



SECTION 3.0

Project Description



3.0 PROJECT DESCRIPTION

3.1 LEAD AGENCY

City of Santa Clarita

23920 Valencia Boulevard, Suite 300

Santa Clarita, California 91355

Contact: Ms. Jessica M. Frank, Associate Planner, Community Development Department

Telephone: (661) 255-4330

Fax: (661) 286-4007

e-mail: jmfrank@santa-clarita.com

3.2 RESPONSIBLE AGENCIES

In addition to the lead agency, there are also Federal, State, regional, and local responsible agencies that may have discretionary authority over specific aspects of the project. The responsible agencies would rely on this EIR when acting on those aspects that require their approval. The following agencies are currently anticipated to use this document in their reviews, although this list is not necessarily exhaustive:

- U.S. Army Corps of Engineers – Section 404 Permit for impacts to Waters of the U.S.
- California Department of Fish & Game – Section 1602 Streambed Alteration Agreement for impacts to Waters of the State.
- Regional Water Quality Control Board – National Pollution Discharge Elimination System (NPDES) Permit and Section 401 Water Quality Certification.
- Southern California Regional Rail Authority (SCRRA) and Los Angeles County Metropolitan Transportation Authority (Metro) – Encroachment Permit for construction of an underpass beneath the Metrolink railroad right-of-way.
- Southern California Gas Company – Encroachment Permit for construction of “D” Street over the existing gas transmission pipeline easement.

Additional coordination and consultation with other agencies that may be required for the project include, but are not limited to, the following:

- Los Angeles County Fire Department
- Los Angeles County Sanitation District
- Southern California Edison
- Santa Clarita Water Company
- Santa Monica Mountains Conservancy



3.3 PROJECT APPLICANT

Robinson Ranch Residential, LP
8447 Wilshire Boulevard, Suite 100
Beverly Hills, California 90211
Contact: Bruce Rothman or Laurent Opman
Telephone: (323) 966-4989

3.4 PROJECT LOCATION AND SETTING

3.4.1 PROJECT LOCATION

The Mancara at Robinson Ranch (Tentative Tract Map No. 063022) project site is located in the County of Los Angeles, within the eastern portion of the City of Santa Clarita (APN 2840-001-118; and 2840-015-031 through 035, 045, and 047). Exhibit 3-1, Regional Location, illustrates the project site in a regional context. The project site is located to the east of the intersection of Oak Spring Canyon Road and Lost Canyon Road, and is generally bounded by the Santa Clara River to the north, Oak Spring Canyon Wash to the west, the Robinson Ranch Golf Club to the south, and unincorporated Los Angeles County and the Angeles National Forest to the east; refer to Exhibit 3-2, Project Site Vicinity.

Regional access to the project area is provided via State Route 14 (SR-14), which is located approximately 0.2-mile north of the project site. Local access routes near the project site include Soledad Canyon Road (situated approximately 0.25-mile north of the site), Sand Canyon Road (located approximately 0.5-mile west of the site), Lost Canyon Road (which terminates at the western boundary of the site), and Oak Spring Canyon Road (which exists along the western and southern boundaries of the site).

3.4.2 PROJECT SETTING

The majority of the 187.3-acre project site is undeveloped. An existing SCRRA and Metro (Metrolink) railroad right-of-way transects the northern portion of the site in an east/west orientation. In addition, a 100-foot wide Southern California Gas Company underground gas transmission pipeline easement and an abandoned Southern Pacific Railroad Company (SPRC) railroad alignment are located in the central portion of the site, also oriented in an east/west direction.

Due to the site's primarily undeveloped nature, the majority of the site is occupied by vegetation in the form of grasses, native shrubs, and oak woodland.¹ The site provides suitable habitat for oak trees (coast live oaks and Tucker's oaks), and a total of 347 jurisdictional oak trees exist on-site. Of these oak trees, 96 would require an Oak Tree Permit under the *City of Santa Clarita Oak Tree Preservation Ordinance* (8 for removal, none of Heritage status, and 88 for encroachment).² The site rests at an approximate elevation of 1,600 feet above mean sea level.

¹ *Biological Constraints and Focused Survey Results, Oak Springs (TTM 063022) Project Site* (February 2007 and revised September 2008) and *Biological Resources Update, Mancara-Robinson Ranch (TTM 063022)* (June 2011), Impact Sciences, Inc.

² *Oak Tree Survey Report and Tree Appraisal: Oak Springs Project, TTM 063022*, Impact Sciences, September 2006 (revised September 2010, April 2011, and July 2011).



With the exception of a few knolls located to the north of the abandoned SPRC railroad alignment on the project property, the site is located on a relatively flat alluvial plain with gentle to level topography (zero to five percent slopes) found over nearly the entire property. Oak Spring Canyon Wash traverses the southwestern portion of the project site.

Uses adjacent to and surrounding the project site include:

- North: The Santa Clara River, SR-14, and residential uses
- South: Robinson Ranch Golf Course and residential uses
- East: Open space, residential, and equestrian uses
- West: Residential and equestrian uses

The proposed project site has two designations under the *City of Santa Clarita General Plan* as follows: 1) Non-Urban 5 (NU5) and Urban Residential 1 (UR1) and two designations under the *Unified Development Code (UDC)*, as follows: 1) Residential Low (RL) and 2) Residential Very Low (RVL). The Metrolink railroad right-of-way, which would not be affected by the project, is designated Open Space (OS); refer to *Exhibit 3-3, City of Santa Clarita General Plan/Unified Development Code Designations*.

