Applicable General Plan Goals and Policies**

City of Santa Clarita General Plan Implementing Goals and Policies	Consistency of Proposed Project
<b>Policy CO 3.6.1:</b> Minimize light trespass, sky-glow, glare, and other adverse impacts on the nocturnal ecosystem by limiting exterior lighting to the level needed for safety and comfort; reduce unnecessary lighting for landscaping and architectural purposes, and encourage reduction of lighting levels during non-business nighttime hours.	<b>Consistent.</b> All lighting must comply with Chapter 17.51.050, Outdoor Lighting Standards, of the City's Unified Development Code, as well as State and local safety and illumination requirements. The Specific Plan's outdoor lighting will be designed to ensure that lighting does not interfere with on-site or surrounding uses.
<b>Policy CO 4.1.3:</b> Require low water use landscaping in new residential subdivisions and other private development projects, including a reduction in the amount of turf-grass.	<b>Consistent.</b> The Specific Plan Landscape Plan builds upon the existing landscaping within the Specific Plan area and focuses primarily on the use of native and drought-tolerant tree and plant species. Plants listed that are non-native or not drought-tolerant will be used sparingly and only in areas that require their unique properties. The use of grass turf will be limited. New irrigation systems will be designed, installed, operated, and maintained in conformance with the State Water Efficient Landscape Ordinance.
<b>Policy CO 4.1.5:</b> Promote the use of low-flow and/or waterless plumbing fixtures and appliances in all new non-residential development and residential development of five or more dwelling units.	<b>Consistent.</b> The Specific Plan requires incorporation of conservation measures and technologies into the design of the buildings that meet State standards for building water usage.
<b>Policy CO 4.3.2:</b> On previously developed sites proposed for major alteration, provide stormwater management improvements to restore natural infiltration, as required by the reviewing authority.	<b>Consistent.</b> Implementation of the Specific Plan would not result in increased local runoff. As part of the Inpatient Building approved under the 2008 Master Plan, a new catch basin will be installed for stormwater runoff per Standard Urban Stormwater Mitigation Plan (SUSMP) requirements.
<b>Policy CO 4.3.7:</b> Reduce the amount of pollutants entering the Santa Clara River and its tributaries by capturing and treating stormwater runoff at the source, to the extent possible.	
<i>Safety Element</i>	
<b>Goal S 1:</b> Protection of public safety and property from hazardous geological conditions, including seismic rupture and ground shaking, soil instability, and related hazards.	<b>Consistent.</b> Development under the Specific Plan is required to meet or exceed all applicable state and local seismic safety design standards including OSHPD requirements.
<b>Policy S 3.1.3:</b> Require adequate fire flow as a condition of approval for all new development, which may include installation of additional reservoir capacity and/or distribution facilities.	<b>Consistent.</b> The Specific Plan's water system is designed to meet the fire flow requirements of OSHPD and LACFD.
<b>Goal S 4:</b> Protection of public safety and property from hazardous materials.	<b>Consistent.</b> Procedures for handling hospital and medical office hazardous waste; and the transport, use, storage, and disposal of all Specific Plan-related hazardous wastes are subject to all local, state, and federal regulations.

## APPENDIX C (Continued)

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# **APPENDIX D**

## *Service Provider Confirmation Emails*



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**From:** Stephan Le Blanc <Stephan.Leb Blanc@sce.com>  
**Sent:** Tuesday, January 12, 2016 6:21 PM  
**To:** Jodie Mendelson  
**Cc:** Richard J. Londergan; John Tierney  
**Subject:** Re: (External):FW: Henry Mayo Specific Plan - Electricity Component

Hello Jodie,  
My apologies for the delay in my response. The statement below in the prior e-mail in true and SCE is complete agreement. If you need more information or have any questions please contact me on my cell phone.

Stephan Le Blanc  
Service Planner  
Valencia District  
805-223-3575

On Jan 12, 2016, at 4:57 PM, Jodie Mendelson <[jmendelson@shpproject.com](mailto:jmendelson@shpproject.com)> wrote:

Hi Stephan,  
Is it possible to let me know if this statement is OK with SCE by tomorrow morning? We are hoping to submit the request to the city this week.

Regards,

**Jodie Mendelson**, AIA, LEED GA  
Project Manager  
<[image001.jpg](#)>  
711 Mission Street, Suite C  
South Pasadena, CA 91030  
[www.SHPproject.com](http://www.SHPproject.com)  
p.626.403.6082

---

**From:** Jodie Mendelson  
**Sent:** Monday, January 11, 2016 12:01 PM  
**To:** Stephan Le Blanc ([Stephan.Leb Blanc@sce.com](mailto:Stephan.Leb Blanc@sce.com))  
**Cc:** 'Richard J. Londergan'; John Tierney ([John.Tierney@sce.com](mailto:John.Tierney@sce.com))  
**Subject:** RE: (External):FW: Henry Mayo Specific Plan - Electricity Component

Stephan,  
Can you confirm SCE agrees with the statement below via email? We don't need this statement to be put on the plans.

*"Implementation of the proposed Specific Plan modifications to the Master Plan would not result in any significant increase in energy consumption because the nature and intensity of uses would not change. It has been confirmed that any increase caused by implementation of the proposed Master Plan modifications would be within the existing service capacities of provider Southern California Edison."*

Thank you so much for your quick reply.

Regards,

**Jodie Mendelson**, AIA, LEED GA

Project Manager

<image005.jpg>

711 Mission Street, Suite C

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---

**From:** Jodie Mendelson <jmendelson@shpproject.com>  
**Sent:** Friday, January 15, 2016 12:44 PM  
**To:** Stein, Tamar C. (tstein@coxcastle.com); Shannon Kimball (shankball@gmail.com); Heather Ivey; Asha Bleier  
**Cc:** Stacey Pray  
**Subject:** FW: Henry Mayo Specific Plan - Gas Usage

Here's the confirmation from So Cal Gas.

Regards,

**Jodie Mendelson**, AIA, LEED GA  
Project Manager



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---

**From:** Foster, Kurtis M [<mailto:KFoster@semprautilities.com>]  
**Sent:** Friday, January 15, 2016 12:18 PM  
**To:** Jodie Mendelson  
**Cc:** Chun, Tony; Blackwell, Brian J.; 'Richard J. Londergan <[londergarj@henrymayo.com](mailto:londergarj@henrymayo.com)> ([londergarj@henrymayo.com](mailto:londergarj@henrymayo.com))'  
**Subject:** RE: Henry Mayo Specific Plan - Gas Usage

Hi Jodie,

Yes, this statement is acceptable.

Kurtis Foster  
Account Executive  
Southern California Gas Company  
818-701-3414



---

**From:** Jodie Mendelson [<mailto:jmendelson@shpproject.com>]  
**Sent:** Friday, January 15, 2016 12:15 PM  
**To:** Foster, Kurtis M  
**Cc:** Chun, Tony; Blackwell, Brian J.; 'Richard J. Londergan <[londergarj@henrymayo.com](mailto:londergarj@henrymayo.com)> ([londergarj@henrymayo.com](mailto:londergarj@henrymayo.com))'  
**Subject:** Henry Mayo Specific Plan - Gas Usage  
**Importance:** High

Hi Kurtis,

We would like to be able to state in the specific plan to the city for the additional inpatient building square footage the following:

“Southern California Gas Company will upsize the existing, on-campus 2” diameter natural gas line with a new 4” diameter line, and upsize their meter adjacent to the new Central Plant accordingly to serve the Inpatient Building. The Company’s existing 6” medium-pressure line on McBean Parkway can supply this load without any required mitigation. No additional demands for natural gas consumption are required by the proposed Specific Plan since the number of total licensed beds on campus, as approved in the 2008 Master Plan and Development Agreement, does not increase.”

Is this statement acceptable to the Southern California Gas Company?

Regards,

**Jodie Mendelson**, AIA, LEED GA  
Project Manager



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This email originated outside of Sempra Energy. Be cautious of attachments, web links, or requests for information.

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**From:** Cris Perez [<mailto:CPerez@valenciawater.com>]  
**Sent:** Friday, January 15, 2016 11:03 AM  
**To:** Jodie Mendelson; Richard J. Londergan  
**Cc:** Gino Garcia  
**Subject:** RE: Henry Mayo Specific Plan - Water supply component

Yes.

**Cris Pérez**

*Vice President - Operations*

661.295.6507 Office      cperez@valenciawater.com  
661.810.0233 Mobile    www.valenciawater.com  
661.294.3806 Fax

**Valencia Water Company**  
24631 Avenue Rockefeller  
Valencia, CA 91355

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**From:** Jodie Mendelson [<mailto:jmendelson@shpproject.com>]  
**Sent:** Friday, January 15, 2016 11:00 AM  
**To:** Cris Perez <[CPerez@valenciawater.com](mailto:CPerez@valenciawater.com)>; Richard J. Londergan <[londergarj@henrymayo.com](mailto:londergarj@henrymayo.com)>  
**Cc:** Gino Garcia <[GGarcia@valenciawater.com](mailto:GGarcia@valenciawater.com)>  
**Subject:** RE: Henry Mayo Specific Plan - Water supply component

Hi Cris,

We would like to be able to state in the specific plan to the city for the additional tower square footage the following:

“The Master Plan EIR found a less than significant impact on Water Supply. Implementation of the proposed Specific Plan modifications to the Master Plan would cause little, if any, increase in the use of water beyond that analyzed in the Master Plan EIR. It has been confirmed that retail purveyor Valencia Water Company would have sufficient capacity to service any increase that might incidentally be caused by implementation of the proposed Specific Plan modifications to the Master Plan. No mitigation measures are required. No new significant effect would occur.”

Is VWC agreeable with this statement?

Regards,

**Jodie Mendelson**, AIA, LEED GA  
Project Manager



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**APPENDIX E**  
*Parking Demand Study*  
*(Fehr and Peers, February 2020)*



# Parking Demand Study

## Henry Mayo Newhall Hospital

Prepared for:  
Henry Mayo Newhall Hospital

February 7, 2020

LA19-3148.00

FEHR  PEERS

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## Appendices

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Appendix A: Existing Parking Occupancy

# 1. Introduction

This parking demand study analyzes parking demand and establishes parking requirements for the Henry Mayo Newhall Hospital 2020 Amended Specific Plan. In this study, we describe the proposed project, document existing conditions, analyze parking utilization by development phase, and establish parking requirements including the utilization of parking management solutions (including paid parking) to support the effective operation of parking on the campus as it continues to build out.

## 1.1 Project Description

The Henry Mayo Newhall Hospital campus is the primary medical center serving the Santa Clarita Valley region. The campus is located along McBean Parkway in the City of Santa Clarita. The current site includes a hospital with inpatient and outpatient services, and several medical office buildings. The City approved a Master Plan in 2008 for the campus which proposed a new inpatient building and three new medical office buildings (one of which is currently open). In 2016, the City approved a Specific Plan for the campus which proposed to increase the square footage of the new inpatient building to comply with updates to the California Building Standards Code. This new inpatient building opened in late 2019.

The campus is proposing an amendment to the Specific Plan to comply with existing and anticipated updates to the Office of Statewide Health Planning and Development (OSHPD) code and Americans with Disabilities Act (ADA) requirements. The amendment proposes a new inpatient building and supporting diagnostics and treatment building. This building expansion will not increase the number of inpatient beds as approved in the Master Plan and will instead increase the space allotted to each bed to comply with the requirements referenced above. The new building space will also provide an additional 30,000 square feet for outpatient services.



## 2. Parking Supply

In 2018, consulting firm Linscott, Law & Greenspan, Engineers (LLG) performed an inventory and utilization survey of on-site parking spaces at the campus. Their inventory found 1,547 existing spaces. This inventory is slightly lower than the 1,644 spaces documented in the Specific Plan. The lower inventory is due to construction activities for the new inpatient building which occupied some surface parking lot areas at the time the inventory was performed.

With the addition of parking spaces approved in the Specific Plan, the total campus parking supply will be 2,227. The amendment to the Specific Plan proposes to add a net 214 spaces, resulting in a total campus parking supply of 2,441 spaces. The following table shows the parking supply as inventoried by LLG, the existing supply documented in the Specific Plan, the approved supply from the Specific Plan, and the proposed supply for the amendment to the Specific Plan.

**Table 1: Existing, Approved and Proposed Parking Supply**

Lot Name	Type of Lot	LLG Inventory	Specific Plan Existing Supply	Specific Plan Approved Supply	Specific Plan Amendment Proposed Supply
PS1	Structure	750	750	750	750
PS2	Structure	-	-	579	579
PS3	Structure	-	-	278	278
PS4	Structure	349	352	352	644
A1	Surface Lot	5	24	-	-
A2	Surface Lot	216	213	44	44
A3	Surface Lot	12	22	22	22
D	Surface Lot	4	54	54	-
F1	Surface Lot	46	46	27	27
F2	Surface Lot	60	60	-	-
G	Surface Lot	29	29	29	29
H	Surface Lot	33	38	38	22
I	Surface Lot	8	8	8	-
J	Surface Lot	-	15	15	15
Center Driveway	On-Street Parallel	25	22	22	22
Internal Driveway	On-Street Parallel	10	11	-	-
West Driveway	On-Street Parallel	-	-	9	9
<b>Total</b>		<b>1,547</b>	<b>1,644</b>	<b>2,227</b>	<b>2,441</b>

Source: Fehr & Peers, 2020, LLG, 2018, Henry Mayo Newhall Hospital Specific Plan, 2016.



The proposed buildings in the approved and amended Specific Plan will be constructed in three phases. Phase 1 will construct the new inpatient/diagnostics and treatment building and an expansion of the PS4 parking structure and will remove Lot D, Lot I and a portion of Lot H. Phase 2 will construct one new medical office building and the PS3 parking structure and remove Lot F2 and a portion of Lots A2 and F1. Phase 3 will construct one new medical office building and the PS2 parking structure and remove Lot A1 and a portion of Lot A2. The following table shows the parking supply that will be available upon buildout of each of these three phases.

**Table 2: Parking Supply by Phase**

Lot Name	Type of Lot	Specific Plan Existing Supply	Phase 1	Phase 2	Phase 3
PS1	Structure	750	750	750	750
PS2	Structure	-	-	-	579
PS3	Structure	-	-	278	278
PS4	Structure	352	644	644	644
A1	Surface Lot	24	24	24	-
A2	Surface Lot	213	213	167	44
A3	Surface Lot	22	22	22	22
D	Surface Lot	54	-	-	-
F1	Surface Lot	46	46	27	27
F2	Surface Lot	60	60	-	-
G	Surface Lot	29	29	29	29
H	Surface Lot	38	22	22	22
I	Surface Lot	8	-	-	-
J	Surface Lot	15	15	15	15
Center Driveway	On-Street Parallel	22	22	22	22
Internal Driveway	On-Street Parallel	11	11	11	-
West Driveway	On-Street Parallel	-	-	-	9
<b>Total</b>		<b>1,644</b>	<b>1,858</b>	<b>2,011</b>	<b>2,441</b>

Source: Fehr & Peers, 2020, Henry Mayo Newhall Hospital Specific Plan, 2016.



## 3. Parking Code Requirement

The City of Santa Clarita includes minimum off-street parking requirements in its municipal code. The code contains parking requirements for Hospital Services, Medical Services, Professional Offices and Warehousing, all of which are included in our calculation of the parking code requirement for the overall campus. The following table shows the parking requirements for the existing site, the approved site in the Specific Plan, and the site including the 2020 amendment to the Specific Plan. For reference, this table also shows the parking supply for each of the three scenarios and notes whether there is a parking surplus or shortfall from the code requirement. In all three scenarios (including the proposed plan amendment), the campus is providing more parking than the municipal code requirement.

**Table 3: Existing, Approved and Proposed Parking Code Requirement**

Land Use	Parking Ratio	Existing [1]		Approved Specific Plan		Proposed Specific Plan Amendment	
		Units	Spaces	Units	Spaces	Units	Spaces
Hospital – Inpatient	2 Spaces/Bed	236 Beds	472	368 Beds	736	NC [2]	736
Hospital – Outpatient	2.5 Spaces/KSF	11.375 KSF	29	15.375 KSF	39	45.375 KSF	114
Medical Services	5 Spaces/KSF	164.916 KSF	825	280.916 KSF	1,405	NC	1,405
Professional Office	4 Spaces/KSF	8.734 KSF	35	0.734 KSF	3	NC	3
Storage	1 Space/KSF	2.384 KSF	3	2.384 KSF	3	NC	3
<b>Parking Code Requirement</b>		<b>1,364</b>		<b>2,186</b>		<b>2,261</b>	
Parking Supply		1,547		2,227		2,441	
<b>Code Surplus/Shortfall</b>		<b>+183</b>		<b>+41</b>		<b>+180</b>	

KSF = 1,000 Square Feet

[1] For the purposes of our analysis, the “Existing” parking code requirement refers to March 2018 – the period which LLG performed its inventory and utilization survey. Therefore, this existing requirement does not consider the new inpatient building which opened in 2019, however the approved bed count is not changing under the 2020 Amended Specific Plan.

[2] NC = No Change from Approved Specific Plan.

Source: *Santa Clarita Municipal Code, Chapters 17.43, 17.44, and 17.45, 2020*





The following table shows the municipal code requirement for each of the three buildout phases of the proposed Specific Plan Amendment. In all three phases, the parking supply exceeds the parking code requirements.

**Table 4: Parking Code Requirement by Phase for Specific Plan Amendment**

		Phase 1		Phase 2		Phase 3	
Land Use	Parking Ratio	Units	Spaces	Units	Spaces	Units	Spaces
Hospital – Inpatient	2 Spaces/Bed	368 Beds	736	368 Beds	736	368 Beds	736
Hospital – Outpatient	2.5 Spaces/KSF	45.375 KSF	114	45.375 KSF	114	45.375 KSF	114
Medical Services	5 Spaces/KSF	164.916 KSF	825	222.916 KSF	1,115	280.916 KSF	1,405
Professional Office	4 Spaces/KSF	8.734 KSF	35	8.734 KSF	35	0.734 KSF	3
Storage	1 Space/KSF	2.384 KSF	3	2.384 KSF	3	2.384 KSF	3
<b>Parking Code Requirement</b>		<b>1,713</b>		<b>2,003</b>		<b>2,261</b>	
Parking Supply		1,858		2,011		2,441	
<b>Code Surplus/Shortfall</b>		<b>+145</b>		<b>+8</b>		<b>+180</b>	

KSF = 1,000 Square Feet

Source: Santa Clarita Municipal Code, Chapters 17.43, 17.44, and 17.45, 2020



## 4. Parking Utilization

The utilization survey LLG collected in March 2018 consisted of two days (Tuesday, March 6<sup>th</sup> and Wednesday, March 7<sup>th</sup>) of hourly occupancy sweeps between the hours of 6 AM and 8 PM. Using the average occupancy over the two days for each hour, we calculated the peak parking utilization for the campus to be 91%, occurring at 11 AM. During this time, about 1,400 spaces were occupied. Details of LLG’s utilization survey can be found in the Appendix.

The following table shows the existing peak utilization for the campus and our projected future peak utilization for each of the three buildout phases of the campus including the proposed amendments.

**Table 5: Existing and Projected Future Peak Parking Utilization by Phase**

	Existing [1]	Phase 1	Phase 2	Phase 3
Parking Code Requirement	1,364	1,713	2,003	2,261
Parking Demand [2]	1,400	1,759	2,055	2,321
Parking Supply	1,547	1,858	2,011	2,441
<b>Parking Utilization</b>	<b>91%</b>	<b>95%</b>	<b>102%</b>	<b>95%</b>

[1] For the purposes of our analysis, the “Existing” parking utilization refers to March 2018 – the period which LLG performed its inventory and utilization survey. Therefore, this existing utilization does not consider the new inpatient building which opened in 2019.

[2] Existing peak parking demand is only about 3% higher than the parking code requirement. Therefore, a 3% adjustment above the code requirement serves as a reasonable proxy for estimating future parking demand on-site.

Source: Fehr & Peers, 2020



## 5. Parking Management Solutions

Although we estimate that the proposed 2,441 parking spaces is adequate to meet future demand at full buildout, at 95% utilization it's likely that campus staff, patients and visitors will face difficulty finding available spaces during periods of peak demand. Typically, utilization above 85-90% causes drivers to spend additional time circling around parking lots and structures scanning for available spaces. This additional time driving can cause frustration for campus patients and visitors and could result in patients being late for appointments. Additionally, as noted in Table 5, we estimate that Phase 2 of the campus buildout will be just over capacity to meet Phase 2 demand (i.e., prior to the construction of the larger PS2 parking structure in Phase 3).

A variety of parking management solutions can effectively reduce peak utilization and are recommended in this study. These implementation measures include transportation demand management (TDM) strategies, parking access and revenue control systems (PARCS), methods for creating additional parking supply, wayfinding and signage, and transportation network company (TNC) partnership programs. The following is a summary of the measures proposed and the projected reduction in parking demand achieved by these measures.

### 5.1 Transportation Demand Management (TDM)

TDM strategies focus on reducing the demand for single-occupancy vehicle (SOV) travel to a site. Fehr & Peers has developed a quick-response tool, *TDM+*, to help land use decisionmakers measure the effects of implementing a variety of TDM strategies. *TDM+* utilizes findings from our technical analysis of greenhouse gas (GHG) mitigation measures for the California Air Resource Board Zero Carbon Buildings Study, and prior work for the California Air Pollution Control Officers Association (CAPCOA) and the Bay Area Air Quality Management District (BAAQMD).

