1. INTRODUCTION

The purpose of this section is to describe the proposed Vista Canyon project in a manner that is meaningful to the public, reviewing agencies and decision makers. For purposes of the California Environmental Quality Act (CEQA; Pub. Resources Code, Section 21000 et seq.), a complete project description must contain the following information:

(a) the precise location and boundaries of the proposed project, shown on a detailed map, along with a regional map of the project's location; (b) a statement of the objectives sought by the proposed project, which should include the underlying purpose of the project; (c) a general description of the project's technical, economic, and environmental characteristics; and (d) a statement briefly describing the intended uses of the EIR, including a list of the agencies that are expected to use the environmental impact report (EIR) in their decision making, a list of permits and other approvals required to implement the project, and a list of related environmental review and consultation requirements imposed by federal, state, or local laws, regulations or policies (State CEQA Guidelines, Section 15124). An adequate project description need not be exhaustive, but should supply the information necessary for the evaluation and review of the project's significant effects on the environment.

2. LEAD AGENCY

Under CEQA, the public agency that is principally responsible for carrying out or approving a proposed project is referred to as the "lead agency" (*State CEQA Guidelines*, Section 15367). While the proposed project is located within unincorporated Los Angeles County, an application will be made to the Local Agency Formation Commission (LAFCO) to annex the project site, and surrounding properties, to the City of Santa Clarita (City). Because the project would be located within the jurisdiction of the City, the City will act as the lead agency. Contact information for the City is as follows:

City of Santa Clarita
23920 Valencia Boulevard, Suite 300
Santa Clarita, California 91355
Contacts: Jeff Hogan and Patrick LeClair, Community Development Department
(661) 255-4330

3. RESPONSIBLE AGENCIES

Under CEQA, a public agency, other than a lead agency, that has discretionary approval power over the proposed project is considered a "responsible agency" (*State CEQA Guidelines*, Section 15381). If the City

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The State CEQA Guidelines are located in Title 14 of the California Code of Regulations at Section 15000 et seq.

approves this project, subsequent implementation of various project components could require discretionary approval authority from responsible agencies including:

- (a) California Department of Transportation (Caltrans);
- (b) California Regional Water Quality Control Board (RWQCB);
- (c) California Department of Fish and Game (CDFG);
- (d) Local Agency Formation Commission (LAFCO);
- (e) South Coast Air Quality Management District (SCAQMD);
- (f) U.S. Fish and Wildlife Service (USFWS);
- (g) U.S. Army Corps of Engineers (USACE); and,
- (h) Metrolink/MTA.

This section is not intended to provide a complete and final listing of all subsequent discretionary actions or approvals that are needed, or may be needed, to implement the proposed project. This section is intended only to identify the responsible agencies that may have subsequent discretionary approval authority over implementation of various project components in the future.

4. PROJECT APPLICANT

The applicant of the proposed project is described below:

Vista Canyon Ranch, LLC 27451 Tourney Road, Suite 100 Santa Clarita, California 91355 Contact: Glenn Adamick (661) 255-3275

a. Property Ownership

Vista Canyon Ranch, LLC, owns or controls approximately 142 acres of the 185-acre site. The City of Santa Clarita owns the remaining 43 acres of the Specific Plan site. Most of the City's property is within the Santa Clara River Corridor. The project applicant would purchase a portion of the City's property (approximately 12 acres), based upon its appraised value, and then dedicate most of this property (approximately 7 acres) back to the City. The Specific Plan site also includes two Santa Clarita Water Division potable water wells located on a small parcel owned in fee by the Castaic Lake Water Agency

(CLWA), Santa Clarita Water Division. Implementation of the Specific Plan would result in the removal of one of the two wells.

5. PROJECT SUMMARY

The Vista Canyon Specific Plan sets forth a comprehensive set of plans, development regulations, design guidelines and implementation programs to develop the Specific Plan site, consistent with the goals and policies of the City of Santa Clarita General Plan and the Draft General Plan Update (One Valley One Vision). The Specific Plan would facilitate residential, mixed-use, and non-residential transit-oriented development, and retain and/or enhance important river resources and other environmental amenities on the project site. In addition, the Specific Plan would result in the construction of a new Metrolink station and bus transfer station.

The project applicant proposes to develop the approximately 185-acre Vista Canyon project site with 1,117 dwelling units (96 single-family residential lots and 1,021 attached condominiums [up to 579 of these attached condominium units may be rented or leased]), and up to 950,000 square feet of commercial and medical office, retail, theater, restaurant, and hotel uses within four Planning Areas (PA). A residential overlay over the corporate office campus site within PA-2, more specifically lots 32-35, would allow for a conversion of up to 250,000 square feet of office floor area to 233 attached residential units. If implemented, this conversion would permit a maximum of 1,350 residential units and 700,000 square feet of commercial floor area.

The project also includes approximately 18 acres of parks/recreation facilities, including the Oak Park, Town Green, Community Garden, River Education/Community Center, private recreation facilities, and project trails. Up to six private recreational facilities would be constructed throughout the Specific Plan site. Further, there are approximately 10 acres of proposed public streets, including the extension of Lost Canyon Road from Fair Oaks Ranch to Vista Canyon Road and the construction of the Vista Canyon Road Bridge to connect Lost Canyon Road and Soledad Canyon Road. Various other off-site improvements would be necessary to implement the Specific Plan. These improvements are described in detail in **subsection 12** below.

The applicant also is proposing construction of a water reclamation plant to be located adjacent to the western project boundary and directly north of Lost Canyon Road, which would provide recycled water for use in the project's landscaped areas and toilets within public restroom areas in commercial areas of the project. A surplus supply of recycled water would be created by the project and would initially be discharged into on-site percolation basins and ultimately utilized by CLWA as part of its recycled water system.

The project also proposes to annex to the City various properties surrounding and including the Vista Canyon project site, which currently are located under the jurisdiction of the County of Los Angeles. In total, the annexation area includes approximately 3,250 acres, including the Vista Canyon project site (approximately 185 acres), Fair Oaks Ranch (approximately 1,082 acres), Jakes Way multi-family area (approximately 260 acres), and the Sand Canyon area (approximately 1,723 acres). Annexation of non-Vista Canyon site areas would require approval of the following entitlements: (a) Pre-Zone No. 07-001b; General Plan Amendment No. 07-001b; and Annexation No. 07-002b (including an amendment to the City's Sphere of Influence). Section 4.24 of the EIR analyzes the environmental impacts, if any, associated with the annexation of the properties surrounding the Vista Canyon site.

The project applicant is requesting approval of the following discretionary entitlements to allow for construction of the Vista Canyon project site: (a) General Plan Amendment No. 07-001a; (b) Pre-Zone No.07-001a; (c) Annexation No. 07-002a (including an amendment to the City's Sphere of Influence); (d) Specific Plan No. 07-001; (e) Tentative Tract Map No. 69164; (f) Conditional Use Permit No. 07-009; and (g) Oak Tree Permit No. 07-019. These project approvals are discussed in further detail later in this section.

Additional subsequent ministerial actions, such as grading permits, building plan review and building permits, would be required by the City prior to actual grading and construction of the proposed Vista Canyon project.

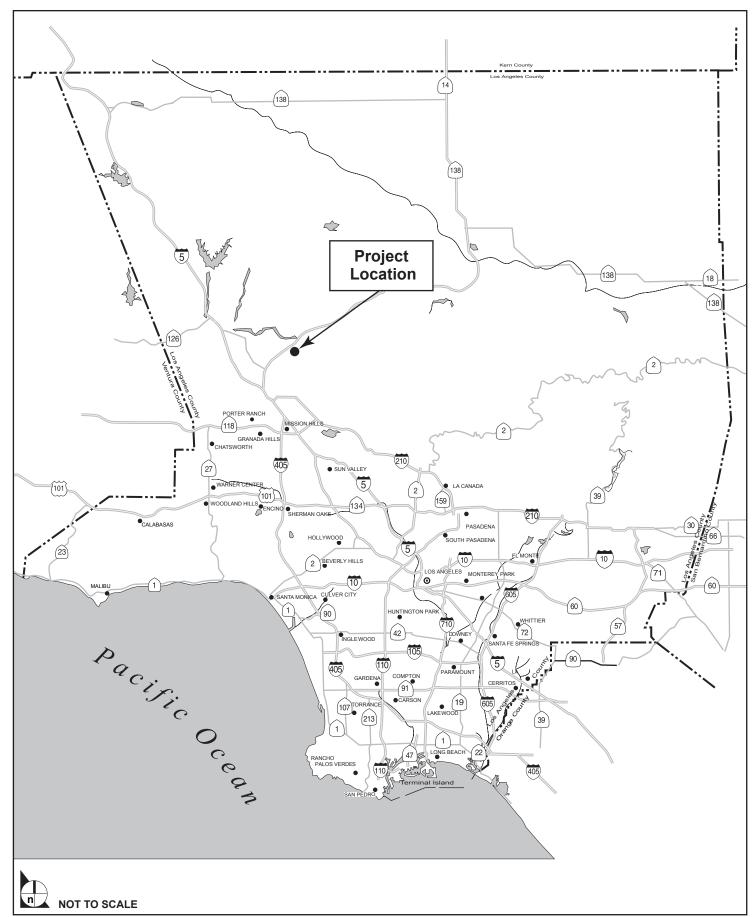
6. PROJECT LOCATION

Figure 1.0-1, Regional Location, illustrates the location of the Vista Canyon project site within a regional context. **Figure 1.0-2, Site Vicinity**, shows that the Vista Canyon project site is located in unincorporated Los Angeles County and the Santa Clarita Valley Planning Area. The Santa Clarita Valley Planning Area is generally surrounded by the Los Padres and Angeles National Forest areas to the north; Agua Dulce and the Angeles National Forest to the east; the major ridgeline of the Santa Susana Mountains, which separates Santa Clarita Valley from the San Fernando and Simi Valleys, to the south; and, the County of Ventura to the west.

Figure 1.0-3, Project Boundary, depicts the boundaries of the Vista Canyon site. The project site is located immediately south of State Route 14 (SR-14), west of La Veda Avenue, north of the Metrolink rail line, and east of the Colony Townhome community. The site also includes a portion of the Santa Clara River.

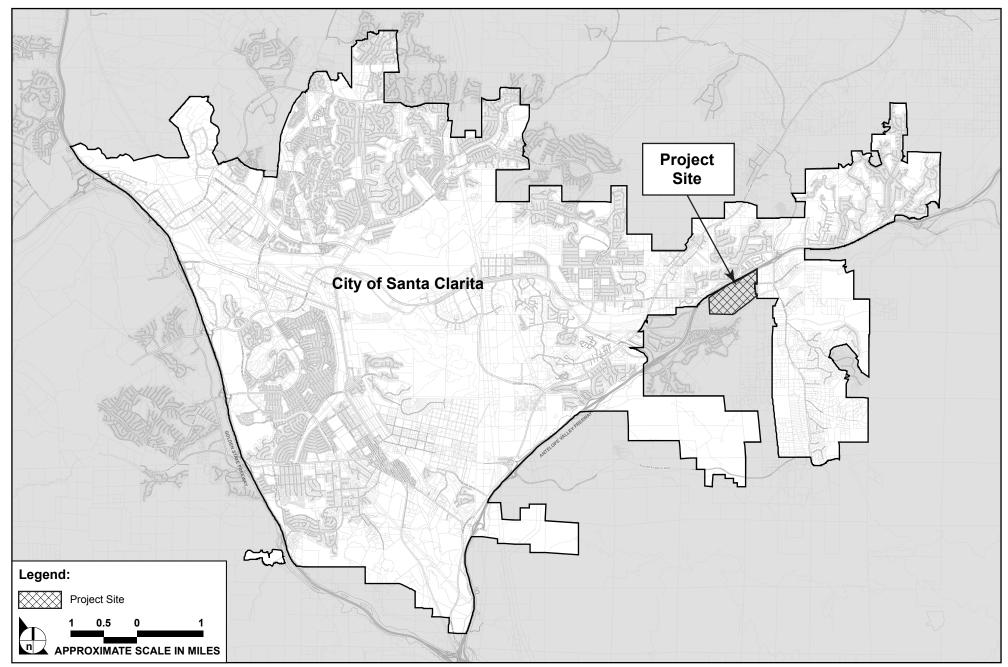
Land uses surrounding the project site include:

- Multi-family residential development (i.e., Colony Townhomes) directly west of the project site;
- SR-14, and residential and commercial development north of the project site;



SOURCE: Impact Sciences, Inc. - February 2007

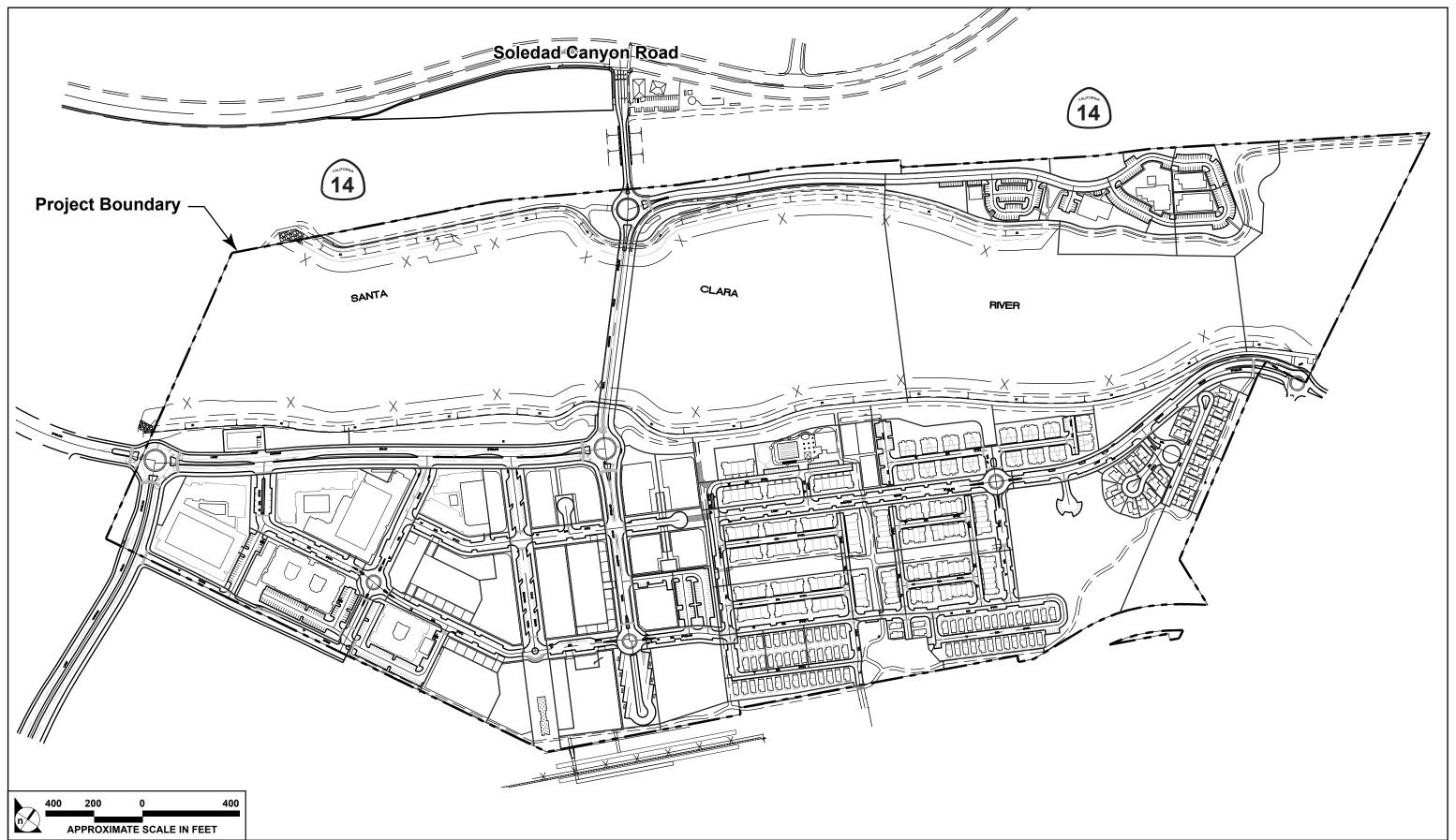
FIGURE 1.0-1



SOURCE: Impact Sciences, Inc. - November 2008

FIGURE 1.0-2

Site Vicinity



SOURCE: Alliance Land Planning and Engineering, Inc. – September 2010

FIGURE 1.0-3

- Single-family residential units along both sides of La Veda Avenue, single-family residential units
 along Lost Canyon Road, a public elementary school, and a private elementary school east of the
 project site;
- The Metrolink rail line and a commercial horse breeding facility property with accessory residential and barn structures south of the eastern portion of the project site; and
- Vacant land, the Metrolink rail line and residential development (Fair Oaks Ranch) south of the western portion of the project site.

7. PROJECT SITE BACKGROUND

The project site includes a portion of the Mitchell ranch, which was first settled in 1860 by Thomas Mitchell. Thomas Mitchell was born in Virginia, subsequently moving to Texas where, in 1852, he served under Sam Houston in the Texas Mounted Volunteers. He went to California shortly thereafter, spending approximately eight years in the Northern California mining districts. In 1860, he moved to the Santa Clarita Valley to start a cattle ranch. Initially he transported a dismantled miner's cabin down from Tehachapi and erected it on the property, more specifically in the southeastern portion of the project site. A few years later he married Martha Taylor and built a more commodious adobe, about 40 feet from the original cabin (Perkins 1961). The adobe was 60 by 45 feet in size and redwood shingled (Worden 1997).

Eventually Mitchell increased his holdings to nearly a thousand acres, raising cattle, producing honey, and farming. With increasing population, and thus children, in the valley, the Sulphur Springs School District was founded, circa 1872. The school opened initially in the kitchen of the Mitchell's adobe, was taught by Mrs. Mitchell, and was the first school building in the Santa Clara Valley area. Circa 1885, the student population had outgrown the single room and a wooden schoolhouse was constructed at Sulphur Springs, on land donated by Mitchell (Worden 1997). The Sulphur Springs school location is directly east of the project site. Mitchell built a two-story home on the project site in 1888, then using the adobe as a guesthouse. Bricks from the adobe were eventually removed from the property and the school/adobe was reassembled at Heritage Junction in Hart Park in Newhall.

In addition to the original miner's cabin, adobe, two-story wooden house, and likely a number of outbuildings, a family cemetery was also present on the Mitchell Ranch. This was used to inter the Mitchell family, and their friends and neighbors. None of the buildings referenced above remain on the project site. The cemetery is still present and would be preserved and enhanced by the project.

8. LAND USE DESIGNATIONS AND ZONING

a. Current Land Use Designations

The approximately 185-acre site is presently located in unincorporated Los Angeles County, directly adjacent to the City of Santa Clarita. The Los Angeles County Land Use Map (as amended through May 13, 2003) designates the property as M (Industry) and W (Floodplain/Floodway). The property is currently zoned M-1.5 (Light Industrial), A-1-1 (Light Agriculture – 1 acre minimum lot size), R-A-8,000 (Residential Agriculture – 8,000 square foot minimum lot size), and A-1-10,000 (Light Agriculture – 10,000 square foot minimum lot size). Under the existing County light industrial zoning designation of M-1.5 and taking into account parking and landscaping requirements, the industrial zoned portion of the project site could be developed with approximately 1.0 million square feet of light industrial use. The agricultural and residential zoned portions of the project site could be developed with approximately 170 single-family residential units.

The project site also was included in the Planning Area of the City of Santa Clarita General Plan. The City's adopted Land Use Map (as amended through April 24, 2007) designates the property as BP (Business Park), with portions of the site covered by a Significant Ecological Area (SEA) overlay. The City's General Plan Land Use Concept (Exhibit L-3 of the General Plan) identifies the project site as a "major sub-center" with Business Park/Office Uses. Under the Business Park designation and taking into account City parking and landscaping requirements, the property could be developed with approximately 4.35 million square feet of business park floor area.

The County of Los Angeles and City of Santa Clarita are presently completing One Valley One Vision (OVOV) – a joint effort, initiated in 2000, between the City and County to create guidelines for the future growth and development of the Santa Clarita Valley while also preserving natural resources. The jurisdictional planning boundaries established in OVOV include the City and its four communities (i.e., Canyon Country, Newhall, Saugus and Valencia), and the County communities of Stevenson Ranch, Castaic, Val Verde, Agua Dulce, and the future Newhall Ranch.

The County of Los Angeles in the draft OVOV Land Use Plan (dated October 2008) designated the project site as UR2 (Urban2 - five dwelling units per acre), with an SEA overlay over portions of the project site. Under this draft land use designation, the project site could be developed with up to 700 residential units. However, various goals and policies within OVOV encourage transit-oriented development (TOD) through the permitting of higher densities and intensities, and would allow for mixed-use, compact development in close proximity to new or existing rail stations and/or multi-modal transit facilities. As proposed, Vista Canyon includes both a new rail station and multi-modal transit facility.

b. Proposed Land Use Designations

The project applicant is proposing a Specific Plan (SP) designation for the project site. The Vista Canyon Specific Plan has been designed to provide a mixed-use, transit-oriented neighborhood for the eastern Santa Clarita Valley. The following land use designation zones would be established by the Specific Plan, and applied to property within the Specific Plan area:

- Residential (R). The R land use designation zone is applied to areas appropriate for a variety of
 attached and detached residential units, including condominiums, apartments, residential flats,
 attached residential with surface parking, and attached residential with parking structure. The R
 designation would permit both for-rent and for-sale residential units. The designation also allows
 recreation, trails, landscaped areas and parking areas to support the residential uses.
- Mixed-Use (MU). The MU land use designation zone is applied to areas appropriate for a wide-range of land uses, and allows multi-family residential, office, retail, and general commercial uses, and office over retail, hotel/lodging, theater, cinema or performing arts, studios, health/fitness facilities, churches, child care facilities, community assembly, and outdoor dining. Civic uses also are encouraged in this zone. Street frontages in the MU zone are pedestrian-oriented, and parking structures, surface parking and subterranean parking are allowed. Further, this zone contemplates a variety of office uses, including areas appropriate for business, financial, professional, business support service, processing, administrative, bank, medical services, and other office and supporting uses. This zone will accommodate local and regional employment needs and enhance the Specific Plan's housing/employment balance objectives. This zone also contemplates a variety of retail uses, including areas appropriate for restaurant, café, coffee shop, market, general retail, food service (including drive-through facilities), and entertainment.
- Open Space (OS). The OS land use designation zone is applied to the Santa Clara River Corridor, including buried bank stabilization areas and the Santa Clara River Trail.
- Other (O). The O land use designation zone is applied to areas containing public facilities, streets, and public recreation areas. Areas covered by the O designation include the water reclamation plant, Oak Park, public streets, Bus Transfer Station, Community Garden and the Town Green.