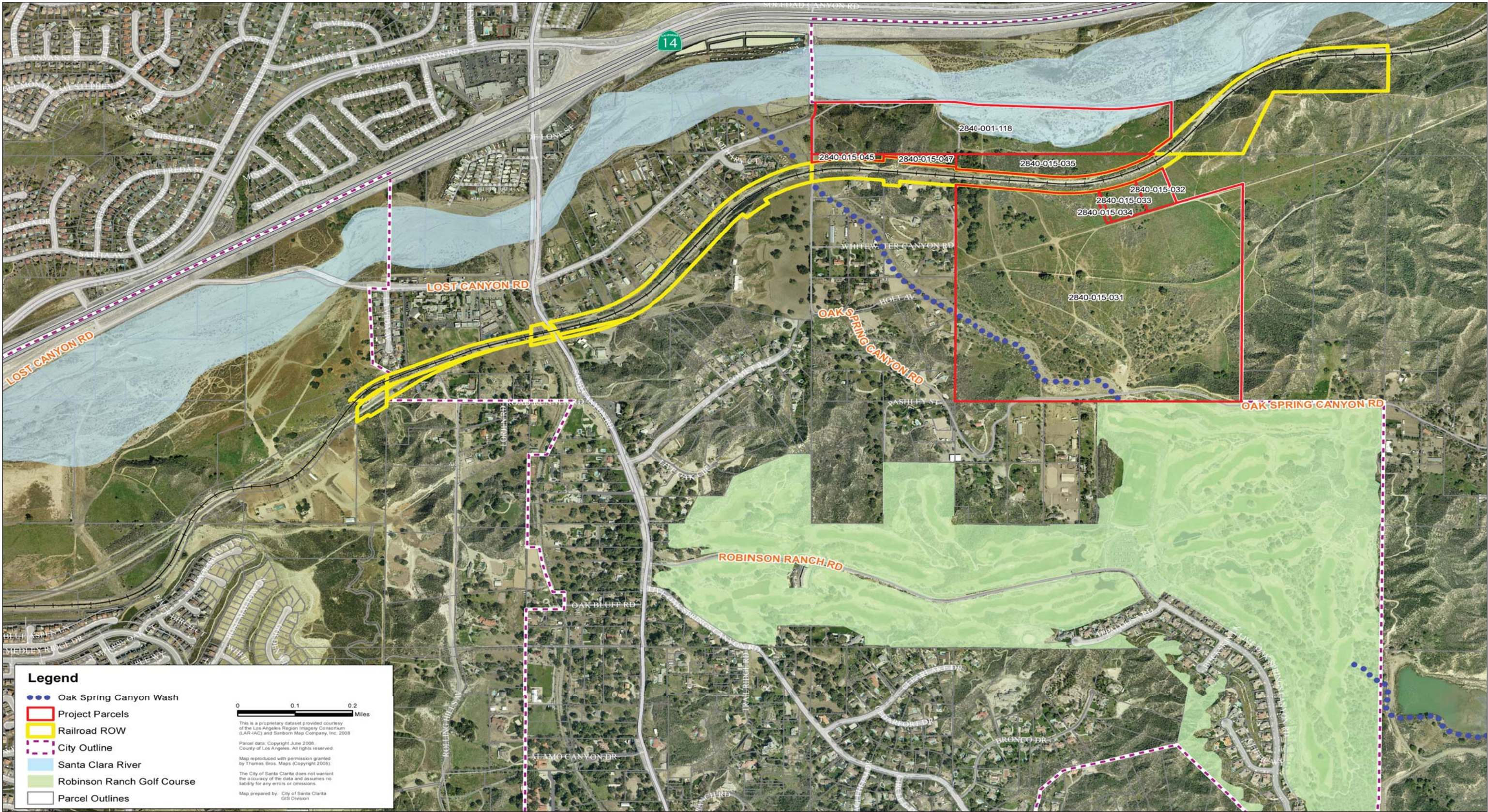
3.4.3 LOCAL HISTORICAL CONTEXT

SAND CANYON COMMUNITY

The subject property is located within the community of Sand Canyon. Sand Canyon is generally located south of SR-14 along Sand Canyon Road, north of Placerita Canyon Road to the west of the Angeles National Forest and the east of Triumph Avenue. Sand Canyon is located within a Special Standards District that is governed by *Unified Development Code (UDC)* Section 17.16.090. The Sand Canyon Special Standards District was established when the City incorporated and completed the first *UDC* in 1992. The Special Standards District was established to maintain, preserve, and enhance the rural and equestrian character of Sand Canyon, and to establish standards to guide future development in the community. The Sand Canyon Special Standards District includes development standards for animal keeping, trails, street standards, lighting standards, gating and fencing.

PREVIOUS APPROVALS FOR THE SUBJECT PROPERTY

In 1990, American Beauty Homes received approval for the annexation and development of a 299 single-family residential project on the subject property from the City of Santa Clarita City Council. The project approval included the channelization of Oak Spring Canyon Wash, a vehicular bridge crossing the Santa Clara River, and the removal of 153 oak trees. The property was annexed into the City; however, the development agreement to construct the units was never initiated, and as a result expired in 2002.



Source: City of Santa Clarita.

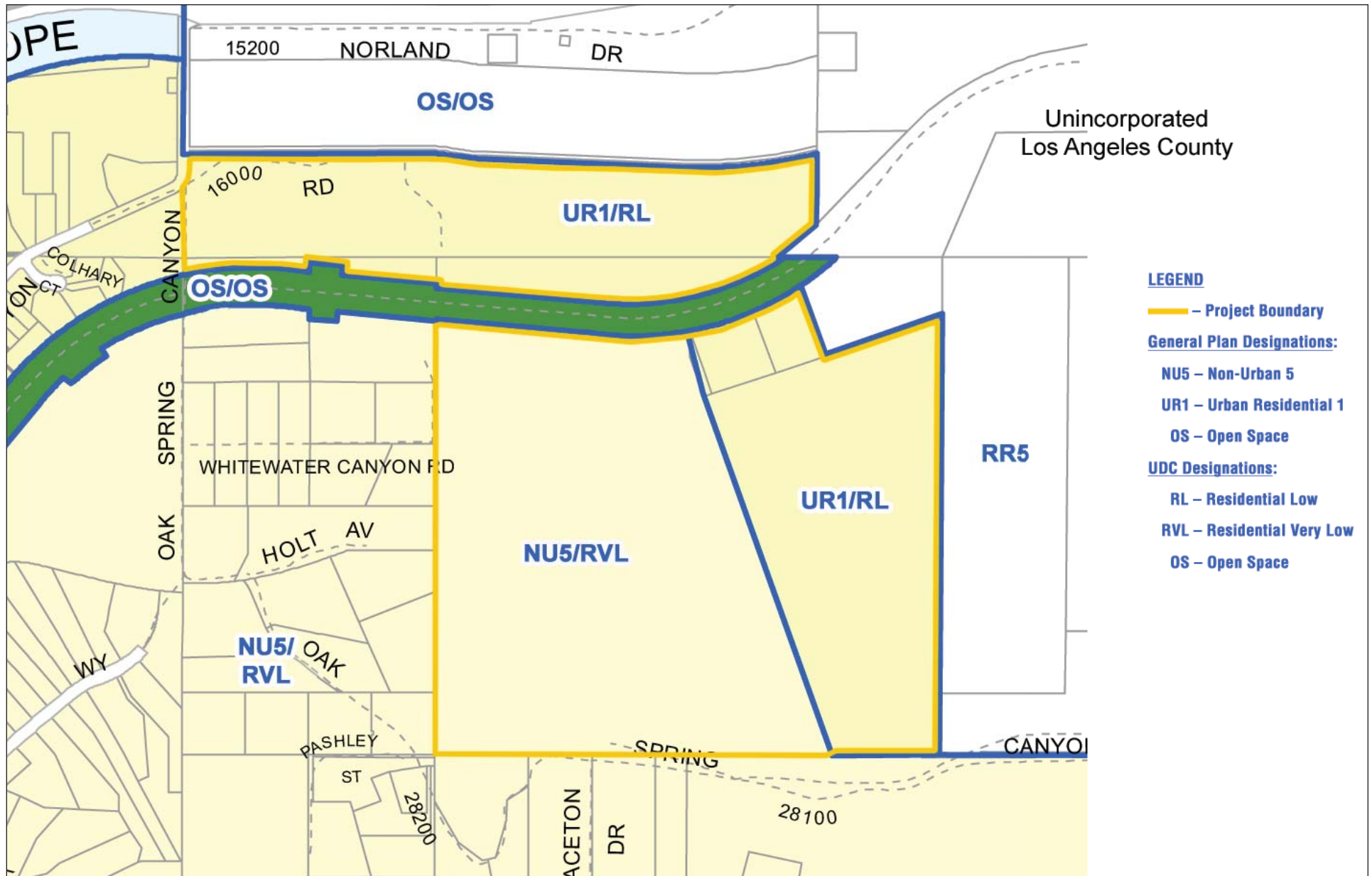
NOT TO SCALE



12/11 • JN 10-104854

MANCARA RESIDENTIAL PROJECT
ENVIRONMENTAL IMPACT REPORT
Project Site Vicinity

Exhibit 3-2



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City of Santa Clarita General Plan/Unified Development Code Designations

MANCARA RESIDENTIAL PROJECT
ENVIRONMENTAL IMPACT REPORT

Exhibit 3-3



3.5 PROJECT OBJECTIVES

The *California Environmental Quality Act (CEQA)* requires that an Environmental Impact Report (EIR) include a statement of the objectives sought by the applicant of the proposed project (*CEQA Guidelines* Section 15124(b)). Mancara at Robinson Ranch is a private development plan proposed by Robinson Ranch Residential, LP. The overall objective of the proposed project is to develop a gated equestrian community with 99 single-family lots and open space areas on 187 acres of primarily undeveloped land. The applicant's objectives for the project include the following items:

LAND USE PLANNING

1. Create a new community that allows for residential development, while preserving significant natural resources and open areas.
2. Provide development that is compatible with surrounding land uses and is consistent with residential communities within the Sand Canyon area.
3. Provide for adequate flood protection for the purposes of public safety and preservation of public and private property.
4. Provide for the long-term maintenance of landscaping, storm drains, etc., that serve the project site.
5. Ensure compatibility with the City's Urban Stormwater Mitigation Plan.
6. Ensure compatibility with the Sand Canyon Special Standards District.