Using the *TDM+* tool, we've developed a comprehensive TDM strategy for the campus. With the implementation of the following TDM measures, we estimate the campus can achieve a parking demand reduction between 3% and 4%. With a reduction of 3%, the peak parking utilization would be about 92%. With a reduction of 4%, the peak parking utilization would be about 91% (same as current peak utilization).

#### 5.1.1 Commute Marketing Program

Commute marketing programs are the traditional TDM program and consist of using staff time (through either a dedicated staff member, or through a transportation management agency) to advertise non-SOV travel options to campus employees, patients and visitors. This may include direct outreach, help with trip planning, development of promotional materials, etc. The efficacy of this measure is affected by the level of investment in the program, the individual staff involved, and the other measures implemented.

Due to the driving-oriented nature of the campus and surrounding community, it's likely that many employees, patients and visitors are unaware of current and potential future non-SOV travel options to



access the campus. Therefore, education and marketing will be crucial to the success of the overall TDM program. For example, campus commuters may not be aware of the Token Transit app, which allows Santa Clarita Transit riders to purchase tickets, including monthly passes, directly from their smartphone. Campus employers can also incentivize transit by purchasing transit passes on behalf of employees as a tax-free benefit. Part of promoting the TDM program may take the form of a rewards program. Commuters can earn points for each day they choose a non-SOV travel option, earning them prizes and recognition.

### **5.1.2 Emergency Ride Home**

Emergency Ride Home is a supportive measure that helps allay anxiety associated with being without private transportation while at work. This program allows for individuals who encounter an emergency (illness, sick child, unplanned overtime) to request a taxi or ride hailing service (e.g., Uber or Lyft) home, and have the cost reimbursed, if they did not drive to work. This service can be coordinated through the commute marketing program.

### **5.1.3 Ridematch Program**

A ridematch program provides a platform for people to identify others to carpool or share rides with. The platform can be developed in-house, or the campus can purchase ready-to-use software or an online subscription service to facilitate ridematching. Since the campus includes large employers, there is a higher likelihood of employees successfully finding ride matches. Employees who create carpools can be eligible for incentives, including financial assistance by campus employers to help pay for fuel and maintenance costs, and priority parking spaces on campus reserved for carpool vehicles.

## **5.2 Parking Access and Revenue Control Systems (PARCS)**

To further reduce parking demand and create revenue to fund TDM measures and parking structure construction and operation costs, the campus shall implement a parking access and revenue control system – or PARCS – within the parking lots and structures on campus. The primary element of PARCS is the introduction of paid parking, which in conjunction with TDM measures can be very effective in reducing parking demand.

In order to accommodate employees, patients and visitors within a paid parking system, a monthly rate and daily rate should be established. While the price-elasticity of parking demand is highly context-specific, typically the introduction of a nominal parking price to an area with existing free parking causes a meaningful reduction in demand. Under this paid parking program, we recommend initially introducing paid parking with a nominal fee (as low as \$2/day, \$20/month) and then monitoring demand annually to see if the desired results are achieved. For example, the campus shall establish a peak parking utilization goal of 85% and increase the price of parking until this goal is reached. Short-term visitors may be allowed up to four hours of free parking with a validated ticket.

Since the parking supply available at the campus is in both structures and surface lots, a combination of enforcement strategies may be necessary. Parking enforcement strategies can be organized into two broad categories: gate and gateless systems. With a gate system, enforcement is managed by moveable gate arms



at the entrances and exits of a parking facility. Typically, a driver entering the facility will be given a paper ticket, which can then be used at a pay kiosk or at the exit gate along with a credit/debit card or cash to pay for parking. With a gateless system, enforcement is managed by personnel driving through the facilities and verifying either physical permits visible through the windows of the vehicle or virtual permits assigned via the license plate number.

Due to the upfront capital costs, gate systems are typically only cost-effective for large structures. While the design and layout of parking structures PS2 and PS3 are unknown, the current parking structures PS1 and PS4 have the potential to use gate systems. Vehicle access to PS1 is restricted to one entrance and one exit. The entrance driveway to PS1 is approximately 100 feet in length, providing a potential queuing area if a gate entrance is installed. Vehicle access to PS4 is restricted to one shared entrance/exit point. The driveway to PS4 is also approximately 100 feet in length and can accommodate potential queueing. A gate system is easy for users to understand and provides immediate enforcement upon implementation.

Gateless systems are useful in locations with small surface lots because they do not require physical infrastructure and they can make efficient use of personnel time if enforcement sweeps do not need to cover significant distance or multiple levels of a structure. With the use of license plate reader (LPR) technology, enforcement of gateless lots only requires an occasional sweep of the parking lots to scan license plates. All parked vehicles' license plates must be linked with a valid parking permit, which can occur at the point-of-sale via a kiosk or online.

The campus currently includes assigned parking for physicians, donors, short-term visitors to the urgent care facility, EV-charging, and ADA. These assigned parking spaces shall be maintained with the implementation of paid parking. Additionally, up to 50% of the surface lots shall be designated as first-come/first-serve free visitor parking. Employees will mostly park within the structures but may also utilize up to 50% of available surface lot parking. These assignments shall be monitored annually to ensure efficient use of parking space.

The implementation of paid parking at the campus may produce unintended parking issues, such as campus users parking on nearby residential streets. However, since most current off-site parking is not convenient for most campus users, this parking spillover effect is not likely to occur. The campus will include the evaluation of parking spillover as part of its annual monitoring program. If off-site impacts are observed, the campus may need to amend its parking management plan to facilitate residential street permits in conjunction with City of Santa Clarita staff.

### **5.3 Additional Parking Supply**

The campus can maximize the space efficiency of its existing and proposed parking facilities by implementing a valet service and enabling tandem (or stacked) parking. A typical lot can accommodate about 45% more vehicles in a tandem-parked valet facility versus a self-parked facility. This significant increase in space efficiency requires the on-going operational costs of a valet service, and therefore may only be cost-effective on a temporary basis, such as periods when the number of spaces available is reduced due to on-site construction or the peak parking constraint in Phase 2 of the campus buildout.



## 5.4 Wayfinding and Signage

The driving and parking experience can be improved through effective wayfinding and signage. Currently, the campus provides adequate signage directing patients and visitors to the PS1 and PS4 parking structures. However, additional information, such as digital displays showing the available spaces in each structure, can reduce the potential for drivers spending additional time searching for available parking. Using sensors within the structures can offer further refined digital displays. For example, upon entering PS1 currently, a sign directs patients/visitors to use the ground level and sub-surface level and employees to use the above-ground levels. However, the ground and sub-surface level can fill up completely during high demand periods. Thus, these static signs may end up directing patients/visitors to areas with no available parking. A digital display, with the support of sensors, can offer a more dynamic wayfinding approach. Digital displays shall be required for all new parking structures.

The on-site shuttle service serves an important wayfinding role as it helps guide visitors between their parked vehicles and on-site uses. This service is on-demand and can offer transportation between any two points on campus. As the campus builds out and a greater proportion of parking is located further away from on-site uses, an expansion of the on-site shuttle service will enhance visitor experience and convenience.

## 5.5 Transportation Network Company (e.g., Uber or Lyft) Partnership Programs

Transportation network companies (TNCs), such as Uber and Lyft, offer partnership programs with businesses and organizations to facilitate subsidizing trips for employees using these ride hailing services. Both Uber and Lyft host an online platform for commute managers to control which employees are eligible for subsidies, set spending limits and geographic zones, and streamline expense reporting. Since these programs are relatively new and little data is publicly available, their effectiveness in reducing parking demand is not currently known. However, given the popularity of these services and the on-demand flexibility, the campus may consider piloting this type of program and compare the cost-effectiveness with a carpool subsidy (as described in the TDM section of this report) or other TDM strategies.

The next frontier of TNCs will take advantage of autonomous vehicle (AV) technology. Many companies are testing the technology and elements of AVs are being incorporated into new vehicle models by most major auto makes. Using completely autonomous vehicles (driverless vehicles) will result in reduced costs to use TNC services since these companies will no longer need to pay drivers. This will likely result in even more use of TNC services. It is speculative to predict when this will occur; however, many experts theorize that driverless AVs could start to penetrate the market in as little as 10 years. This study does not include parking demand reductions for the introduction of AVs since the technology is still in the early stages of development.



## 5.6 Summary of Parking Management Solutions

The following table summarizes the effectiveness of each parking management solution in reducing peak parking utilization. The campus may decide which combination of these solutions is the best approach and add or drop programs to further refine and optimize. As previously mentioned, a paid parking program is most effective in conjunction with TDM measures.

**Table 6: Summary of Parking Management Solutions**

Parking Management Solution	Demand Reduction	Supply Increase
TDM Measures	3% - 4% (70 - 100 spaces)	-
PARCS (Paid Parking)	Up to 5% (115 spaces)	-
Tandem Parking with Valet Service	-	10% - 15% (200 - 300 spaces)
Wayfinding and Signage	-	[1]
TNC Partnership Programs [2]	1.5% (35 spaces)	-

[1] While wayfinding and signage alone do not increase supply, they can significantly improve operations and reduce driver frustration during periods of peak utilization.

[2] The effectiveness of TNC partnership programs is not currently known. The reduction for carpool/vanpool subsidies (1% - 2%) may serve as a reasonable proxy.

Source: Fehr & Peers, 2020

The following table shows how applying these parking management solutions can reduce the demand for parking to 85% utilization for each of the three buildout phases.

**Table 7: Estimated Parking Utilization with Parking Management by Phase**

	Existing [1]	Phase 1	Phase 2	Phase 3
Parking Demand	1,400	1,759	2,055	2,321
<i>TDM Measures</i>	-	(70)	(70)	(100)
<i>PARCS (Paid Parking)</i>	-	(115)	(115)	(115)
<i>Tandem Parking with Valet</i>	-	-	(200)	-
<i>TNC Partnership</i>	-	-	(35)	(35)
<b>Adjusted Parking Demand [2]</b>	<b>1,400</b>	<b>1,574</b>	<b>1,635</b>	<b>2,071</b>
Parking Supply	1,547	1,858	2,011	2,441
<b>Parking Utilization</b>	<b>91%</b>	<b>85%</b>	<b>81%</b>	<b>85%</b>

[1] For the purposes of our analysis, the "Existing" parking utilization refers to March 2018 – the period which LLG performed its inventory and utilization survey. Therefore, this existing utilization does not consider the new inpatient building which opened in 2019.

[2] The parking demand reductions in this table are estimates. Certain parking management strategies, such as tandem parking with valet, can be scaled up or down as necessary to achieve the desired 85% utilization goal.

Source: Fehr & Peers, 2020



As shown in Table 7, Phase 2 of the buildout will likely require the most extensive package of parking management solutions, potentially including tandem parking with valet service to maximize space efficiency within on-site parking facilities. Phase 3 of the buildout will likely have a greater emphasis on TDM strategies than Phases 1 and 2, which hopefully can be achieved through a maturing program. Alternatively, incremental parking price increases may help to meet the 85% utilization target.

## **5.7 Monitoring Program**

Since these findings are estimates, annual on-site monitoring of parking demand shall occur to ensure the parking management solutions in place are meeting the 85% utilization target through each phase of campus buildout. Additionally, a detailed parking utilization report shall be completed prior to the issuance of each new building permit. These parking utilization reports should include new on-site parking counts and be subject to City staff review and approval. Further parking management and TDM strategies can be explored over time as part of these monitoring efforts. While the strategies contained in this report are expected to accommodate the projected parking demand, technology solutions will continue to evolve and new strategies will likely become available.





**APPENDIX A:  
Existing Parking Occupancy**

**WEEKDAY PARKING ACCUMULATION SURVEYS**

HENRY MAYO NEWHALL MEMORIAL HOSPITAL

TUESDAY, MARCH 06, 2018

PARKING LOCATION	NO. OF SPACES	TIME OF DAY														
		6:00 AM	7:00 AM	8:00 AM	9:00 AM	10:00 AM	11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM	6:00 PM	7:00 PM	8:00 PM
<b>PARKING STRUCTURE - PS1</b>																
<b>GROUND LEVEL</b>																
* VISITOR STANDARD	116	30	51	83	100	115	116	116	114	116	115	110	95	83	67	59
* VISITOR COMPACT	4	3	4	4	4	4	3	4	4	4	4	4	3	3	3	3
* STANDARD	32	7	21	30	32	32	32	32	32	32	31	30	26	19	9	5
* COMPACT	4	0	2	4	4	4	3	3	4	4	4	4	4	2	1	0
* HANDICAP	8	1	1	6	5	8	6	8	8	7	8	8	7	3	3	0
<b>LEVEL 1</b>																
* VISITOR STANDARD	68	29	53	62	68	68	68	65	67	68	66	60	55	50	53	49
* VISITOR COMPACT	2	2	2	2	2	2	2	2	2	2	2	2	2	1	1	1
* STANDARD	64	49	63	64	64	64	63	62	64	64	62	61	51	48	45	38
* COMPACT	4	2	3	4	4	4	4	4	4	4	4	4	4	3	3	3
* STANDARD RESERVED	1	0	0	0	0	1	1	1	1	1	0	0	0	0	0	0
* HANDICAP	7	0	2	3	5	5	7	5	6	6	6	6	6	6	6	6
<b>LEVEL 2</b>																
* STANDARD	146	56	108	143	145	146	144	141	144	145	143	138	110	83	70	53
* COMPACT	7	2	3	7	7	7	7	5	7	7	5	6	5	4	4	2
* MOTORCYCLE	1	0	1	1	1	1	1	1	1	1	1	0	0	0	0	0
<b>LEVEL 3</b>																
* STANDARD	147	6	23	57	82	104	108	108	103	105	101	95	80	64	37	22
* COMPACT	8	0	1	4	6	6	6	6	6	6	5	4	4	2	2	2
<b>LEVEL 4</b>																
* CONTRACTOR STANDARD	82	43	80	80	80	80	79	77	80	79	34	26	12	9	3	1
* CONTRACTOR COMPACT	4	4	4	4	4	4	4	4	4	4	2	2	1	1	0	0
* STANDARD	45	5	11	13	12	15	13	12	12	10	8	4	4	4	3	2
<b>TOTAL STRUCTURE</b>	<b>750</b>	<b>239</b>	<b>433</b>	<b>571</b>	<b>625</b>	<b>669</b>	<b>667</b>	<b>656</b>	<b>663</b>	<b>665</b>	<b>602</b>	<b>565</b>	<b>469</b>	<b>385</b>	<b>310</b>	<b>246</b>

PARKING LOCATION	NO. OF SPACES	TIME OF DAY														
		6:00 AM	7:00 AM	8:00 AM	9:00 AM	10:00 AM	11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM	6:00 PM	7:00 PM	8:00 PM
<b>PARKING STRUCTURE - PS4</b>																
<b>GROUND</b>																
* VISITOR STANDARD	71	32	57	64	71	71	71	71	71	68	71	71	70	67	71	64
* VISITOR COMPACT	7	6	7	7	7	7	7	7	7	7	7	7	7	5	5	6
* HANDICAP	12	1	2	6	12	12	12	12	12	12	11	10	6	4	3	3
* ELECTRIC	6	0	1	1	3	3	3	3	3	3	3	3	1	1	1	1
<b>LEVEL B1</b>																
* STANDARD	45	28	40	45	45	45	45	42	45	45	45	42	42	40	41	41
* COMPACT	4	1	4	4	4	4	4	4	4	4	3	3	3	3	3	2
* PHYSICIAN STANDARD	44	11	28	36	44	45	44	40	41	36	39	39	34	32	32	27
* PHYSICIAN COMPACT	4	3	4	4	4	4	4	4	4	4	4	4	4	2	2	2
* VISITOR STANDARD	4	2	4	4	4	4	4	4	4	4	4	4	4	4	3	4
<b>LEVEL B2</b>																
* STANDARD	93	16	52	75	84	90	91	91	91	91	85	79	73	63	51	39
* COMPACT	8	0	7	8	8	8	8	8	8	7	8	7	6	6	4	4
<b>LEVEL B3</b>																
* STANDARD	49	0	11	24	41	43	48	48	48	48	48	39	26	15	10	5
* COMPACT	2	0	1	1	2	2	2	2	2	2	2	2	2	1	0	0
<b>TOTAL STRUCTURE</b>	<b>349</b>	<b>100</b>	<b>218</b>	<b>279</b>	<b>329</b>	<b>338</b>	<b>343</b>	<b>336</b>	<b>340</b>	<b>331</b>	<b>331</b>	<b>310</b>	<b>278</b>	<b>243</b>	<b>226</b>	<b>198</b>

PARKING LOCATION	NO. OF SPACES	TIME OF DAY													
		6:00 AM	7:00 AM	8:00 AM	9:00 AM	10:00 AM	11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM	6:00 PM	7:00 PM

SURFACE LOTS																
<b>LOT A1 / CONSTRUCTION</b>																
* STANDARD	3	1	3	4	4	4	4	4	4	4	3	1	0	0	0	0
* HANDICAP	2	0	0	1	1	1	1	1	1	1	1	0	0	0	0	0
<b>TOTAL PARKING</b>	<b>5</b>	<b>1</b>	<b>3</b>	<b>5</b>	<b>5</b>	<b>5</b>	<b>5</b>	<b>5</b>	<b>5</b>	<b>5</b>	<b>4</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>LOT A2</b>																
* STANDARD	112	32	44	71	104	109	111	111	107	103	95	90	77	55	48	40
* COMPACT	39	3	6	10	17	31	29	28	24	24	22	17	18	15	8	4
* VISITOR STANDARD	41	3	10	16	28	28	27	28	23	26	21	18	16	15	12	9
* HANDICAP	24	4	4	8	11	16	12	13	12	11	11	11	9	7	7	4
<b>TOTAL PARKING</b>	<b>216</b>	<b>42</b>	<b>64</b>	<b>105</b>	<b>160</b>	<b>184</b>	<b>179</b>	<b>180</b>	<b>166</b>	<b>164</b>	<b>149</b>	<b>136</b>	<b>120</b>	<b>92</b>	<b>75</b>	<b>57</b>
<b>LOT A3 / CONSTRUCTION</b>																
* STANDARD	10	9	9	7	10	10	10	10	10	10	8	8	8	7	5	5
* HANDICAP	2	2	2	2	2	2	2	2	2	2	1	1	0	0	0	0
<b>TOTAL PARKING</b>	<b>12</b>	<b>11</b>	<b>11</b>	<b>9</b>	<b>12</b>	<b>12</b>	<b>12</b>	<b>12</b>	<b>12</b>	<b>12</b>	<b>9</b>	<b>9</b>	<b>8</b>	<b>7</b>	<b>5</b>	<b>5</b>
<b>LOT D / CONSTRUCTION</b>																
* STANDARD	2	2	2	1	1	2	2	2	2	2	2	2	2	2	2	2
* HANDICAP	2	0	0	1	2	2	2	2	2	0	0	0	0	0	0	0
<b>TOTAL PARKING</b>	<b>4</b>	<b>2</b>	<b>2</b>	<b>2</b>	<b>3</b>	<b>4</b>	<b>4</b>	<b>4</b>	<b>4</b>	<b>2</b>	<b>2</b>	<b>2</b>	<b>2</b>	<b>2</b>	<b>2</b>	<b>2</b>
<b>LOT E1</b>																
* STANDARD	22	0	3	14	19	18	20	20	21	20	16	13	9	5	2	2
* COMPACT	24	0	0	10	23	23	24	24	23	23	14	10	8	5	1	1
<b>TOTAL PARKING</b>	<b>46</b>	<b>0</b>	<b>3</b>	<b>24</b>	<b>42</b>	<b>41</b>	<b>44</b>	<b>44</b>	<b>44</b>	<b>43</b>	<b>30</b>	<b>23</b>	<b>17</b>	<b>10</b>	<b>3</b>	<b>3</b>
<b>LOT F2</b>																
* STANDARD	46	1	2	10	30	36	42	38	27	28	30	30	24	15	14	14
* COMPACT	6	0	0	0	1	4	5	5	3	4	3	4	2	2	1	1
* HANDICAP	8	0	0	1	2	3	7	6	4	3	2	1	1	0	0	0
<b>TOTAL PARKING</b>	<b>60</b>	<b>1</b>	<b>2</b>	<b>11</b>	<b>33</b>	<b>43</b>	<b>54</b>	<b>49</b>	<b>34</b>	<b>35</b>	<b>35</b>	<b>35</b>	<b>27</b>	<b>17</b>	<b>15</b>	<b>15</b>
<b>LOT G</b>																
* VISITOR STANDARD	15	4	6	5	9	12	12	11	10	11	11	9	7	8	10	8
* HANDICAP	14	1	3	4	8	12	14	10	11	9	8	6	3	2	1	1
<b>TOTAL PARKING</b>	<b>29</b>	<b>5</b>	<b>9</b>	<b>9</b>	<b>17</b>	<b>24</b>	<b>26</b>	<b>21</b>	<b>21</b>	<b>20</b>	<b>19</b>	<b>15</b>	<b>10</b>	<b>10</b>	<b>11</b>	<b>9</b>
<b>LOT H</b>																
* VISITOR STANDARD	15	9	14	15	15	15	15	14	15	12	12	11	11	12	14	12
* HANDICAP	18	4	6	15	18	18	17	17	17	15	12	10	8	9	11	10
<b>TOTAL PARKING</b>	<b>33</b>	<b>13</b>	<b>20</b>	<b>30</b>	<b>33</b>	<b>33</b>	<b>32</b>	<b>31</b>	<b>32</b>	<b>27</b>	<b>24</b>	<b>21</b>	<b>19</b>	<b>21</b>	<b>25</b>	<b>22</b>
<b>LOT I</b>																
* HANDICAP	8	0	1	3	7	8	7	7	7	5	4	3	3	3	3	3
<b>TOTAL PARKING</b>	<b>8</b>	<b>0</b>	<b>1</b>	<b>3</b>	<b>7</b>	<b>8</b>	<b>7</b>	<b>7</b>	<b>7</b>	<b>5</b>	<b>4</b>	<b>3</b>	<b>3</b>	<b>3</b>	<b>3</b>	<b>3</b>
<b>LOT J</b>																
CLOSED / CONSTRUCTION	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
<b>TOTAL PARKING</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>PARALLEL PARKING AREAS</b>																
<b>CENTER DRIVE WAY</b>																
* STANDARD	25	9	11	10	10	10	12	12	12	12	12	12	12	12	10	9
<b>TOTAL PARKING</b>	<b>25</b>	<b>9</b>	<b>11</b>	<b>10</b>	<b>10</b>	<b>10</b>	<b>12</b>	<b>12</b>	<b>12</b>	<b>12</b>	<b>12</b>	<b>12</b>	<b>12</b>	<b>12</b>	<b>10</b>	<b>9</b>
<b>INTERNAL DRIVEWAY</b>																
* STANDARD	10	3	4	9	9	10	9	10	10	8	8	7	7	4	3	3
<b>TOTAL PARKING</b>	<b>10</b>	<b>3</b>	<b>4</b>	<b>9</b>	<b>9</b>	<b>10</b>	<b>9</b>	<b>10</b>	<b>10</b>	<b>8</b>	<b>8</b>	<b>7</b>	<b>7</b>	<b>4</b>	<b>3</b>	<b>3</b>
<b>20 MIN ZONE</b>																
* STANDARD (APPROX)	5	1	2	2	3	4	5	4	3	4	3	4	2	1	1	1
<b>TOTAL PARKING</b>	<b>5</b>	<b>1</b>	<b>2</b>	<b>2</b>	<b>3</b>	<b>4</b>	<b>5</b>	<b>4</b>	<b>3</b>	<b>4</b>	<b>3</b>	<b>4</b>	<b>2</b>	<b>1</b>	<b>1</b>	<b>1</b>
<b>TOTAL SURFACE LOT - PARALLEL</b>	<b>453</b>	<b>88</b>	<b>132</b>	<b>219</b>	<b>334</b>	<b>378</b>	<b>389</b>	<b>379</b>	<b>350</b>	<b>337</b>	<b>299</b>	<b>268</b>	<b>227</b>	<b>179</b>	<b>153</b>	<b>129</b>
<b>GRAND TOTAL</b>	<b>1552</b>	<b>427</b>	<b>783</b>	<b>1069</b>	<b>1288</b>	<b>1385</b>	<b>1399</b>	<b>1371</b>	<b>1353</b>	<b>1333</b>	<b>1232</b>	<b>1143</b>	<b>974</b>	<b>807</b>	<b>689</b>	<b>573</b>