The project site would be divided into four planning areas, with a residential conversion overlay in PA-2 and an SEA overlay for the Santa Clara River Corridor corresponding to the OS land use designation. The PAs are described in more detail in **subsection 12**, below.

c. Phasing

The Vista Canyon project would be completed in multiple phases in order to correlate infrastructure and amenities with site development. The sequence of development would be influenced by the following factors: (a) market conditions and changes; (b) economic conditions; and (c) rate of growth in the immediate vicinity and region. As these factors change during buildout of the project, adjustments in corresponding infrastructure requirements may be needed. However, it is anticipated that there would be

four phases of development with the initial phase being occupied in 2012 and the last phase being completed in 2015. Each phase would have specific infrastructure and amenity requirements to ensure that the project is adequately served and City standards are met. For purposes of this EIR, phasing is projected as follows:

Phase 1 – Construction of 600 residential units within PA-1, and 80 residential units and 25,000 square feet of retail within PA-2. Access would be provided by an extension of Lost Canyon Road from its present terminus in Fair Oaks Ranch to the project site, and an extension of Jakes Way to the project site. Construction of the water reclamation plant, all flood protection improvements and the Santa Clara River trail would be constructed in Phase 1. The entire site would be graded in conjunction with development of Phase 1. The property for the City multi-modal transit station and the Oak Park would also be offered for dedication to the City in conjunction with development of Phase 1. Phase 1 would not include the construction of the Vista Canyon Bridge over the Santa Clara River or an extension of Lost Canyon Road easterly to its present terminus near La Veda Avenue.

Phase 2 – Construction of 297 residential units and the Oak Park within PA-3, and 140 residential units and the Community Garden within PA-2. The Vista Canyon Road Bridge would be constructed and operational by the 150th occupancy within PA-3, and Lost Canyon Road would be extended to its easterly terminus near La Veda Avenue. Street improvements to the segment of Lost Canyon Road between the project site and Sand Canyon Road and intersection improvements to Lost Canyon Road and Sand Canyon Road would be completed prior to the opening of the connection of Lost Canyon Road to La Veda Avenue.

Phase 3 – Construction of 56,000 square feet of commercial (office and retail) in PA-4, and 50,000 square feet of commercial retail, 100,000 square feet of commercial office, and the initial phase of the Metrolink Station/Bus Transfer Station and accessory parking within PA-2.

Phase 4 – All remaining land uses within the Specific Plan (e.g., 719,000 square feet of commercial floor area and accessory parking within PA-2, and the Town Green).

d. Lost Canyon Road (east of the Vista Canyon project site)

This segment of Lost Canyon Road presently has one lane in each direction with a posted speed limit of 30 miles per hour (mph) (25 mph when children are present). A continuous sidewalk is provided on the south side of the street, from Sand Canyon Road to the Vista Canyon project site. Sulphur Springs Elementary School and Pinecrest School both take vehicular access from this segment of Lost Canyon Road. Presently, this segment of Lost Canyon Road is congested when school is in session during the morning when students are being dropped off and in the afternoon when students are being picked up.

The project would implement several improvements to this segment of Lost Canyon Road, which would improve the existing operation of the roadway and accommodate project traffic. These improvements include:

- Pavement widening and striping of this segment of Lost Canyon Road to accommodate one travel
 lane in each direction with a median turn lane, a trail along the north side of the roadway and a
 roundabout at the intersection of La Veda Avenue and Lost Canyon Road. These improvements
 would be completed within the existing right-of-way.
- Permit on-street parallel parking on the south side of the roadway.
- Restrict the outbound-only driveways at each school to right-turns to minimize conflicting turning movements.
- Construct a narrow raised median at the easterly Pinecrest School driveway and include signage in the median prohibiting u-turns near this driveway.
- Coordinate with the Sulphur Springs School District to consider permitting parents to park in the
 off-street parking lot east of the school buildings to pick-up and drop-off students.

e. Lost Canyon Road/Sand Canyon Road Intersection Design Options

The proposed project would implement one of four intersection design options analyzed in the EIR. A more detailed discussion and exhibit of each design option is included in Section 4.3, Traffic and Access, and Appendix 4.3, Final Transportation Impact Study for Vista Canyon Transit-Oriented Development (May 2010). The four options are as follows:

- Four-Way Stop this design option is presently in place at the intersection. The intersection is presently congested in the morning and afternoon when school is in session due to student drop off and pick up. Under this design option, the operation of this intersection in the future would worsen to a Level of Service (LOS) F with or without the Vista Canyon project.
- Signalized Intersection (Look Ahead Signal) this design option would result in a signalized intersection, with a "look ahead" signal at the southwest corner to address only northbound "line of sight" requirements. Therefore, this option would not meet all of the City's design criteria for a signalized intersection. Minimal widening of the intersection would occur with this design option, with some right-of-way necessary at the southeast corner. Encroachment within the protected zone of the heritage oak tree located along the eastern edge of Sand Canyon Road would increase slightly as compared to the existing condition. Even with future growth (including the Vista Canyon project), this design option would result in the improved operation of the intersection in the future (LOS D), as compared to the existing four-way stop design.

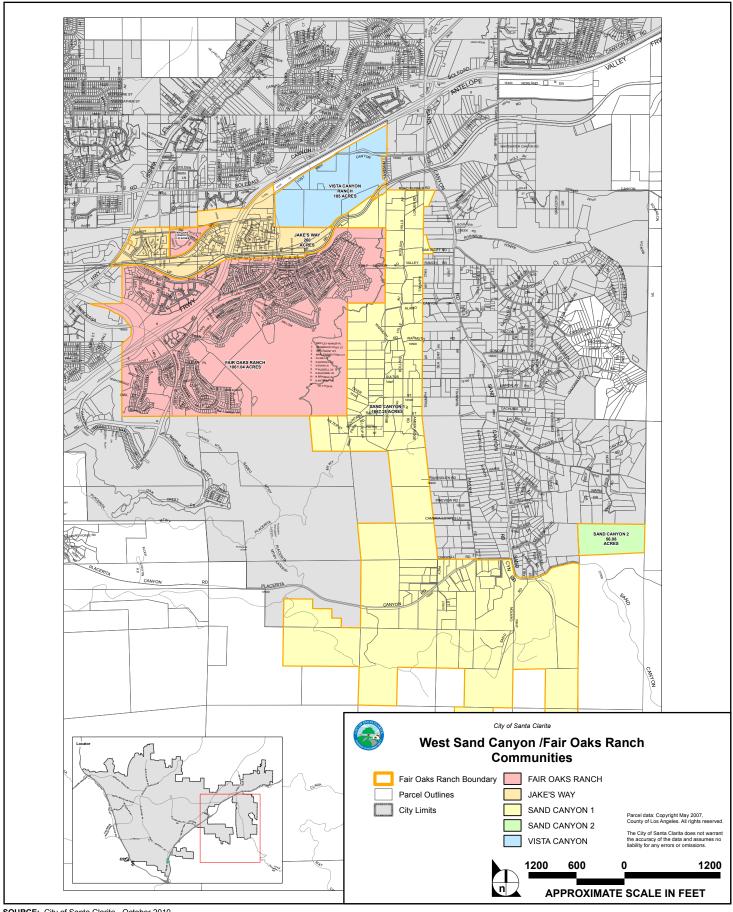
- Roundabout this design option would include the installation of a "roundabout" or traffic circle at the intersection. This option would involve the relocation of the intersection to the north and west to adhere to northbound "line of sight" requirements and would necessitate right-of-way acquisition on the northwest, southwest and southeast corners. Encroachment within the protected zone of the heritage oak tree located along the eastern edge of Sand Canyon Road would still occur, consistent with the existing condition. From a traffic operational standpoint, this design option would be the best of the four, improving the future LOS F under the existing design to LOS C in the AM peak hour and LOS B in the PM peak hour even with future growth (including the Vista Canyon project).
- Signalized Intersection (Standard) this design option improves the intersection of Lost Canyon Road/Sand Canyon Road with a fully signalized intersection complying with all of the City's standard intersection design criteria. This option would require the acquisition of right-of-way on all four corners of the intersection and would require the removal of the heritage oak tree located along the eastern edge of Sand Canyon Road. Similar to the "Look Ahead Signal" design option, this option would result in the improved operation of the intersection (LOS D), as compared to the existing design, even with future growth (including the Vista Canyon project).

9. REQUESTED PROJECT APPROVALS

As indicated above, annexation of the properties surrounding Vista Canyon would require approval of the following entitlements: Pre-Zone No. 07-001b; General Plan Amendment No. 07-001b; and Annexation No. 07-002b (including an amendment to the City's Sphere of Influence). Section 4.24, Ancillary Annexation Areas, of the EIR analyzes the environmental impacts, if any, associated with the annexation of these properties. Please see Figure 1.0-4, Annexation Sub-Areas.

The project applicant is requesting the project approvals described below, which would govern development of the proposed Vista Canyon project. Prior to issuing the project approvals, the City must certify that this EIR: (a) has been reviewed and considered; (b) has adequately analyzed the potential impacts of the proposed project; (c) has been completed in compliance with CEQA, the *State CEQA Guidelines*, and the City's Environmental Guidelines; and (d) reflects the independent judgment of the City Council. The requested project approvals are described in further detail below.

General Plan Amendment No. 07-001a. The City's General Plan currently designates the site for business park uses, with an SEA overlay over portions of the site. The applicant proposes to amend the City's Land Use and Circulation Elements of the General Plan.



SOURCE: City of Santa Clarita - October 2010

FIGURE **1.0-4**

Specifically, the applicant requests that the City approve a General Plan Land Use Element Amendment to revise (i) the land use designation for the property from BP (Business Park) to SP, and (ii) the SEA overlay boundaries to correspond to the area designated SP-OS (Santa Clara River Corridor). The SP designation is requested because the land uses proposed for the project site support a transit-oriented development and allow for a mixed-use project, which would not be permitted under the business park designation. The applicant also requests City approval of a General Plan Circulation Element Amendment to (a) establish the alignment for Lost Canyon Road/Vista Canyon Road, (b) reduce Lost Canyon Road from a six-lane roadway (Major Highway) to a four-lane roadway (Secondary Highway) from its westerly terminus in Fair Oaks Ranch to the future intersection of Lost Canyon Road and Vista Canyon Road, (c) designate the proposed Vista Canyon Road Bridge across the Santa Clara River as a limited secondary highway permitting a total of two travel lanes and one auxiliary lane with a sidewalk and a bike path on one side, and (d) remove from the City's General Plan Circulation Element as a Major Highway Lost Canyon Road from Vista Canyon Road to Sand Canyon Road.

Pre-Zone No. 07-001a. The applicant is proposing to amend the City Zoning Map by pre-zoning the site to SP. The City Council must approve the pre-zone prior to LAFCO consideration of the applicant's proposed annexation request. The SP zoning would take effect upon approval of the annexation by LAFCO.

Specific Plan No. 07-001. The proposed Specific Plan would establish the zoning; development plans, regulations, guidelines and implementation provisions within Specific Plan area. The Specific Plan document serves as a guide to implement the goals, policies, and objectives of the City's General Plan. Key components of the Specific Plan include a land use plan, permitted use chart, development standards, circulation plan, conceptual landscape plan, pedestrian plan, trails plan, and sustainability plan.

Annexation No. 07-002a. The project site is currently located in unincorporated Los Angeles County, directly adjacent to the City of Santa Clarita. The applicant is requesting annexation of the project site into the City. The Specific Plan site is not currently within the City's Sphere of Influence as defined by LAFCO. The Los Angeles County LAFCO will act as a responsible agency under CEQA and consider the City's Sphere of Influence amendment and the applicant's annexation request.

Tentative Tract Map No. 69164. The applicant is proposing to subdivide the property into a total of 162 lots to facilitate construction of 1,117 residential units (96 single-family units and 1,021 attached, multi-family, condominium units), up to 950,000 square feet of commercial and office, retail, theater, restaurant, and hotel uses, and other lots for parks, landscape/open space, River Corridor, bank protection, parking structures, and private drives. The applicant's tract map application includes a

request for approval of a proposed large lot parcel map for conveyance purposes. In addition, under the Specific Plan, a residential overlay is included over the Mixed-Use (MU) land use designation within PA-2 on Lots 32-35. This overlay allows for the conversion of up to 250,000 square feet of commercial/office uses to a maximum of 233 attached residential units. If this conversion is implemented, the allowable development mix under the Specific Plan would be a maximum of 1,350 residential units and 700,000 square feet of office uses.

Conditional Use Permit No. 07-009. The applicant is requesting approval of a Conditional Use Permit (CUP) to allow for the import of up to 500,000 cubic yards of dirt to accommodate development within the project site. Import materials are to be from one or both of the following borrow sites: (a) the George Caravalho Santa Clarita Sports Complex Expansion, and (b) the Center Pointe Business Park. Development on both of the borrow sites has been previously approved by the City.

Oak Tree Permit No. 07-002. Implementation of the project would result in the impact of oak trees on and off the project site.

On site, the applicant is requesting an oak tree permit to allow for removal of 10 (four of which are heritage size) of the 41 oak trees located on the project site. One of the non-heritage trees proposed for removal would be relocated within the project site. The request would also permit the encroachment into the protected zone of 10 oak trees. Pruning or trimming of seven of these 10 oak trees would also be permitted under the oak tree permit. A total of 31 oak trees located on the project site, including 17 heritage-sized oak trees, would not be removed by the project.

Impacts to the two oak trees located off site would differ depending upon the intersection design option selected for Lost Canyon Road/Sand Canyon Road. Under the Sand Canyon Road/Lost Canyon Road intersection design options 1-3, the requested oak tree permit would allow for encroachment into the protected zone of two oak trees. Encroachment for one tree is needed for the construction of a trail along Lost Canyon Road, east of the project site, and encroachment for the Sand Canyon Road tree is required by improvements to the intersection of Lost Canyon Road and Sand Canyon Road. Trimming or pruning would also be permitted on the tree along Lost Canyon Road to provide for trail clearance requirements.

Under the forth intersection design option (standard signalized intersection), the heritage oak tree located along Sand Canyon Road would be removed. Removal of the tree would permit the construction of a signalized intersection conforming to all of the City's standard design criteria. Trimming of the oak tree along Lost Canyon Road would be permitted under this design option as well.

In total, the project's oak tree permit could permit the removal of up to 11 oak trees (five of which are heritage), the encroachment into the protected zone of up to 12 oak trees, including the trimming or pruning of up to eight of the 12 oak trees.

10. PERMITS AND APPROVALS

Table 1.0-1, Future Agency Actions, identifies other permits and approvals known to be needed, or which may be needed, in order to implement various project components in the future.

Table 1.0-1
Future Agency Actions¹

Agency	Action Required			
Regional Water Quality Control Board	National Pollutant Discharge Elimination System Permit; Section 401 permit under the federal Clean Water Act; and permits related to the reclamation plant and use of recycled water			
California Department of Fish and Game	Streambed Alteration Agreement per Fish & Game Code Section 1602			
U.S. Department of the Army, Corps of Engineers	Section 404 Permit under the federal Clean Water Act			
South Coast Air Quality Management District	Various permits for air emissions regulation found in the Air Quality Management Plan			
Los Angeles County Local Agency Formation Commission	Approval of Annexation No. 07-002 (a) (Vista Canyon), including a modification to the City's Sphere of Influence			
Southern California Regional Rail Authority (SCRRA) /Metrolink/Metropolitan Transit Authority (MTA)	Approval of Vista Canyon Metrolink Station, including property acquisition and various grading, drainage and trail easements			

This table is not intended to provide the complete and final listing of future actions required to implement the project. This is an attempt to identify those actions that are known at this time to be required in the future.

11. PROJECT OBJECTIVES

CEQA requires that an EIR include a statement of the objectives sought by the proposed project (*State CEQA Guidelines*, Section 15124(b)):

A statement of objectives sought by the proposed project. A clearly written statement of objectives will help the Lead Agency develop a reasonable range of alternatives to evaluate in the EIR and will aid the decision makers in preparing findings or a statement of overriding considerations, if necessary. The statement of objectives should include the underlying purpose of the project.

The project objectives are consistent with the Vista Canyon Specific Plan objectives, and include the following:

a. Land Use Planning Objectives

- 1. Create a new transit-oriented community with interrelated neighborhoods that allows for residential, retail/commercial, office, hotel, and recreational uses, while preserving and enhancing significant natural and historical resources.
- 2. Provide a sensitive and protective interface with the Santa Clara River by utilizing appropriate setback, grading, landscape, buried bank stabilization, and water quality treatments.
- 3. Provide development and transitional land use patterns that do not conflict with surrounding communities and land uses.
- 4. Arrange land uses to reduce vehicle miles traveled and energy consumption, and to encourage the use of transit.
- 5. Design neighborhoods to create a unique identity and sense of place.
- 6. Design neighborhoods to locate a variety of residential and non-residential land uses in close proximity to each other and major road corridors, transit, and trails.
- 7. Provide a rich set of public spaces, including roadways that range from lively streetscapes to pedestrian passages.
- 8. Implement sustainable development principles, including greater energy efficiency, waste reduction, drought-tolerant landscaping, use of water efficiency measures, and use of recycled materials and renewable energy sources.
- Create and enhance opportunities for non-vehicular travel and encourage pedestrian mobility by
 providing an internal pedestrian circulation system that links residential neighborhoods to nearby
 schools, neighborhood parks, trail systems, neighborhood retail/commercial and adjacent park and
 recreation areas.
- 10. Foster the design and integration of a mutually beneficial relationship between the natural and built environments, and implement sensitive land use transition treatments, attractive streetscapes, and high quality design themes.
- 11. Provide a meandering trail with public access adjacent to the Santa Clara River Corridor.
- 12. Integrate a new community into the City's existing and planned circulation network.
- 13. Provide a landscape design emphasizing a pleasant neighborhood character and inviting streetscapes.

- 14. Facilitate the expansion of transit facilities by providing property and participate in the funding of a new City/Metrolink transit center and associated facilities, and direct pedestrian access to such facilities from the Specific Plan's commercial, retail, office, and residential areas.
- 15. Provide neighborhood parks and improvements that offset park dedication requirements and meet the recreation needs of local residents.

b. Economic Objectives

- 1. Enhance and augment the housing market by providing a variety of housing types and densities to meet the varying needs of future residents.
- Adopt development regulations that provide flexibility to respond and adjust to changing economic and market conditions.
- 3. Provide a tax base to support public services and infrastructure.
- 4. Provide a project jobs/housing balance of at least two jobs for every one residential unit.
- 5. Adopt development regulations and guidelines that allow site, parking and facility sharing, and other innovations that reduce the costs of providing public services.

c. Resource Conservation Objectives

- 1. Restore and minimize impacts to important biotic resources.
- 2. Maintain the use of the Santa Clara River as a major east/west open space corridor.
- 3. Establish a Santa Clara River Corridor and adopt measures to maintain, enhance, and protect important river habitat values and functions.
- 4. Provide native revegetation of river and setback areas when temporarily disturbed due to development activities.
- 5. Minimize impacts to the Santa Clara River and its resources.
- 6. Minimize impacts to oak trees and incorporate, where possible, oak trees into public spaces.

12. TECHNICAL, ECONOMIC, AND ENVIRONMENTAL CHARACTERISTICS

The *State CEQA Guidelines* require an EIR to provide "[a] general description of the project's technical, economic, and environmental characteristics, considering the principal engineering proposals if any and supporting public service facilities." (*State CEQA Guidelines*, Section 15124 (c)).

The proposed project includes a mix of single-family, multi-family, transit, recreational, commercial, office and open space uses. The project would provide land uses that support the local vicinity and

region, particularly with the incorporation of a Metrolink and multi-modal transit hub, corporate office campus, and retail "Main Street" into the project design. The Metrolink and multi-modal transit hub would provide transit accessibility to both existing and future residents within the eastern portion of the Santa Clarita Valley. New housing would be provided to support existing and new employment opportunities expected to occur in the region. Up to 950,000 square feet of commercial uses would be developed on site, providing employment opportunities for future residents of the project and existing residents within the Santa Clarita Valley. The proposed trail system and public and private parks would provide local recreational support for new and existing residents. The retail commercial uses would support the proposed residential uses as well as the existing residents in the local area.

a. Proposed Land Uses

The following discussion describes the types and amounts of new land uses proposed by the applicant and the infrastructure improvements necessary to construct the development. This description is intended to provide a sufficient level of detail from which an evaluation and review of the environmental impacts of the proposed project can be made.

The Specific Plan and Tentative Tract Map identify the arrangement of land uses, lots, grading limits, and supporting infrastructure/improvements on the tract map site. As depicted in **Figure 1.0-5**, **Vista Canyon Tentative Tract Map No. 69164**, the project site is divided into four Planning Areas (consistent with the Specific Plan). The Planning Areas of the site are shown on **Figures 1.0-6a–d**, **Planning Areas**. The Specific Plan permits modifications to building locations, building footprints and product types shown on TTM 69164 subject to the requirements of the Specific Plan.

Planning Area 1 (PA-1). PA-1 consists of approximately 12 acres located within the southwest portion of the Specific Plan site. This area would be built out to accommodate a maximum of 600 attached, multi-family condominiums. Up to 359 of these condominium units may be leased or rented. Three private recreational areas (.39 acre), trails, water quality improvements, surface and structure parking, and other open areas would be provided within PA-1. The water reclamation plant would also be located in PA-1. The maximum building height in PA-1 is 50 feet (excluding architectural elements, which can extend up to a maximum height of 60 feet). This Planning Area is depicted on Figure 1.0-6a-Planning Area 1.

VISTA CANYON **TENTATIVE TRACT MAP 69164** SITE DEVELOPMENT PLAN PA-1

SOURCE: Alliance Land Planning & Engineering, Inc.