ECONOMIC

1. Develop the site to include lots of varying sizes.
2. Create an economically feasible project that offers single-family residential lots to serve the current and projected market.

MOBILITY

1. Provide a safe, efficient, and aesthetically attractive street system, which is consistent with all requirements of the Sand Canyon Special Standards District.
2. Provide two points of ingress and egress that minimize impacts on adjacent residential neighborhoods.
3. Provide equestrian trails throughout the project which connect with the City's equestrian backbone trail system.

PARKS AND RECREATION

1. Provide space for an equestrian-oriented City park.
2. Provide space for an equestrian trail head that connects to the City's equestrian backbone trail system.
3. Provide space for a City community park.



RESOURCE CONSERVATION

1. Maintain approximately 44 acres of open space.
2. Provide a site-specific evaluation of the biotic resources of the site in compliance with the provisions of the City's *Unified Development Code* and *General Plan* with regard to significant ecological areas.

3.6 PROJECT CHARACTERISTICS

3.6.1 OVERVIEW OF PROPOSED LAND USES

The proposed project is a rural residential equestrian-based community that involves the development of 99 single-family graded residential lots and open space areas within 105 lots on approximately 187.3 gross acres of land (refer to Table 3-1, Proposed Project Land Uses). The 105 proposed lots would be utilized for the following purposes:

- **Lots 1 through 99:** Lots 1 through 99 would consist of single-family residential lots. A portion of Lots 9 and 10 (along the eastern boundary of the site) would contain a temporary drainage desilting basin that would be utilized until a proposed "future" street is extended easterly to accommodate a proposed development on the adjacent property to the east. Each of the 99 proposed residential lots would be developed with custom homes, consistent with the character of the Sand Canyon community and in compliance with the requirements of the *UDC*, including the Sand Canyon Special Standards District.
- **Lots 100 through 104:** Lots 100 through 104 would be open space and equestrian lots within the northernmost and southernmost portions of the project site. Specifically, Lots 101 and 102 (just north of the Metrolink railroad right-of-way) are proposed for a City park and equestrian uses/equestrian trailhead. Further north (along the northern boundary of the site), Lots 100 and 103 would compose an open space area within the Santa Clara River floodplain. Lot 104, along the southern boundary of the project site, would be utilized for open space (golf course) purposes.
- **Lot 105:** Lot 105 would be utilized for a drainage/desilting water quality basin within the southeastern portion of the project site.
- **Not a Part:** The southwestern corner of the project site (approximately 14.7 acres) is under the ownership of the Project Applicant, but would not be developed as part of the proposed project. This area, labeled as Not a Part, is generally located north of Oak Spring Canyon Road, and southwest of the Oak Spring Canyon Wash (refer to Exhibit 3-4, Tentative Tract Map, for an illustration of the proposed project).

As such, approximately 172.6 acres of the 187.3-acre project site would be affected by development.



**Table 3-1
Proposed Project Uses**

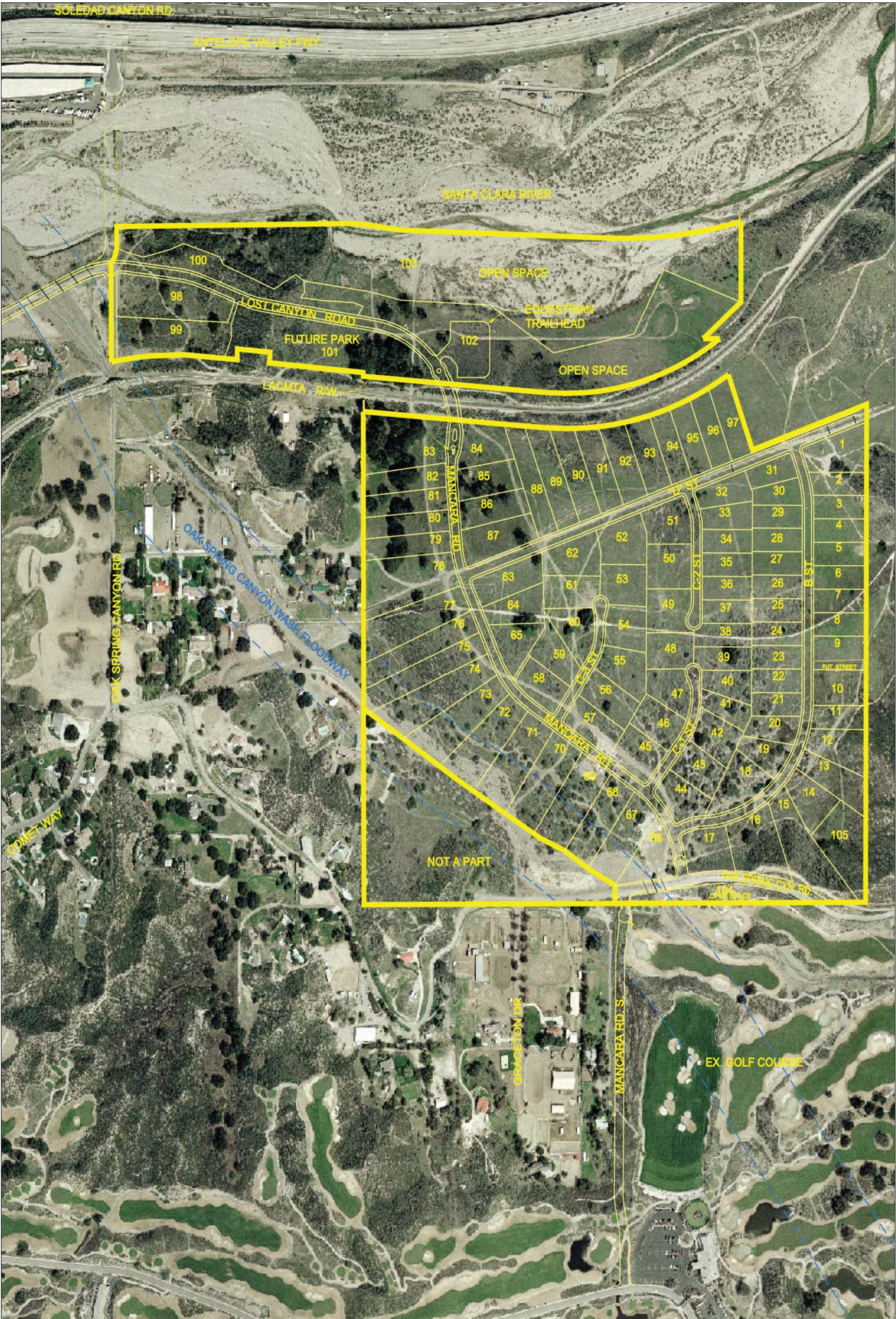
Use	Dwelling Units	Lot Numbers	Gross Acres ¹
Single-Family Residential	99	1 to 99	130.0
City Park		101	5.0
Equestrian Area & Trailhead		102	4.90
Open Space			7.3
Open Space		100	2.2
Open Space		103	22.8
Open Space (Golf Course)		104	1.4
Drainage/Desilting Water Quality Basin		105	0.4
Lot Area			172.6
Not a Part		Not applicable	14.7
Total	99		187.3

¹ Acreages have been rounded to up or down to closest 0.1.
Notes: Street acreage totals approximately 17 acres and is included within the 172.6-acre total.