**WEEKDAY PARKING ACCUMULATION SURVEYS**  
HENRY MAYO NEWHALL MEMORIAL HOSPITAL  
WEDNESDAY, MARCH 07, 2018

PARKING LOCATION	NO. OF SPACES	TIME OF DAY														
		6:00 AM	7:00 AM	8:00 AM	9:00 AM	10:00 AM	11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM	6:00 PM	7:00 PM	8:00 PM
<b>PARKING STRUCTURE - PS1</b>																
<b>GROUND LEVEL</b>																
* VISITOR STANDARD	116	28	57	79	92	115	116	113	115	116	110	101	80	72	58	55
* VISITOR COMPACT	4	3	3	3	4	4	4	4	4	4	1	1	1	2	2	2
* STANDARD	32	4	19	24	28	32	32	32	32	32	32	32	25	17	6	4
* COMPACT	4	0	1	2	3	4	4	4	4	4	3	3	2	0	0	0
* HANDICAP	8	1	0	4	5	7	8	7	8	5	5	5	4	4	3	2
<b>LEVEL 1</b>																
* VISITOR STANDARD	68	21	49	57	68	68	68	66	67	68	65	64	58	57	55	50
* VISITOR COMPACT	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2
* STANDARD	64	47	64	64	64	64	64	64	64	64	60	60	55	54	50	45
* COMPACT	4	1	4	4	4	4	4	4	4	4	4	3	3	3	2	2
* STANDARD RESERVED	1	0	0	0	0	1	1	1	1	0	0	0	0	0	0	0
* HANDICAP	7	0	1	2	3	4	4	4	4	4	2	2	2	1	1	0
<b>LEVEL 2</b>																
* STANDARD	146	42	97	122	129	146	146	144	144	143	139	130	101	75	64	60
* COMPACT	7	2	4	6	7	7	7	7	7	7	7	7	6	5	5	3
* MOTORCYCLE	1	0	1	1	1	1	1	1	1	1	1	1	1	0	0	0
<b>LEVEL 3</b>																
* STANDARD	147	3	17	52	74	93	95	97	101	101	104	101	75	50	24	20
* COMPACT	8	0	1	2	5	6	6	5	5	5	5	5	4	3	3	3
<b>LEVEL 4</b>																
* CONTRACTOR STANDARD	82	49	77	80	80	80	80	77	78	79	27	20	5	4	0	0
* CONTRACTOR COMPACT	4	4	4	4	4	4	4	4	4	4	3	2	2	0	0	0
* STANDARD	45	3	15	18	20	21	21	21	21	20	17	15	10	8	3	3
<b>TOTAL STRUCTURE</b>	<b>750</b>	<b>210</b>	<b>416</b>	<b>526</b>	<b>593</b>	<b>663</b>	<b>667</b>	<b>657</b>	<b>666</b>	<b>663</b>	<b>587</b>	<b>554</b>	<b>436</b>	<b>357</b>	<b>278</b>	<b>251</b>

PARKING LOCATION	NO. OF SPACES	TIME OF DAY														
		6:00 AM	7:00 AM	8:00 AM	9:00 AM	10:00 AM	11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM	6:00 PM	7:00 PM	8:00 PM
<b>PARKING STRUCTURE - PS4</b>																
<b>GROUND</b>																
* VISITOR STANDARD	71	26	45	53	71	71	71	69	70	71	71	71	67	65	59	51
* VISITOR COMPACT	7	3	7	7	7	7	7	7	7	6	7	7	7	7	7	6
* HANDICAP	12	0	1	7	10	12	12	11	10	10	9	8	7	7	5	4
* ELECTRIC	6	1	1	1	2	2	2	3	3	4	5	4	3	2	1	1
<b>LEVEL B1</b>																
* STANDARD	45	25	38	45	45	45	45	43	45	45	44	44	41	42	39	37
* COMPACT	4	3	2	4	4	4	4	4	4	4	3	4	4	3	3	
* PHYSICIAN STANDARD	44	15	24	30	38	42	42	42	42	40	38	35	34	32	30	28
* PHYSICIAN COMPACT	4	2	4	4	4	4	4	4	4	4	4	4	4	4	4	3
* VISITOR STANDARD	4	3	4	4	4	4	4	4	4	4	4	4	3	2	2	2
<b>LEVEL B2</b>																
* STANDARD	93	10	48	62	85	92	93	92	91	89	85	82	75	58	42	35
* COMPACT	8	0	5	8	8	8	8	7	8	8	8	7	6	5	5	5
<b>LEVEL B3</b>																
* STANDARD	49	1	15	29	37	46	48	47	46	47	48	44	30	19	11	8
* COMPACT	2	0	0	0	1	2	2	2	2	2	2	2	1	1	0	0
<b>TOTAL STRUCTURE</b>	<b>349</b>	<b>89</b>	<b>194</b>	<b>254</b>	<b>316</b>	<b>339</b>	<b>342</b>	<b>335</b>	<b>336</b>	<b>334</b>	<b>329</b>	<b>315</b>	<b>282</b>	<b>248</b>	<b>208</b>	<b>183</b>

PARKING LOCATION	NO. OF SPACES	TIME OF DAY													
		6:00 AM	7:00 AM	8:00 AM	9:00 AM	10:00 AM	11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM	6:00 PM	7:00 PM

SURFACE LOTS																
<b>LOT A1 / CONSTRUCTION</b>																
* STANDARD	3	1	3	3	4	4	4	4	4	4	2	2	2	1	1	1
* HANDICAP	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
<b>TOTAL PARKING</b>	<b>5</b>	<b>1</b>	<b>3</b>	<b>3</b>	<b>4</b>	<b>4</b>	<b>4</b>	<b>4</b>	<b>4</b>	<b>4</b>	<b>2</b>	<b>2</b>	<b>2</b>	<b>1</b>	<b>1</b>	<b>1</b>
<b>LOT A2</b>																
* STANDARD	112	34	53	70	94	110	110	105	101	94	88	84	77	55	47	40
* COMPACT	39	2	4	11	17	24	24	24	23	22	20	18	15	10	4	2
* VISITOR STANDARD	41	6	7	14	25	38	38	35	30	27	23	21	20	17	14	11
* HANDICAP	24	5	5	7	12	15	15	12	11	10	9	8	5	3	0	0
<b>TOTAL PARKING</b>	<b>216</b>	<b>47</b>	<b>69</b>	<b>102</b>	<b>148</b>	<b>187</b>	<b>187</b>	<b>176</b>	<b>165</b>	<b>153</b>	<b>140</b>	<b>131</b>	<b>117</b>	<b>85</b>	<b>65</b>	<b>53</b>
<b>LOT A3 / CONSTRUCTION</b>																
* STANDARD	10	5	6	8	9	10	10	10	10	9	9	8	8	7	7	6
* HANDICAP	2	2	2	2	2	2	2	2	2	1	1	1	1	1	1	1
<b>TOTAL PARKING</b>	<b>12</b>	<b>7</b>	<b>8</b>	<b>10</b>	<b>11</b>	<b>12</b>	<b>12</b>	<b>12</b>	<b>12</b>	<b>10</b>	<b>10</b>	<b>9</b>	<b>9</b>	<b>8</b>	<b>8</b>	<b>7</b>
<b>LOT D / CONSTRUCTION</b>																
* STANDARD	2	2	2	2	2	2	2	2	2	2	2	2	2	2	1	1
* HANDICAP	2	0	0	2	2	2	2	2	0	0	0	0	0	0	0	0
<b>TOTAL PARKING</b>	<b>4</b>	<b>2</b>	<b>2</b>	<b>4</b>	<b>4</b>	<b>4</b>	<b>4</b>	<b>4</b>	<b>2</b>	<b>2</b>	<b>2</b>	<b>2</b>	<b>2</b>	<b>2</b>	<b>1</b>	<b>1</b>
<b>LOT E1</b>																
* STANDARD	22	0	5	12	18	22	22	21	20	19	18	17	15	9	3	2
* COMPACT	24	0	0	15	20	24	24	22	22	21	20	18	14	7	1	1
<b>TOTAL PARKING</b>	<b>46</b>	<b>0</b>	<b>5</b>	<b>27</b>	<b>38</b>	<b>46</b>	<b>46</b>	<b>43</b>	<b>42</b>	<b>40</b>	<b>38</b>	<b>35</b>	<b>29</b>	<b>16</b>	<b>4</b>	<b>3</b>
<b>LOT F2</b>																
* STANDARD	46	2	5	18	32	44	44	42	40	40	38	34	30	18	13	8
* COMPACT	6	0	0	0	1	4	5	4	3	4	2	4	2	1	1	1
* HANDICAP	8	0	0	2	3	3	3	4	4	5	5	4	2	1	0	0
<b>TOTAL PARKING</b>	<b>60</b>	<b>2</b>	<b>5</b>	<b>20</b>	<b>36</b>	<b>51</b>	<b>52</b>	<b>50</b>	<b>47</b>	<b>49</b>	<b>45</b>	<b>42</b>	<b>34</b>	<b>20</b>	<b>14</b>	<b>9</b>
<b>LOT G</b>																
* VISITOR STANDARD	15	1	2	4	8	11	15	15	12	11	13	11	10	8	7	7
* HANDICAP	14	1	2	5	9	12	14	14	13	12	10	9	7	5	2	2
<b>TOTAL PARKING</b>	<b>29</b>	<b>2</b>	<b>4</b>	<b>9</b>	<b>17</b>	<b>23</b>	<b>29</b>	<b>29</b>	<b>25</b>	<b>23</b>	<b>23</b>	<b>20</b>	<b>17</b>	<b>13</b>	<b>9</b>	<b>9</b>
<b>LOT H</b>																
* VISITOR STANDARD	15	7	13	14	15	15	15	15	14	15	14	13	11	12	13	13
* HANDICAP	18	1	5	14	14	18	18	18	16	18	16	14	11	10	8	10
<b>TOTAL PARKING</b>	<b>33</b>	<b>8</b>	<b>18</b>	<b>28</b>	<b>29</b>	<b>33</b>	<b>33</b>	<b>33</b>	<b>30</b>	<b>33</b>	<b>30</b>	<b>27</b>	<b>22</b>	<b>22</b>	<b>21</b>	<b>23</b>
<b>LOT I</b>																
* HANDICAP	8	1	2	4	7	8	8	7	7	5	5	4	4	4	3	3
<b>TOTAL PARKING</b>	<b>8</b>	<b>1</b>	<b>2</b>	<b>4</b>	<b>7</b>	<b>8</b>	<b>8</b>	<b>7</b>	<b>7</b>	<b>5</b>	<b>5</b>	<b>4</b>	<b>4</b>	<b>4</b>	<b>3</b>	<b>3</b>
<b>LOT J</b>																
CLOSED / CONSTRUCTION	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
<b>TOTAL PARKING</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>PARALLEL PARKING AREAS</b>																
<b>CENTER DRIVE WAY</b>																
* STANDARD	25	7	10	12	12	14	14	15	15	15	15	15	12	9	5	5
<b>TOTAL PARKING</b>	<b>25</b>	<b>7</b>	<b>10</b>	<b>12</b>	<b>12</b>	<b>14</b>	<b>14</b>	<b>15</b>	<b>15</b>	<b>15</b>	<b>15</b>	<b>15</b>	<b>12</b>	<b>9</b>	<b>5</b>	<b>5</b>
<b>INTERNAL DRIVEWAY</b>																
* STANDARD	10	5	8	8	8	9	9	9	9	9	9	9	7	5	3	2
<b>TOTAL PARKING</b>	<b>10</b>	<b>5</b>	<b>8</b>	<b>8</b>	<b>8</b>	<b>9</b>	<b>9</b>	<b>9</b>	<b>9</b>	<b>9</b>	<b>9</b>	<b>9</b>	<b>7</b>	<b>5</b>	<b>3</b>	<b>2</b>
<b>20 MIN ZONE</b>																
* STANDARD (APPROX)	5	2	3	3	4	4	4	3	2	2	2	2	2	2	2	2
<b>TOTAL PARKING</b>	<b>5</b>	<b>2</b>	<b>3</b>	<b>3</b>	<b>4</b>	<b>4</b>	<b>4</b>	<b>3</b>	<b>2</b>	<b>2</b>	<b>2</b>	<b>2</b>	<b>2</b>	<b>2</b>	<b>2</b>	<b>2</b>
<b>TOTAL SURFACE LOT - PARALLEL</b>	<b>453</b>	<b>84</b>	<b>137</b>	<b>230</b>	<b>318</b>	<b>395</b>	<b>402</b>	<b>385</b>	<b>360</b>	<b>345</b>	<b>321</b>	<b>298</b>	<b>257</b>	<b>187</b>	<b>136</b>	<b>118</b>
<b>GRAND TOTAL</b>	<b>1552</b>	<b>383</b>	<b>747</b>	<b>1010</b>	<b>1227</b>	<b>1397</b>	<b>1411</b>	<b>1377</b>	<b>1362</b>	<b>1342</b>	<b>1237</b>	<b>1167</b>	<b>975</b>	<b>792</b>	<b>622</b>	<b>552</b>



# **APPENDIX F**

## *Exterior Master Sign Program and Amendments*





**HENRY MAYO NEWHALL**  
Memorial Hospital

**Exterior Master Sign Program**

City of Santa Clarita Submittal

March 17, 2011

# HENRY MAYO NEWHALL Memorial Hospital

## HENRY MAYO NEWHALL Memorial Hospital

### Tenant Signs/Names

Type style, and color to vary based upon Tenant Logotype

*XYZ Medical Group*






### Frutiger - 57 Condensed

ABCDEFGHIJKLMNOPQRSTUVWXYZ  
abcdefghijklmnopqrstuvwxyz  
0123456789&

### Frutiger - 67 Bold Condensed

ABCDEFGHIJKLMNOPQRSTUVWXYZ  
abcdefghijklmnopqrstuvwxyz  
0123456789&

### Colors / Finishes / Lighting

	C-1 Grey Pantone 430		C-9 Sandstone tile
	C-2 Blue Pantone 293		C-10 Slate Slate tile
	C-3 Aluminum Brushed #4 grain		C-11 Matte Black
	C-4 Silver Paint Matthews Paint Silver rose metallic 48102		C-12 Red Pantone 485 Emergency Background Color
	C-5 Sandstone paint TBD		
	C-6 Stainless Steel Brushed #4 grain		L-1 Lighting Color White
	C-7 White Translucent Acrylic, Vinyl, Paint		L-2 Lighting Color Red

### Building Address Numbering

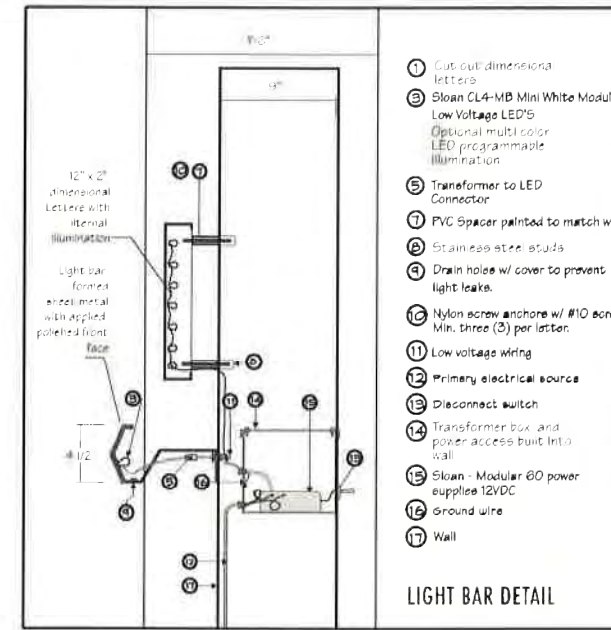
Reduction of leading digit size, change of color and a font change.  
Frutiger 55 & Frutiger - 75 Extra Black

23845



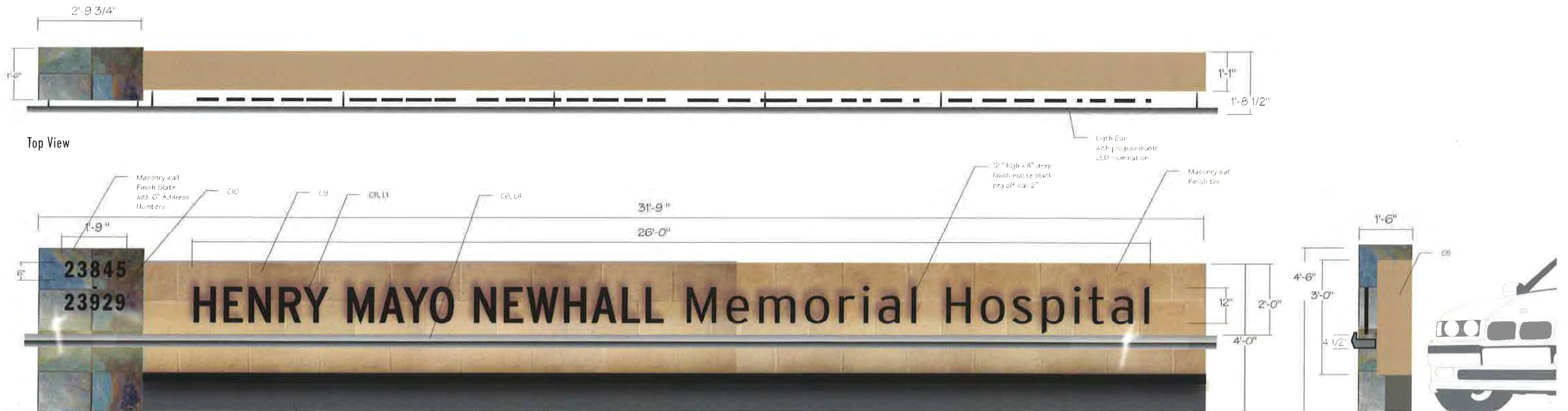
Sign Type 1.01: Entry Monument  
Right side of entry

Street End



Sign Type: 1.01

Cabinet wall structure "skinned" with sandstone tile. Halo illuminated letters and custom light bar with programmable LED illumination. Slate stone veneer support column and pointed concrete base. Power for sign to be on a dedicated circuit and either 110 or 277 volt. All construction and assembly and mounting hardware to be non-corrosive.