FIGURE 1.0-5

NOT TO SCALE

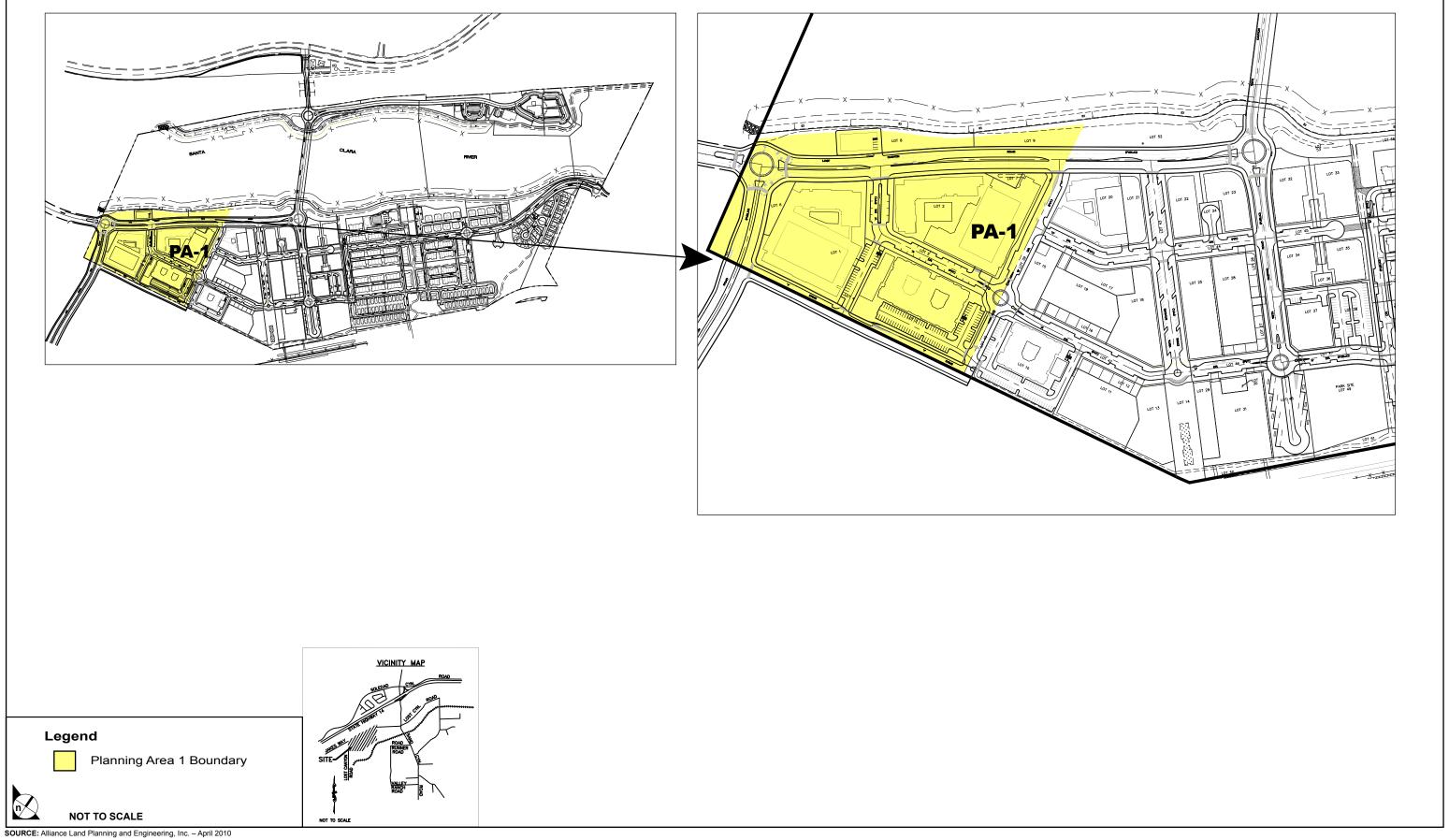


FIGURE 1.0-6a

Planning Area 2 (PA-2). PA-2 consists of approximately 30 acres, located within the south central portion of the Specific Plan site. PA-2 would be built out to accommodate up to 220 attached residential condominium units (e.g., apartments, live/work units, residential flats, residential over retail), including two private recreation areas. All of the residential units in PA-2 may be leased or rented. In addition, PA-2 includes 596,000 square feet of office space; 158,000 square feet of retail space (including restaurants and a theater); a 200-room hotel (approximately 140,000 square feet in size); up to four parking structures (the height of the parking structures will be within the height limitation of 55 feet) and accessory surface and subterranean parking; the City/Metrolink transit center; Vista Square; Pedestrian Plaza; Community Garden; and associated parks, trails, swales, private drives, and public streets. Recreational areas, trails, water quality improvements and other open areas also would be provided within PA-2. The maximum height in PA-2 for retail/commercial buildings is 55 feet (excluding architectural elements, which can extend up to a maximum height of 66 feet). Excluding architectural elements, the maximum heights for the office buildings in PA-2 located north of "C" Street is 95 feet, and 60 feet for office buildings located south of "C" Street. Architectural elements could extend up to 114 feet and 72 feet, respectively. All residential structures within PA-2 would have a maximum height of 50 feet (excluding architectural elements, which could extend up to 60 feet).

PA-2 includes a mix of office, retail, and residential uses oriented around "Main Street" (Vista Square), "Plaza at Vista Square," and theater. Vista Square includes diagonal on-street parking, sidewalks, landscaping, public amenities (e.g., benches, bike racks, and outdoor dining), and other traffic calming features to balance the needs of pedestrians with those of cars, and to enable pedestrian-oriented shops, restaurants, and services. The approximately 1.6 acre Town Green is also in PA-2.

Within the MU land use designation in PA-2, Lots 32-35, there is a residential overlay that allows for the conversion of up to 250,000 square feet of office uses to a maximum of 233 attached residential units. If the conversion is implemented under the Specific Plan, the allowable development under the Specific Plan would be a maximum of 1,350 residential units and 700,000 square feet of commercial uses. Planning Area 2 is depicted on **Figure 1.0-6b**, **Planning Area 2**.

Planning Area 3 (PA-3). PA-3 consists of approximately 46 acres, located within the southeast portion of the Specific Plan site. PA-3 would be built out to accommodate up to 297 single-family or multi-family attached units. The tentative tract map, as designed, includes 201 attached for-sale units and 96 detached single-family units. The maximum height of the residential structures within PA-3 is 35 feet. The 7-acre "Oak Park" within PA-3 would be improved and dedicated to the City, while the private park and recreation facilities (1.7 acres) would be utilized by future residents and maintained by a homeowners' association. Trails, water quality improvements and other open areas also would be provided within PA-3. To support the residential uses within PA-3, the area includes one public street (Lost Canyon Road) and private streets/drives. This Planning Area is depicted on Figure 1.0-6c, Planning Area 3.

Planning Area 4 (PA-4). PA-4 consists of approximately 12.8 acres, located within the northeast portion of the Specific Plan site. PA-4 would be built out to accommodate up to 56,000 square feet of office, retail/commercial, restaurant, and recreation uses. Residential units are prohibited in PA-4. This area also would include the River Education/Community Center overlooking the Santa Clara River Corridor. Additionally, trails, water quality improvements and other open areas would be provided within PA-4. The maximum height of buildings within PA-4 is 35 feet (excluding architectural elements, which could extend up to 42 feet).

The existing Mitchell Family cemetery is located within PA-4, between SR-14 and the Santa Clara River. The applicant would preserve and restore the cemetery and integrate it with other development in PA-4.

The proposed project includes other areas within its boundaries. These areas include the buried bank stabilization/protection along the north and south side of the Santa Clara River, as shown on Tentative Tract Map No. 69164; the reach of the Santa Clara River Corridor within the Specific Plan boundary; and the public streets and private drives in and out of the Specific Plan site. Planning Area 4 is depicted on **Figure 1.0-6d, Planning Area 4**.

Table 1.0-2, on a planning area-by-planning area basis, identifies the applicable land use designations, use types, acreage, number of dwelling units, and total non-residential square footage.

(1) Off-Site Improvements

To facilitate development of the project, the applicant is responsible for the construction of off-site improvements. These improvements consist of:

- The extension of Lost Canyon Road (approximately 800 feet), from its present terminus at the northerly abutment of the bridge over the Metrolink railroad tracks within Fair Oaks Ranch, across adjacent properties to the Specific Plan site. The right-of-way for this road is proposed at 95 feet, which would accommodate two vehicular lanes in each direction, a raised landscaped median, parkway, sidewalk and Class III bike lanes. Approximately 160,000 cubic yards of grading, inclusive of remedial grading and import, would be necessary to complete this improvement.
- The extension of Jakes Way (approximately 250 feet), from its present terminus directly west of the Specific Plan site, to the proposed roundabout at Lost Canyon Road and Jakes Way. The right-of-way for this road is proposed at 92 feet, which would accommodate one vehicular lane in each direction, parkway, sidewalk and Class III bike lanes. Approximately 2,000 cubic yards of grading would be necessary to complete this improvement. Buried bank stabilization also would be necessary along this roadway extension, and would connect to the existing concrete-gunite flood protection located directly north of the existing Jakes Way.
- Grading on portions of the adjacent southerly property for slope and drainage purposes.

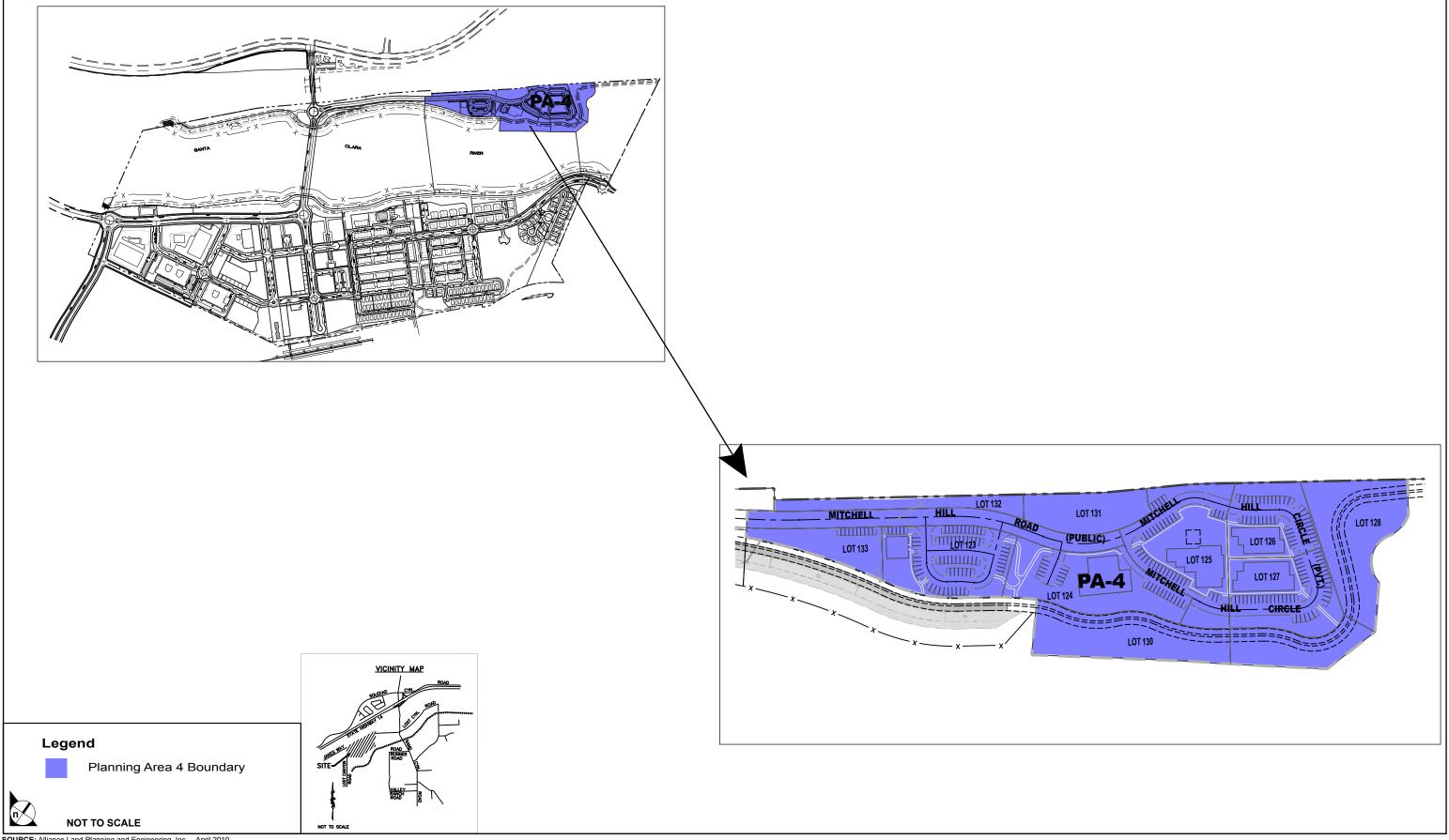


FIGURE 1.0-6b



SOURCE: Alliance Land Planning and Engineering, Inc. – April 2010

FIGURE 1.0-6c



SOURCE: Alliance Land Planning and Engineering, Inc. – April 2010

FIGURE 1.0-6d

Planning Area 4

- Extension of the Santa Clara River Regional Trail easterly from the Specific Plan site along Lost Canyon Road to Sand Canyon Road. This trail, up to 10-foot-wide, would consist of decomposed granite or a similar surface, and include a pedestrian bridge crossing over the Sand Canyon wash.
- The widening and completion of roadway improvements on Lost Canyon Road under SR-14 within the existing right-of-way. This roadway is presently partially improved and used for public access. Proposed improvements would include the addition of pavement, curb, gutter, and sidewalk (east side).
- The import of up to 500,000 cubic yards of dirt from one or both of the following borrow sites: (a) the George Caravalho Santa Clarita Sports Complex; and (b) the Center Pointe Business Park. Development on both of the borrow sites was previously approved.
- Construction of the platform and accessory station improvements within the Metrolink right-of-way as part of a new City/Metrolink transit center.
- Grading and various trail and drainage improvements within the Metrolink right-of-way adjacent to the project site.
- Construction of various off-site traffic mitigation improvements discussed in further detail in **Section 4.3**, **Traffic and Access**, of this Draft EIR.

Table 1.0-2 Vista Canyon Statistical Summary By Planning Area

	Land Use		Acres	Dwelling		Square Footage	
Planning Areas	Designation ¹	Use Type	(gross)	Units	Retail	Office	Hotel
PA-1	R	Attached Residential, Private Recreation Areas	11	600			
	O	Water Reclamation Plant	1				
PA-2	MU	Offices, Retail, Hotel, ² Theater, Parking Structures (including Metrolink Parking), Residential, Private Recreation Areas	27.5	220	158,000	596,000	140,000
	О	Community Garden, Town Green, Bus Transfer Station	2.5				
PA-3	R	Attached and Detached Residential, Private Recreation Area	39	297			
	О	Oak Park	7				
PA-4	MU	Retail, Offices, River Education Center	12.8		6,000	50,000	
Other Areas							
River Corridor	OS	River	74.5				
Public Streets	0	Streets	10				
TOTAL			185.3	1,117	164,000	646,000	140,000
(Total with Overla	(y) ³			1,350	164,000	396,000	140,000

Notes:

Source: Tentative Tract Map No. 69164

¹ R = Residential; MU = Mixed Use; OS = Open Space; and O = Other. OS includes the Santa Clara River Corridor, including buried bank stabilization areas.

² PA-2 includes a four-story hotel with 200 rooms on Lots 23 and 24, as shown on Tentative Tract Map No. 69164.

³ The Specific Plan allows a residential overlay over MU within PA-2, Lots 32-35. This overlay allows for the conversion of up to 250,000 square feet of commercial/office uses to a maximum of 233 attached residential units. If the conversion is implemented under the Specific Plan, the allowable development under the Specific Plan will be a maximum of 1,350 residential units and 700,000 square feet of commercial/office uses.

b. Mobility Plan

(1) Introduction

The Vista Canyon Specific Plan's Mobility Plan provides for a transportation/circulation system utilizing existing routes and new facilities. The Mobility Plan incorporates vehicular and non-vehicular modes of transportation in a system of roads, bike lanes, trails, and pedestrian pathways. Roads are arranged in a hierarchy, organized by function, to facilitate access within and around the Specific Plan site. The Mobility Plan seeks to implement street sections that slow traffic, create a pleasant walking environment, and reduce the urban "heat island" effect.² The Specific Plan's circulation network described below is conceptually shown on **Figure 1.0-7**, **Specific Plan Circulation Network**.

The Specific Plan also is guided by its proximity and access to the existing Metrolink rail right-of-way, located adjacent to the southern boundary of the Specific Plan site. The project proposes to provide land and partner with the City and Metrolink on the development of a new multi-modal transit hub within PA-2, surrounded by residential, retail, live/work residences, employment opportunities, park/recreation amenities, and services. The Mobility Plan achieves project objectives by creating and enhancing opportunities for non-vehicular travel, encouraging pedestrian mobility, and facilitating transit use with the new City/Metrolink transit center, including directing pedestrian access to such facilities from the Specific Plan's office, commercial, retail, hotel, and residential areas.

As previously noted, the Specific Plan consists of a mix of office, commercial, retail, and residential uses surrounded by a "Main Street" thoroughfare (Vista Square), which like the rest of the project is designed as a pedestrian-oriented environment. This town center design is intended to calm traffic to balance the needs of pedestrians to those with cars, and to enable pedestrian-oriented shops, restaurants, and services. Parking along "Vista Square" is addressed through diagonal on-street parking, along with other parking in PA-2 in the form of strategically located "park-once" parking structures in close proximity to "Vista Square."

(2) Existing Regional Circulation and Access

The City of Santa Clarita is served by an existing network of highways, roadways, multi-use trails, commuter rail, and transit service. Primary regional access in the Santa Clarita Valley is provided by I-5,

Impact Sciences, Inc. 1.0-30 Vista Canyon Draft EIR 0112.024 October 2010

The term "heat island" describes built environment areas that are hotter than nearby rural or open areas. Heat islands can affect communities by increasing summertime peak energy demand, air conditioning costs, greenhouse gas emissions, heat-related illness and mortality, and water quality. (See U.S. Environmental Protection Agency website, Heat Island Effect, http://www.epa.gov/heatisland/index.htm [last visited October 4, 2010]).

located south and west of the Specific Plan site. SR-14, located north of the Specific Plan site, also provides a regional link between the Los Angeles basin and the high desert communities of Palmdale and Lancaster. SR-14 connects to I-5 southwest of the Specific Plan site. Soledad Canyon Road provides secondary regional access, paralleling SR-14 along the northern boundary of the Specific Plan site, and extending north to Palmdale and Lancaster.

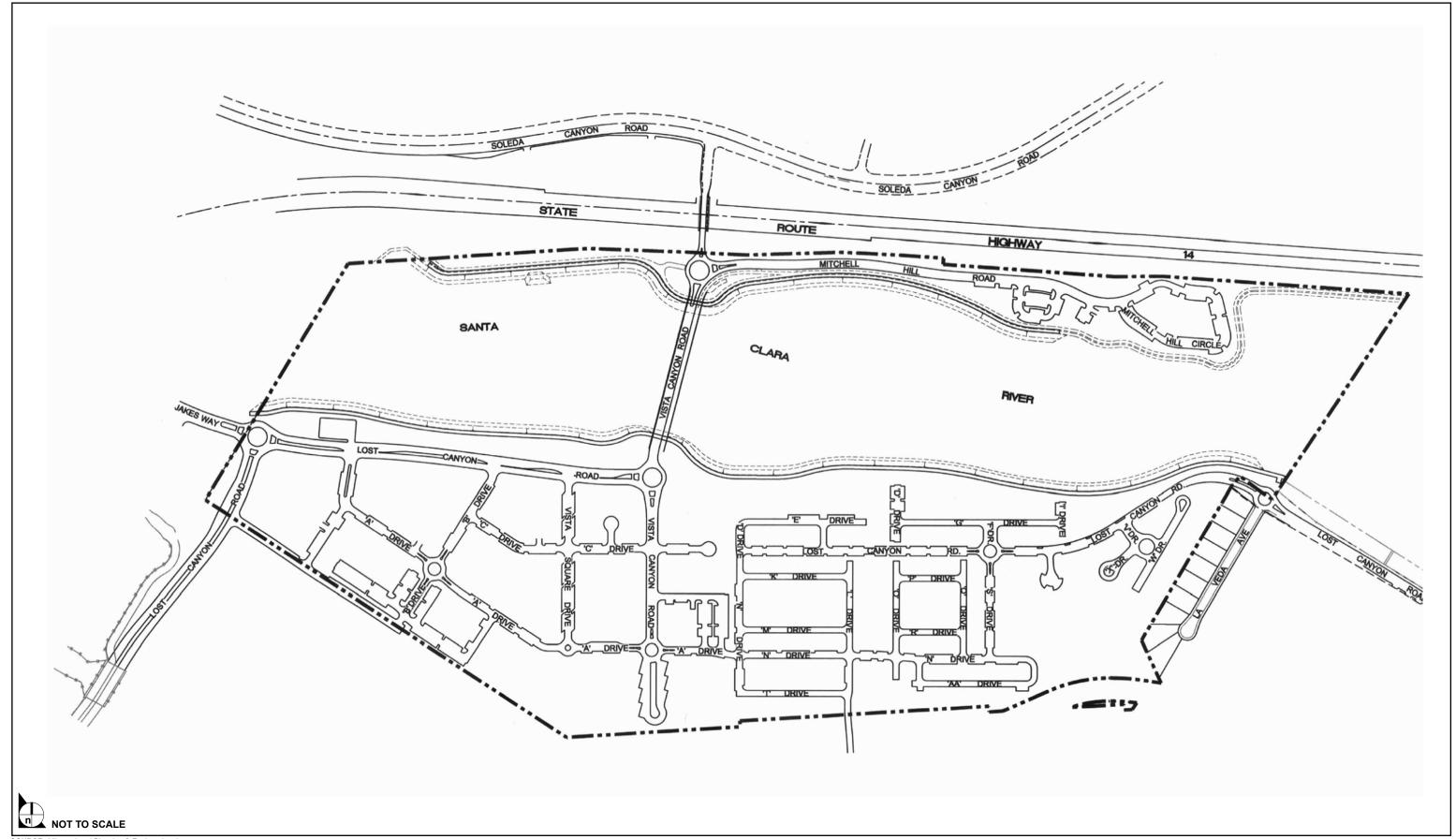
The Metrolink Antelope Valley line serves the region by connecting the Antelope Valley with points south, including Santa Clarita, to Union Station in downtown Los Angeles. The City also is presently served by a city-owned and operated bus service. Santa Clarita Transit (SCT) provides local and regional bus service, operating local routes within the Santa Clarita Valley and regional routes to and from Los Angeles, Antelope Valley, Van Nuys, and Warner Center. The Los Angeles Department of Transportation (LADOT) also provides commuter service with stops in the Santa Clarita Valley.

The Specific Plan would connect to the Santa Clarita backbone trail network as well as the County of Los Angeles trail system.

(3) Local Roadway Circulation and Access

The Mobility Plan's roadway network is designed as an orderly extension of the regional circulation pattern in the Santa Clarita Valley. The proposed development patterns within the Specific Plan site, integrated with existing and proposed roadway alignments, form the basis for this network. The Mobility Plan is designed to integrate modes of travel, accommodate anticipated traffic demands generated by the Specific Plan and surrounding development, and provide roadway extensions and improvements that ultimately would connect existing development and the proposed project to the SR-14 corridor and the rest of the Santa Clarita Valley.

Vehicular access to and from the Specific Plan site is proposed from four existing roadways. Primary access to the site would be from: (a) Soledad Canyon Road, via the proposed Vista Canyon Road Bridge, to the north; (b) the westerly Lost Canyon Road within Fair Oaks Ranch, which would be extended to the Specific Plan site, connecting with Jakes Way at a roundabout; (c) Jakes Way, which would be extended easterly from its present terminus to Lost Canyon Road; and (d) after the Lost Canyon Road/Jakes Way roundabout, Lost Canyon Road would traverse easterly through the Specific Plan site and terminate at a roundabout with the new Vista Canyon Road. Secondary access to and from the Specific Plan site would be from the easterly terminus of Lost Canyon Road near existing La Veda Avenue; this access is designed with a roundabout, an internal traffic loop, and other traffic calming features to minimize "cut-through" traffic.



SOURCE: Alliance Land Planning & Engineering, Inc

FIGURE **1.0-7**

a. Lost Canyon Road

Lost Canyon Road from Via Princessa to Sand Canyon Road is the only roadway within the Specific Plan boundary designated as a "Major/Secondary Highway" in the Santa Clarita General Plan Circulation Element and the Los Angeles County Master Plan of Highways. Both the City and County anticipate Lost Canyon Road continuing through the Specific Plan site.

The project requires a General Plan Amendment to the City's Circulation Element to:

- Establish the Lost Canyon Road alignment;
- Reduce Lost Canyon Road from a six-lane Major Highway to a four-lane Secondary Highway from
 its westerly terminus in Fair Oaks Ranch to the future roundabout at Lost Canyon Road and the
 proposed Vista Canyon Road;
- Designate the Vista Canyon Road Bridge as a Limited Secondary Highway, with this roadway
 consisting of two travel lanes, a striped median/emergency lane, sidewalk and a Class I bike path on
 the east side; and,
- Remove Lost Canyon Road, from Vista Canyon Road to Sand Canyon Road, from the City's General Plan Circulation Element as a Major Highway.