The site would accommodate approximately 43.6 acres of open space and 17 acres of streets, yielding a net site area of 112 acres. The residential lots would range in size from approximately 0.7-acre to over two acres, with an average lot size of 1.1 acres (excluding open space lots). The proposed project is consistent with the *City of Santa Clarita General Plan* designations of Non-Urban 5 (NU5) and Urban Residential 1 (UR1) designations, and *Unified Development Code (UDC)* designations of Residential Very Low (RVL) and Residential Low (RL). Under the *UDC*, residential lots located within the RL zone on the eastern portion of the site would have a minimum width of 75 feet and minimum lot area of 20,000 square feet, while lots located within the RVL zone on the western portion of the site would have a minimum lot width of 100 feet and minimum lot area of 40,000 square feet. In both the RL and RVL zones, the minimum front yard setback is 20 feet, minimum rear yard setback is 15 feet, minimum side yard setback is five feet, and maximum building height is 35 feet without a Conditional Use Permit (CUP).

As stated above, the approximately 187.3-acre project site includes 172.6 acres that would be affected by development. Of this 172.6 acres, 123.6 acres are designated by the *UDC* as RVL and 49 acres are designated RL. The RVL designation allows for a maximum density of 1.0 dwelling unit per gross acre, while the RL designation allows for a maximum of 2.2 dwelling units per gross acre. Thus, applying applicable densities to each designation, a total of 232 single-family dwelling units would be allowed under the *UDC*.³ Thus, at 99 residential units, the proposed project represents a reduction from the maximum development potential of the site under the *UDC*.

³ 123.6 acres of RVL x 1.0 dwelling unit/acre = 124 dwelling units; 49 acres of RL x 2.2 dwelling units/acre = 108 dwelling units.



Source: Sikand, August 2010.

NOT TO SCALE



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3.6.2 ACCESS AND CIRCULATION

ACCESS

Access to the project site is proposed to occur via three vehicular gate-controlled points. The first would occur along a proposed easterly extension of Lost Canyon Road (at the northwestern corner of the site), immediately south of the existing Los Angeles County Metropolitan Transportation Authority (Metro) Metrolink railroad right-of-way. The second vehicular gate would occur at the southern boundary of the site, further south along the Lost Canyon Road extension just north of Oak Spring Canyon Road, and the third would occur to the south of Oak Spring Canyon Road on the adjacent property on the proposed Mancara Road; refer to Exhibit 3-4. All 99 residential units would feature vehicular gated access with the exception of Lots 98 and 99, portions of which would be located just north of the proposed gate along the Lost Canyon Road Extension. Pedestrian and equestrian access through the project site would remain open to the public.

CIRCULATION

On-Site Improvements

- **Lost Canyon Road Extension:** Lost Canyon Road currently terminates at the northwestern corner of the project site at its intersection with Oak Spring Canyon Road. The project proposes to extend Lost Canyon Road to the southeast, creating a public roadway that traverses the entire site in a northwest to southeast orientation. The Lost Canyon Road Extension would proceed from the existing easterly terminus of Lost Canyon Road in an easterly/southeasterly direction. The extension would consist of a two-lane undivided roadway with a 60-foot right-of-way (ROW). North of the proposed extension, a publicly-accessible recreational trail would be implemented, adjacent to proposed open space/equestrian uses to the north.

The Lost Canyon Road Extension would require a new undercrossing beneath the existing Metrolink railroad right-of-way. The undercrossing would consist of a two-lane roadway with a 72-foot ROW, with a minimum 15-foot vertical clearance. The Lost Canyon Road Extension would terminate at a vehicular access gate proposed immediately south of the rail undercrossing. South of the access gate, the roadway would continue into the residential development as Mancara Road.

- **Mancara Road:** As stated above, the extension of Lost Canyon Road would be named Mancara Road south of the proposed vehicular access gate. Mancara Road would consist of a two-lane undivided roadway with a 60-foot ROW. This roadway would provide direct residential access to numerous parcels to the west/southwest (in addition to "D" Street, "B" Street, and local cul-de-sacs described below). Mancara Road is proposed to terminate to the south at Oak Spring Canyon Road, where another vehicular access gate would be constructed.
- **"D" Street:** This proposed new roadway would occur within the existing 100-foot Southern California Gas Company gas transmission pipeline easement, and would traverse the site generally in an east to west orientation. "D" Street is proposed to be extended to the east in the future upon implementation of a proposed adjacent



residential development (Tentative Tract No. 065159). “D” Street is proposed to consist of a two-lane undivided roadway with a 64-foot ROW. This roadway would provide a connection between Mancara Road and “B” Street, in addition to a local cul-de-sac (described below).

- **“B” Street:** This proposed new roadway would occur along the eastern boundary of the project site and would connect “D” Street to Mancara Road. “B” Street would consist of a two-lane undivided roadway with a 60-foot ROW. This roadway would provide direct residential access to parcels along both sides of the street.
- **Local Cul-De-Sacs:** The proposed project would include three new on-site cul-de-sacs providing direct access to residential units. Two cul-de-sacs would extend to the north from Mancara Road, while one would extend to the south from “D” Street. Each of these roadways would consist of two lanes, with a 58-foot ROW.
- **Oak Spring Canyon Road Improvements:** Most of Oak Spring Canyon Road from Lost Canyon Road at the north of the project site extending south and connecting to the future Mancara Road would remain unimproved, with the exception of approximately 360 linear feet of Oak Spring Canyon Road (along the southern boundary of the site). This segment would be improved to Sand Canyon Special Standards District requirements under the UDC to provide a secondary all weather vehicular connection to a new access road (Mancara Road South, described below). The Oak Spring Canyon Road improvements would consist of two-lane undivided roadway with a 60-foot ROW. A 12-foot equestrian trail would be provided along northern side of the roadway. A portion of this 360-foot segment would include a bridge crossing over Oak Spring Canyon Wash. The bridge would be designed to allow for a 10-foot clearance.

Off-Site Improvements

- **Lost Canyon Road Improvements and Bridge Crossing:** The existing portion of Lost Canyon Road from Sand Canyon Road to its easterly terminus at Oak Spring Canyon Road would be improved with pavement reconstruction. From Sand Canyon Road east, Lost Canyon Road would consist of a two-lane undivided roadway with a 60-foot ROW, which would include 20- to 26-feet of paved road with parking on no, one, or both sides of the road. The width of the on-street parking area would vary from 0 to 8 feet. A 4-foot pedestrian path is proposed on the north side of the ROW beginning at Cohary Court, and a City multi-use recreational (pedestrian, bicycle, equestrian) trail is proposed on the northerly or southerly sides of the ROW (refer to Exhibit 3-6a through Exhibit 3-6d, Lost Canyon Road Improvements).

Exhibits 3-6a through 3-6d include typical roadway sections for the specific portion of Lost Canyon Road depicted on the exhibit. Each section identifies the street right-of-way, width of paved road, width of parking, width of pedestrian path, and width of trail. The width of the pedestrian path is 4-feet, and the width of the trail varies from 5- to 12-feet.

Beginning at Sand Canyon Road and going east, the 5- to 12-foot trail would be located on the southerly side of Lost Canyon Road (refer to *Exhibits 3-6a, 3-6b*, and top graphic on *3-6c*), but would be located on the northerly side of the ROW after the new bridge on Lost Canyon Road that would cross Oak Spring Canyon Wash (refer lower graphic on *Exhibit 3-6c*). At this point, there is 12-foot trail and no 4-foot pedestrian path.



Lost Canyon Road Bridge Segment

This segment of Lost Canyon Road would require a 60-foot ROW with a 38-foot wide bridge crossing that includes two vehicular lanes, a 4-foot pedestrian path on the north side of the bridge and an 8-foot trail on the south side of the bridge over Oak Spring Canyon Wash, approximately 0.1-mile west of the site. The bridge would have a clearance of 14 feet. The multi-use recreation trail then extends east onto the project site. See the Trails discussion in Section 3.6.3 for a description of the City trail through the project site.