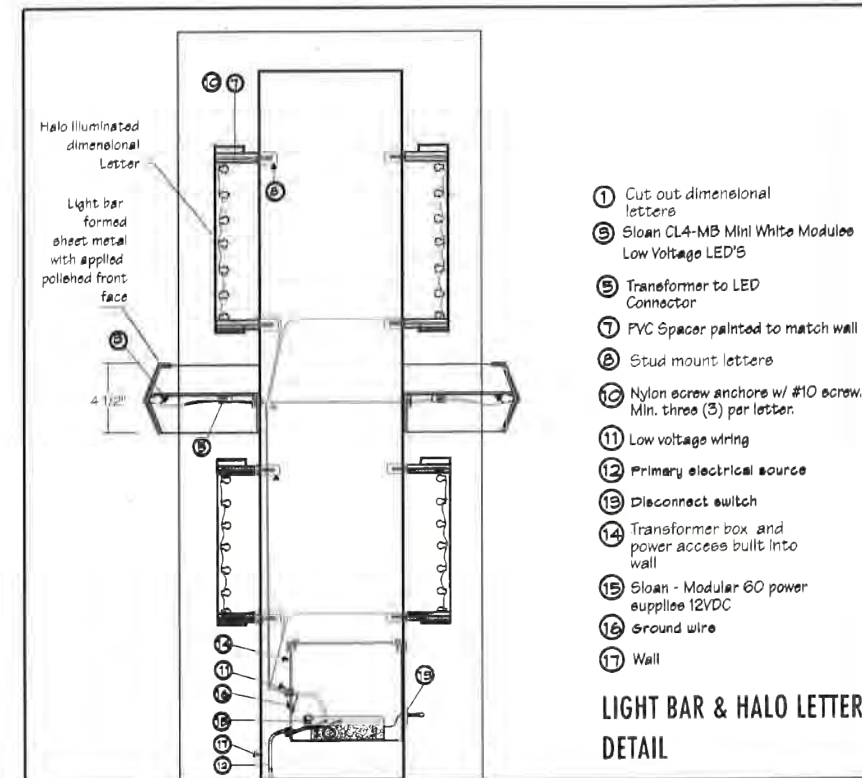


Sign Type 1.01: Entry Monument, Front View  
Left side of entry

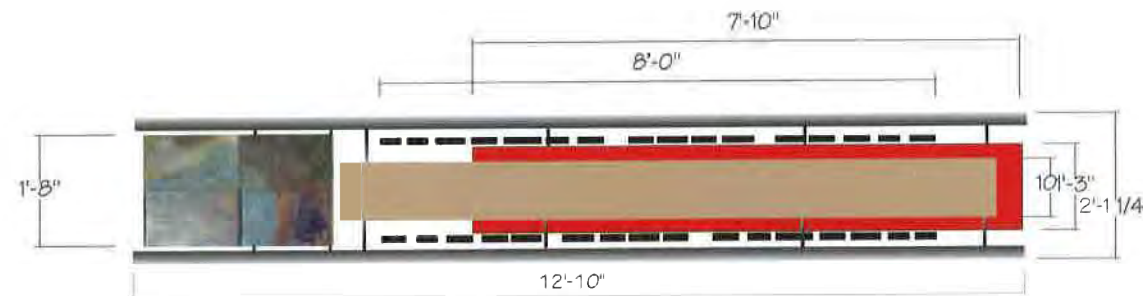
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Sign Type: 1.02A

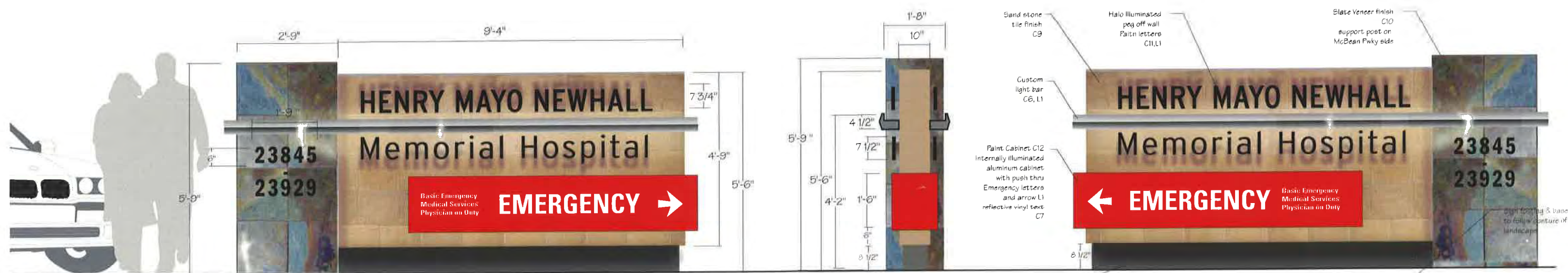
Fabricated aluminum cabinet sign double sided finished in sandstone. Halo illuminated letters and custom light bar with LED illumination. Stone veneer support and painted concrete base. Internally illuminated sign cabinet with routeout copy. Power for sign to be on a dedicated circuit and either 110 or 277 volt. All construction and assembly and mounting hardware to be non-corrosive.



LIGHT BAR & HALO LETTER DETAIL



Top View

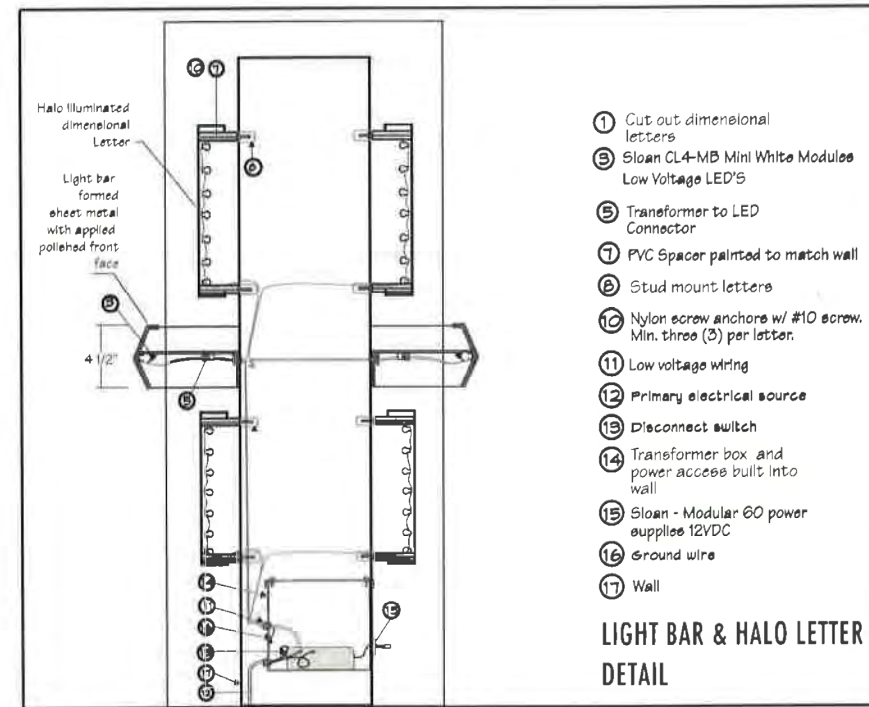


Sign Type 1.02A: Secondary Mounment

Interior End View

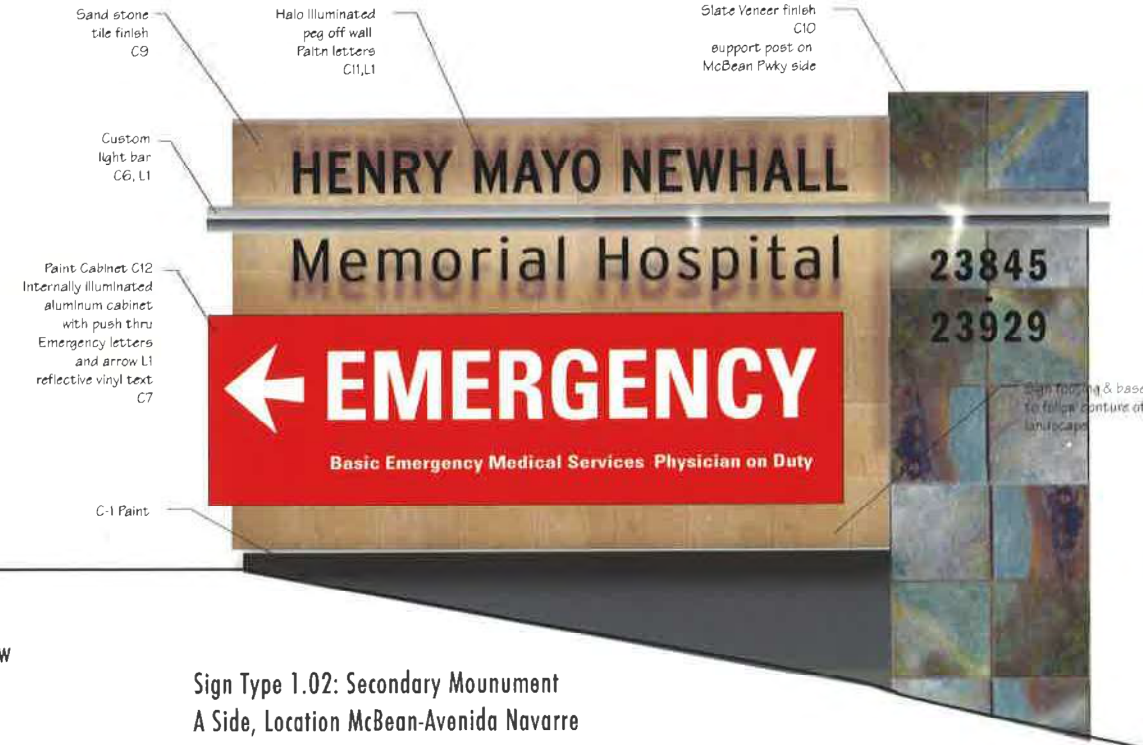
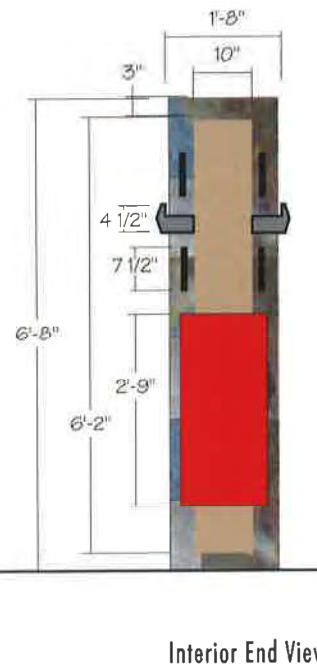
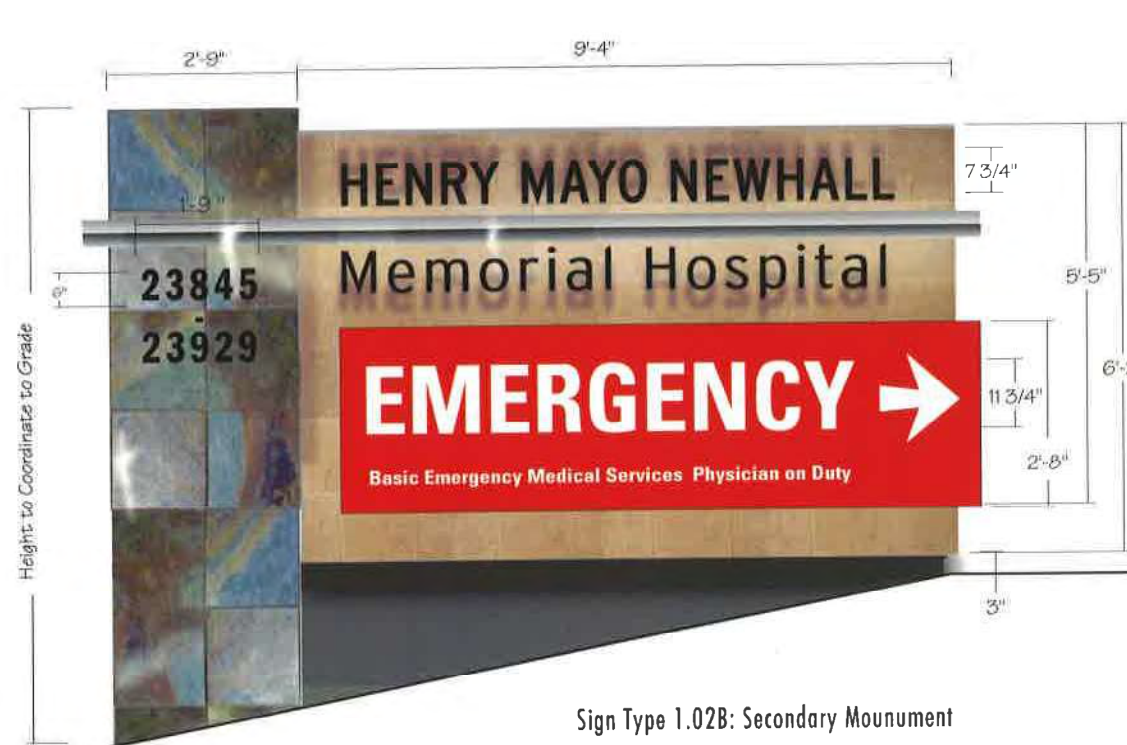
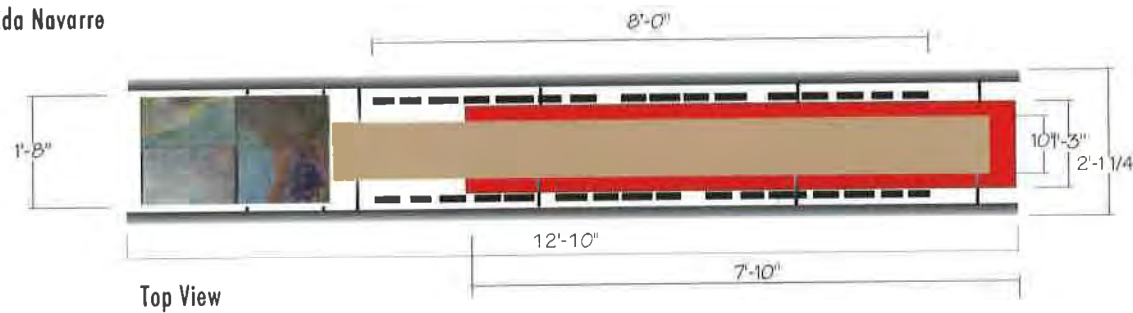


Sign Type 1.02B: Secondary Mounment  
B Side, Location McBean-Avenida Navarre

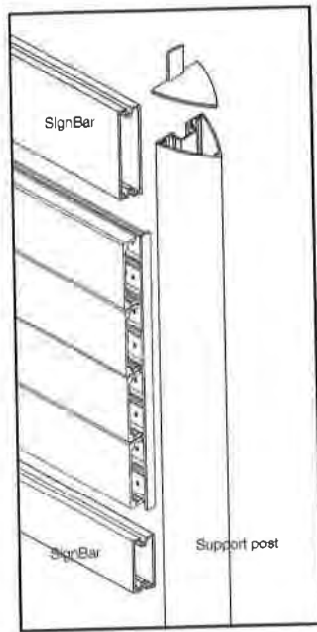


Sign Type: 1.02B

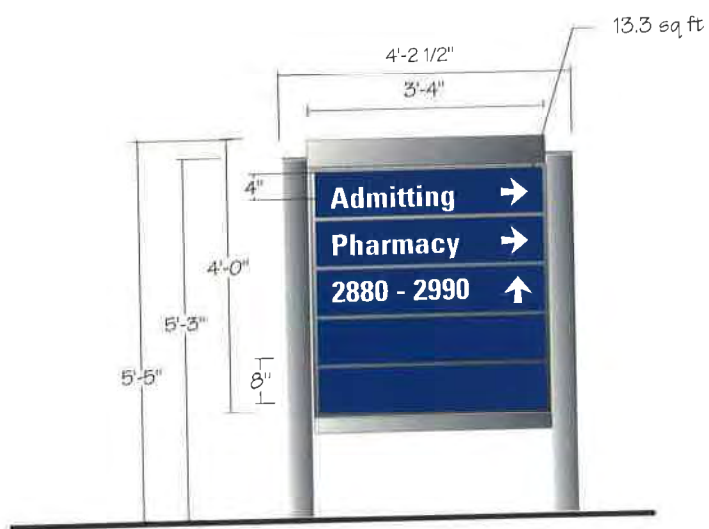
Tall-fabricated aluminum cabinet sign double sided finished in sandstone. Halo illuminated letters and custom light bar with LED illumination. Stone veneer support and painted concrete base. Internally illuminated sign cabinet with routeout copy. Power for sign to be on a dedicated circuit and either 110 or 277 volt. All construction and assembly and mounting hardware to be non-corrosive.



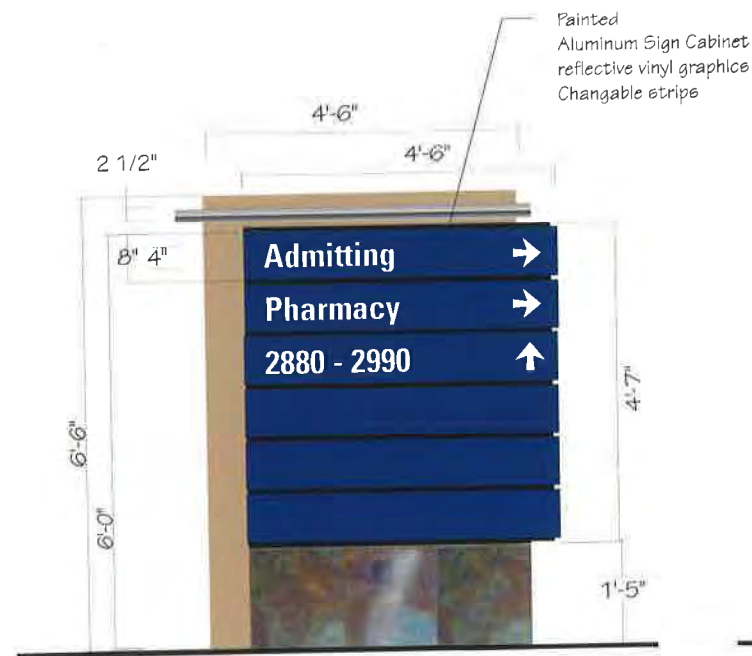
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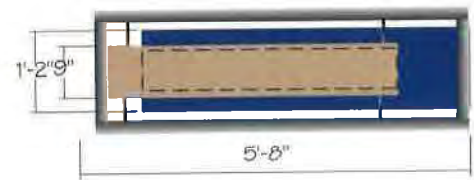
Top View



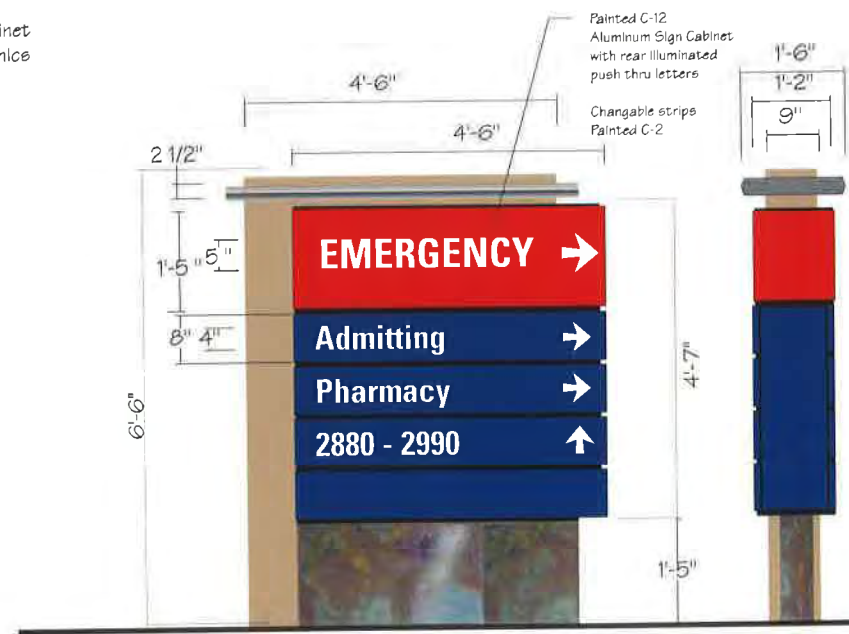
Front View  
Sign Type 2.04: Vehicular Directional



Front View  
Sign Type 2.03: Vehicular Directional



Top View



Front View  
Sign Type 2.02: Vehicular Directional



**Sign Type: 2.02**

Fabricated aluminum cabinet sign. Double faced Internally illuminated sign cabinets with changable strips with push through letters. Paint and stone veneer suport and base. Power for sign to be on a dedicated circuit and either 110 or 277 volt. All construction and assembly and mounting hardware to be non-corrosive.