If the General Plan Circulation Element Amendments are adopted, the Specific Plan would implement the Mobility Plan shown in **Figure 1.0-8**, **Vista Canyon Mobility Plan**. As shown, the following Lost Canyon Road segments would be constructed:

(1) Lost Canyon Road South of Jakes Way

Lost Canyon Road would be extended (approximately 800 feet), from its existing terminus at the northerly abutment of the bridge over the Metrolink railroad tracks within Fair Oaks Ranch, north across adjacent properties to the south and west of the Specific Plan site. This segment is a public street in a maximum 95-foot right-of-way that would accommodate two vehicular lanes in each direction, a raised median, curb and gutter, parkways, sidewalks and Class III bike lanes on both sides. This road is depicted in cross-section and plan view in **Figure 1.0-9**, **Road Section (Lost Canyon South of Jakes Way)**.

(2) Roundabout "A": Lost Canyon Road at Jakes Way

This section of Lost Canyon Road would terminate on the west side of the Specific Plan site at a roundabout, located at Lost Canyon Road and Jakes Way (Roundabout "A"). The roundabout's overall radius is 79 feet, and consists of two vehicular travel lanes. The plan view of this roundabout is depicted in Figure 1.0-10, Road Section (Roundabout "A": Lost Canyon Road at Jakes Way).

(3) Lost Canyon Road (Jakes Way to Vista Canyon Road)

Jakes Way would be extended (approximately 250 feet) from its existing terminus directly west of the Specific Plan site to the proposed roundabout at Lost Canyon Road and Vista Canyon Road. This segment is a public street in a maximum 92-foot right-of-way that would accommodate two vehicular lanes in each direction, a raised median, curb and gutter, parkways, a sidewalk on one side and Class III bike lanes. Buried bank stabilization is needed along the north side of this roadway extension, connecting to the existing concrete-gunite flood protection located directly north of the existing Jakes Way. This road is depicted in cross-section view in Figure 1.0-11, Road Section (Lost Canyon Road, Jakes Way to Vista Canyon Road).

(4) Lost Canyon Road to La Veda Avenue

The portion of Lost Canyon Road located within PA-3 would traverse to existing La Veda Avenue to the east. This segment is a public street in a maximum 68-foot right-of-way that would accommodate one vehicular lane in each direction, parking on both sides of the street, curb and gutter, parkway and sidewalks on both sides in addition to various traffic calming improvements. This road is depicted in cross-section view in Figure 1.0-12, Road Section (Lost Canyon Road to La Veda Avenue).

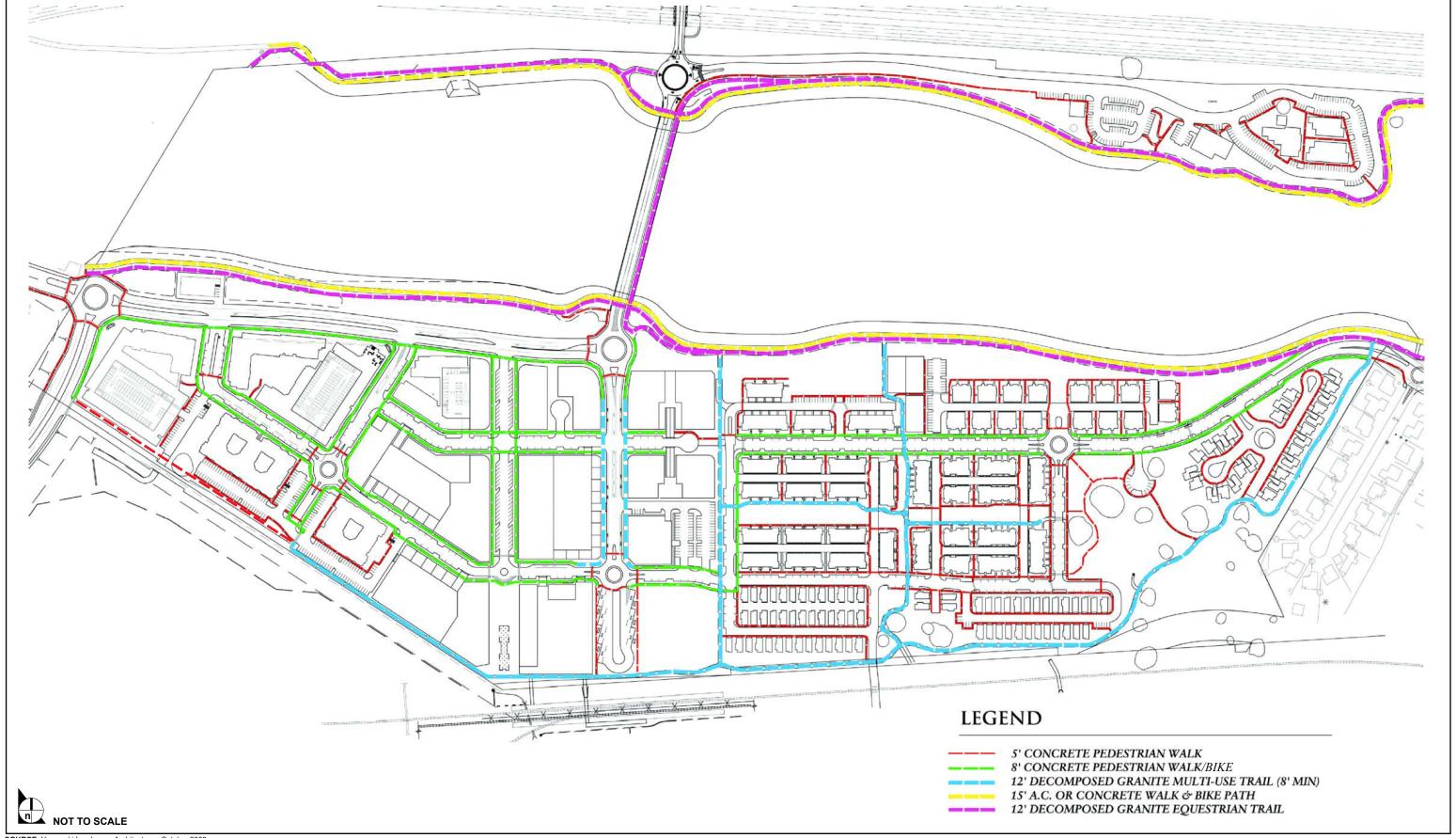
(5) Lost Canyon Road East of La Veda Avenue

As summarized earlier in this Section, Lost Canyon Road would be improved east of the existing La Veda Avenue beyond the easterly portion of the Specific Plan site. This segment is a public street in a maximum 66-foot right-of-way that would accommodate one vehicular lane in each direction, a center stacking or left-turn lane, curb and gutter, a sidewalk on the south side in order to create a continuous walkway to Sand Canyon Road further east, and a trail on the north side. This segment would provide access to the east side of the Specific Plan, serving primarily PA-3. This road is depicted in cross-section view in Figure 1.0-13, Road Section (Lost Canyon Road East of La Veda Avenue).

b. Vista Canyon Road

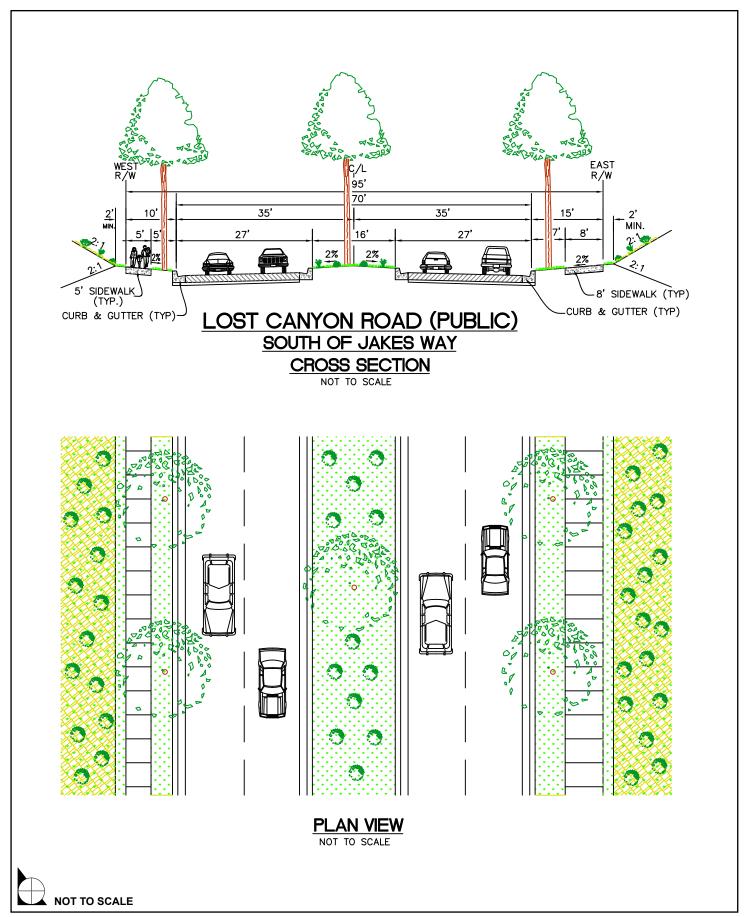
(1) Vista Canyon Road/Vista Canyon Road Bridge (North)

At the Lost Canyon Road/Vista Canyon Road roundabout, Vista Canyon Road would traverse both north and south. The northerly portion of Vista Canyon Road would consist of two lanes in a maximum right-of-way width of 64 feet, with a painted median/emergency lane, curb and gutter, a sidewalk on one side, and a Class 1 bike trail. This road would cross the Santa Clara River (Vista Canyon Road Bridge). The segment would connect, via a roundabout, to Mitchell Hill Road, located in PA-4 on the north side of the Specific Plan site. This road is depicted in cross-section view in **Figure 1.0-14**, **Road Section (Vista Canyon Road/Vista Canyon Road Bridge – North)**.



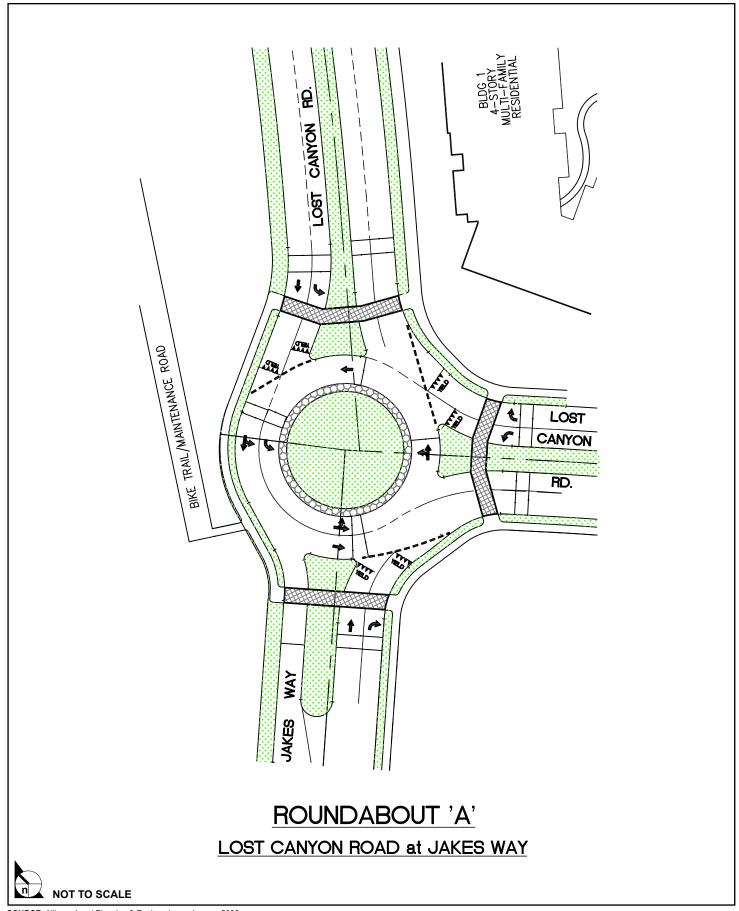
SOURCE: Yamasaki Landscape Architecture - October 2009

FIGURE 1.0-8



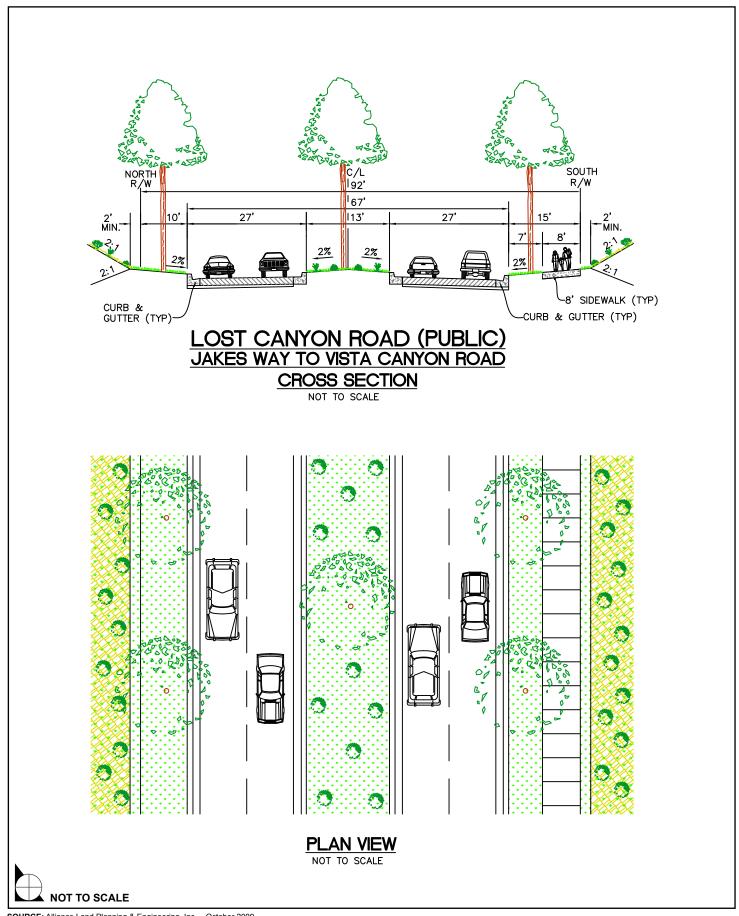
SOURCE: Alliance Land Planning & Engineering, Inc. – January 2006, Impact Sciences, Inc. – November 2009

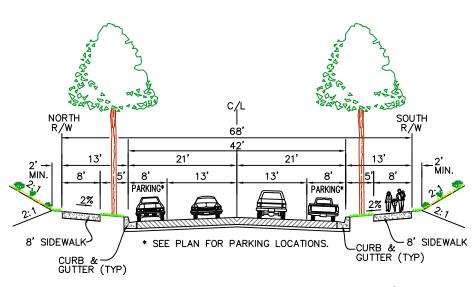
FIGURE 1.0-9



SOURCE: Alliance Land Planning & Engineering – January 2006

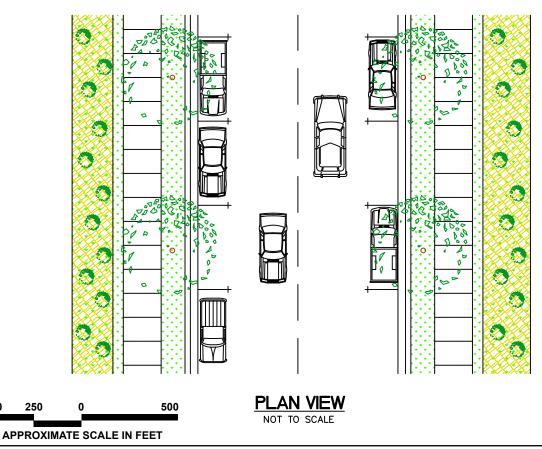
FIGURE 1.0 - 10





LOST CANYON ROAD (PUBLIC) VISTA CANYON ROAD TO LA VEDA AVE. **CROSS SECTION**

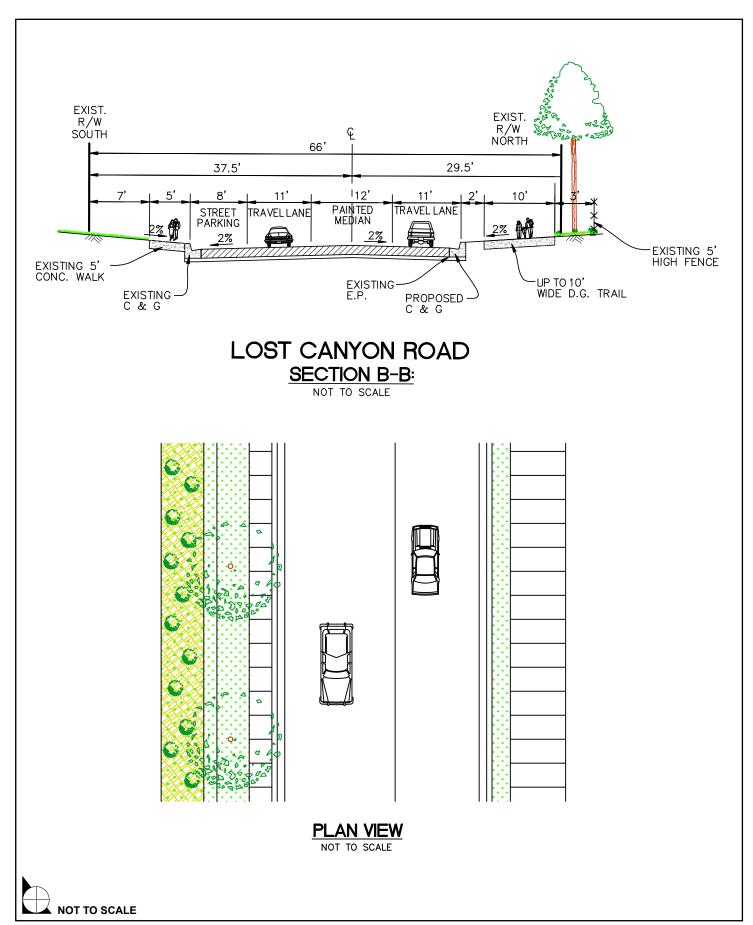
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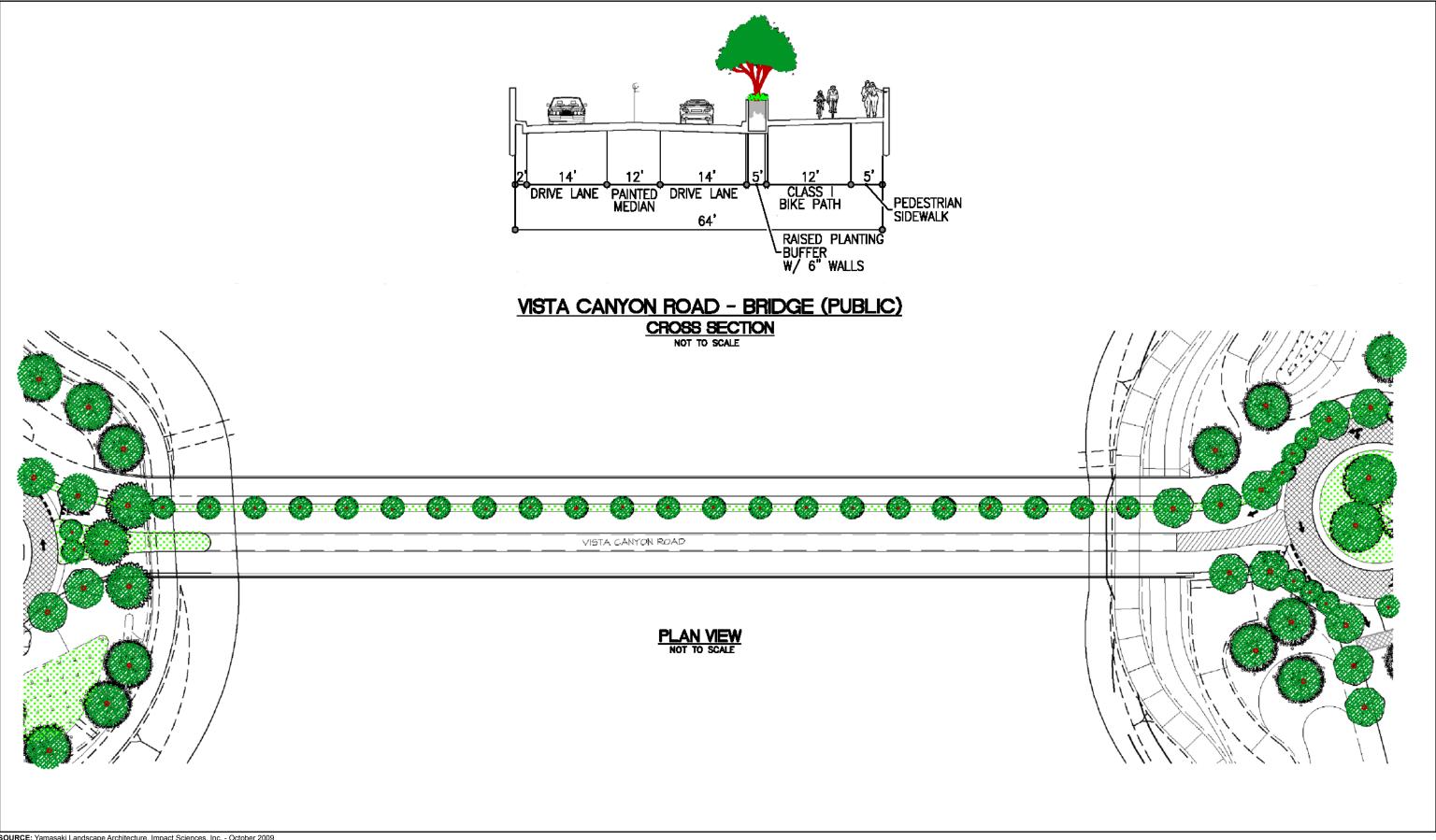


SOURCE: Alliance Land Planning & Engineering, Inc. - January 2006

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SOURCE: Yamasaki Landscape Architecture, Impact Sciences, Inc. - October 2009

(2) Vista Canyon Road SR-14 Underpass to Soledad Canyon Road Improvements

Vista Canyon Road would then proceed north past Mitchell Hill Road under SR-14 and ultimately connect to Soledad Canyon Road. As part of the Specific Plan, the applicant would widen and complete roadway improvements, within the existing City right-of-way, on Vista Canyon Road under SR-14. This roadway is currently partially improved and used for public access. Proposed improvements would include the addition of pavement, curb and gutter, and a sidewalk on one side. This road is depicted in cross-section view in Figure 1.0-15, Road Section (Vista Canyon Road SR-14 Underpass to Soledad Canyon Road Improvements).

(3) Vista Canyon Road (South)

The southerly portion of Vista Canyon Ranch Road would be a two-lane public street in a maximum right-of-way width of 79 feet, painted median/left turn lane, curb and gutter, sidewalks and Class II bike lanes on both sides, and would traverse southeast through the Specific Plan site connecting to "A" Drive at a roundabout. This segment would serve primarily the office, hotel, retail, and residential live/work uses within PA-2. This road would serve as the primary access point to the City/Metrolink transit center, park site, and parking structures in the eastern portion of PA-2. This road is depicted in cross-section view in Figure 1.0-16, Road Section (Vista Canyon Road – South).