- **Mancara Road South:** Mancara Road would be extended south of the project site from the improved Oak Spring Canyon Road (at the southern boundary of the site) to Robinson Ranch Road, located approximately 0.3-mile south of the site within the Robinson Ranch Golf Course. Mancara Road South would extend south to connect with Robinson Ranch Road and ultimately Sand Canyon Road to provide the project site secondary all-weather access. Vehicular gate-controlled access along Mancara Road South would be provided just south of the southerly site boundary. Mancara Road South would consist of a two-lane undivided roadway with a 46-foot ROW.
- **Sand Canyon Road/Lost Canyon Road Intersection Improvements:** In the event intersection improvements have not been carried forward by others at the time of implementation of the Mancara at Robinson Ranch Project, the proposed project would include the implementation of a roundabout at the intersection of Sand Canyon Road and Lost Canyon Road. The roundabout would involve the relocation of the intersection to the north and west to adhere to northbound "line of sight" requirements and would necessitate ROW acquisition on the northwest, southwest, and southeast corners; refer to Section 5.3, Traffic and Circulation, for additional information regarding the proposed roundabout.

Implementation of the Sand Canyon Road/Lost Canyon Road roundabout was also included as mitigation within the Environmental Impact Report for the Vista Canyon Project, a proposed 185-acre Specific Plan including 1,100 dwelling units, up to 950,000 square feet of non-residential development, and 18 acres of park/recreational facilities.⁴ The responsibility for implementation of the roundabout would apply to whichever project (i.e., Mancara at Robinson Ranch or Vista Canyon) triggers the need for the improvement first.

The City of Santa Clarita and County of Los Angeles have established a fee program to fund construction of new significant transportation infrastructure improvements. This program consists of six Bridge & Major Thoroughfare (B&T) Fee Districts, which provide an equitable financing mechanism by which new development within an identified area will share the cost of providing full mitigation improvements by payment of appropriate fees or construction of eligible improvements. The proposed project is located within the Eastside B&T District. As such, the party responsible for construction of the proposed Sand Canyon Road/Lost Canyon Road roundabout (i.e., the project applicant for Mancara at Robinson Ranch or Vista Canyon) would become eligible for a B&T District credit, which can be used to offset District fee payments.

⁴ Vista Canyon Draft Environmental Impact Report, Impact Sciences, Inc., October 2010.



Future Improvements

- **“D” Street Extension and Future Street:** The eastern terminus of “D” Street would be extended into a proposed residential development east of the project site (Tentative Tract No. 065159). Based on Los Angeles County Fire Department requirements, a fence is required at the eastern side of the intersection of “D” Street and “B” Street until the easterly extension of “D” Street occurs. The fence would have a minimum 26-foot wide gate with an approved Fire Department locking device. All gate accessory hardware would be clear of the accessway when the gate is in the fully opened position.
- **Drainage Desilting Basin Conversion:** The drainage desilting basin between Lots 9 and 10 would be converted into a roadway leading to Tentative Tract No. 065159 to the east of the project site. The desilting basin would be relocated to Tentative Tract No. 065159.

Typical roadway sections are depicted in Exhibit 3-5, Typical Roadway Sections, while the proposed street and trail improvements for Lost Canyon Road are shown on Exhibit 3-6a through Exhibit 3-6d, Lost Canyon Road Improvements. In addition, refer to Section 5.3, Traffic and Circulation, for additional information regarding existing/forecast traffic conditions at the Sand Canyon Road/Lost Canyon Road intersection.

3.6.3 PARKS, RECREATION, AND OPEN SPACE

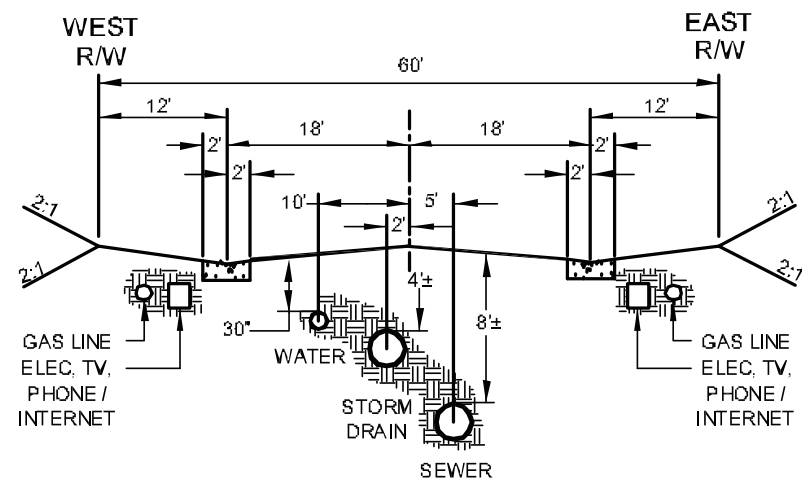
The proposed project would include a range of parks, recreation, and open space areas on-site. The majority of these uses would occur within the northern portion of the site (north of the Metrolink railroad right-of-way), with a small portion of open space proposed along the southern boundary of the site.

Within the northern portion of the project site (north of the Metrolink railroad right-of-way), a total of approximately 43.6 acres would be dedicated to City park, equestrian, and open space uses. Specifically, Lot 101 would be utilized as a five-acre City neighborhood park facility equipped with a restroom/maintenance structure, playgrounds, picnic areas with shade structures, and playfield/turf areas. Lot 102 would be utilized for a five-acre equestrian area, and would include an equestrian trailhead. Lots 100 and 103, located within the Santa Clara River floodplain, would be utilized for open space purposes.

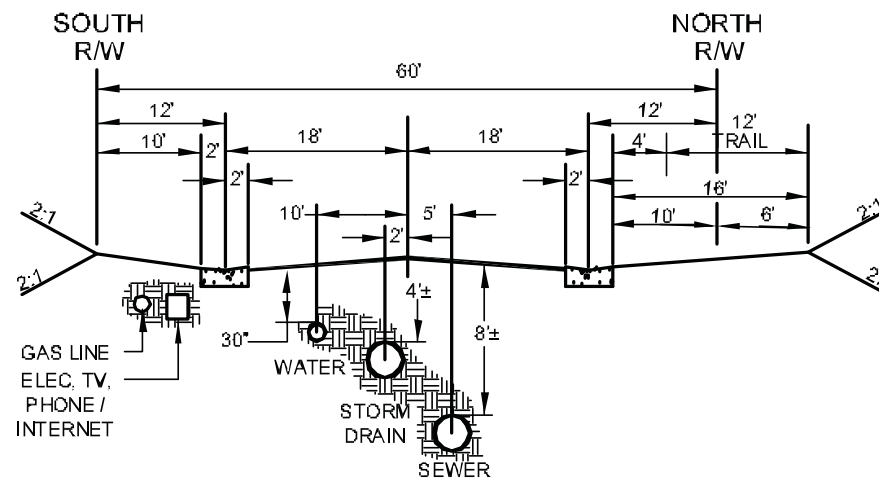
Lot 104, located along the southern boundary of the project site, is located immediately north of Robinson Ranch Golf Club. Lot 104 would be deeded to the Robinson Ranch Golf Club for continued use for recreational/golf activities, since the existing golf course currently encroaches onto the project site.

TRAILS

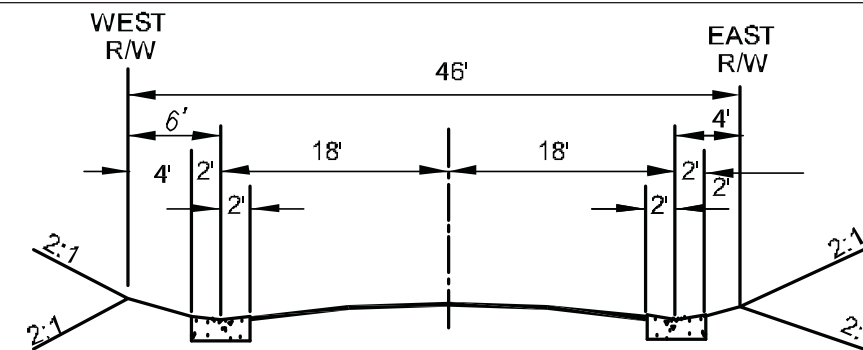
The proposed project includes a network of on-site multi-use trails, which would connect to all residential lots on the site and the extension of an off-site trail on Lost Canyon Road from Sand Canyon Road to the project site. The majority of on-site trails would be owned and operated by the Homeowners’ Association and would be accessible to the general public with the exception of a 30-foot wide City dedicated and maintained trail easement. Refer to Exhibit 3-7, On-Site Recreation Trails.