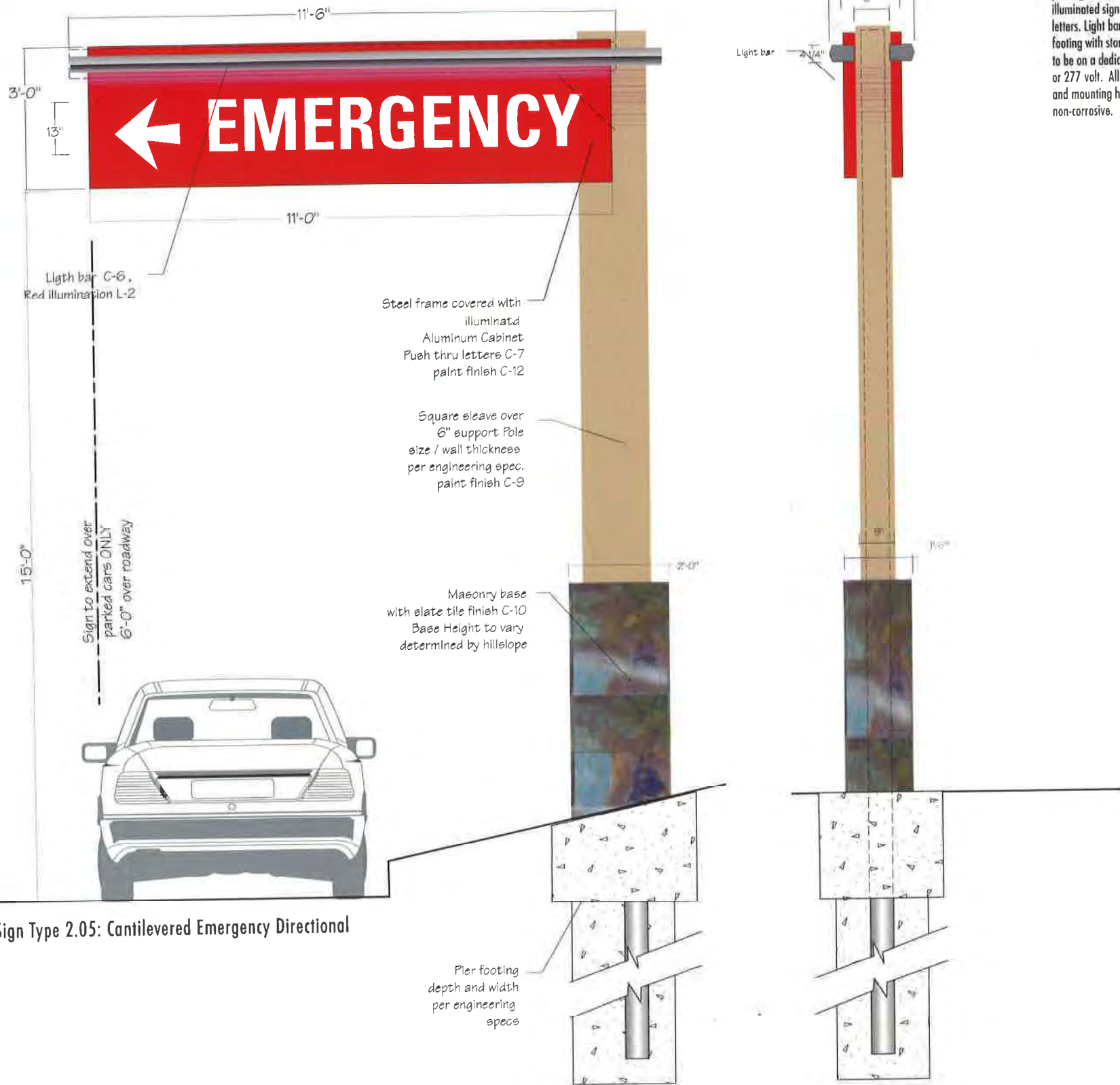
**Sign Type: 2.03**

Fabricated aluminum cabinet sign. Double faced Internally illuminated sign cabinets with changable strips with push through letters. Paint and stone veneer suport and base. Power for sign to be on a dedicated circuit and either 110 or 277 volt. All construction and assembly and mounting hardware to be non-corrosive

**Sign Type: 2.04**

Post and panel sign Double sided. Fabricated aluminum panel changable graphic insert panels as required. With reflective vinyl .

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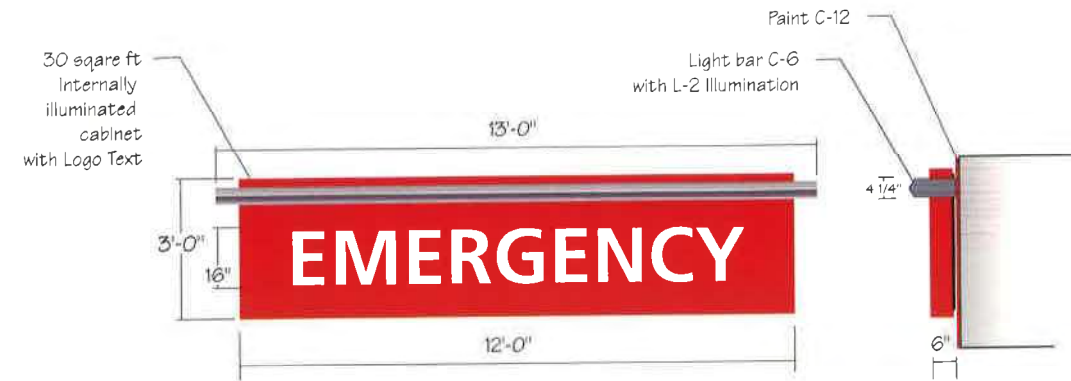
**Sign Type: 2.05**

Fabricated aluminum and steel cantilevered pole sign. Double faced internally illuminated sign cabinet with push through letters. Light bar illumination in red. Pylon footing with stone veneer. Power for sign to be on a dedicated circuit and either 110 or 277 volt. All construction and assembly and mounting hardware to be non-corrosive.



**Sign Type 2.05: Cantilevered Emergency Directional**

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Sign Type 4.02 Wall Cabinet  
SCALE: 1/4" = 1'



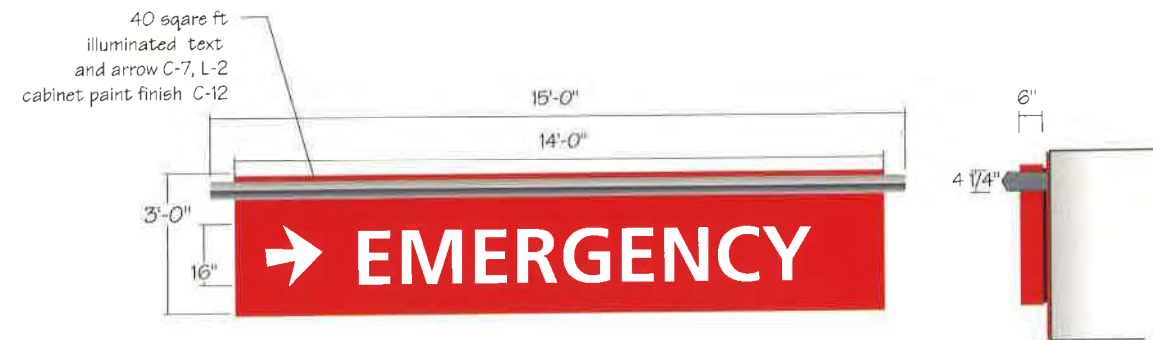
Sign Type 4.02 Wall Cabinet, Alternate Color

**Sign Type: 4.02**

Internally illuminated aluminum fabricated cabinet with push thru copy. Indirect illuminated accent bar. Power for sign to be on a dedicated circuit and either 110 or 277 volt. All construction and assembly and mounting hardware to be non-corrosive. All building penetrations are to be sealed and water tight.

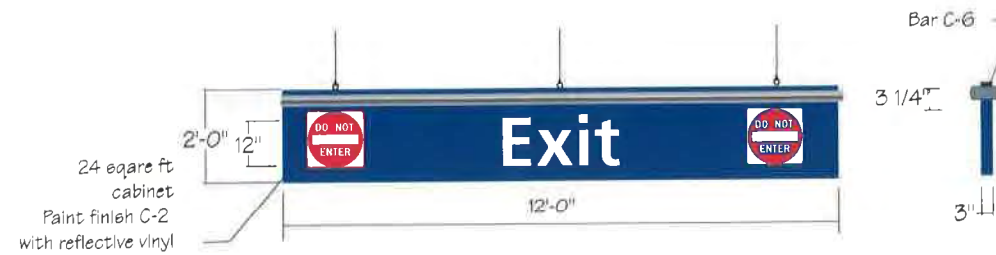
**Sign Type: 4.03**

Internally illuminated aluminum fabricated cabinet with push thru copy. Indirect illuminated accent bar. Power for sign to be on a dedicated circuit and either 110 or 277 volt. All construction and assembly and mounting hardware to be non-corrosive. All building penetrations are to be sealed and water tight.

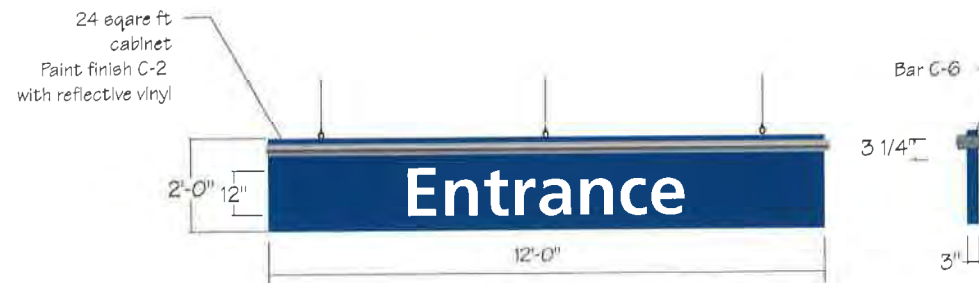


Sign Type 4.03 Wall Cabinet  
SCALE: 1/4" = 1'

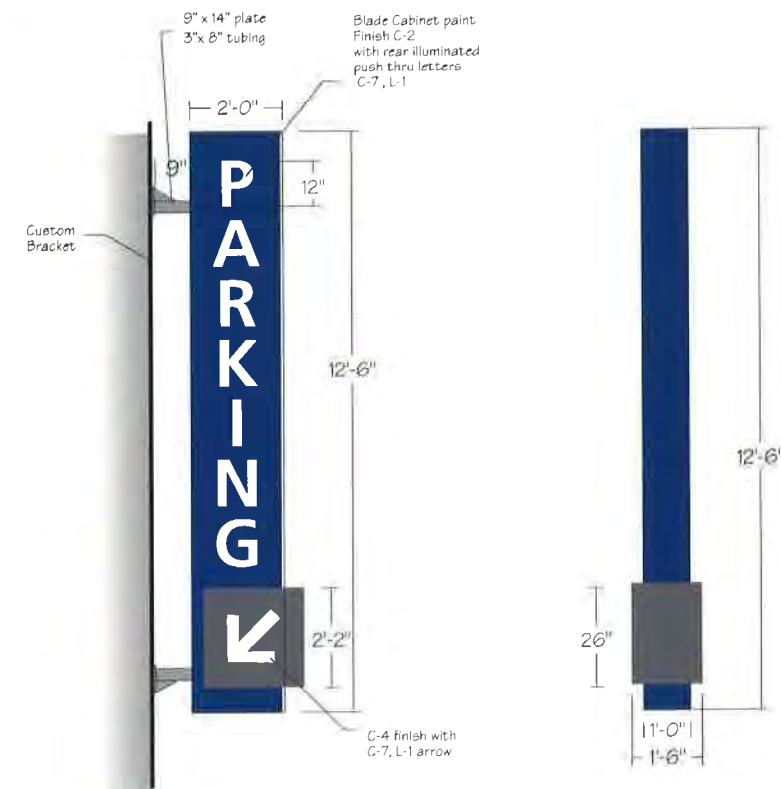




Sign Type 7.01 Hanging Cabinet  
SCALE: 1/4" = 1'



Sign Type 7.01 Hanging Cabinet  
SCALE: 1/4" = 1'



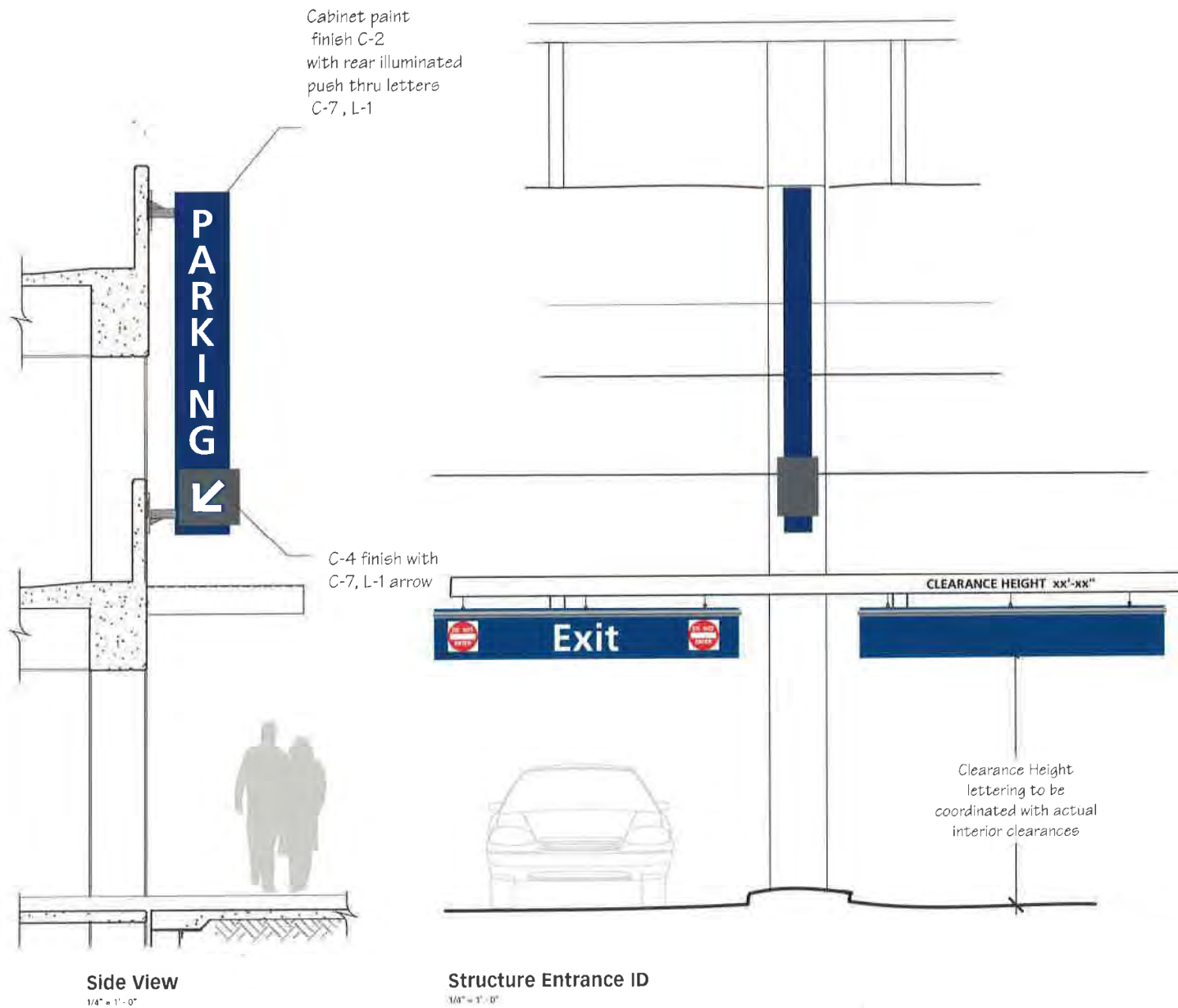
Sign Type 7.02 Vertical Wall Cabinet  
SCALE: 1/4" = 1'

**Sign Type: 7.01**

Alluminum fabricated cabinet with reflective vinyl copy. All construction and assembly and mounting hardware to be non-corrosive.

**Sign Type: 7.02**

Internally illuminated aluminum fabricated two sided blade cabinet with push thru copy. Power for sign to be on a dedicated circuit and either 110 or 277 volt. All construction and assembly and mounting hardware to be non-corrosive. All building penetrations are to be sealed and water tight.



Side View  
1/4" = 1'-0"

Structure Entrance ID  
1/8" = 1'-0"

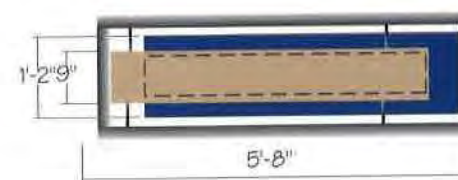
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### Sign Type: 6.01

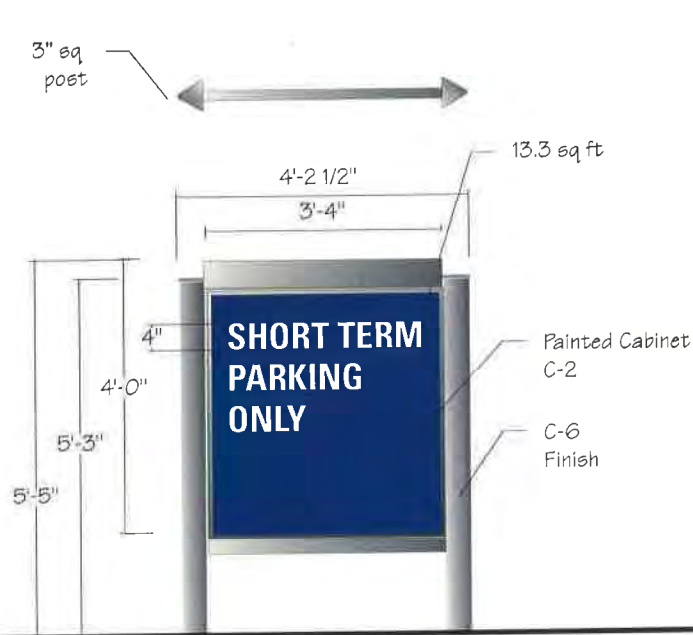
Fabricated aluminum cabinet sign. Double faced internally illuminated sign cabinet with push through letters. Paint and stone veneer support and base. Power for sign to be on a dedicated circuit and either 110 or 277 volt. All construction and assembly and mounting hardware to be non-corrosive

### Sign Type: 6.02

Aluminum post and panel non-illuminated sign. Double sided. Fabricated aluminum panel graphic insert panels as required. Reflective vinyl text.



Top View



Front View

Sign Type 6.02: Parking Lot Identification

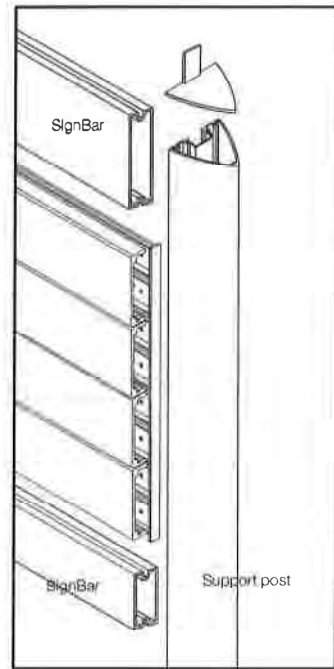


Front View

Sign Type 6.01: Parking Lot Identification



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**Sign Type: 3.03**

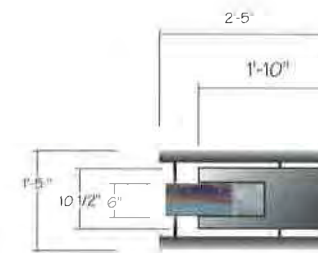
Post and panel sign Double sided. Fabricated aluminum panel changable graphic insert panels as required. With reflective vinyl text.

**Sign Type: 3.04**

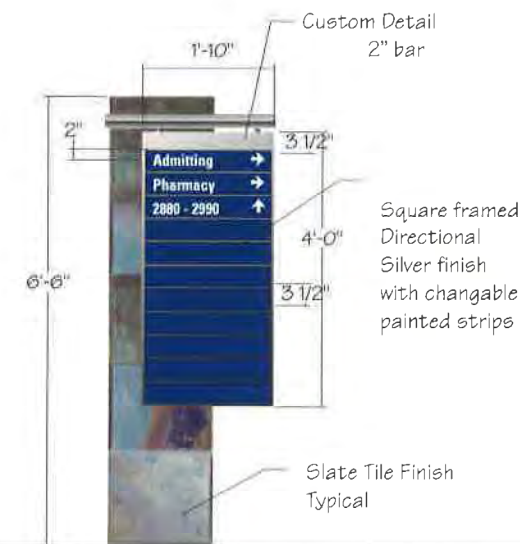
Post and panel sign Double sided. Fabricated aluminum panel changable graphic insert panels as required. With reflective vinyl text.

**Sign Type: 3.02**

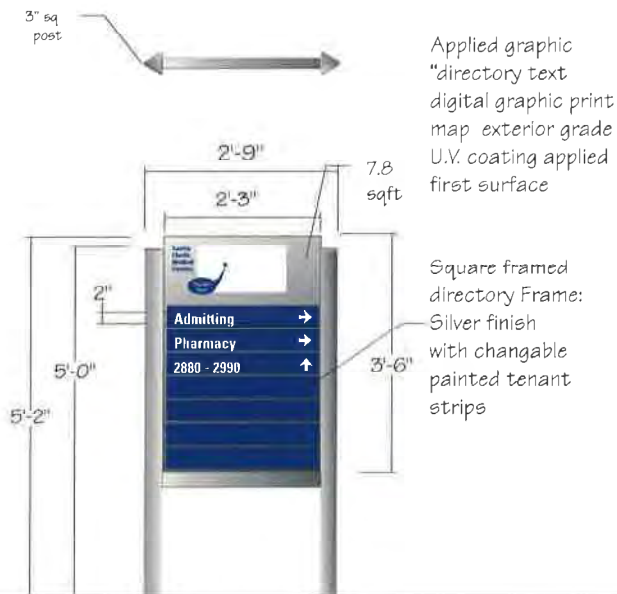
Double sided Fabricated aluminum frame . with insert panels vinyl text. Custom bar hanging bracket, stone veneer support and painted concrete base. All construction and assembly and mounting hardware to be non-corrosive.