(4) Roundabout at Vista Canyon Road and "A" Drive

The roundabout at Vista Canyon Road and "A" Drive would consist of two vehicular traffic lanes. The plan view of this roundabout is depicted in Figure 1.0-17, Road Section (Roundabout at Vista Canyon Road and Lost Canyon Road Drive).

c. Vista Square Drive (Main Street)

Vista Square Drive would be a two-lane private street, serving as the "Main Street" within PA-2 to facilitate traffic circulation, but also would enable pedestrian-oriented shops, restaurants, and services. This private street would be a maximum 88-foot right-of-way with diagonal on-street parking and sidewalks on both sides, landscape parkway, curb and gutter, and streetscape amenities (e.g., benches, bike racks, and outdoor dining). This design is intended to facilitate traffic calming to balance the needs of pedestrians with those of cars. This road is depicted in cross-section view in **Figure 1.0-18**, **Road Section (Vista Square Drive – Main Street)**.

d. Mitchell Hill Road/Mitchell Hill Circle

Mitchell Hill Road would be a two-lane public street in a maximum 45-foot right-of-way with curb and gutter, and a parkway. Directly adjacent to the right-of-way on the south side is an equestrian trail, walkway, and bike path/maintenance road. This road would become Mitchell Hill Circle, a private street, which provides the internal circulation for the commercial uses in PA-4. This private street would consist of two lanes in a maximum 26-foot right-of-way and includes various traffic calming features. These roads are depicted in cross-section view in **Figure 1.0-19**, **Road Section (Mitchell Hill Road/Mitchell Hill Circle)**.

e. Other Private Neighborhood Streets

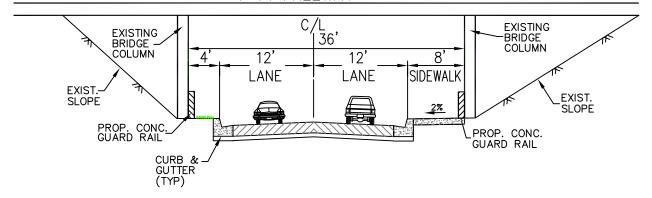
The alignment, location, and design of the below listed private streets and drives may be modified or eliminated based upon final development plans per the Vista Canyon Specific Plan.

"A," "B," and "C" Drives would be two-lane private streets, serving as the internal circulation for the office, hotel, commercial, retail, and residential land uses in PA-1 and PA-2. The private drives would be in a maximum 74-foot right-of-way with parking, parkways, and sidewalks on both sides, and curb and gutter. These private drives are depicted in cross-section view in **Figure 1.0-20**, **Road Section (A, B, and C Drives)**.

"D" and "F" Drives would be two-lane private streets, serving as the internal circulation for the residential uses in PA-3. "D" Drive would be a two-lane roadway in a maximum 60-foot right-of-way with a landscaped median, parkways, and sidewalk on one side. "F" Drive would be a two-lane roadway with a maximum 82-foot right-of-way with a landscaped median and parkways and sidewalks on both sides. These private drives are depicted in cross-section view in **Figure 1.0-21**, **Road Section (D Drive)**, and **Figure 1.0-22**, **Road Section (F Drive)**.

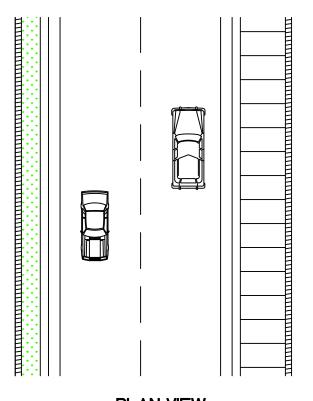
These drives would be two-lane private streets, serving as the internal circulation for the residential land uses in PA-3. They would be in a maximum 31-foot right-of-way with curb and gutter on both sides and sidewalk on one side. These private drives are depicted in cross-section view in **Figure 1.0-23**, **Road Section (E, G, H, I, K, L, M, O, P, Q, R, and U Drives)**.

I-14 FREEWAY



VISTA CANYON ROAD I-14 UNDERPASS IMPROVEMENT DETAIL CROSS SECTION

NOT TO SCALE



PLAN VIEW
NOT TO SCALE



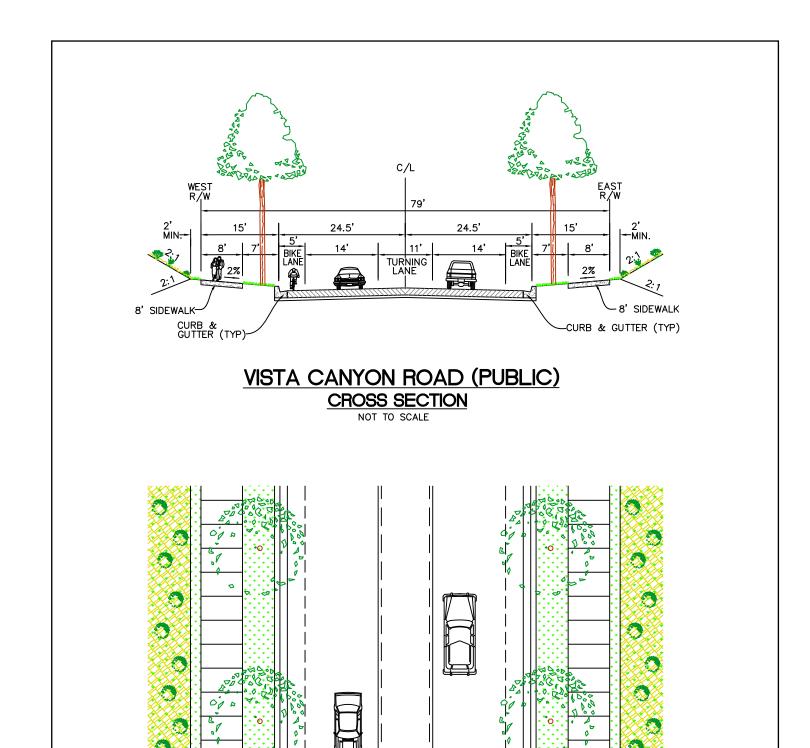
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SOURCE: Alliance Land Planning & Engineering – January 2006

FIGURE 1.0-15



Road Section (Vista Canyon Road SR-14 Underpass to Soledad Canyon Road Improvements)

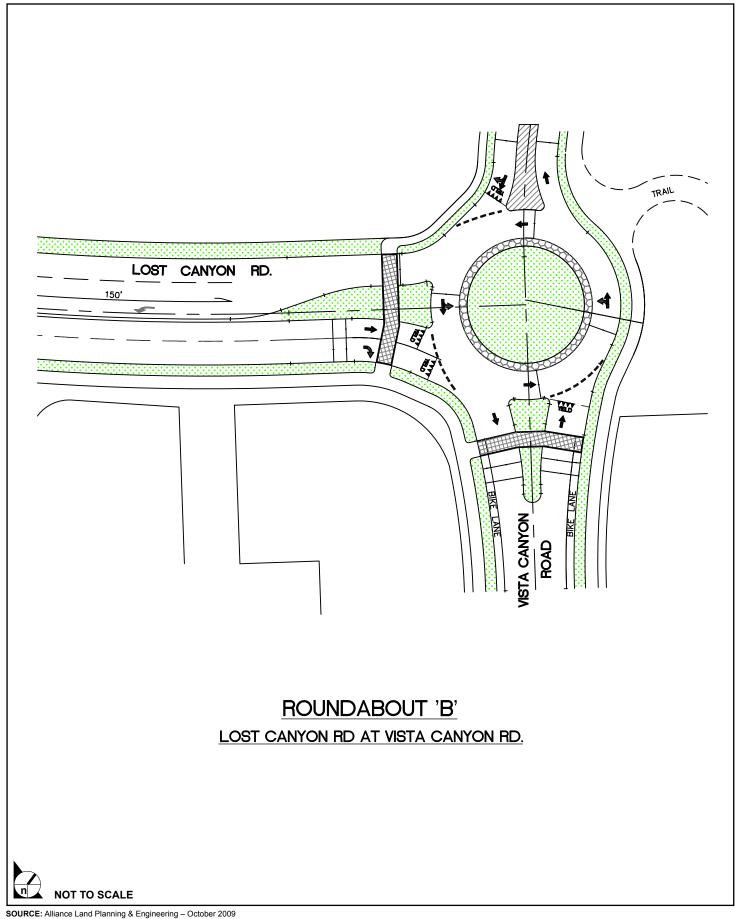


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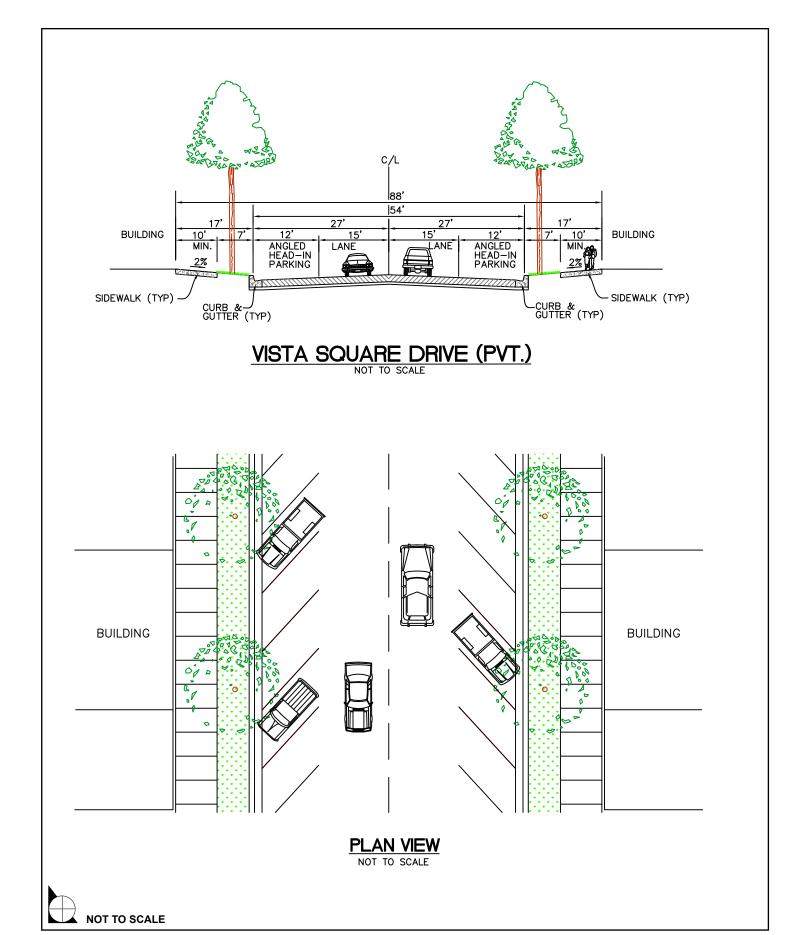
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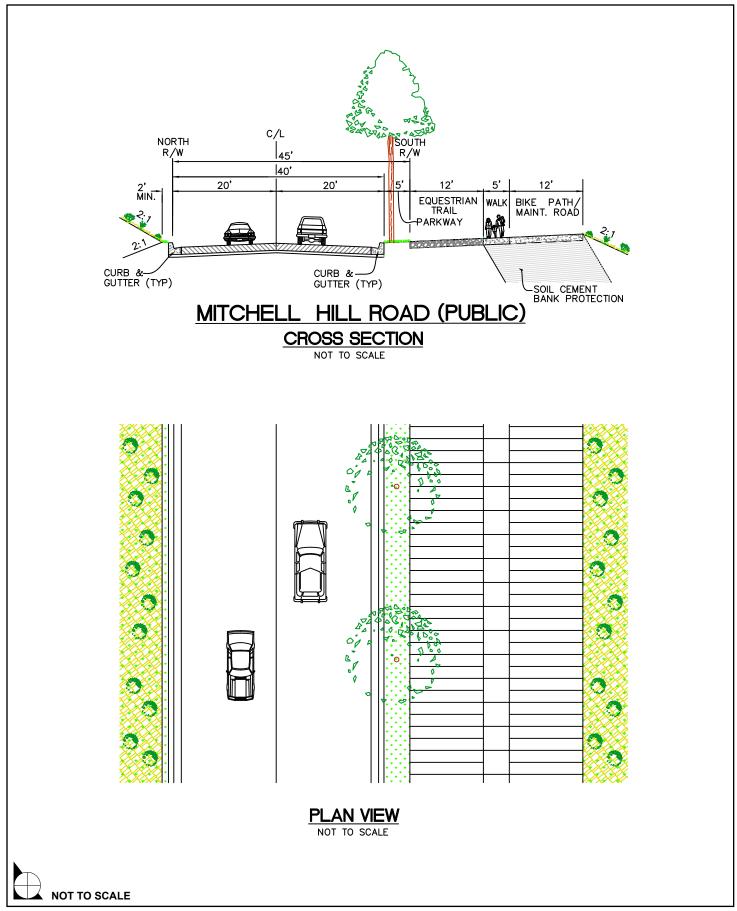
FIGURE 1.0 - 16

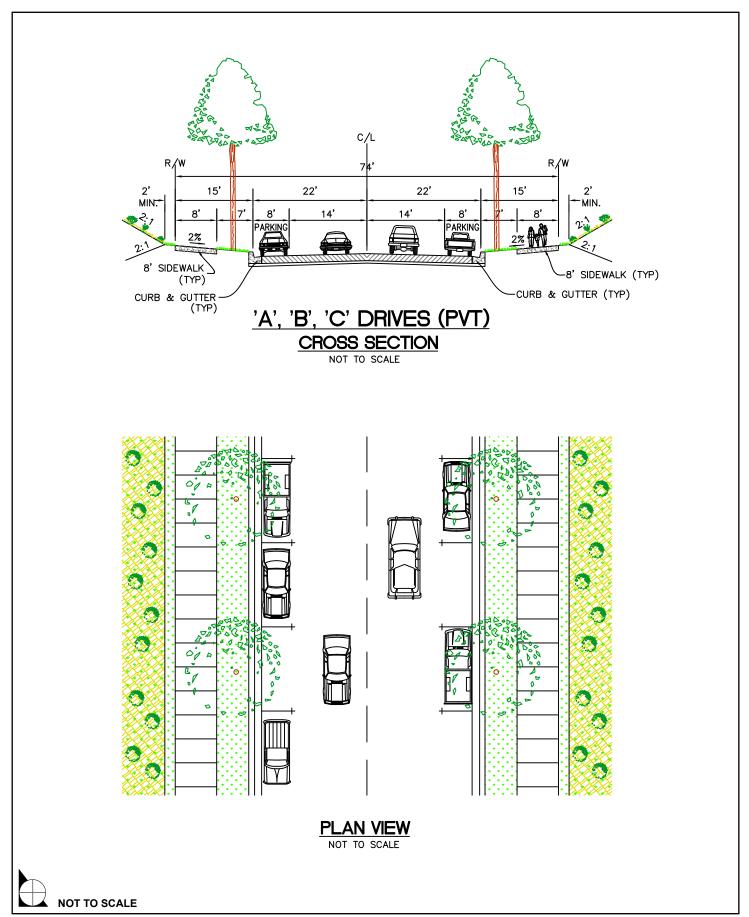


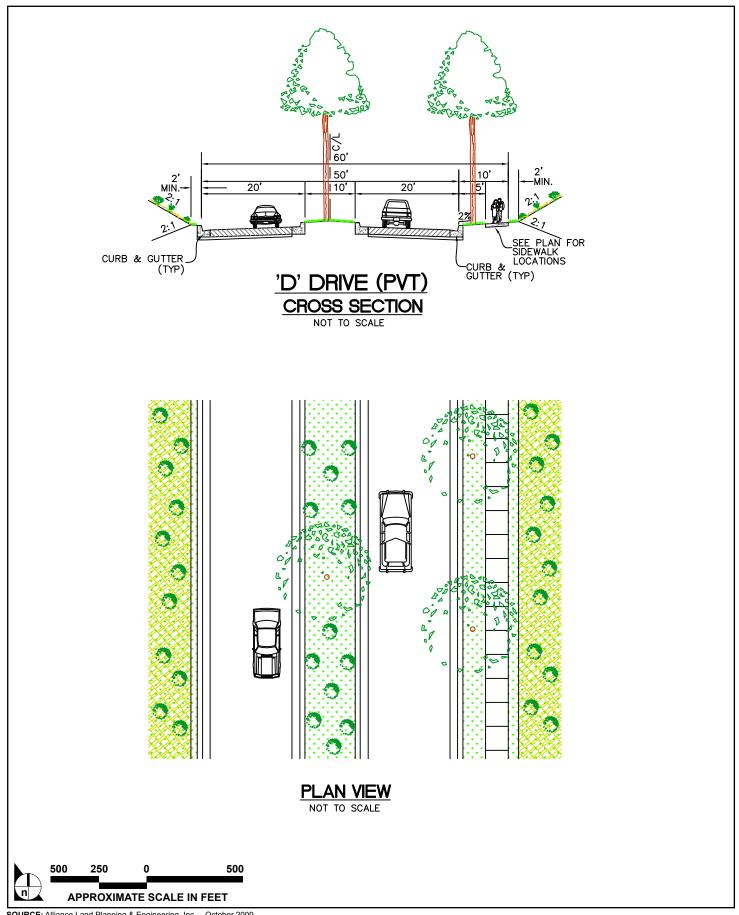


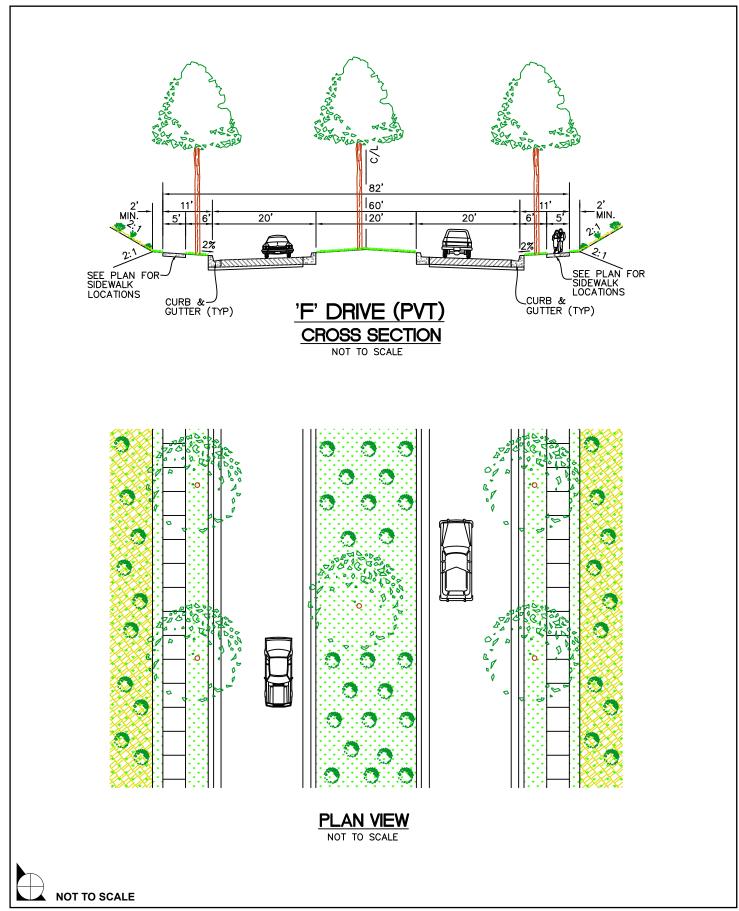


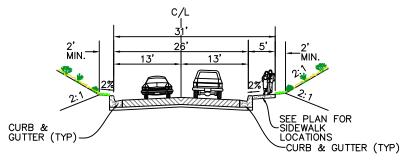








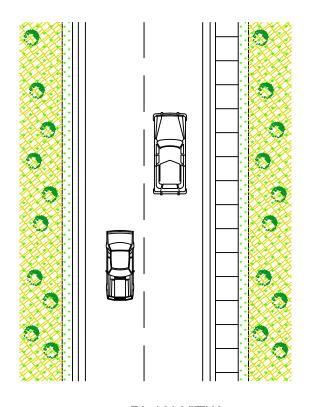




'E', 'G', 'H', 'I', 'K', 'L', 'M', 'O', 'P', 'Q', 'R', 'U' DRIVES (PVT.)

CROSS SECTION

NOT TO SCALE



PLAN VIEW NOT TO SCALE

Note: Landscape pockets would be provided in locations along private drives.

NOT TO SCALE

SOURCE: Alliance Land Planning & Engineering, Inc. – October 2009

 $\mathsf{FIGURE}\ 1.0\text{-}23$



(4) "I," "N," "S," and "T" Drives

These drives would be two-lane private drives, serving as the internal circulation for the residential land uses in PA-3. The private drives would be in a maximum 52-foot right-of-way with curb and gutter, parallel parking on both sides, and a sidewalk on one side. These private drives are depicted in cross-section view in Figure 1.0-24, Road Section (J, N, S, and T Drives).

(5) "V" Drive

"V" Drive would be a two-lane private street, serving as the internal circulation for the residential land uses in PA-3. This private drive would be in a maximum 60-foot right of-way with a raised median, curb and gutter, and sidewalk on both sides. The road for this private drive is depicted in cross-section view in Figure 1.0-25, Road Section (V Drive).

(6) "W" and "X" Drives

"W" and "X" Drives would be two-lane private streets, serving as the internal circulation for the residential land uses in PA-3. These private drives would be in a maximum 46-foot right-of-way with curb and gutter, and optional parallel parking and sidewalks on both sides. These private drives are depicted in cross-section view in **Figure 1.0-26**, **Road Section (W and X Drives)**.

(4) Parking Program

PA-1 and PA-2 of the Vista Canyon Specific Plan includes a Parking Demand Analysis that reflects the transit-oriented, mixed-use characteristics of the proposed project. This Parking Demand Analysis establishes parking requirements in PA-1 and PA-2 of the Vista Canyon mixed-use, transit-oriented development (TOD). Planning Areas 3 and 4 of the Vista Canyon project would comply with existing City of Santa Clarita Unified Development Code (UDC) parking requirements, which have been included in the Vista Canyon Specific Plan. For a more detailed discussion of the Parking Demand Analysis, please see Draft EIR Section 4.3. Traffic and Access and Appendix 4.3 (Parking Demand Analysis Vista Canyon Transit-Oriented Development [Planning Areas 1 and 2] April 2010).

(5) Transit Plan

The Specific Plan's transit component is intended to create a variety of alternatives to the use of automobiles. The project proposes relocation of the Via Princessa Metrolink Station to the Vista Canyon project site as part of this "Transit Oriented Community." This would be accomplished by providing the land and partnering with the City and Metrolink on facilities needed for the City/Metrolink transit center, located north of the existing Metrolink rail line along the Specific Plan's southern boundary in PA-2. The transit center would consist of a Metrolink passenger rail station and a City transit station.