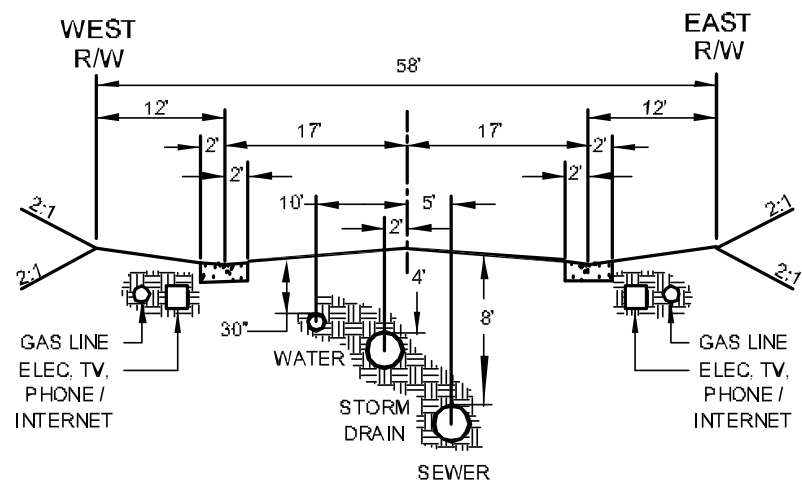
B STREET & MANCARA RD. (ONSITE)
RURAL SECTION
(NO SIDEWALK OR STREET LIGHT)
TYPICAL STREET SECTION
NOT TO SCALE



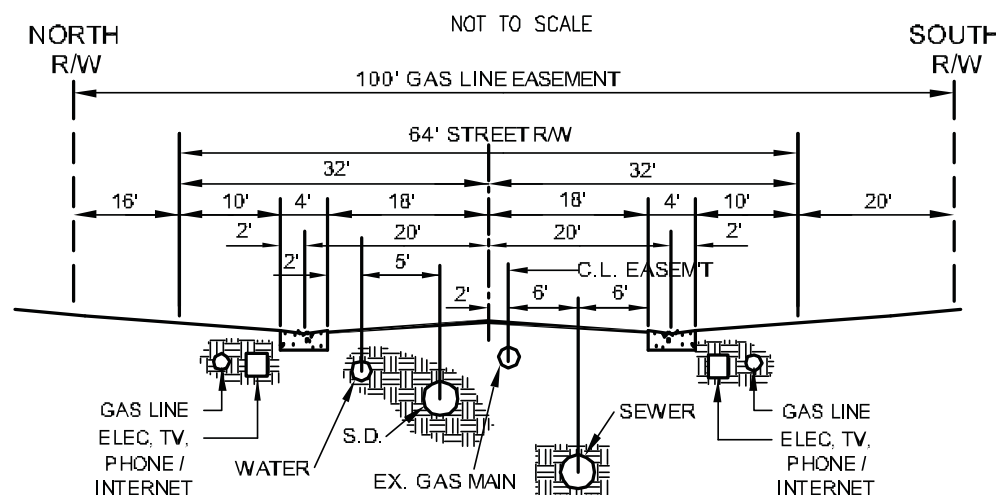
LOST CYN ROAD ADJACENT TO LOTS 98-101
TYPICAL STREET SECTION
NOT TO SCALE



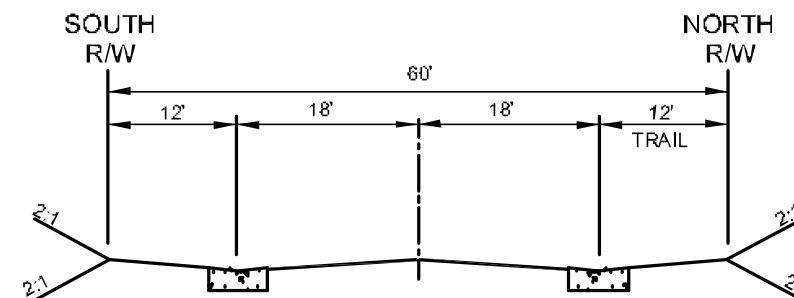
MANCARA ROAD S. (OFFSITE)
(SOUTH OF OAK SPRINGS CYN. RD.)
RURAL SECTION
(NO SIDEWALK OR STREET LIGHT)
TYPICAL STREET SECTION
NOT TO SCALE



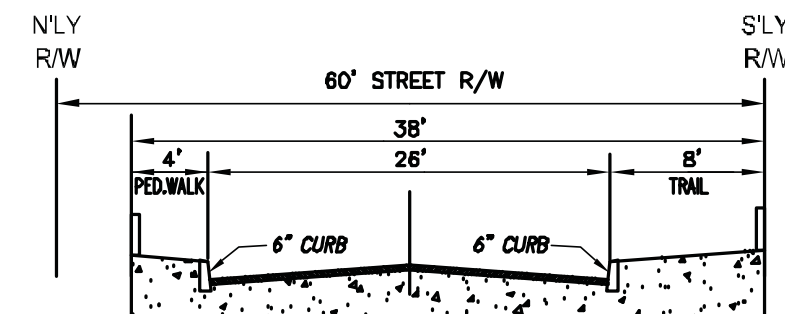
C-1, C-2 AND C-3 STREET
RURAL SECTION
(NO SIDEWALK OR STREET LIGHT)
TYPICAL STREET SECTION
NOT TO SCALE



D STREET
TYPICAL STREET SECTION
NOT TO SCALE



OAK SPRING CANYON ROAD
(BETWEEN MANCARA RD. S & MANCARA RD. ONSITE)
LOST CANYON ROAD
(BETWEEN SAND CANYON RD. AND LOST CANYON BRIDGE)
RURAL SECTION
(NO SIDEWALK OR STREET LIGHT OUTSIDE OF BRIDGE LIMIT)
TYPICAL STREET SECTION



LOST CANYON ROAD
STA. 26+80± TO STA. 30+20±
BRIDGE OVER OAK SPRINGS BRIDGE (340')
TYPICAL STREET SECTION
NOT TO SCALE

Note: Partial listing of roadway sections shown on
Sheet 1 of 5 Tentative Tract No. 060322. Refer to
Sheets 1 and 5 for additional roadway sections.

Source: Sikand, November 28, 2011.