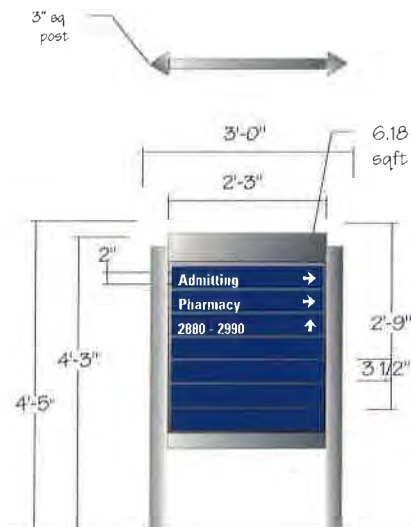
Top view



3.02: Pedestrian Directional



3.03: Pedestrian Directional

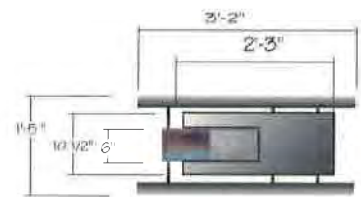


3.04: Pedestrian Directional

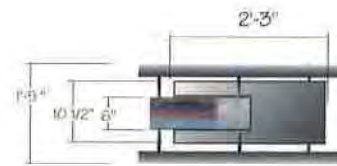




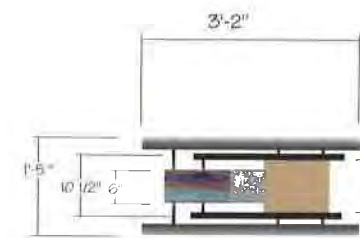
Sign Type 5.02: Directory & Campus Map  
Typical Condition



Top view



Top view



Top view

**Sign Type: 9.01**

Double sided fabricated aluminum frame with insert panels and applied text. Custom bar hanging bracket, stone veneer support and painted concrete base. All construction and assembly and mounting hardware to be non-corrosive

**Sign Type: 5.03**

Double sided fabricated aluminum frame with insert panels and applied text. Custom bar hanging bracket, stone veneer support and painted concrete base. All construction and assembly and mounting hardware to be non-corrosive

**Sign Type: 5.01**

Double sided fabricated aluminum frame with insert panels and applied text. Custom bar hanging bracket, stone veneer support and painted concrete base. All construction and assembly and mounting hardware to be non-corrosive

**Sign Type: 5.02**

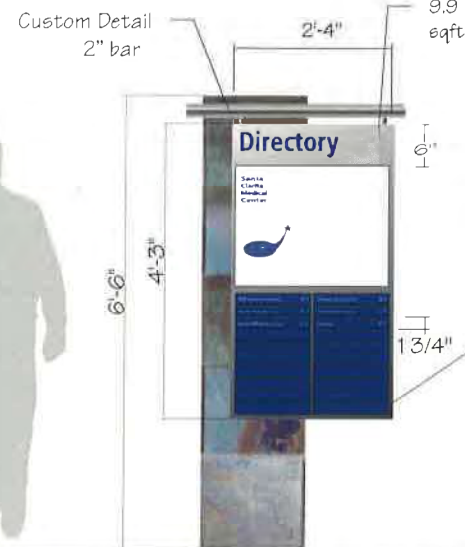
Wall mounted directory with map and applied text. All construction and assembly and mounting hardware to be non-corrosive.

Applied graphic "directory" and Address number text, digital graphic print map exterior grade U.V. coating applied first surface.

Single sided Square framed directory Frame: Silver finish with changable painted tenant strips



Custom Detail 2" bar

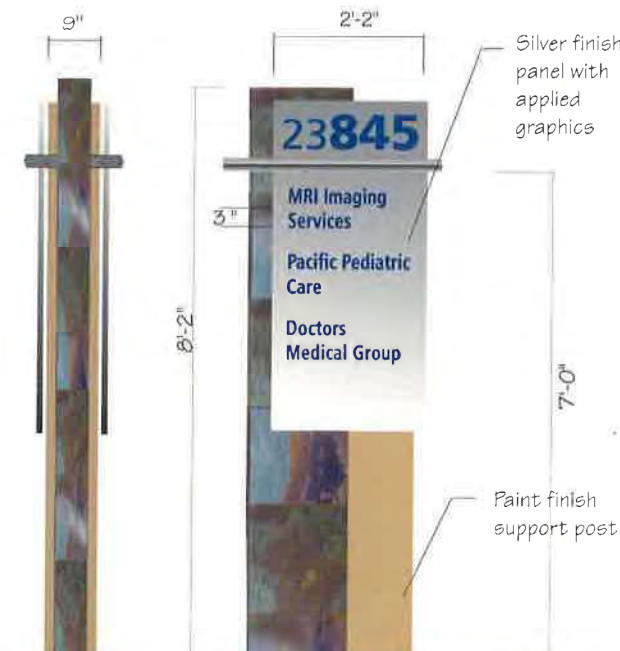


Applied graphic "directory text" digital graphic print map exterior grade U.V. coating applied first surface

Square framed directory Frame: Silver finish with changable painted tenant strips



Square framed directory Silver finish with changable painted tenant strips



Silver finish panel with applied graphics

Paint finish support post

Sign Type 5.02: Directory & Map  
Wall Mount

Sign Type 5.01: Campus Map

Sign Type 5.03: Address- Building ID  
Tenant Directory

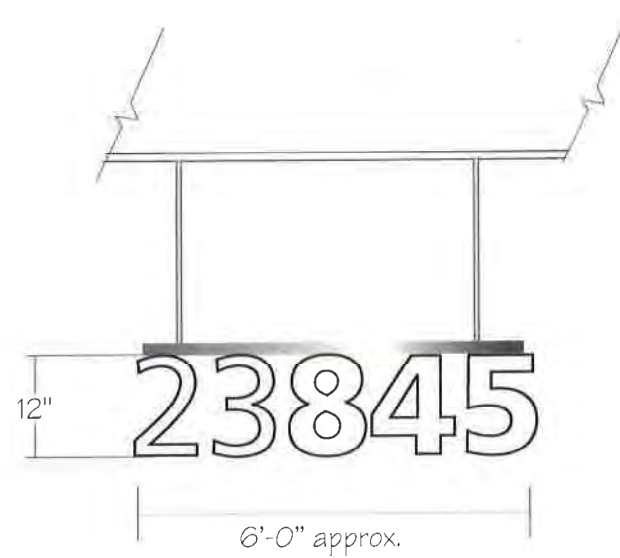
Sign Type 9.01: Address-Building ID  
Anchor Tenant/Service ID

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MOB 1 North Elevation -  
SCALE: 1/16" = 1'-0"

Pendant hung internally  
illuminated  
building address numbers



Sign Type: 12.02 - Pendant Hung Building Address Numbers  
SCALE: 1/2" = 1'



Letters mounted to support top  
bar Pendant mount assembly to  
canopy cross bar. Finish C-3

Letters 12" high Internally  
Illuminated L-1, white face  
dimensional letters with sides and  
back in Finish C-2

6 3/4" & 8" x 1/2" thick  
Non-illuminated aluminum  
fabricated numbers mounted to  
building surface.  
8" Finish C-3 & 6 3/4" Finish C2

23845

Sign Type: 12.03 - Building Address Numbers

10" & 12" x 1" thick non-illuminated  
aluminum fabricated numbers  
mounted to building surface.  
12" Finish C-3 & 10" Finish C2

23845

Sign Type: 12.01 - Building Address Numbers  
SCALE: 1" = 1'

**Sign Type: 12.02**  
Face illuminated fabricated numbers with  
LED internal illumination. Mounted to  
support top bar Pendant mount assembly to  
canopy cross bar. Numbers faces are white  
and sides and backs are painted per  
specified color. All mounting hardware to  
be non-corrosive.

**Sign Type: 12.01**  
Non-illuminate fabricated metal numbers.  
Number are painted per specified color and  
finish. All mounting hardware to be  
non-corrosive.

**Sign Type: 12.03**  
Non-illuminate fabricated metal numbers.  
Number are painted per specified color and  
finish. All mounting hardware to be  
non-corrosive.

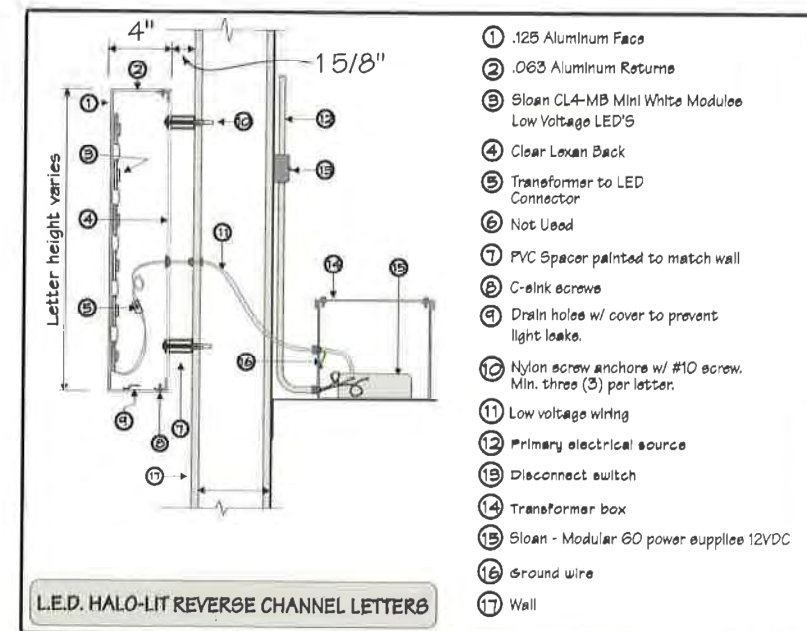
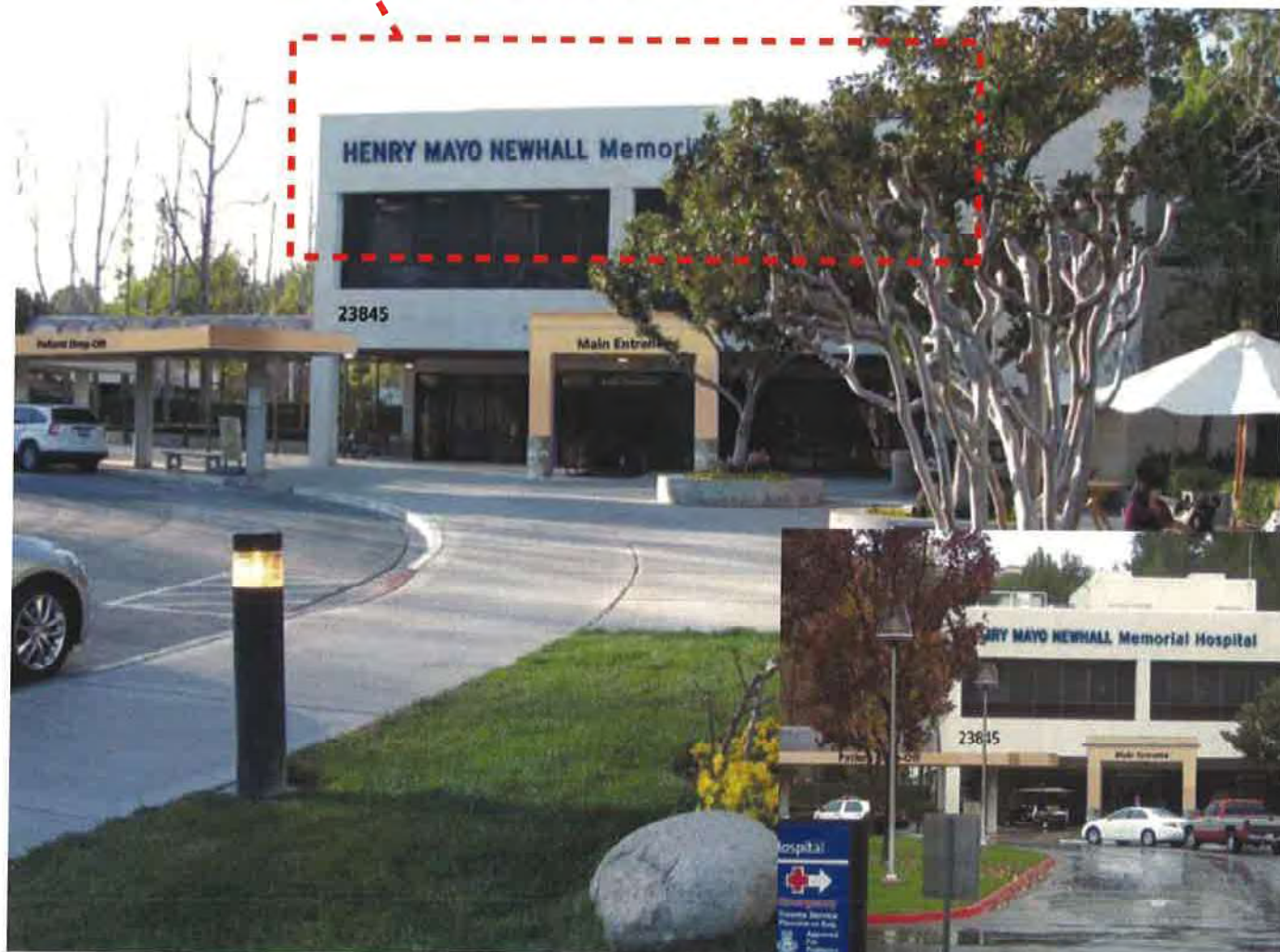
39'-10"

24"

# HENRY MAYO NEWHALL Memorial Hospital

Sign Type 11.01 Building Letters Hospital Entrance  
SCALE: 1/4" = 1'

L-1  
Halo LED Illuminated reverse pan channel letters  
C-2 Paint finish face and sides



Sign Type: 11.01

Halo-Lit LED Reverse Pan Channel Letters. Letters are aluminum construction (.125 faces with .063 returns) painted per specified color. Letters have 3/16" clear backs. Halo-lit with white LED's per Sloan layout to remote transformer(s) behind wall. Mount letters 1 5/8" from wall. All assembly and mounting hardware to be non-corrosive. All building penetrations are to be sealed and water tight. Mounting connection to building to be coordinated with building surface and letter weight and approved by OSHPD inspector.

Sign Type: 11.02

Halo-Lit LED Reverse Pan Channel Letters. Letters are aluminum construction (.125 faces with .063 returns) painted per specified color. Letters have 3/16" clear backs. Halo-lit with white LED's per Sloan layout to remote transformer(s) behind wall. Mount letters 1 5/8" from wall. All assembly and mounting hardware to be non-corrosive. All building penetrations are to be sealed and water tight. Mounting connection to building to be coordinated with building surface and letter weight and approved by OSHPD inspector.

2'-6"

# HENRY MAYO NEWHALL Memorial Hospital

Sign Type 11.02 Hospital Name on New Building  
Location, finish and size to be determined by building architecture.

L-1  
Halo LED Illuminated reverse pan channel letters.  
Paint color or metal finish to coordinate with building finishes.



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# 1'-0" Main Entrance

Sign Type 11.03: Non-illuminated Dimensional Letters

Building Entrance Identification  
Building Identification

SCALE: 1/2" = 1'

# 1'-0" Patient Drop Off

Sign Type 11.04: Non-illuminated Dimensional Letters

Building Entrance Identification  
Building Identification,  
Operational Identification

SCALE: 1/2" = 1'

# 6" Service Entry Identification Donor Recognition Name

Sign Type 11.05: Non-illuminated Dimensional Letters

Building Entrance Identification  
Building Identification,  
Operational Identification  
Donor Recognition on a building

SCALE: 1/2" = 1'



## Sign Type: 11.03

Dimensional fabricated non-illuminated metal letters. Letters are painted per specified color. Letters have 1/4" stand off from wall. Stub mount at min. three points. All mounting hardware to be non-corrosive. All building penetrations are to be sealed and water tight.

## Sign Type: 11.04

Dimensional fabricated non-illuminated metal letters. Letters are painted per specified color. Letters have 1/4" stand off from wall. Stub mount at min. three points. All mounting hardware to be non-corrosive. All building penetrations are to be sealed and water tight.

## Sign Type: 11.05

Dimensional cut out or fabricated non-illuminated metal letters. Letters are painted per specified color. Letters have 1/4" stand off from wall. Stub mount at min. three points. All mounting hardware to be non-corrosive. All building penetrations are to be sealed and water tight.

**Sign Type: H08.02**

Illuminated letters with translucent faces. Lighting is by LED internal illumination. Letters sides are painted per tenants specified color. All mounting hardware to be non-corrosive. All building penetrations are to be sealed and water tight.



MOB 1 South Elevation  
SCALE: 1/32" = 1'-0"

Tenant Sign Type 8.02  
Cap Height: 18" max.  
Sign field show at 2' H x 26' Long Maximum

Tenant Sign Type 8.02  
Cap Height: 18" max.  
Sign field show at 2' H x 26' Long Maximum



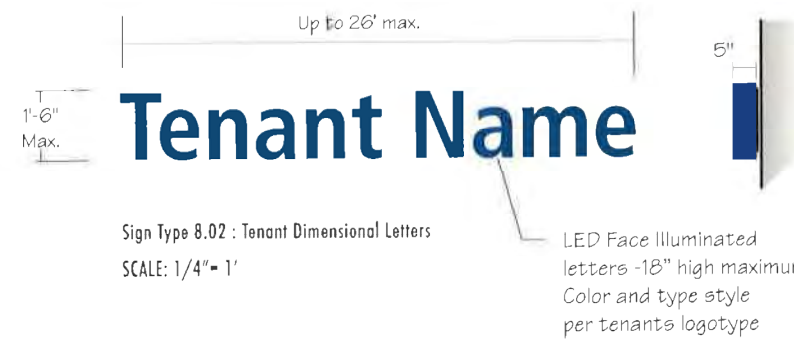
MOB 1 North Elevation  
SCALE: 1/32" = 1'-0"

Tenant Sign Type 8.02  
Cap Height: 18" max.  
Sign field show at 2' H x 26' Long Maximum

Tenant Sign Type 8.02  
Cap Height: 18" max.  
Sign field show at 2' H x 26' Long Maximum



MOB 1 West Elevation -  
SCALE: 1/32" = 1'-0"



Sign Type 8.02 : Tenant Dimensional Letters  
SCALE: 1/4" = 1'

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Existing Condition MOB - 23929



Existing Condition MOB - 23929

Existing Tenant Signs  
Previously approved by City

Existing Tenant Signs  
Previously approved by City

**\*\* Existing letters do not exceed 18" cap. height. Future letters at existing tenant locations are not to exceed 18" cap height as that is the tenant standard for all buildings.**



Sign Type 8.01 : Tenant Dimensional Letters  
SCALE: 1/4" = 1'

Color and type style  
per tenants logotype

**Sign Type: H08.01**

Existing Tenant Identification

Illuminated letters with translucent faces. Lighting is by LED internal illumination. Letters sides are painted per tenants specified color. All mounting hardware to be non-corrosive. All building penetrations are to be sealed and water tight.