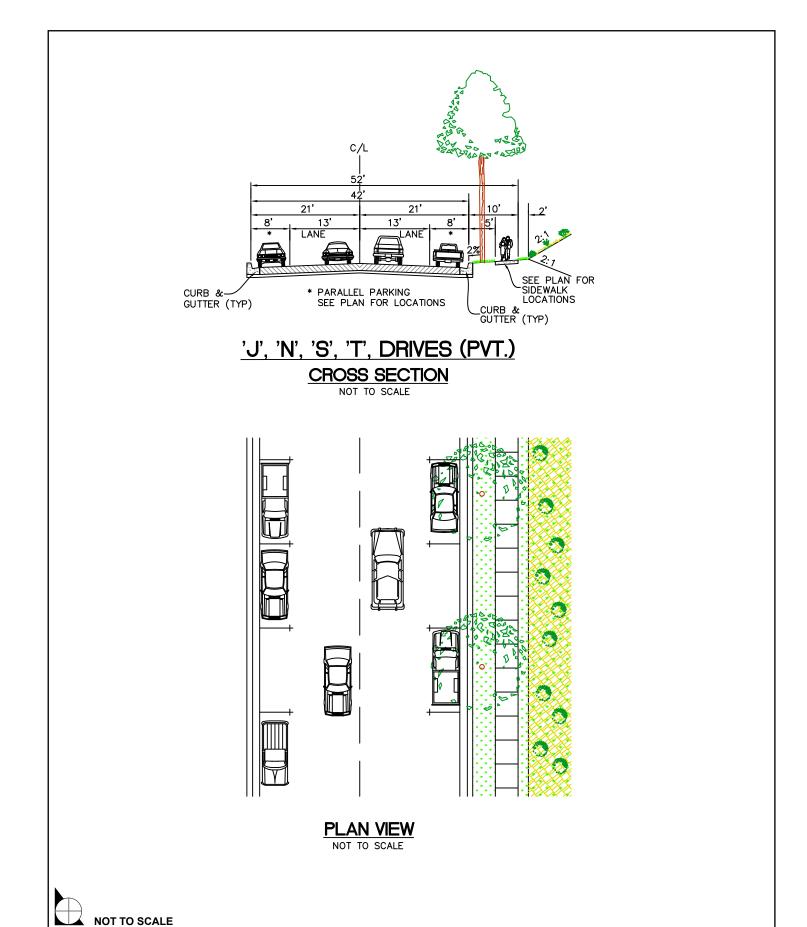
The Metrolink station component of the center would include construction of the platforms and accessory station improvements within the Metrolink right-of-way, including: construction of approximately 3,500 feet of a second main line; new turnout and a new signal; construction of approximately 1,000 linear feet of intertrack fencing; and, associated grading. The station will likely be constructed in two phases with the first phase including construction of the north platform, portions of the second main line, and adjacent surface parking. The second phase would include construction of the south platform, the pedestrian overpass and undercrossing and the adjacent parking structure and bus-transfer station.

At buildout, the Specific Plan would include one 4-story, 5-level parking structure, providing 750 parking spaces, which would be utilized for transit users during Metrolink's hours of operation and be part of the shared parking pool for PA-2 during evening and weekend hours. The parking structure also would include restroom facilities and a security/waiting room. Vehicular access to the Metrolink station would be from Vista Canyon Road and the Specific Plan's "Main Street" (Vista Square Drive). In addition, a pedestrian overpass from the third level of the parking structure to the north platform and a pedestrian underpass to the south platform would be constructed.

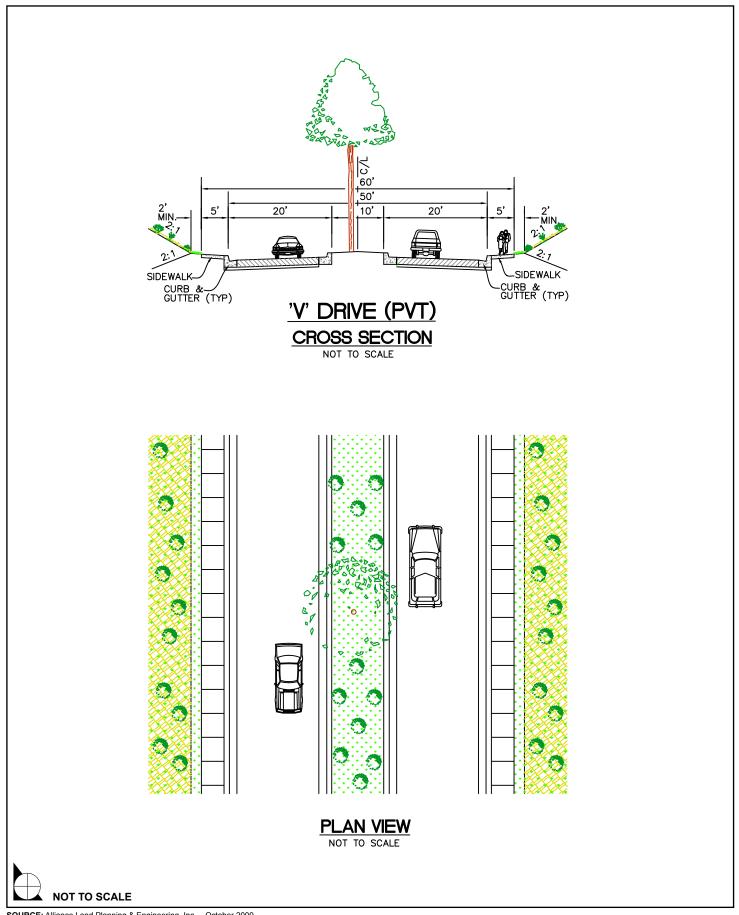
The City's transit station component of the center would include a bus transfer facility, similar to the station currently operated by the City at McBean Parkway and Valencia Boulevard. The bus transfer facility would consist of seven bus bays arranged around a loop road with covered passenger shelters. Vehicular access to the transit station is the same as the access to the Metrolink station. The transit facility is depicted on **Figure 1.0-27**, **Vista Canyon Transit Plan**.

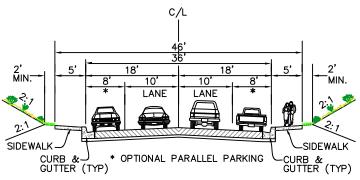
(6) Trails Plan

The Specific Plan's Trails Plan provides an extensive, comprehensive trail system throughout the project site linking the PAs to each other and the Santa Clara River Regional Trail. In summary, the project proposes over 4 miles of bicycle, pedestrian, and equestrian trails throughout the Specific Plan site. The primary trails within the Specific Plan include extensions of the Santa Clara River Trail and the Oak Park Trail, which would extend from Oak Park in PA-3 westerly to the town center in PA-2. Another trail, the Loop Trail would begin at Lost Canyon Road near the existing La Veda Avenue and extend along the Specific Plan's easterly and southerly boundaries terminating in PA-1. The trail system also would include direct connections to the City's existing regional trail system and the Santa Clara River Regional Trail, providing recreation opportunities for local residents and the region. The trails would provide connectivity to the living, shopping, work, entertainment, office, park, and recreation facilities throughout the Specific Plan site. Figure 1.0-28 illustrates the typical trail sections included as part of the proposed Specific Plan.

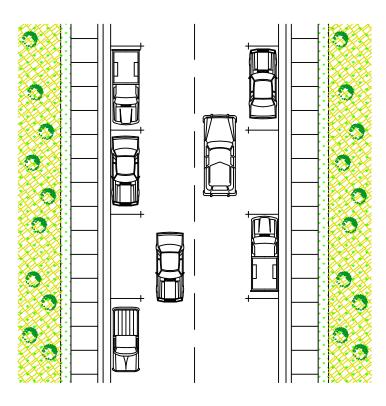








"W', 'X' DRIVES (PVT.) CROSS SECTION NOT TO SCALE



PLAN VIEW

NOT TO SCALE



NOT TO SCALE

SOURCE: Alliance Land Planning & Engineering, Inc. – October 2009

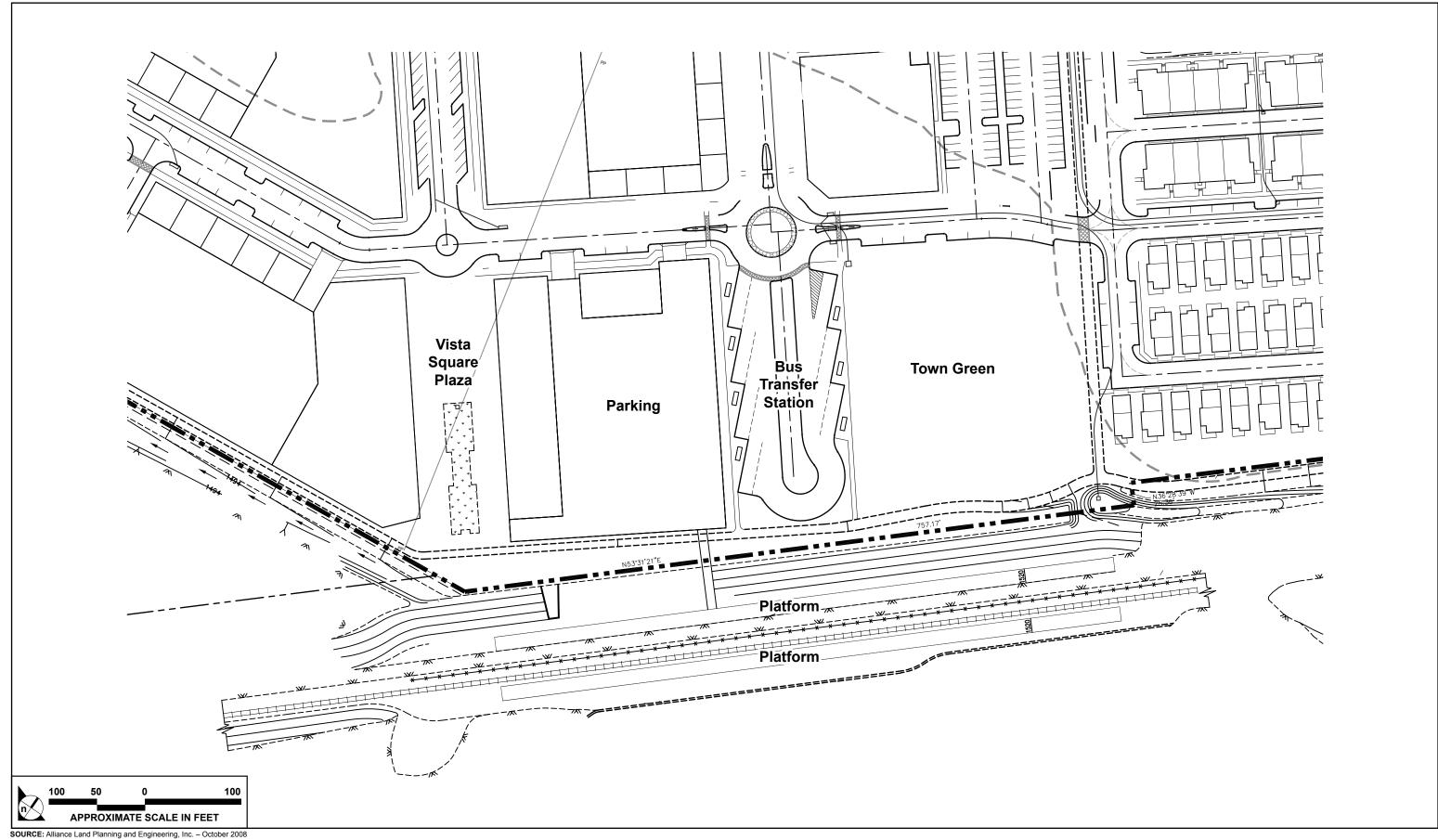
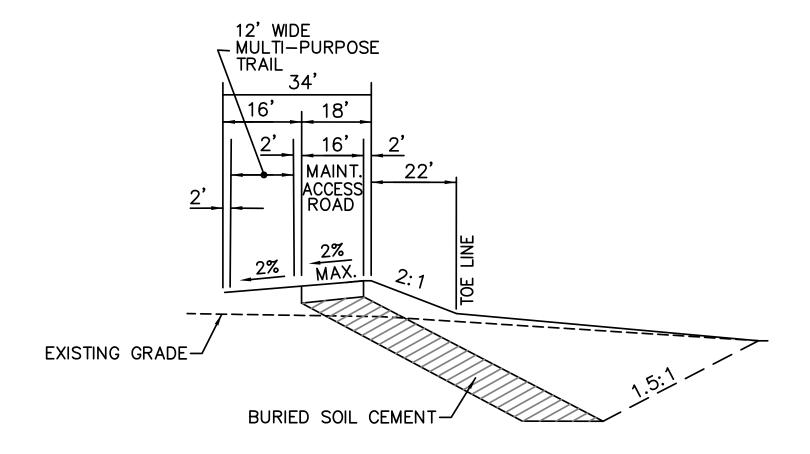


FIGURE 1.0-27

Vista Canyon Transit Plan



SECTION A-A * TYPICAL SECTION-BANK STABILIZATION

NOT TO SCALE



* SEE THE VTTM FOR ACTUAL CROSS-SECTION LOCATION

SOURCE: Alliance Land Planning & Engineering, Inc. - October 2009

FIGURE 1.0 - 28

Typical Trail Sections

a. Santa Clara River Regional Trail

The Santa Clara River Regional Trail is located on both the north and south sides of the Santa Clara River, and would provide a recreational amenity and open space within the Specific Plan for local residents and residents from surrounding communities. The Specific Plan's extension of this regional trail would represent an important recreational feature of the Specific Plan, allowing both active and passive enjoyment along the Santa Clara River. The Specific Plan trail design would accommodate combined pedestrian, bicycle, and equestrian uses.

The extension of the Santa Clara River Regional Trail also would provide a direct connection to Vista Canyon Road, providing access into Vista Square and PA-2. This trail extension would be 34-feet wide, located along the Santa Clara River, and provide access for the maintenance of bank stabilization, water quality improvements, utilities, and other flood control improvements.

b. Community Trails

The Specific Plan would include numerous pedestrian and bicycle community trails, internal to the project, providing trail access to the Santa Clara River Regional Trail, Oak Park Trail and Loop Trail. These trails would provide access to the Specific Plan's amenities and serve to link the various land uses within the Specific Plan site.

c. Drainage/Water Quality/Bank Stabilization

The Vista Canyon Drainage and Water Quality Plan incorporates methodologies to meet or exceed the ongoing National Pollutant Discharge Elimination System (NPDES) Permit requirements. The plan includes a comprehensive series of drainage, flood control, and water quality improvements designed to allow for a system to both protect development and preserve the Santa Clara River.

The proposed Vista Canyon Drainage Concept is designed to provide drainage and flood protection, and to maintain storm water flows from the project during and after buildout. As proposed, on-site surface runoff would be conveyed to a network of treatment structures, including bioretention areas, water quality basins and vegetated swales, prior to discharge into the Santa Clara River. In each PA, parking lot and roof runoff would be directed through landscaped parkways and grassy swales or through sections of pervious pavement to provide initial treatment prior to discharge into the drainage system. Please refer to **Section 4.2**, **Flood**, of this EIR for a detailed discussion of existing and post-development drainage conditions and related improvements on the project site.

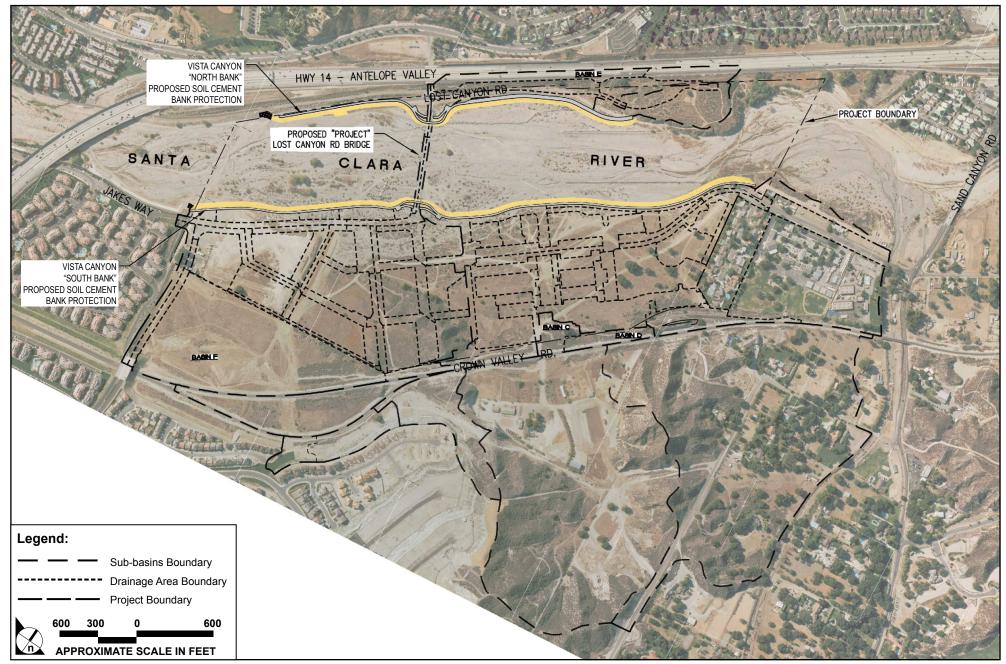
Project Design Features (PDFs) incorporated into the project to address water quality and hydrologic impacts include site design, source control, treatment control, and hydromodification control Best

Management Practices (BMPs). As currently planned, stormwater runoff from all developed areas within the project would be routed to bioretention areas, vegetated swales, and/or infiltration treatment control BMPs. These BMP's would be designed to operate off-line, receiving dry weather flows, small storm flows, and the initial portion of large storm flows. Please refer to **Section 4.8.1**, **Water Quality**, of this EIR for detailed discussion of the water quality PDFs incorporated into the project drainage concept.

As shown on Figure 1.0-29, above, the location of the proposed buried bank stabilization extends along both sides of the Santa Clara River. The proposed soil cement bank protection on the north bank of the Santa Clara River is located south of SR-14. The bank protection alignment begins at the westerly edge of PA-4 at Mitchell Hill. Mitchell Hill is an exposed bedrock formation that is approximately 40 feet above the elevation of the River and, based upon its geologic formation, does not require riverbank erosion protection. The proposed "north bank" extends approximately 3,000 linear feet from Mitchell Hill downstream and terminates near the project's northwest boundary, adjacent to SR-14. The bank protection is designed to protect the north bank against potential erosion and flooding, and also is necessary to protect the Vista Canyon Road Bridge north abutment from erosion and flooding. Portions of the north bank stabilization would replace existing rip-rap flood control improvements, generally located west of the existing Lost Canyon Road/SR-14 undercrossing.

The proposed soil cement bank protection on the south bank of the Santa Clara River is located between the easterly project boundary near existing La Veda Avenue and the westerly project boundary near the existing Colony Townhomes. The proposed "south bank" is approximately 4,500 linear feet with the horizontal alignment extending from approximately 1,400 feet downstream of Sand Canyon Bridge to 1,100 feet upstream of SR-14 Bridge. The bank protection is designed to protect the proposed project and the southerly abutment of the Vista Canyon Road Bridge from potential erosion and flooding.

The proposed bank protection on the north and south banks would also result in the removal of debris fencing along both sides of the active channel on portions of the project site. The proposed bank stabilization would consist of an 8-foot-wide soil cement section with varied height (top and toe as required by the City/County) and a maximum 1.5:1 slope. Once installed, the soil cement would be backfilled (buried) with native soils on a 3:1 or flatter slope. The excavation required to construct the bank protection would be backfilled and returned to existing grade, except as overlaid by the 3:1 or flatter fill slope. The final slope would be revegetated with native species and temporarily irrigated until the vegetation is established.



SOURCE: PACE - February 2009

Four storm drains, two through the north bank and two through the south bank, also are proposed to outlet via Reinforced Concrete Pipe (RCP). To reduce storm flow velocities and prevent erosion at stormwater discharge points into the Santa Clara River, energy dissipaters consisting of either rip-rap or other larger reinforced concrete impact-type energy dissipaters would be constructed at storm drain outlets into the River. The energy dissipaters would slow the rate of flow of runoff into the River to prevent erosion of the stream channel.

Figure 1.0-30, **Buried Bank Stabilization Cross Section**, depicts a typical cross-section for buried bank stabilization. As shown, the buried bank stabilization approach uses soil cement, which is buried beneath the existing banks of the Santa Clara River to resist future scouring.

Figure 1.0-31, **Bank Stabilization Techniques**, illustrates the exposed and buried bank stabilization techniques proposed for the project. This figure also depicts the relationship between the Santa Clara River, buried bank stabilization, and trail areas. The representative photographs used in this figure are taken from previously constructed projects located in the Valencia community, in which exposed and buried bank stabilization were used.

d. Santa Clara River Corridor

(1) Resource Description

The Specific Plan area includes a reach of the Santa Clara River between the terminus of existing Jakes Way to the west and existing La Veda Avenue to the east (see **Figure 1.0-32**, **Mitigation Areas**). This reach, which is under the jurisdiction of the USACE and CDFG, is dry except after periods of heavy rainfall, generally occurring during the winter months.

As part of the Specific Plan, a River Corridor has been delineated that corresponds to the area designated SP-OS. The proposed River Corridor is designed to be sufficiently wide to handle flooding, while retaining and enhancing the majority of the riparian habitat that exists in the River on the project site. In areas where riparian habitat must be disturbed, the Specific Plan would require such areas to be mitigated through restoration and enhancement activities.

The Santa Clara River also is part of the designated SEA 23. The SEA designation is one of several land use designations set forth in the City and County General Plans, and generally identifies lands having important biological resources. The intent of the City and County is to preserve and enhance SEAs, to the extent possible. Both the City and County recognize that measures necessary to preserve and enhance the biological resources within SEAs will vary depending upon the nature of resource values present and the degree of threat implied by potentially incompatible development. In summary, development is

permitted within SEAs; however, any development should be designed in a manner that minimizes impacts to the biological resources in the SEA and assures its ongoing viability.

The Specific Plan includes dedication of the River Corridor to the City, requires mitigation of project impacts within the River Corridor through restoration and enhancement, provides for transition areas between the River Corridor and development, restricts recreational uses as discussed below, and includes long-term monitoring and maintenance of the River Corridor.

(2) Mitigation Requirements

Mitigation for impacts of the Specific Plan on jurisdictional waters and riparian habitat would include restoration and enhancement activities.

a. Mitigation Through Restoration/Enhancement

The Vista Canyon California Rapid Assessment Methodology Report (CRAM), prepared by Dudek and Associates, Inc. (Dudek 2008), is found in **Appendix 4.6** of the Draft EIR. The purpose of this assessment is to determine the functional condition of the wetlands within the project area in order to support planning and design of the proposed Vista Canyon project and future habitat restoration planning.

The riparian habitat within the Santa Clara River that is impacted by the Specific Plan would require mitigation through restoration and enhancement activities; however, minimization of impacts to such resources was the primary goal during design of the Specific Plan. Accordingly, impacts to jurisdictional waters and riparian resources would be minimized and mitigated by implementation of the Specific Plan's *Conceptual Wetland Mitigation and Monitoring Plan*, prepared by Dudek & Associates, Inc. (Dudek 2009). This plan is found in **Appendix 4.6** of this Draft EIR.