NOT TO SCALE

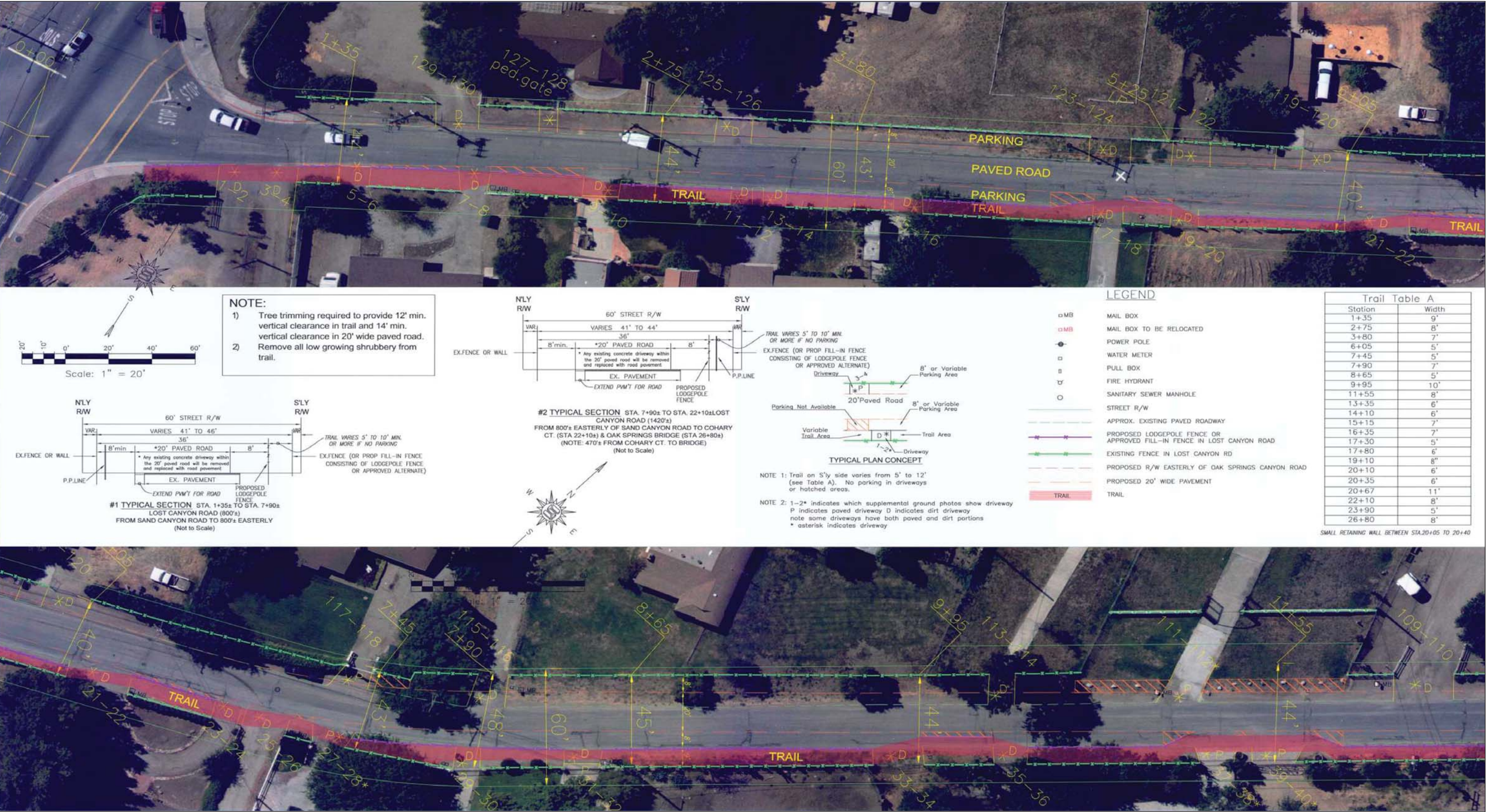
RBF
CONSULTING

12/11 • JN 10-104854

MANCARA RESIDENTIAL PROJECT
ENVIRONMENTAL IMPACT REPORT

Typical Roadway Sections

Exhibit 3-5

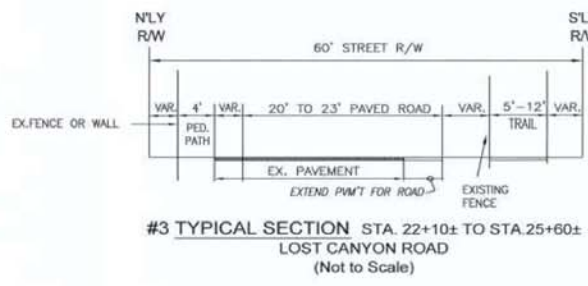
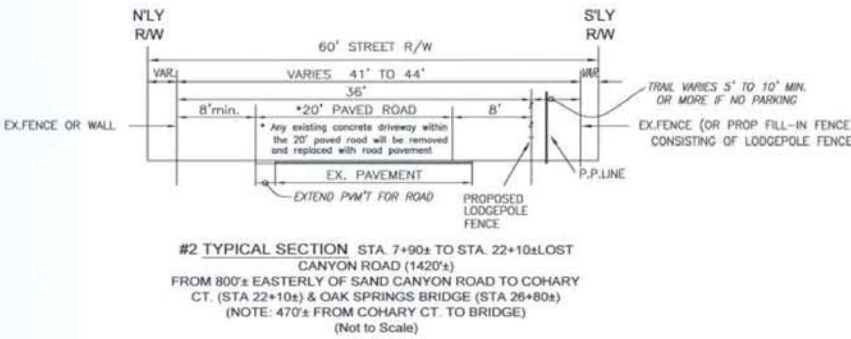
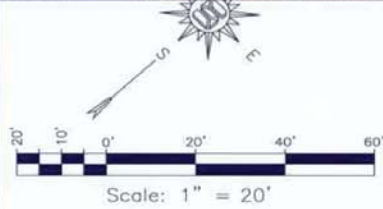
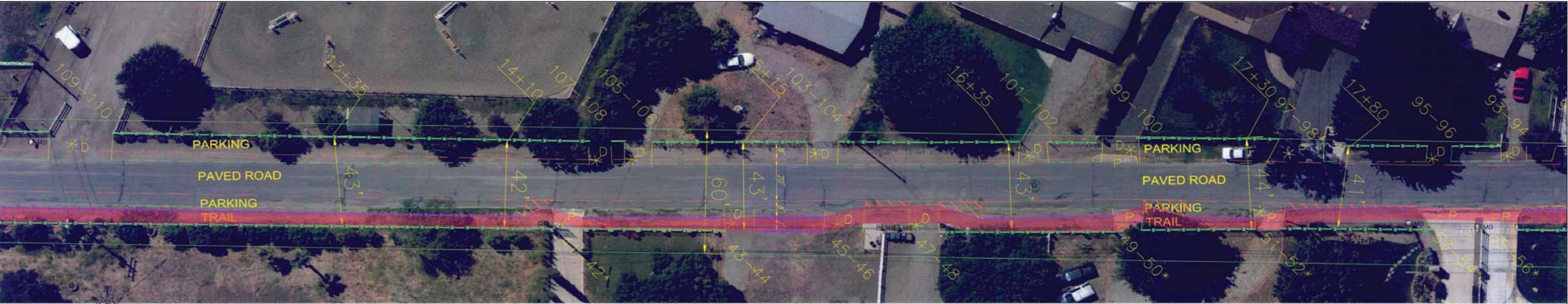


Source: Sikand, September 22, 2011.

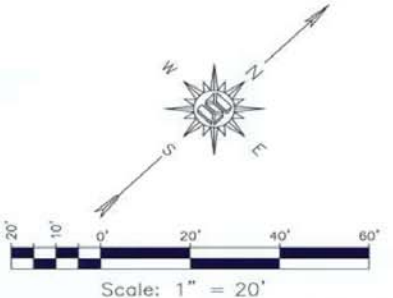
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NOTE:
SEE SHEET 1 FOR LEGEND AND
TYPICAL PLAN CONCEPT