Location M-239  
Sign type 4.05

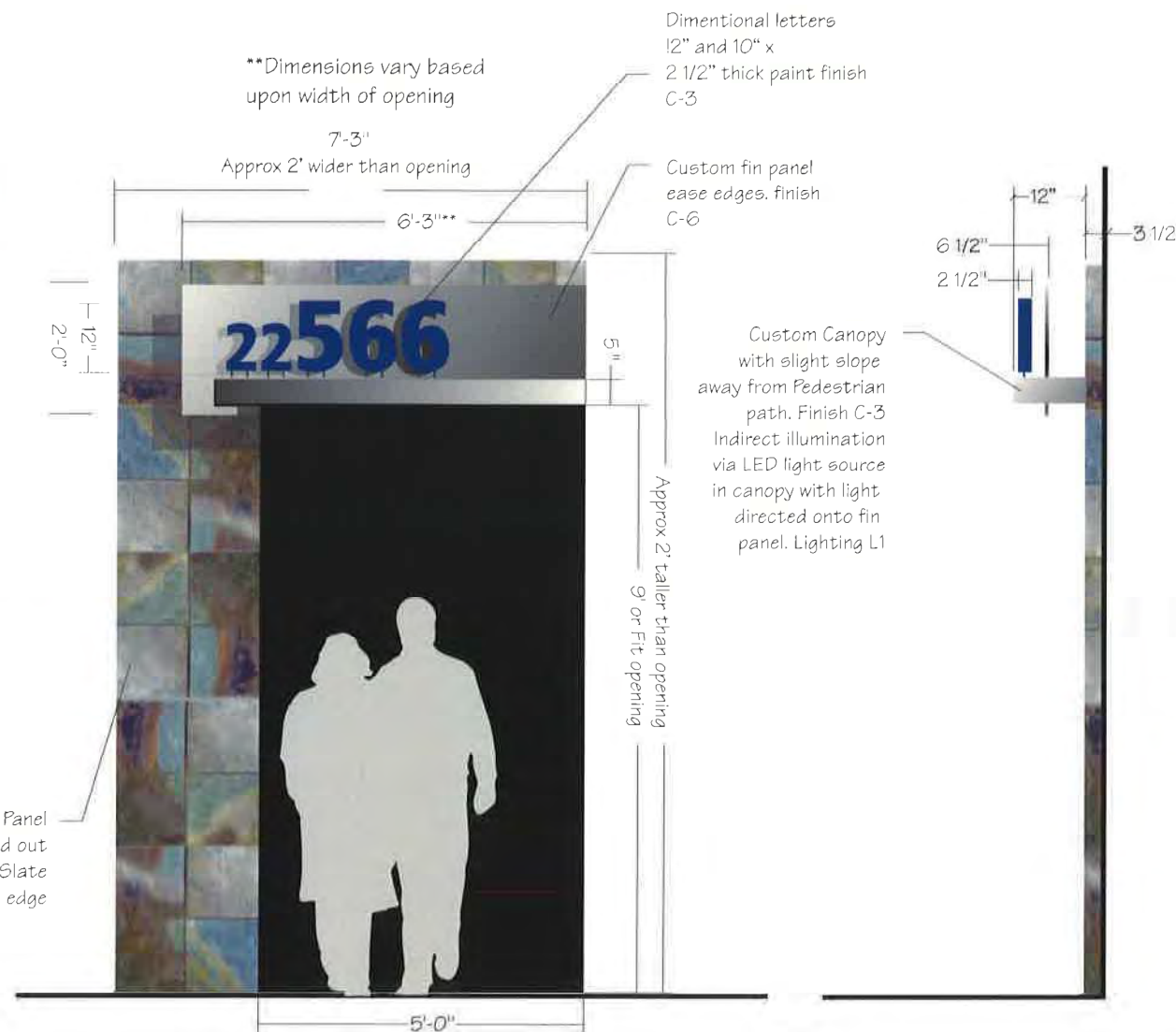
Location M-316  
Sign Type 8-02

Location M 240  
Sign Type 9.01

MOB Entry - 23861 Shown

**Sign Type: 4.05**

Entry Surround Causom to each opening. 3" build out of slate Veneer. Fabricated aluminum Canopy extend from building 12". Back fin plate behind 2 1/2" thick dimensional letters 12 and 10" high Paint finish. Indirect illumination on fin panel. All construction and assembly and mounting hardware to be non-corrosive

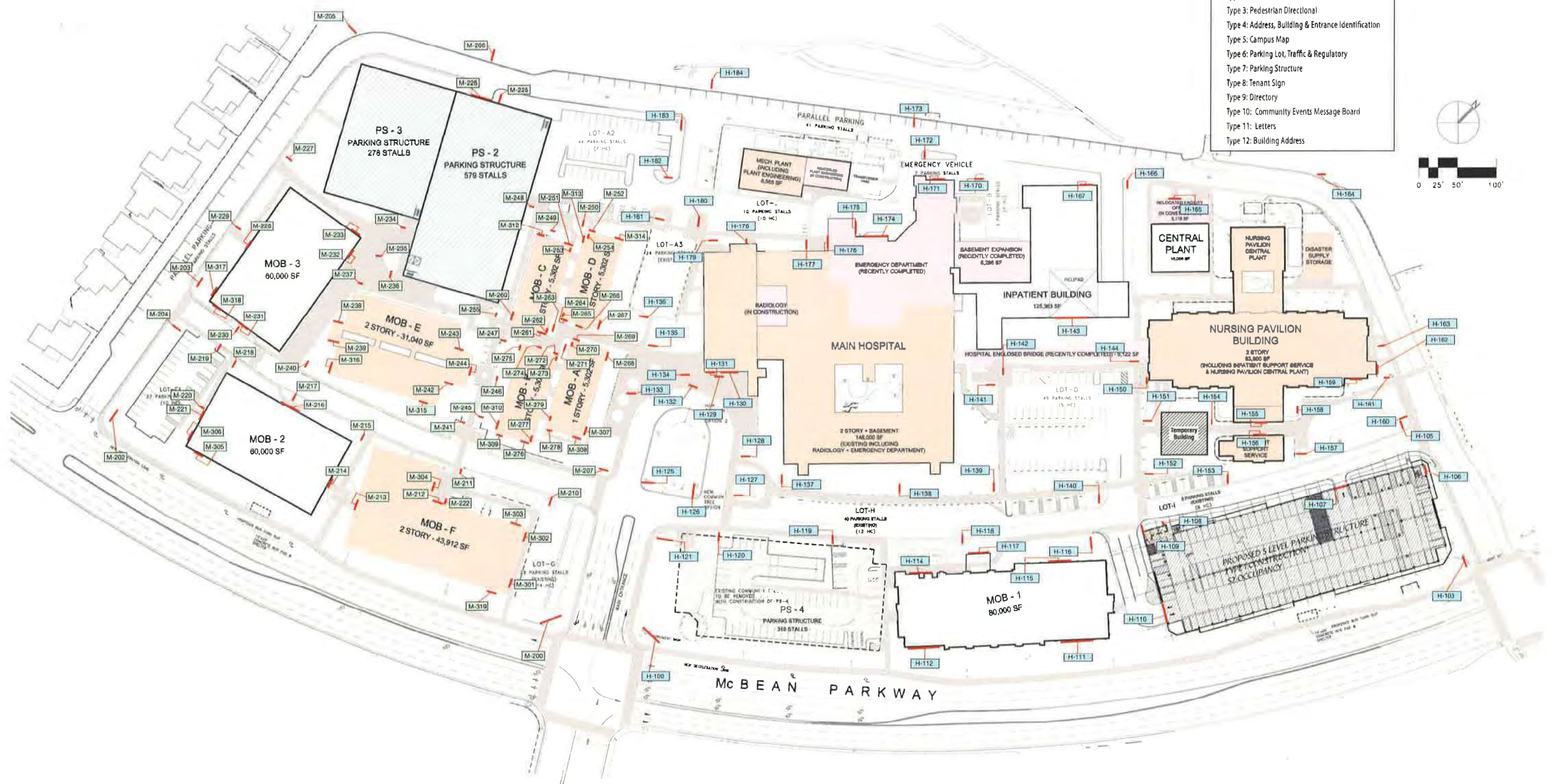
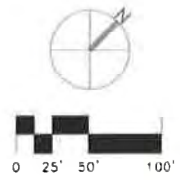


Custom Top & Side Panel to fit opening. build out depth 3" Finish Slate C-10 face and exposed edge

**4.05: Building Entry ID with Address Number**

**SIGN TYPE KEY:**

<b>H</b>	Signs on Hospital Site
<b>M</b>	Signs on Medical Office Building Site
Type 1	Monument / Freestanding Identification Sign
Type 2	Vehicular Directional
Type 3	Pedestrian Directional
Type 4	Address, Building & Entrance Identification
Type 5	Campus Map
Type 6	Parking Lot, Traffic & Regulatory
Type 7	Parking Structure
Type 8	Tenant Sign
Type 9	Directory
Type 10	Community Events Message Board
Type 11	Letters
Type 12	Building Address



Area	Sign Location	Sign Type	Function	Description	Emergency Dept Info	Graphic Width	Graphic Height	Graphic Sq. Ft.	Sign Width	Sign Height	Sign Sq. Ft.	Illumination	Double Sided
H	100	1.01	Entry Monument	Horizontal Monument with Hospital Identity at Site Entry		26'-0"	1'-0"	26.0	31'-9"	4'-0"	127.0	yes	no
H	105	2.02	Vehicular & Emergency Directional	Directional sign with information for drivers	yes	4'-6"	4'-7"	20.5	4'-9"	6'-6"	30.8	yes	no
H	121	2.02	Vehicular Directional	Directional sign with information for drivers	yes	4'-6"	4'-7"	20.5	4'-9"	6'-6"	30.8	yes	yes
H	125	2.02	Vehicular Directional	Directional sign with information for drivers	yes	4'-6"	4'-7"	20.5	4'-9"	6'-6"	30.8	yes	yes
H	160	2.02	Vehicular & Emergency Directional	Directional sign with information for drivers	yes	4'-6"	4'-7"	20.5	4'-9"	6'-6"	30.8	yes	yes
H	164	2.02	Vehicular & Emergency Directional	Directional sign with information for drivers	yes	4'-6"	4'-7"	20.5	4'-9"	6'-6"	30.8	yes	no
H	172	2.02	Vehicular & Emergency Directional	Directional sign with information for drivers	yes	4'-6"	4'-7"	20.5	4'-9"	6'-6"	30.8	yes	yes
H	184	2.02	Vehicular & Emergency Directional	Directional sign with information for drivers	yes	4'-6"	4'-7"	20.5	4'-9"	6'-6"	30.8	yes	yes
H	116	2.03	Vehicular Directional	Directional sign with information for drivers	yes	4'-6"	4'-7"	20.5	4'-9"	6'-6"	30.8	yes	yes
H	120	2.03	Vehicular Directional	Directional sign with information for drivers		4'-6"	4'-7"	20.5	4'-9"	6'-6"	30.8	yes	yes
H	126	2.03	Vehicular Directional	Directional sign with information for drivers		4'-6"	4'-7"	20.5	4'-9"	6'-6"	30.8	yes	yes
H	152	2.04	Vehicular Directional	Directional sign with information for drivers		3'-4"	4'-0"	13.3	4'-2"	5'-5"	13.3	yes	yes
H	153	2.04	Vehicular Directional	Directional sign with information for drivers		3'-4"	4'-0"	13.3	4'-2"	5'-5"	13.3	yes	yes
H	157	2.04	Vehicular Directional	Directional sign with information for drivers		3'-4"	4'-0"	13.3	4'-2"	5'-5"	13.3	yes	yes
H	163	2.04	Vehicular Directional	Directional sign with information for drivers		3'-4"	4'-0"	13.3	4'-2"	5'-5"	13.3	yes	yes
H	166	2.04	Vehicular Directional	Directional sign with information for drivers		3'-4"	4'-0"	13.3	4'-2"	5'-5"	13.3	yes	yes
H	182	2.04	Vehicular Directional	Directional sign with information for drivers		3'-4"	4'-0"	13.3	4'-2"	5'-5"	13.3	yes	yes
H	183	2.04	Vehicular Directional	Directional sign with information for drivers		3'-4"	4'-0"	13.3	4'-2"	5'-5"	13.3	yes	yes
H	173	2.05	Emergency Directional	Directional to Emergency	yes	11'-0"	3'-0"	33.0	11'-0"	18'-0"	33.0	yes	yes
H	127	3.02	Pedestrian Directional	Directional sign providing pedestrians directional information		1'-10"	4'-0"	7.1	2'-6"	6'-6"	13.9	no	yes
H	128	3.02	Pedestrian Directional	Directional sign providing pedestrians directional information		1'-10"	4'-0"	7.1	2'-6"	6'-6"	13.9	no	yes
H	133	3.02	Pedestrian Directional	Directional sign providing pedestrians directional information		1'-10"	4'-0"	7.1	2'-6"	6'-6"	13.9	no	yes
H	135	3.02	Pedestrian Directional	Directional sign providing pedestrians directional information		1'-10"	4'-0"	7.1	2'-6"	6'-6"	13.9	no	yes
H	137	3.02	Pedestrian Directional	Directional sign providing pedestrians directional information		1'-10"	4'-0"	7.1	2'-6"	6'-6"	13.9	no	yes
H	136	3.03	Pedestrian Directional	Directional sign providing pedestrians directional information		2'-3"	3'-6"	7.8	2'-9"	5'-2"	7.8	no	yes
H	177	3.03	Pedestrian Directional	Directional sign providing pedestrians directional information		2'-3"	3'-6"	7.8	2'-9"	5'-2"	7.8	no	yes
H	138	3.04	Pedestrian Directional	Directional sign providing pedestrians directional information		2'-3"	2'-9"	6.2	3'-0"	4'-5"	6.2	no	yes
H	141	3.04	Pedestrian Directional	Directional sign providing pedestrians directional information		2'-3"	2'-9"	6.2	3'-0"	4'-5"	6.2	no	yes
H	142	3.04	Pedestrian Directional	Directional sign providing pedestrians directional information		2'-3"	2'-9"	6.2	3'-0"	4'-5"	6.2	no	yes
H	144	3.04	Pedestrian Directional	Directional sign providing pedestrians directional information		2'-3"	2'-9"	6.2	3'-0"	4'-5"	6.2	no	yes
H	150	3.04	Pedestrian Directional	Directional sign providing pedestrians directional information		2'-3"	2'-9"	6.2	3'-0"	4'-5"	6.2	no	yes
H	178	3.04	Pedestrian Directional	Directional sign providing pedestrians directional information	yes	2'-3"	2'-9"	6.2	3'-0"	4'-5"	6.2	no	yes
H	179	3.04	Pedestrian Directional	Directional sign providing pedestrians directional information	yes	2'-3"	2'-9"	6.2	3'-0"	4'-5"	6.2	no	yes
H	175	4.02	Emergency Entrance Identification	Emergency Identification on Canopy	yes	3'-0"	12'-0"	36.0	3'-0"	12'-0"	36.0	yes	no
H	176	4.02	Emergency Entrance Identification	Emergency Identification on Canopy	yes	3'-0"	12'-0"	36.0	3'-0"	12'-0"	36.0	yes	no
H	174	4.03	Emergency Identification	Emergency Directional	yes	3'-0"	14'-0"	42.0	3'-0"	14'-0"	42.0	yes	no

Area	Sign Location	Sign Type	Function	Description	Emergency Dept Info	Graphic Width	Graphic Height	Graphic Sq. Ft.	Sign Width	Sign Height	Sign Sq. Ft.	Illumination	Double Sided
H	108	5.01	Campus Map	Freestanding Pedestrian Level Map of Site		2'-4"	4'-3"	9.9	3'-2"	6'-6"	20.3	no	yes
H	134	5.01	Campus Map	Freestanding Pedestrian Level Map of Site		2'-4"	4'-3"	9.9	3'-2"	6'-6"	20.3	no	yes
H	139	5.01	Campus Map	Freestanding Pedestrian Level Map of Site		2'-4"	4'-3"	9.9	3'-2"	6'-6"	20.3	no	yes
H	151	5.01	Campus Map	Freestanding Pedestrian Level Map of Site		2'-4"	4'-3"	9.9	3'-2"	6'-6"	20.3	no	yes
H	154	5.03	Directory	Freestanding Sign Listing Main Tenants Occupying a Particular Building/Facility		2'-4"	4'-3"	9.9	3'-2"	6'-6"	20.3	no	yes
H	119	6.01	Parking Lot Identification	Sign Identifying Parking Lot		4'-6"	5'-3"	23.6	4'-9"	7'-6"	33.8	yes	yes
H	140	6.01	Parking Lot Identification	Sign Identifying Parking Lot		4'-6"	5'-3"	23.6	4'-9"	7'-6"	33.8	yes	yes
H	170	6.02	Parking Lot Identification	Sign Identifying Parking Lot		3'-4"	4'-0"	13.3	4'-2"	5'-5"	13.3	yes	yes
H	180	6.02	Parking Lot Identification	Sign Identifying Parking Lot		3'-4"	4'-0"	13.3	4'-2"	5'-5"	13.3	yes	yes
H	181	6.02	Parking Lot Identification	Sign Identifying Parking Lot		3'-4"	4'-0"	13.3	4'-2"	5'-5"	13.3	yes	yes
H	107	7.01	Parking Exit Identification	"Exit Only" sign at PS-1 exit		12'-0"	2'-0"	24.0	12'-0"	2'-0"	24.0	no	yes
H	110	7.01	Parking Entrance Identification	Parking Entrance Identification on Building at Entry to PS-1		12'-0"	2'-0"	24.0	12'-0"	2'-0"	24.0	no	yes
H	109	7.02	Parking Identification Blade Sign	Parking Entrance Identification with arrow		2'-0"	12'-0"	24.0	2'-0"	12'-0"	24.0	yes	yes
H	111	8.02	Tenant Identification	Tenant Identification on Building Façade of MOB 1		up to 26'-0"	cap. height 1'-6" max.	52.0	varies	2'-0" max.	52 max.	yes	no
H	112	8.02	Tenant Identification	Tenant Identification on Building Façade of MOB 1		up to 26'-0"	cap. height 1'-6" max.	52.0	varies	2'-0" max.	52 max.	yes	no
H	114	8.02	Tenant Identification	Tenant Identification on Building Façade of MOB 1		up to 26'-0"	cap. height 1'-6" max.	52.0	varies	2'-0" max.	52 max.	yes	no
H	115	8.02	Tenant Identification	Tenant Identification on Building Façade of MOB 1		up to 26'-0"	cap. height 1'-6" max.	52.0	varies	2'-0" max.	52 max.	yes	no
H	118	9.01	Directory	Freestanding Sign Listing Services Occupying a Particular Building		2'-2"	4'-9"	10.5	2'-8"	8'-2"	24.5	no	yes
H	158	9.01	Directory	Freestanding Sign Listing Services/Tenants Occupying Building		2'-2"	4'-9"	10.5	2'-8"	8'-2"	24.5	no	yes
H	161	9.01	Directory	Freestanding Sign Listing Services/Tenants Occupying Building		2'-2"	4'-9"	10.5	2'-8"	8'-2"	24.5	no	yes
H	130	11.01	Hospital Name	Hospital Identification on Building at Main Entrance		53'-0"	2'-0"	106.0	53'-0"	2'-0"	106.0	yes	no
H	143	11.02	Hospital Name	Hospital Identification on new Patient Tower		unknown	up to 2'-6"	180.0	unknown	up to 2'-6"	180.0	yes	no
H	129	11.03	Entrance Identification	Hospital Main Entrance Identification		15'-0"	1'-0"	12.8	15'-0"	1'-0"	12.8	no	no
H	171	11.03	Designated Vehicle Use Identification	Ambulance Entrance Sign	yes	15'-0"	1'-0"	12.8	15'-0"	1'-0"	12.8	no	no
H	132	11.04	Designated Vehicle Use Identification	Patient Drop Off Canopy		13'-6"	10"	11.2	13'-6"	10"	11.2	no	no
H	162	11.04	Entrance Identification	Entry Identification at Behavioral Health Entrance		15'-0"	10"	12.8	15'-0"	10"	12.8	no	no
H	155	11.05	Building Identification	Identification at Entry Doors to Nursing Pavilion		8'-0"	6"	4.0	8'-0"	6"	4.0	no	no
H	156	11.05	Building Identification	Identification at Entry Doors to Inpatient Support Building		8'-0"	6"	4.0	8'-0"	6"	4.0	no	yes
H	165	11.05	Entrance Identification	Identification at Entrance to Facilities Building		8'-0"	6"	4.0	8'-0"	6"	4.0	no	no
H	106	12.01	Address	Building Street Address		6'-0"	1'-0"	6.0	6'-0"	1'-0"	6.0	no	no
H	131	12.01	Address	Building Street Address		6'-0"	1'-0"	6.0	6'-0"	1'-0"	6.0	no	no
H	159	12.01	Address	Building Street Address		6'-0"	1'-0"	6.0	6'-0"	1'-0"	6.0	no	no
H	167	12.01	Address	Building Street Address		6'-0"	1'-0"	6.0	6'-0"	1'-0"	6.0	no	no
H	117	12.02	Address	Address Numbers (MOB 1) Pendant		6'-0"	1'-0"	6.0	6'-0"	1'-0"	6.0	no	no
H	103	1.02 B	Entry & Emergency Monument	Hospital Identity & Emergency Identification at Site Entry	yes	9'-4"	4'-9"	57.5	12'-1"	6'-2"	75.6	yes	yes