Mitigation and monitoring activities within the Specific Plan area also would be subject to requirements imposed during the local planning and environmental review process by the City, as the lead agency. In addition, the Specific Plan would require subsequent federal and state permitting from responsible agencies, including the CDFG streambed alteration agreement required by Fish and Game Code section 1600, et seq., and the USACE' section 404 permit, as well as consultation with the RWQCB. The Mitigation and Monitoring Plan would include the following parameters:

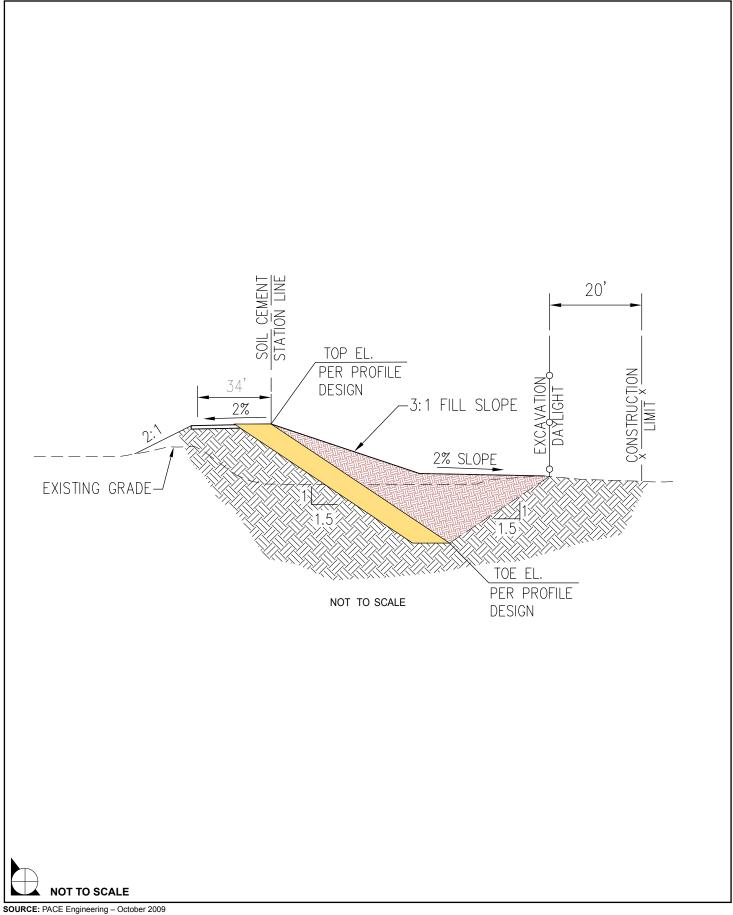








Photo 2



Photo 3



Photo 4



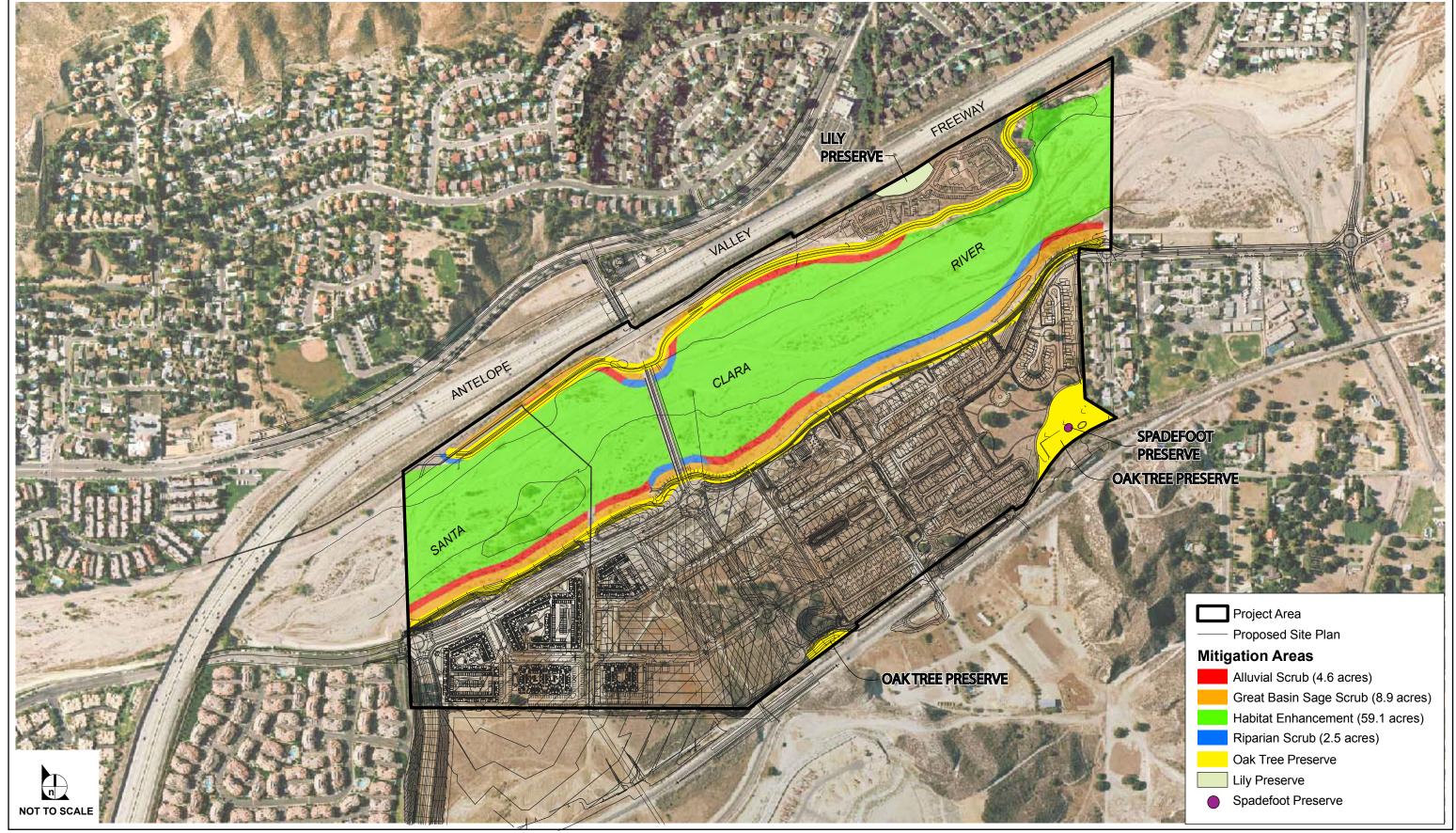
Photo 5



Photo 6

Photo 1-5: Bank Stabilization at Creekside Photo 6: Bank Stabilization at Bridgeport (These photos depict Buried Bank stabilization)

SOURCE: Impact Sciences, Inc. – October 2009



SOURCE: Alliance Land Planning and Engineering, Inc., Dudek – October 2009

FIGURE 1.0 - 32

(1) Riparian Habitat to be Established

The riparian vegetation communities proposed to be established in the River Corridor are alluvial scrub and riparian scrub in temporary impact areas within the channel, and Great Basin sage scrub on the channel banks. The character and species composition of the restored/enhanced vegetation communities is intended to be consistent with the character and species composition of the existing alluvial scrub and riparian scrub.

(2) Long-Term Monitoring

To ensure that the restoration and enhancement areas in the River Corridor meet success criteria to be established by the permitting agencies, a five-year monitoring period would be implemented. During this time, the River Corridor site would be monitored by a Habitat Restoration Specialist, who would then make recommendations to a Restoration Contractor to perform maintenance tasks necessary to keep the River Corridor site in compliance with success criteria. Should the River Corridor not meet the final sign-off criteria by the end of year five, the monitoring period would be extended until agency sign-off is obtained, which shall not be unreasonably withheld.

(3) Recommended Plant Palette for Mitigation Areas

The restoration areas would be planted with seed and container plants. Enhancement areas generally would not be planted, but instead would rely on natural recruitment. Some limited planting in enhancement areas may occur in places where large areas are disturbed from the removal of revetment structures or non-native invasive species.

(4) Mitigation Implementation

The Conceptual Wetland Mitigation and Monitoring Plan (Dudek 2009) sets forth the mitigation implementation plan and schedule. In summary, no public access would be provided to the mitigation areas, which would be fenced and posted with appropriate signage. Restoration areas would require re-contouring of temporary impact areas following installation of buried bank protection. After completion of bank protection installation, stockpiled native topsoil would be used on the surface of the temporary impact areas and channel banks.

The primary goal of restoration and enhancement mitigation efforts is to establish native vegetation communities capable of maintaining and supporting themselves in perpetuity. A temporary, on-grade irrigation system would be installed to enhance overall success of the restoration and enhancement efforts.

(5) Maintenance Activities During Monitoring Period

The Conceptual Wetland Mitigation and Monitoring Plan (Dudek 2009) sets forth the maintenance activities required in the mitigation areas during the specified monitoring period. In summary, the maintenance activities include weed control, trash removal, and irrigation maintenance.

(6) Monitoring Plan

The Conceptual Wetland Mitigation and Monitoring Plan (Dudek 2009) sets forth the monitoring plan for the mitigation areas in the River Corridor. In summary, the monitoring plan includes a 120-day plant establishment period within a five-year long-term monitoring phase. The monitoring would be based on specified performance standards and success criteria. Monitoring would be both qualitative and quantitative, and follow a specified schedule.

(7) Reporting/Costs

The Conceptual Wetland Mitigation and Monitoring Plan (Dudek 2009) sets forth the annual reporting and agency notification requirements for the River Corridor. The Specific Plan applicant or designee would be financially responsible for all costs associated with the implementation, maintenance, and monitoring of the mitigation areas within the River Corridor.

(8) Contingency Measures/Adaptive Management

The Conceptual Wetland Mitigation and Monitoring Plan (Dudek 2009) includes contingency measure provisions to be implemented to bring the mitigation areas within the River Corridor into compliance with performance standards and success criteria, if such criteria are not met by the end of the five-year long-term monitoring period. In addition, adaptive management must be implemented in the event of unforeseen or unpredictable circumstances.

b. Management Plan

Upon final approval of the Specific Plan, the River Corridor would be designated as public open space and zoned SP-OS. There shall be no public access to, or use of, the mitigation areas within the River Corridor. The Los Angeles County Department of Public Works would require flood control maintenance access in some areas along the bank protection to inspect and maintain the public drainages/outlets. The limits of the mitigation areas within the River Corridor would be established outside of the flood control maintenance access areas. The new bridge across the Santa Clara River also would have adequate barriers at the bridge perimeters to discourage access to the mitigation areas within the River Corridor.

e. Water/Wastewater

The Santa Clarita Water Division of the CLWA would be the retail water company providing potable water to the project site. The Vista Canyon Preliminary Potable and Recycled Water System Infrastructure is shown on Figure 1.0-33.

As shown on **Figure 1.0-33**, the proposed water delivery system consists of a network of varying sized water mainlines that generally follow major roadways. A network of smaller lines located within the planned roadway network would distribute the water for connection to laterals located on individual lots. Potable water storage would come from the existing Santa Clarita Water Division infrastructure system.

Potable water demands for Vista Canyon would be met by using groundwater produced from the Alluvial aquifer from existing Santa Clarita Water Division wells located in the vicinity and State Water Project water from the CLWA. The Specific Plan site also includes two Santa Clarita Water Division potable water wells located on a small parcel owned in fee by the CLWA, Santa Clarita Water Division. Implementation of the Specific Plan would result in the removal of one of the two wells.

The Vista Canyon project proposes to use recycled water for landscape irrigation purposes and other allowable uses, such as public restroom facilities for office and commercial uses. The proposed delivery system for recycled (non-potable) water is illustrated on **Figure 1.0-33**. Currently, recycled water is not available to the project site. However, the Vista Canyon Wastewater Treatment Plant (WRP) would be constructed and operational prior to the first occupancy.

As proposed, the WRP would be sized to treat approximately 395,411 gallons per day (gpd) and would be owned and operated by the City of Santa Clarita. As such, it would be considered a "municipal wastewater treatment plant" or publically owned treatment work (POTW). The project site is not currently within the boundary of the Santa Clarita Valley Sanitation District, but is within the Santa Clarita Water Division of CLWA service boundaries. The project applicant proposes to construct the WRP in conjunction with the project, and provide a turn-key facility to City. The City would likely contract for operation of the WRP, And all costs associated with the ongoing maintenance of the WRP would be paid for by future residents and property owners within the Vista Canyon project through the formation of an assessment district.

The Vista Canyon WRP would treat the wastewater generated by the project along with a portion of flows from a City of Santa Clarita sewer line crossing the project site, with all solids from the water factory sent to the Santa Clarita Valley Sanitation District's Saugus and Valencia water reclamation plants for processing. Recycled water from the WRP would ultimately be delivered to CLWA as the wholesale

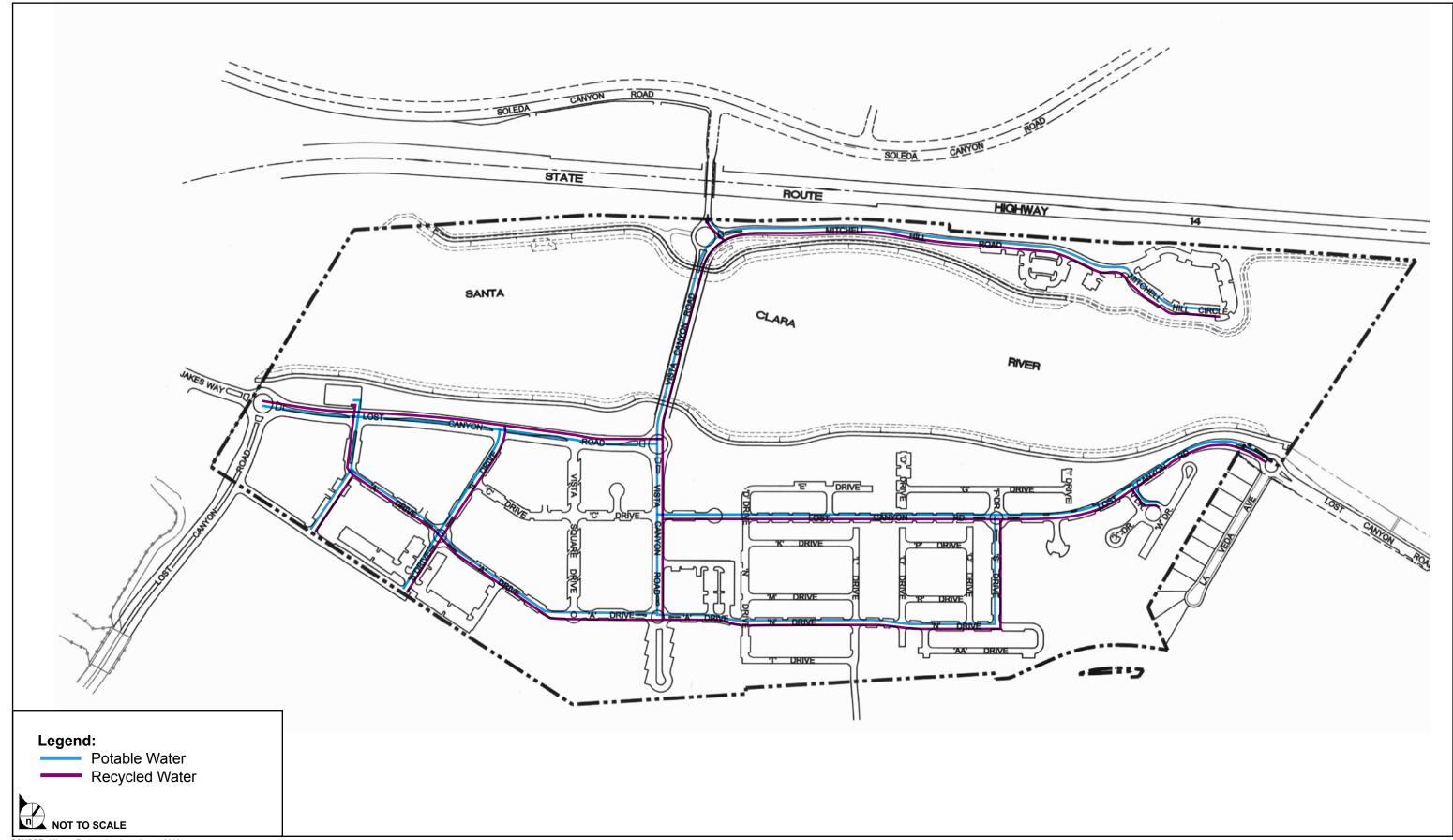
water agency for the Santa Clarita Valley. This water would be distributed by CLWA through its reclaimed water distribution system both within and outside of the project boundary. Initially, some of the water may be directed to the percolation pond/s, or infiltration basin/s, adjacent to the WRP until the CLWA recycled system comes on-line.

f. Sustainability Plan

(1) Introduction

The Vista Canyon project includes project design features to reduce greenhouse gas (GHG) emissions associated with site development, and promote a sustainable community. Before incorporating these project design features into the site design, the applicant, in cooperation with the City, investigated several sustainability strategies. These sustainability strategies are included within the Vista Canyon Sustainability Plan. For example, because mobile source emissions (i.e., emissions resulting from the fossil fuel combustion associated with motor vehicle travel) account for roughly half of GHG emissions from residential developments, the proposed project includes a number of design features intended to reduce the number of vehicle miles traveled by future residents and occupants of Vista Canyon. Project-related traffic reduction measures include the creation of a mixed-use community (with jobs in close proximity to homes), the placement of retail land uses within 0.5 mile of commercial/residential/office/recreation areas, the accommodation and implementation of public transit options (Metrolink station and bus transfer station), and the provision of trails, bicycle lanes and sidewalks to encourage non-vehicular options. Additionally, substantial reductions in mobile source emissions also are anticipated from the enactment of more ambitious fuel economy and vehicle efficiency standards by relevant federal and state regulatory bodies.

The other primary contributor of GHG emissions within the project would be the energy use required for residential and non-residential buildings. The project applicant has committed to requiring that all future residential and non-residential buildings exceed the 2008 Title 24 standards by a minimum of 20 percent. Additionally, the project applicant has committed to the installation of 80,000 square feet of solar on rooftops or parking structures within the project. The project applicant also has committed to the installation of ENERGY STAR appliances and the use of solar heating for pools within the project.



SOURCE: Alliance Engineering, Inc – August 2010

(2) Application of Project Design Features to Vista Canyon

Foremost among the design features that would reduce GHG emissions and promote sustainability is the project's provision of property and a portion of the funding for a new multi-modal transit center to facilitate transit, reduce automobile trips, and direct pedestrian access to and from such facilities through trails and sidewalks that connect to the project's residential, retail/commercial, and office areas. In addition, the project would provide opportunities for new employment in the eastern Santa Clarita Valley, with office, retail, and live/work uses integrated within a street system oriented around a "Main Street" (Vista Square Drive). A diversity of housing types are also provided, in conjunction with a proposed trail system and public and private parks connecting to the town center design and the new City/Metrolink transit center.

As shown, the project would incorporate various components of a sustainable community, including the following:

- (a) **Mix of Land Uses.** Vista Canyon would include a broad range of housing types, along with retail/commercial/office and associated facilities. The residential component includes a diverse range of housing consisting of 1,117 residential units (96 single-family unit; 1,021 multi-family condominium units). To minimize and shorten vehicular trips, all residential units would be within walking distances to the Specific Plan community's mixed-use, retail/commercial, and office uses. The Specific Plan also would create opportunities for non-vehicular travel and encourage pedestrian mobility by providing an internal trail/sidewalk circulation system that links residential neighborhoods to nearby schools, area parks, and adjacent park and recreation areas.
- (b) **Provision of Jobs.** The project is projected to provide up to 4,000 new jobs (based upon information provided by the project applicant) in its retail/commercial and office areas. When combined with other job centers in the Santa Clarita Valley, Vista Canyon would help the Santa Clarita Valley's jobs-housing base, which is critical to establishing a sustainable community because it allows people to work close to home and minimize vehicle miles traveled.
- (c) Locating Residential in Close Proximity to Commercial/Retail/Public Spaces. All of the residential units in the project would be located within walking distance of retail/commercial and office areas. Residents within Vista Canyon would be able to utilize trails, sidewalks, and the Santa Clara River Regional Trail to walk to retail/commercial centers, offices, public and private recreation facilities, parks, and nearby schools. As stated above, this traditional neighborhood approach, designed around a mix of other land uses, would minimize vehicle trips.
- (d) **Transit-Oriented Development.** The project site is uniquely situated in proximity to Metrolink rail lines, affords the opportunity to plan a new City/Metrolink transit center that would facilitate transit and reduce automobile trips, and creates a lively place to live, work, play, and shop.

- (e) **Open Space, Recreation, and Parks.** The project includes extensive open space, recreation opportunities, and public/private parks to serve the neighborhoods within Vista Canyon. These areas are linked by the project's trail system, which promotes walking and biking while minimizing vehicle trips.
- (f) **Reducing Impermeable Surfaces.** To curtail urban runoff and maximize groundwater recharge, the project would utilize smaller street sections where possible, native landscape areas, permeable pavement in various parking areas and non-structural water quality treatments. This design reduces impermeable surfaces and minimizes urban "heat island" effects.
- (g) Water Conservation and Reuse. Vista Canyon includes the construction of a WRP that would provide for the project's irrigation needs and also provide additional recycled water for off-site use. As proposed, the project would create a water demand of approximately 333.7 acre-feet per year. The proposed Vista Canyon Water Reclamation Plant would generate approximately 442.9 acre-feet per year of recycled water, completely offsetting the demand of the project. Additionally, Vista Canyon would require compliance with numerous water efficiency strategies, including the minimization of turf, use of drought-tolerant landscaping, automated irrigation systems, and water-efficient appliances and fixtures.
- (h) **Traffic/Transportation Improvements.** The project's circulation plan minimizes vehicle trips and reduces GHG emissions through the design of internal roads that place homes, schools, retail/commercial areas, offices, and trail system in close proximity. Transit also is uniquely incorporated into the traditional neighborhood and town center design. Trails and bike paths leading to close-to-home jobs, neighborhood-serving retail, offices, and nearby schools encourage residents to reduce vehicle miles traveled.
- (i) **Energy Efficiency.** The project applicant has committed to requiring that all future residential and non-residential buildings exceed the 2008 Title 24 standards, which became effective January 1, 2010, by a minimum of 20 percent. In addition, the project applicant, or its designee, would provide ENERGY STAR appliances and equipment for any standard items (e.g., clothes dryers, dishwashers, and refrigerators) included within the residential units at the project site. ENERGY STAR equipment (e.g., monitors, printers, copiers) also would be provided in the office buildings.
- (k) **Renewable Energy.** The project applicant has committed to install 80,000 square feet of solar panels on rooftops or parking structures within the project site.
- (l) **Solar Energy System Option.** Consistent with Governor Schwarzenegger's Million Solar Roofs Plan, the project applicant or designee, acting as the seller of the single-family residences constructed as part of the development, would offer a solar energy system option to all customers that enter negotiations to purchase a new home; such offer would include the total installed cost of the solar energy system option and the customer's estimated cost savings.

g. Landscape Plan

The Vista Canyon Conceptual Landscape Plan is shown on **Figure 1.0-34**. The conceptual landscape plan for Vista Canyon focuses primarily on the use of native and drought tolerant tree and plant species to create a natural and vibrant environment. All plant species listed, both native and non-native have been

chosen due to their ability to thrive in the Santa Clarita climate and their potential to add complexity and texture to the open space/landscape areas within the project. Plants listed that are non-native or not drought tolerant will be used sparingly and only in areas that require their unique properties such as: bio-swales, rain gardens, northern building exposures, and commercial high use areas. The use of turf shall be very limited and only used in locations where it will serve for passive or active recreation. Minimum turf will be used in road medians or along major streets.

The irrigation systems will be designed, installed, operated, and maintained in conformance with the State Water Efficient Landscape Ordinance. The main objective for the irrigation design is to minimize water use, maximize efficiency, and explore the feasible uses of recycled water. These objectives will be met through the use of Smart ET Based controllers, hydro-zoning, moisture sensors, rain-shut-off devices, cisterns, and drip irrigation. Although much of the native planting areas may receive temporary irrigation, a permanent irrigation system is important for a majority of the landscape planting in order to comply with the Los Angeles County Fire Department Fuel Modification Guidelines.

There are two major varieties of oaks that will be planted as mitigation oak trees: the evergreen Coast Live Oak (Quercus agrifolia) and the deciduous Valley Oak (Quercus lobata). Open space has been established for the planting of oak trees. The largest of the areas is a 2-acre portion of the proposed Oak Park. This area will provide abundant open space for up to 100 mitigation oak trees. The Town Green Park has also been designed to include an open space area near the project's southern boundary for the planting of oak trees. The open space along the river trails, the project trails, and the Santa Clarita River revegetation zone will also create opportunities for mitigation oak tree planting.

h. Parks and Recreation Plan

The project provides opportunities for active and passive recreational opportunities for its residents. Figure 1.0-35, Parks and Recreation Plan, illustrates the locations of public and private parks and recreation facilities in relation to other designated open space within the project site. The three primary recreational amenities associated with Vista Canyon include the 7-acre Oak Park, 1.6-acre Town Green, Community Garden and River Education Center. Up to six private recreational facilities would be provided in the project.

The Vista Canyon Parks and Recreation Plan includes the following active and passive parks that would be accessible to the public:

• The proposed 7-acre Oak Park would be located in the southeast portion of the project site in PA-3. The park would include an approximately 2-acre oak tree preserve for passive uses, and the remaining 5-acres would be developed with improvements similar to a neighborhood park. The park would be dedicated to the City.

• The proposed 1-acre River Education Center would be located north of the Santa Clara River within PA-4. The center would consist of a building ranging in size from 1,000 to 2,500 square feet of building space and would be dedicated to the City.

The Vista Canyon Parks and Recreation Plan includes the following private parks:

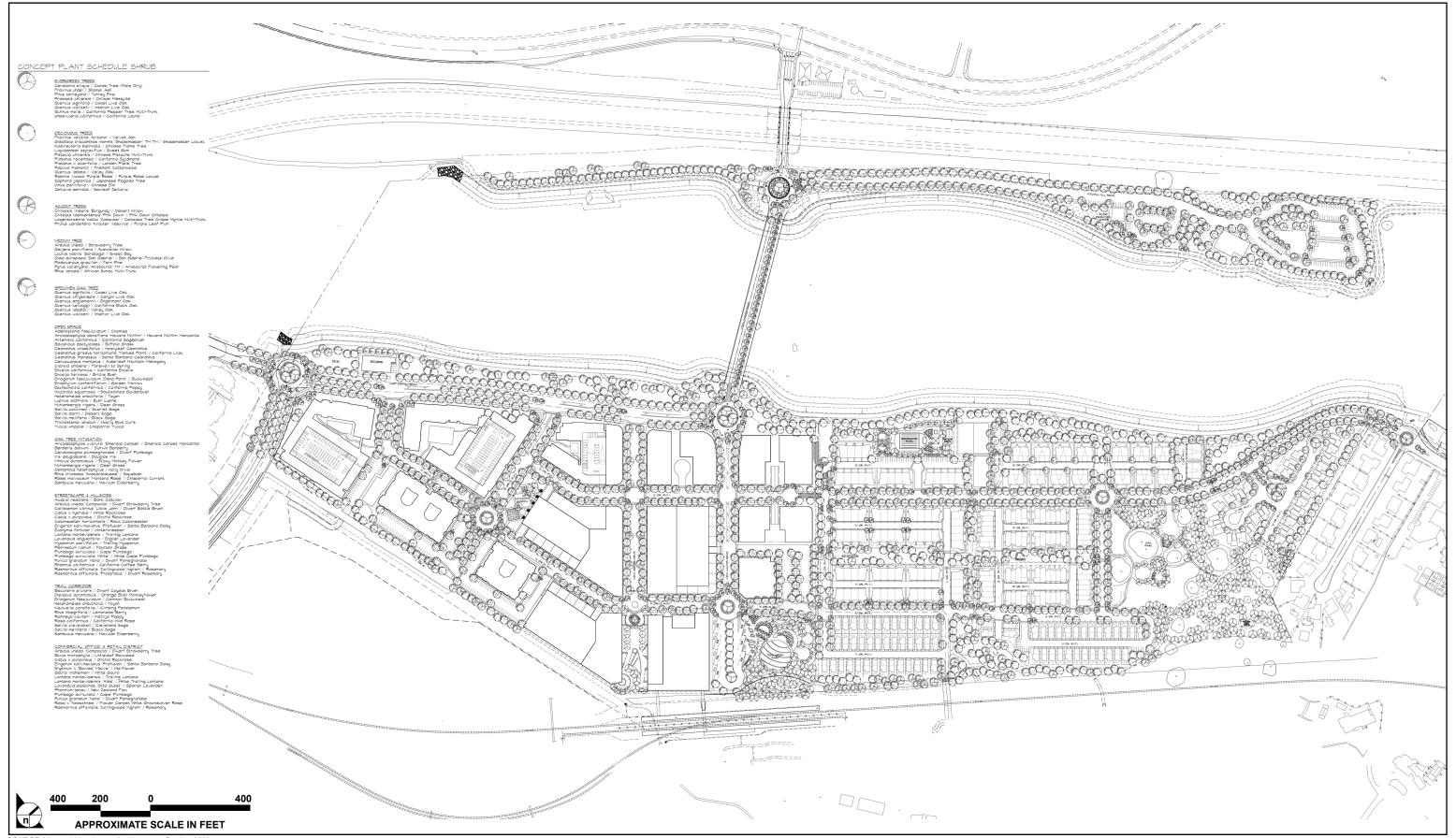
- The project site would include the following private recreation facilities totally approximately 4.18 acres: The Community Recreation Area would be located within PA-3, and would consist of a pool, clubhouse, and three tennis courts. Multi-family recreation areas and courtyards are located adjacent to multi-family uses on the western portion of the project site in PA-1 and PA-2.
- The proposed 0.4-acre Community Garden would be located within PA-2, and would be centrally located for residences on the site. The garden and amenities would be available for residents and would be owned and maintained by a property owners' association.
- The proposed 1.6-acre Town Green would abut the Metrolink transit center on the southern portion of the project site. The Town Green could be developed with a small amphitheater, water feature, picnic area, and sitting area. The area would be accessible to the public, though it would be owned and maintained by a property owners' association.

Figures 1.0-36, **1.0-37**, and **1.0-38** illustrate the conceptual park concept designs for the Oak Park, Lot 49 Park, and Community Garden.

i. Grading Plan

The total amount of soil to be cut from the project site is estimated at 590,000 cubic yards (cy). The total amount of fill is estimated at 830,000 cy. This cut and fill grading would be in addition to 1.7 million cubic yards of remedial grading required for the project. Finally, approximately 500,000 cy of soil would be imported to the site; including the 240,000 cy difference between the project's cut and fill and the additional fill needed to compensate for soil shrinkage associated with soil compaction. The Grading Plan is depicted on **Figure 1.0-39**.

Additionally, grading activities will require import of dirt from southern portions of the project site to northern portions of the project site for the construction of the buried bank stabilization improvements. To facilitate this action, a temporary at-grade haul route, at a width of 35 feet, would be constructed within the Vista Canyon Road bridge corridor and used for up to nine months.



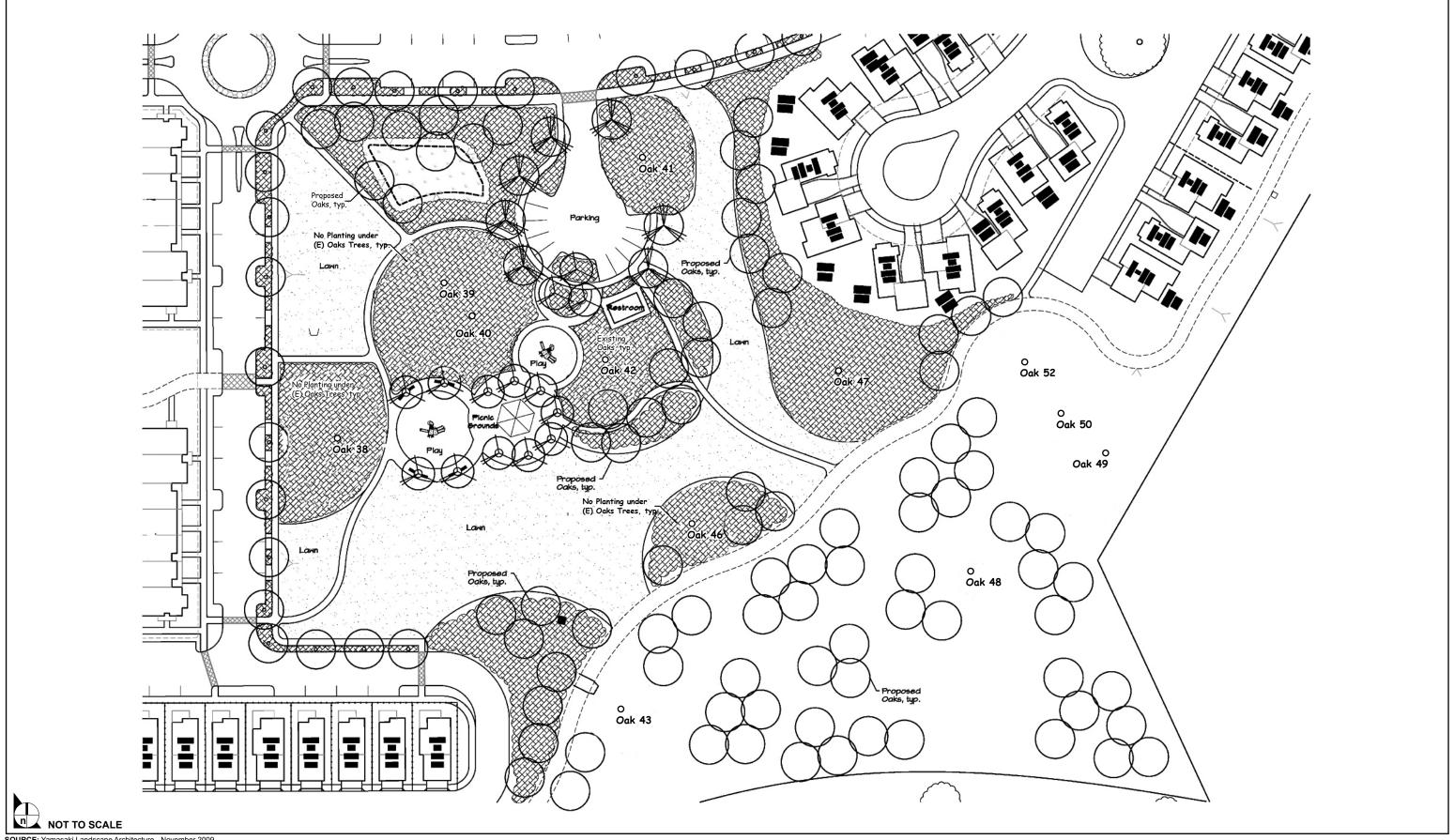
SOURCE: Yamasaki Landscape Architecture – October 2009



SOURCE: Yamasaki Landscape Architecture

FIGURE 1.0 - 35

Parks and Recreation Plan



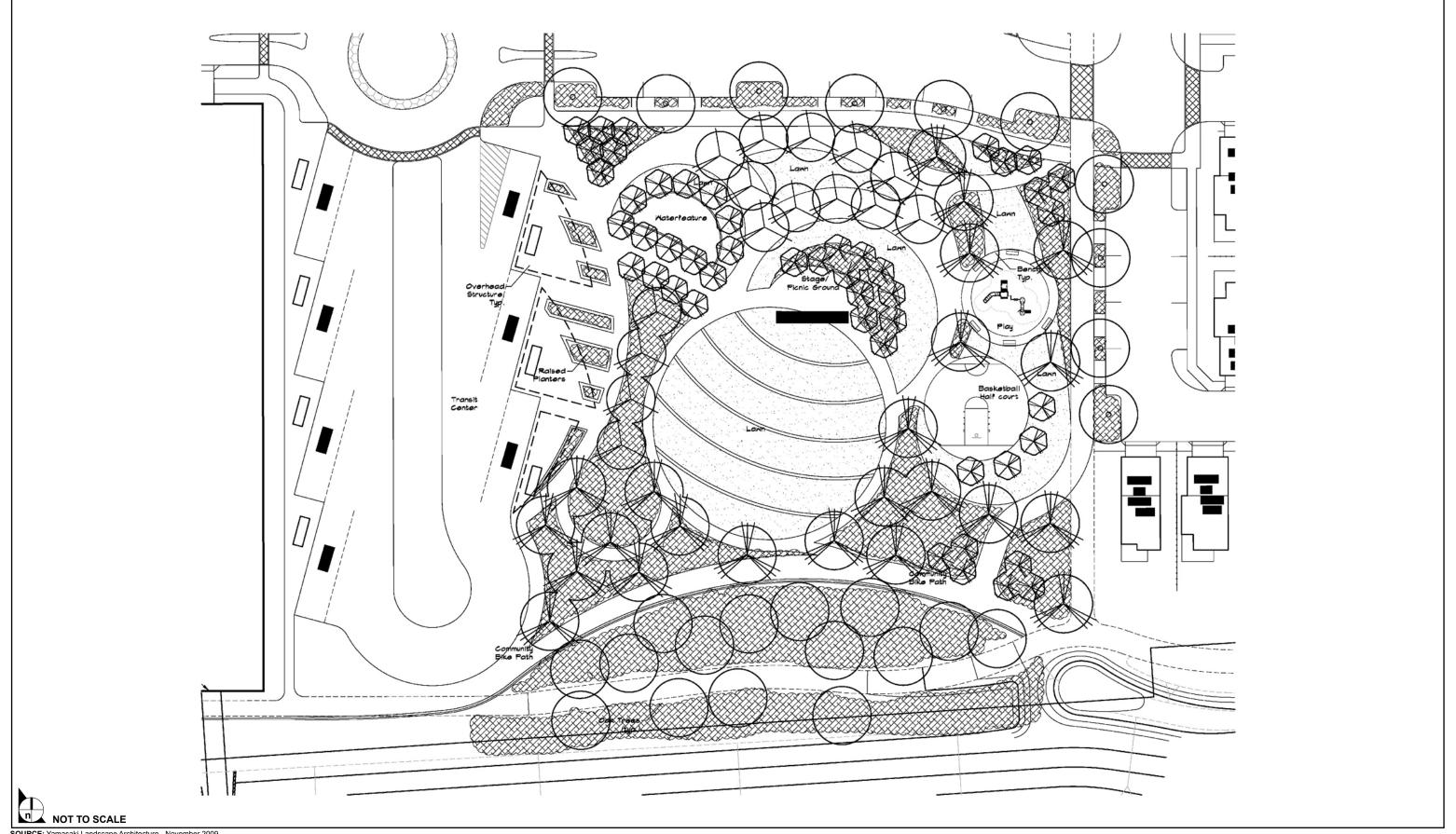


FIGURE 1.0 - 37

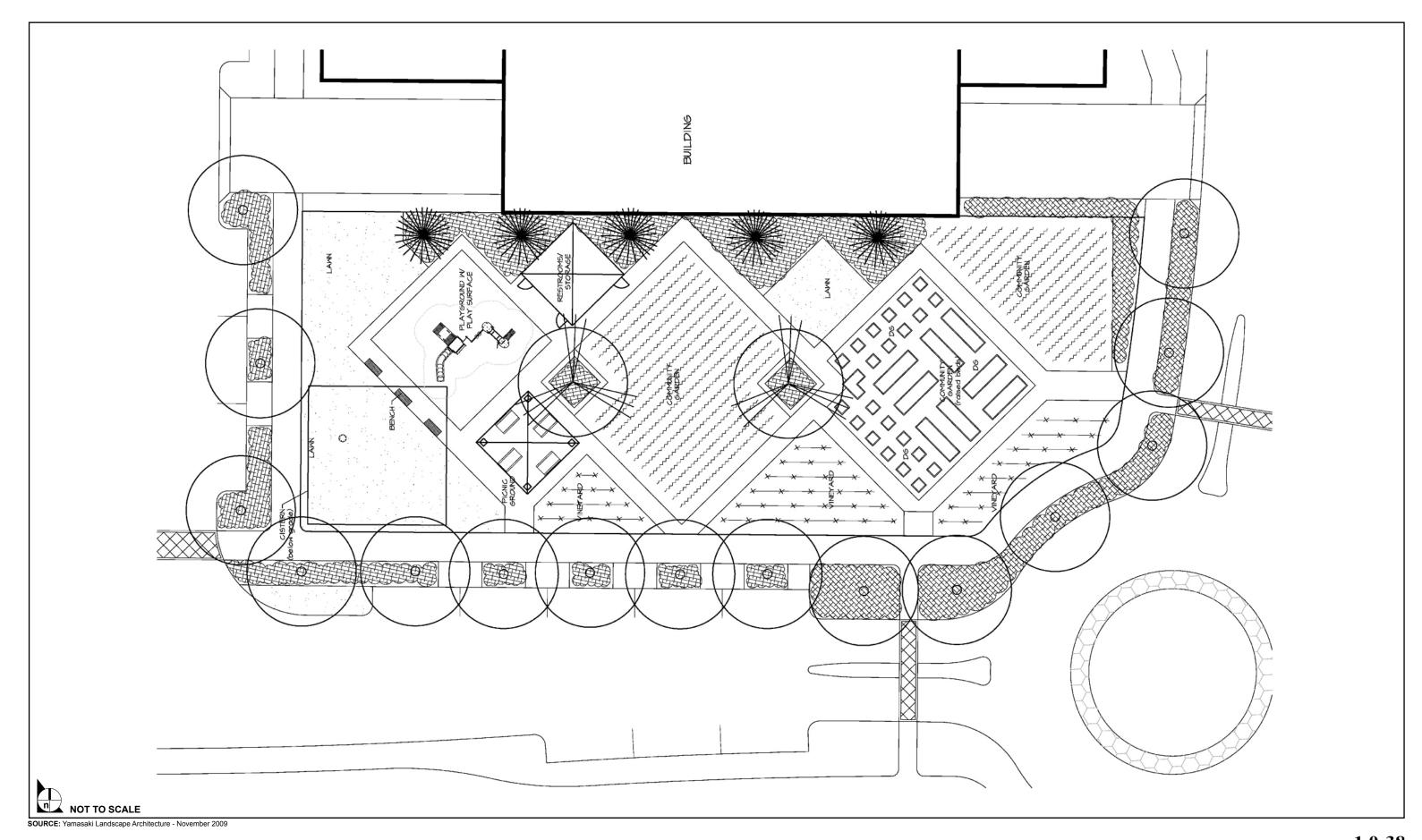


FIGURE **1.0-38**

Conceptual Park Concepts Design (Community Garden)

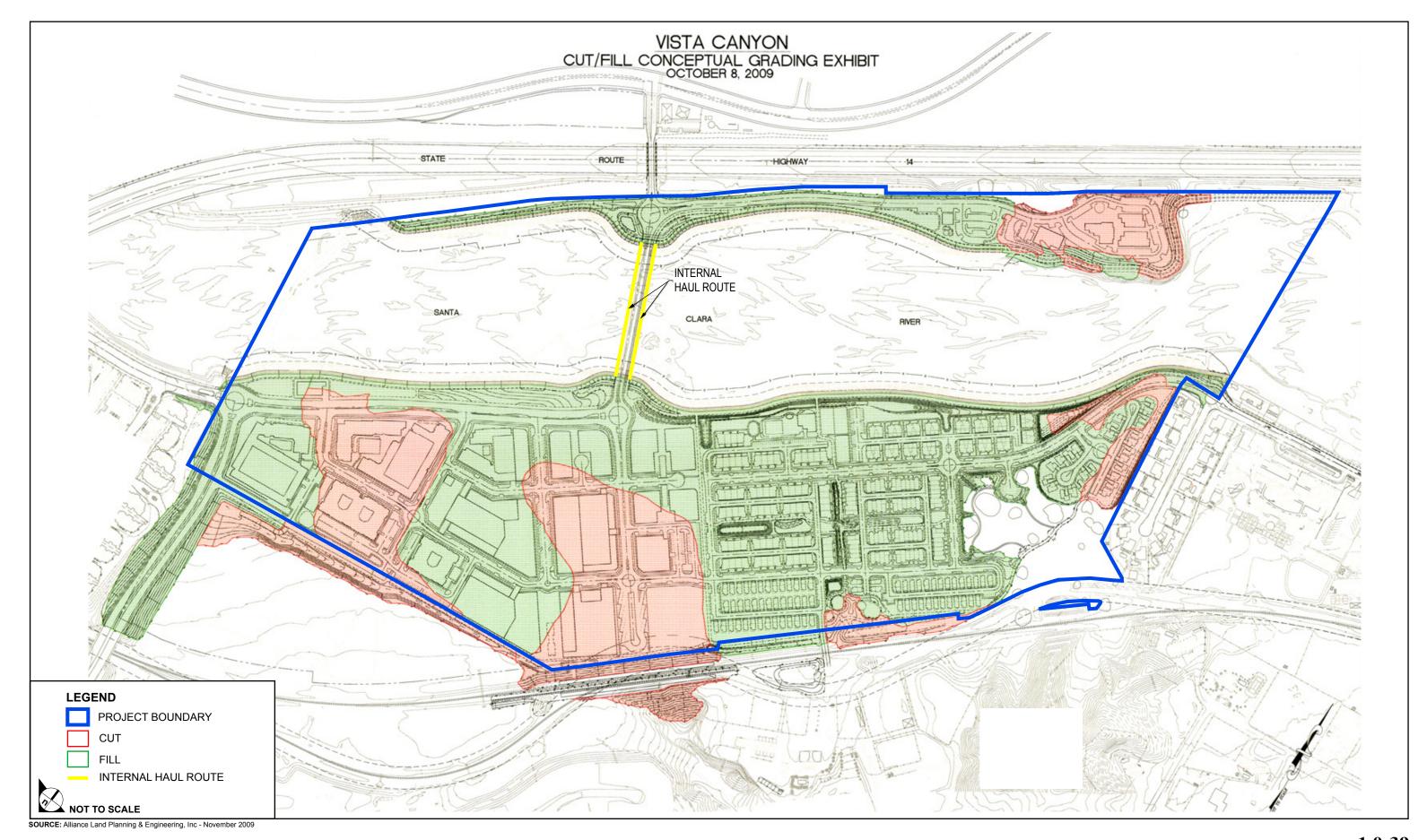


FIGURE **1.0-39**

Conceptual Grading Plan

j. Utilities

The project site is located within the Southern California Edison service area. Primary service would come from Soledad Canyon Road via the existing power lines, which presently cross the River and the site. This existing line would be extended to serve the project's initial development phases. Upon its construction, power lines would be placed within the Vista Canyon Road Bridge. Additionally, service lines exist within Lost Canyon Road to the southwest and Lost Canyon Road at La Veda Avenue to the east, both of which would likely be extended into the project site.

Phone service to the project site would be from AT&T. Primary service would come from Soledad Canyon Road via the existing line, which crosses the River and the site, which would be placed in the Vista Canyon Road Bridge upon its construction. Telephone lines would be constructed underground throughout the project within phases as development commences. Existing service lines are also located within Lost Canyon Road in the southwest corner of the site and Lost Canyon Road near La Veda Avenue. These localized lines may also be extended to the site and utilized for the early phases of the project.

The project is within the Time Warner cable service area. Existing service lines are located in Soledad Canyon Road to the north, Jakes Way to the west and Lost Canyon Road to the east. A main line feeder from Soledad Canyon Road would be placed in the Vista Canyon Road Bridge. Existing service lines are also located within Lost Canyon Road in the southwest corner of the site and Lost Canyon Road near La Veda Avenue. These localized lines may be utilized for the early phases of the project.

The project is within the Southern California Gas Company service area. Existing service lines are located in Soledad Canyon Road to the north, Lost Canyon Road to the southwest, and Lost Canyon Road near La Veda Avenue to the east. Gas service to the project would be extended to the site from one of these existing sources. A gas line would also be placed into the Vista Canyon Road Bridge.