Area	Sign Location	Sign Type	Function	Description	Emergency Dept Info	Graphic Width	Graphic Height	Graphic Sq. Ft.	Sign Width	Sign Height	Sign Sq. Ft.	Illumination	Double Sided
M	200	1.01	Entry Monument	Horizontal Monument with Hospital Identity at Site Entry		26'-0"	1'-0"	26.0	31'-9"	4'-0"	127.0	yes	no
M	203	2.02	Vehicular & Emergency Directional	Directional sign with information for drivers	yes	4'-6"	4'-7"	20.5	4'-9"	6'-6"	30.8	yes	no
M	204	2.02	Vehicular & Emergency Directional	Directional sign with information for drivers	yes	4'-6"	4'-7"	20.5	4'-9"	6'-6"	30.8	yes	yes
M	205	2.02	Vehicular & Emergency Directional	Directional sign with information for drivers	yes	4'-6"	4'-7"	20.5	4'-9"	6'-6"	30.8	yes	no
M	206	2.02	Vehicular & Emergency Directional	Directional sign with information for drivers	yes	4'-6"	4'-7"	20.5	4'-9"	6'-6"	30.8	yes	yes
M	207	2.02	Vehicular Directional	Directional sign with information for drivers	yes	4'-6"	4'-7"	20.5	4'-9"	6'-6"	30.8	yes	yes
M	210	2.02	Vehicular & Emergency Directional	Directional sign with information for drivers	yes	4'-6"	4'-7"	20.5	4'-9"	6'-6"	30.8	yes	yes
M	219	2.04	Vehicular Directional	Directional sign with information for drivers		3'-4"	4'-0"	13.3	4'-2"	5'-5"	13.3	yes	yes
M	227	2.04	Vehicular Directional	Directional sign with information for drivers		3'-4"	4'-0"	13.3	4'-2"	5'-5"	13.3	yes	yes
M	215	3.02	Pedestrian Directional	Directional sign providing pedestrians directional information		1'-10"	4'-0"	7.1	2'-6"	6'-6"	13.9	no	yes
M	218	3.02	Pedestrian Directional	Directional sign providing pedestrians directional information		1'-10"	4'-0"	7.1	2'-6"	6'-6"	13.9	no	yes
M	236	3.03	Pedestrian Directional	Directional sign providing pedestrians directional information		2'-3"	3'-6"	7.8	2'-9"	5'-2"	7.8	no	yes
M	246	3.03	Pedestrian Directional	Directional sign providing pedestrians directional information		2'-3"	3'-6"	7.8	2'-9"	5'-2"	7.8	no	yes
M	263	3.03	Pedestrian Directional	Directional sign providing pedestrians directional information		2'-3"	3'-6"	7.8	2'-9"	5'-2"	7.8	no	yes
M	266	3.03	Pedestrian Directional	Directional sign providing pedestrians directional information		2'-3"	3'-6"	7.8	2'-9"	5'-2"	7.8	no	yes
M	271	3.03	Pedestrian Directional	Directional sign providing pedestrians directional information		2'-3"	3'-6"	7.8	2'-9"	5'-2"	7.8	no	yes
M	274	3.03	Pedestrian Directional	Directional sign providing pedestrians directional information		2'-3"	3'-6"	7.8	2'-9"	5'-2"	7.8	no	yes
M	234	3.04	Pedestrian Directional	Directional sign providing pedestrians directional information		2'-3"	2'-9"	6.2	3'-0"	4'-5"	6.2	no	yes
M	237	3.04	Pedestrian Directional	Directional sign providing pedestrians directional information		2'-3"	2'-9"	6.2	3'-0"	4'-5"	6.2	no	yes
M	248	3.04	Pedestrian Directional	Directional sign providing pedestrians directional information		2'-3"	2'-9"	6.2	3'-0"	4'-5"	6.2	no	yes
M	255	3.04	Pedestrian Directional	Directional sign providing pedestrians directional information		2'-3"	2'-9"	6.2	3'-0"	4'-5"	6.2	no	yes
M	212	4.05	MOB Building Identification	Building Entry & Address Identification		6'-3" varies	1'-0"	5.0	5'-0"	1'-0"	5.0	yes	no
M	213	4.05	MOB Building Identification	Building Entry & Address Identification		6'-3" varies	1'-0"	5.0	5'-0"	1'-0"	5.0	yes	no
M	214	4.05	MOB Building Identification	Building Entry & Address Identification		6'-3" varies	1'-0"	5.0	5'-0"	1'-0"	5.0	yes	no
M	216	4.05	MOB Building Identification	Building Entry & Address Identification		6'-3" varies	1'-0"	5.0	5'-0"	1'-0"	5.0	yes	no
M	221	4.05	MOB Building Identification	Building Entry & Address Identification		6'-3" varies	1'-0"	5.0	5'-0"	1'-0"	5.0	yes	no
M	222	4.05	MOB Building Identification	Building Entry & Address Identification		6'-3" varies	1'-0"	5.0	5'-0"	1'-0"	5.0	yes	no
M	228	4.05	MOB Building Identification	Building Entry & Address Identification		6'-3" varies	1'-0"	5.0	5'-0"	1'-0"	5.0	yes	no
M	231	4.05	MOB Building Identification	Building Entry & Address Identification		6'-3" varies	1'-0"	5.0	5'-0"	1'-0"	5.0	yes	no
M	232	4.05	MOB Building Identification	Building Entry & Address Identification		6'-3" varies	1'-0"	5.0	5'-0"	1'-0"	5.0	yes	no
M	233	4.05	MOB Building Identification	Building Entry & Address Identification		6'-3" varies	1'-0"	5.0	5'-0"	1'-0"	5.0	yes	no
M	238	4.05	MOB Building Identification	Building Entry & Address Identification		6'-3" varies	1'-0"	5.0	5'-0"	1'-0"	5.0	yes	no
M	239	4.05	MOB Building Identification	Building Entry & Address Identification		6'-3" varies	1'-0"	5.0	5'-0"	1'-0"	5.0	yes	no
M	242	4.05	MOB Building Identification	Building Entry & Address Identification		6'-3" varies	1'-0"	5.0	5'-0"	1'-0"	5.0	yes	no
M	243	4.05	MOB Building Identification	Building Entry & Address Identification		6'-3" varies	1'-0"	5.0	5'-0"	1'-0"	5.0	yes	no
M	244	4.05	MOB Building Identification	Building Entry & Address Identification		6'-3" varies	1'-0"	5.0	5'-0"	1'-0"	5.0	yes	no
M	251	4.05	MOB Building Identification	Building Entry & Address Identification		6'-3" varies	1'-0"	5.0	5'-0"	1'-0"	5.0	yes	no
M	252	4.05	MOB Building Identification	Building Entry & Address Identification		6'-3" varies	1'-0"	5.0	5'-0"	1'-0"	5.0	yes	no

Area	Sign Location	Sign Type	Function	Description	Emergency Dept Info	Graphic Width	Graphic Height	Graphic Sq. Ft.	Sign Width	Sign Height	Sign Sq. Ft.	Illumination	Double Sided
M	261	4.05	MOB Building Identification	Building Entry & Address Identification		6'-3" varies	1'-0"	5.0	5'-0"	1'-0"	5.0	yes	no
M	264	4.05	MOB Building Identification	Building Entry & Address Identification		6'-3" varies	1'-0"	5.0	5'-0"	1'-0"	5.0	yes	no
M	269	4.05	MOB Building Identification	Building Entry & Address Identification		6'-3" varies	1'-0"	5.0	5'-0"	1'-0"	5.0	yes	no
M	272	4.05	MOB Building Identification	Building Entry & Address Identification		6'-3" varies	1'-0"	5.0	5'-0"	1'-0"	5.0	yes	no
M	276	4.05	MOB Building Identification	Building Entry & Address Identification		6'-3" varies	1'-0"	5.0	5'-0"	1'-0"	5.0	yes	no
M	278	4.05	MOB Building Identification	Building Entry & Address Identification		6'-3" varies	1'-0"	5.0	5'-0"	1'-0"	5.0	yes	no
M	235	5.01	Campus Map	Freestanding Pedestrian Level Map of Site		2'-4"	4'-3"	9.9	3'-2"	6'-6"	20.3	no	yes
M	245	5.01	Campus Map	Freestanding Pedestrian Level Map of Site		2'-4"	4'-3"	9.9	3'-2"	6'-6"	20.3	no	yes
M	247	5.01	Campus Map	Freestanding Pedestrian Level Map of Site		2'-4"	4'-3"	9.9	3'-2"	6'-6"	20.3	no	yes
M	249	5.01	Campus Map	Freestanding Pedestrian Level Map of Site		2'-4"	4'-3"	9.9	3'-2"	6'-6"	20.3	no	yes
M	253	5.02	Directory	Wall mounted directory listing tenants/services in a building		1'-3"	4'-0"	5.0	1'-3"	4'-0"	5.0	no	no
M	254	5.02	Directory	Wall mounted directory listing tenants/services in a building		1'-3"	4'-0"	5.0	1'-3"	4'-0"	5.0	no	no
M	262	5.02	Directory	Wall mounted directory listing tenants/services in a building		1'-3"	4'-0"	5.0	1'-3"	4'-0"	5.0	no	no
M	265	5.02	Directory	Wall mounted directory listing tenants/services in a building		1'-3"	4'-0"	5.0	1'-3"	4'-0"	5.0	no	no
M	270	5.02	Directory	Wall mounted directory listing tenants/services in a building		1'-3"	4'-0"	5.0	1'-3"	4'-0"	5.0	no	no
M	273	5.02	Directory	Wall mounted directory listing tenants/services in a building		1'-3"	4'-0"	5.0	1'-3"	4'-0"	5.0	no	no
M	277	5.02	Directory	Wall mounted directory listing tenants/services in a building		1'-3"	4'-0"	5.0	1'-3"	4'-0"	5.0	no	no
M	279	5.02	Directory	Wall mounted directory listing tenants/services in a building		1'-3"	4'-0"	5.0	1'-3"	4'-0"	5.0	no	no
M	220	5.03	Tenant Directory	Freestanding Sign Listing Main Tenants Occupying a Particular Building/Facility		2'-4"	4'-3"	9.9	3'-2"	6'-6"	20.3	no	yes
M	241	5.03	Tenant Directory	Freestanding Sign Listing Main Tenants Occupying a Particular Building/Facility		2'-4"	4'-3"	9.9	3'-2"	6'-6"	20.3	no	yes
M	250	5.03	Tenant Directory	Freestanding Sign Listing Main Tenants Occupying a Particular Building/Facility		2'-4"	4'-3"	9.9	3'-2"	6'-6"	20.3	no	yes
M	226	7.01	Parking Entrance Identification	Parking Entrance Identification on Building at Entry		12'-0"	2'-0"	24.0	12'-0"	2'-0"	24.0	no	yes
M	225	7.02	Parking Identification Blade Sign	Parking Entrance Identification (with Directional Arrow) on building.		2'-0"	12'-0"	24.0	2'-0"	12'-0"	24.0	yes	yes
M	301	8.01	Existing Tenant Identification	Existing Tenant Identification on Building Façade		up to 22'-0"	cap. height 1'-6" max.	33.0	varies	2'-0" max.	33 max.	yes	no
M	302	8.01	Existing Tenant Identification	Existing Tenant Identification on Building Façade		up to 22'-0"	cap. height 1'-6" max.	33.0	varies	2'-0" max.	33 max.	yes	no
M	303	8.01	Existing Tenant Identification	Existing Tenant Identification on Building Façade		up to 22'-0"	cap. height 1'-6" max.	33.0	varies	2'-0" max.	33 max.	yes	no
M	304	8.01	Existing Tenant Identification	Existing Tenant Identification on Building Façade		up to 22'-0"	cap. height 1'-6" max.	33.0	varies	2'-0" max.	33 max.	yes	no
M	305	8.01	Tenant Identification	Tenant Identification on Building Façade		up to 22'-0"	cap. height 1'-6" max.	33.0	varies	2'-0" max.	33 max.	yes	no
M	306	8.01	Tenant Identification	Tenant Identification on Building Façade		up to 22'-0"	cap. height 1'-6" max.	33.0	varies	2'-0" max.	33 max.	yes	no
M	307	8.01	Tenant Identification	Tenant Identification on Building Façade		up to 22'-0"	cap. height 1'-6" max.	33.0	varies	2'-0" max.	33 max.	yes	no
M	308	8.01	Tenant Identification	Tenant Identification on Building Façade		up to 22'-0"	cap. height 1'-6" max.	33.0	varies	2'-0" max.	33 max.	yes	no
M	309	8.01	Tenant Identification	Tenant Identification on Building Façade		up to 22'-0"	cap. height 1'-6" max.	33.0	varies	2'-0" max.	33 max.	yes	no
M	310	8.01	Tenant Identification	Tenant Identification on Building Façade		up to 22'-0"	cap. height 1'-6" max.	33.0	varies	2'-0" max.	33 max.	yes	no
M	312	8.01	Tenant Identification	Tenant Identification on Building Façade		up to 22'-0"	cap. height 1'-6" max.	33.0	varies	2'-0" max.	33 max.	yes	no
M	313	8.01	Tenant Identification	Tenant Identification on Building Façade		up to 22'-0"	cap. height 1'-6" max.	33.0	varies	2'-0" max.	33 max.	yes	no
M	314	8.01	Tenant Identification	Tenant Identification on Building Façade		up to 22'-0"	cap. height 1'-6" max.	33.0	varies	2'-0" max.	33 max.	yes	no
M	315	8.01	Tenant Identification	Tenant Identification on Building Façade		up to 22'-0"	cap. height 1'-6" max.	33.0	varies	2'-0" max.	33 max.	yes	no
M	316	8.01	Tenant Identification	Tenant Identification on Building Façade		up to 22'-0"	cap. height 1'-6" max.	33.0	varies	2'-0" max.	33 max.	yes	no

Area	Sign Location	Sign Type	Function	Description	Emergency Dept Info	Graphic Width	Graphic Height	Graphic Sq. Ft.	Sign Width	Sign Height	Sign Sq. Ft.	Illumination	Double Sided
M	317	8.01	Tenant Identification	Tenant Identification on Building Façade		up to 22'-0"	cap. height 1'-6" max.	33.0	varies	2'-0" max.	33 max.	yes	no
M	318	8.01	Tenant Identification	Tenant Identification on Building Façade		up to 22'-0"	cap. height 1'-6" max.	33.0	varies	2'-0" max.	33 max.	yes	no
M	319	8.01	Existing Tenant Identification	Existing Tenant Identification on Building Façade		up to 22'-0"	cap. height 1'-6" max.	33.0	varies	2'-0" max.	33 max.	yes	no
M	217	9.01	Directory	Freestanding Sign Listing Services/Tenants Occupying Building		2'-2"	4'-9"	10.5	2'8"	8'-2"	24.5	no	yes
M	229	9.01	Directory	Freestanding Sign Listing Services/Tenants Occupying Building		2'-2"	4'-9"	10.5	2'8"	8'-2"	24.5	no	yes
M	211	9.01	Directory	Freestanding Sign Listing Services/Tenants Occupying Building		2'-2"	4'-9"	10.5	2'8"	8'-2"	24.5	no	yes
M	230	9.01	Directory	Freestanding Sign Listing Services/Tenants Occupying Building		2'-2"	4'-9"	10.5	2'8"	8'-2"	24.5	no	yes
M	240	9.01	Directory	Freestanding Sign Listing Main Services/Tenants Occupying a Particular Building/Facility		2'-2"	4'-9"	10.5	2'8"	8'-2"	24.5	no	yes
M	260	12.03	Address	Building Street Address		3'-0"	8"	1.8	3'-0"	8"	1.8	no	no
M	267	12.03	Address	Building Street Address		3'-0"	8"	1.8	3'-0"	8"	1.8	no	no
M	268	12.03	Address	Building Street Address		3'-0"	8"	1.8	3'-0"	8"	1.8	no	no
M	275	12.03	Address	Building Street Address		3'-0"	8"	1.8	3'-0"	8"	1.8	no	no
M	202	1.02 A	Entry & Emergency Monument	Hospital Identity & Emergency Identification at Site Entry	yes	9'-4"	4'-9"	44.3	12'-1"	5'-9"	59.6	yes	yes



# HENRY MAYO NEWHALL MEMORIAL HOSPITAL

## MASTER SIGN PROGRAM OVERVIEW & JUSTIFICATION

A master sign program has been developed of the entire Henry Mayo Newhall Memorial Hospital campus. This includes both Henry Mayo Newhall Memorial Hospital and G&L Realty medical office buildings.

The way-finding program includes directional signs for drivers to direct them to the service or building they are seeking. There is a way-finding sign program for pedestrians with directional signs, "you are here" maps and directories to direct them to the various buildings and entrances. Directional signs are throughout the site to get people to the Emergency Entrance from any site entrance off McBean Parkway.

~~Electronic message displays have been provided in the program to accommodate the community event notices the hospital has been providing for years. The various parking structures and surface parking lots are identified to aid visitors in parking as well as capture all the Code, Liability and DMV sign requirements. The program has also identified parameters for Donor Recognition signing on buildings.~~

Sign criteria/standards have been developed for the various medical office buildings to identify the specific location for tenant signs as well as their size, square footage, type of sign and method of illumination. (Low energy LED lighting will be used throughout.) This program includes the existing signs as well as provisions for future signs on future buildings.

Along McBean Parkway the sign program has been designed to present a low-key stonewall look with night illumination being provided with indirect (halo) light sources. The only direct illumination is identification and directions to Emergency and that is accomplished by only illuminating the letters.

Within the front drive and throughout the front of the site the signs continue the use of stone trim. Internal illumination, when used, is only for the lettering. As one moves further into the site, the sign program reduces in scale (size and height) and shifts to non-illuminated aluminum post and panel signs, maps and directories.

The plan also includes options for enhancing building entrances (at the discretion of the building owners).

Because the Emergency Entrance is located in the rear of the hospital and recurring problems with the community finding the entrance, a strong presentation has been made in the way-finding program to direct people to the walk-in entrance and emergency parking.

This master sign program has been developed to accommodate both the current site and future site conditions and future buildings as is best known today, 2010. In several cases, the buildings have not yet been designed, the site conditions, landscaping, walkways and driveways are also yet to be designed so schematic sign concepts have been applied as best that can be anticipated. Some portions of the sign program, in future years, may require adjustments to accommodate changes that may come about when final designs are approved by the City for construction.

Implementation of the master sign program is not tied to a specific time or "event" schedule and will take place over the campus Master Plan period as Henry Mayo Newhall Memorial Hospital and G&L Realty move forward with various projects and improvements. Unless required by Code, implementation of the master sign program will be at the discretion of the hospital and G&L Realty.

The goal of the sign program is to present to the City of Santa Clarita a professional, non-commercial appearance complementing the high quality of care provided by the hospital and on-campus medical practices.

NOTE: Emergency signing, and specific text, is required by the state of California.  
California Code of Regulation, Title 22, Division 5, Chapter 1, Article 6, Section 70413, Paragraph J.



Orchard Drive Entry

**OPTION 1a**

Curved concrete  
accent/screen wall  
with painted finish

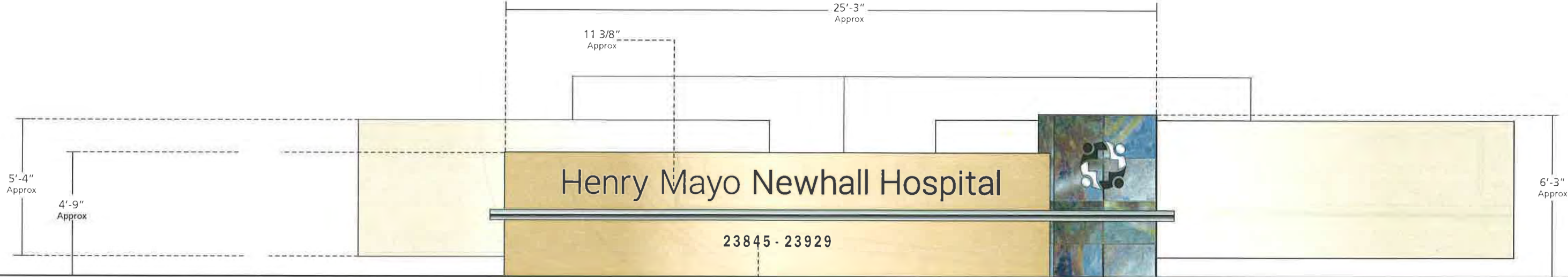
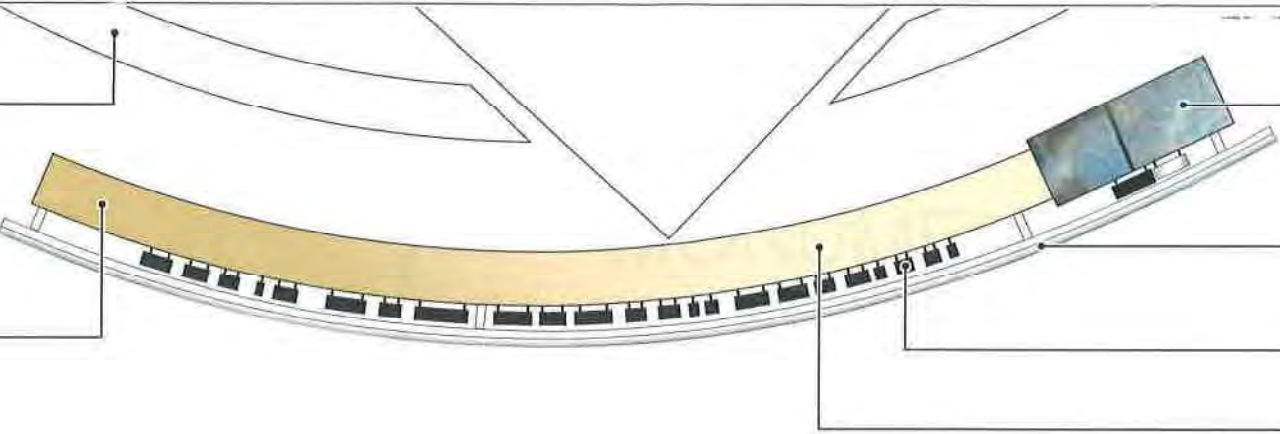
Fabricated aluminum  
monument sign  
with painted finish

Masonry pylon with  
stone cladding

Curved Stainless  
Steel light bar

Reverse -channel letters  
with Halo-illumination

Reverse -channel logo  
with Halo-illumination



Scale: 1/4" = 1'-0"

MCBean Parkway - Northbound View



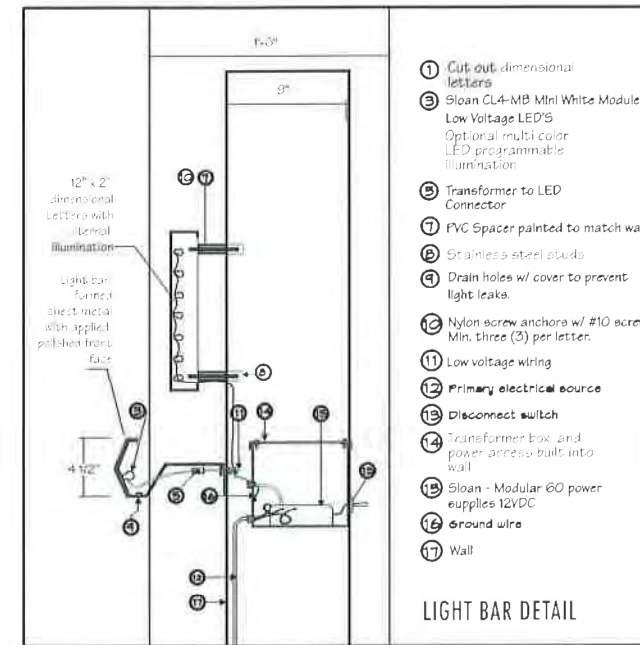
Scale: 1/16" = 1'-0"

Orchard Village Road - Westbound View



Sign Type 1.01: Entry Monument  
Right side of entry

Street End



Sign Type: 1.01

Cabinet wall structure "skinned" with sandstone tile. Halo illuminated letters and custom light bar with programmable LED illumination. Slate stone veneer support column and painted concrete base. Power for sign to be on a dedicated circuit and either 110 or 277 volt. All construction and assembly and mounting hardware to be non-corrosive.



Sign Type 1.01: Entry Monument, Front View  
Left side of entry

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