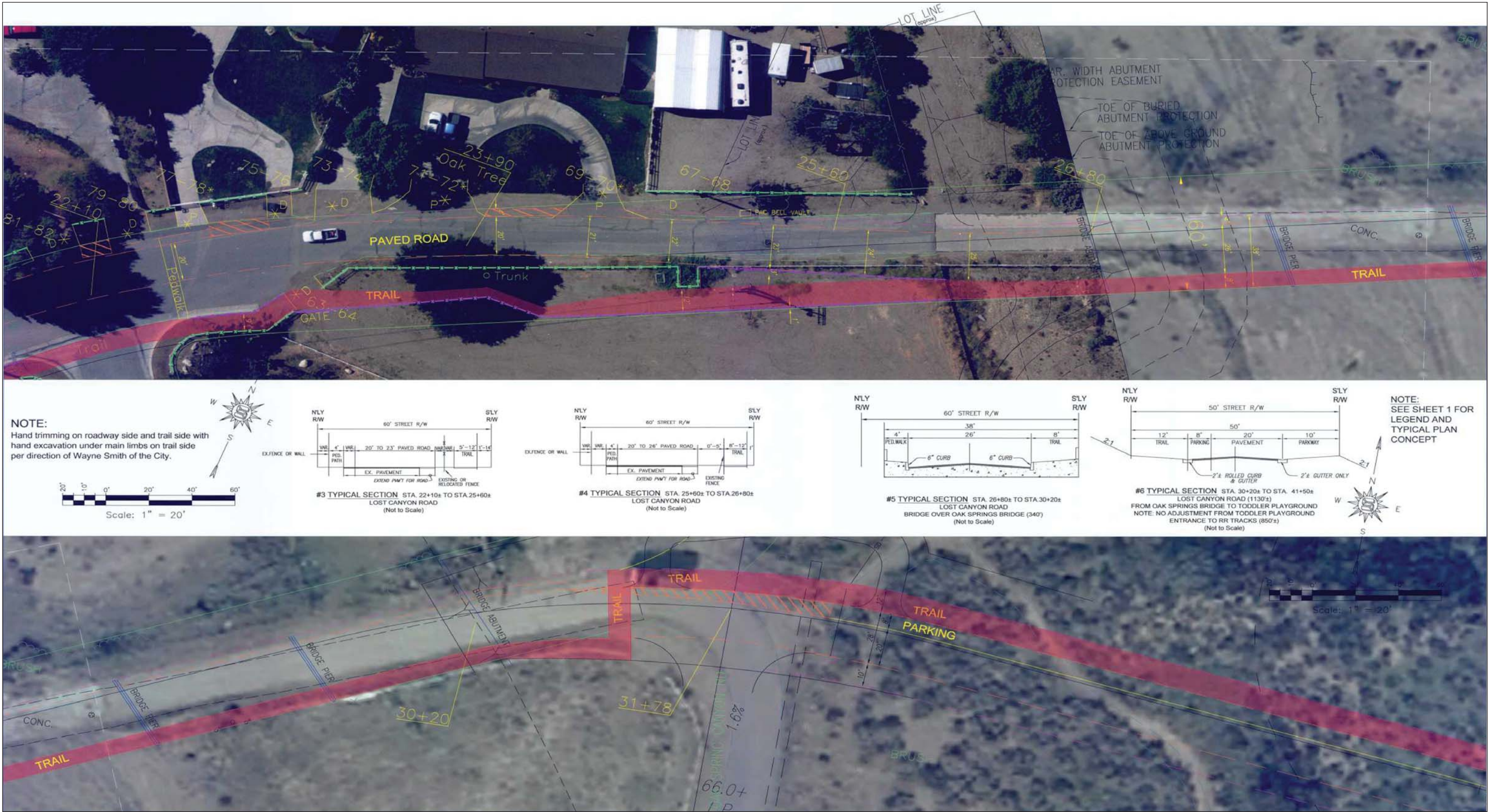


Trail Table A	
Station	Width
1+35	9'
2+75	8'
3+80	7'
6+05	5'
7+45	5'
7+90	7'
8+65	5'
9+95	10'
11+55	8'
13+35	6'
14+10	6'
15+15	7'
16+35	7'
17+30	5'
17+80	6'
19+10	8'
20+10	6'
20+35	6'
20+67	11'
22+10	8'
23+90	5'
26+80	8'

SMALL RETAINING WALL BETWEEN STA. 20+05 TO 20+40



Source: Sikand, September 22, 2011.



Source: Sikand, September 22, 2011.

NOT TO SCALE

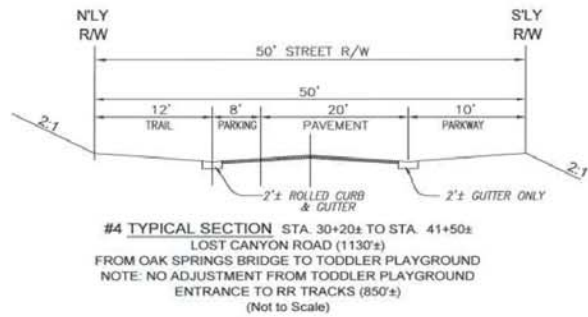
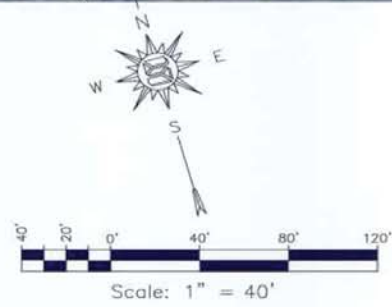
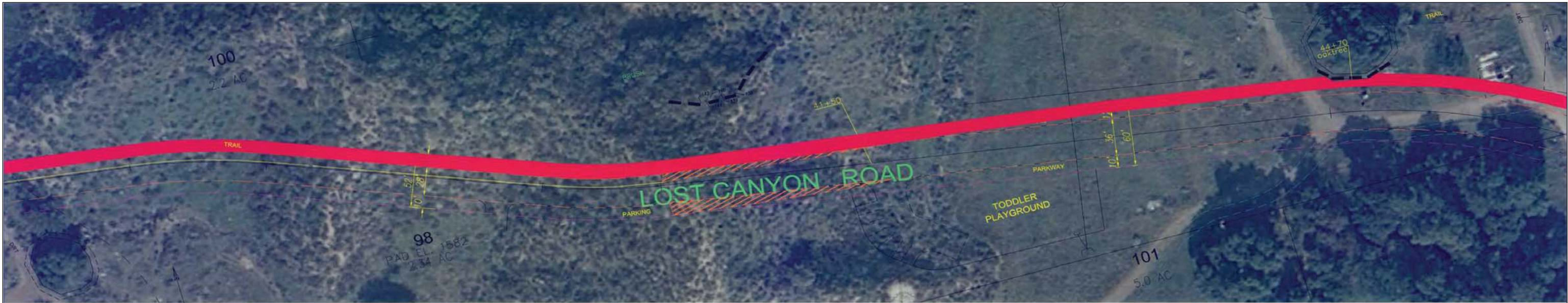


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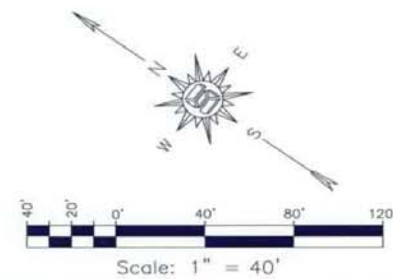
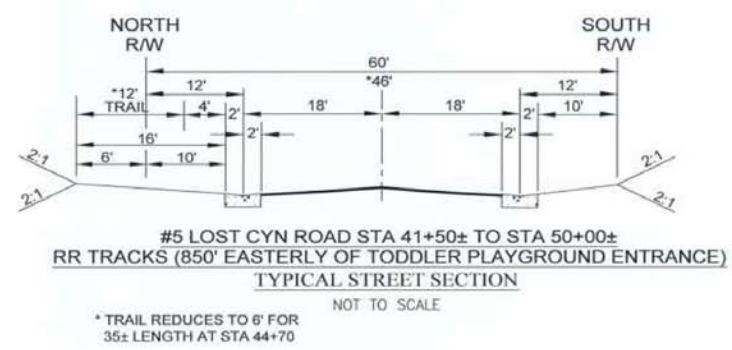
MANCARA RESIDENTIAL PROJECT
ENVIRONMENTAL IMPACT REPORT

Lost Canyon Road Improvements

Exhibit 3-6c



NOTE:
 SEE SHEET 1 FOR LEGEND AND
 TYPICAL PLAN CONCEPT



Source: Sikand, September 22, 2011.

NOT TO SCALE

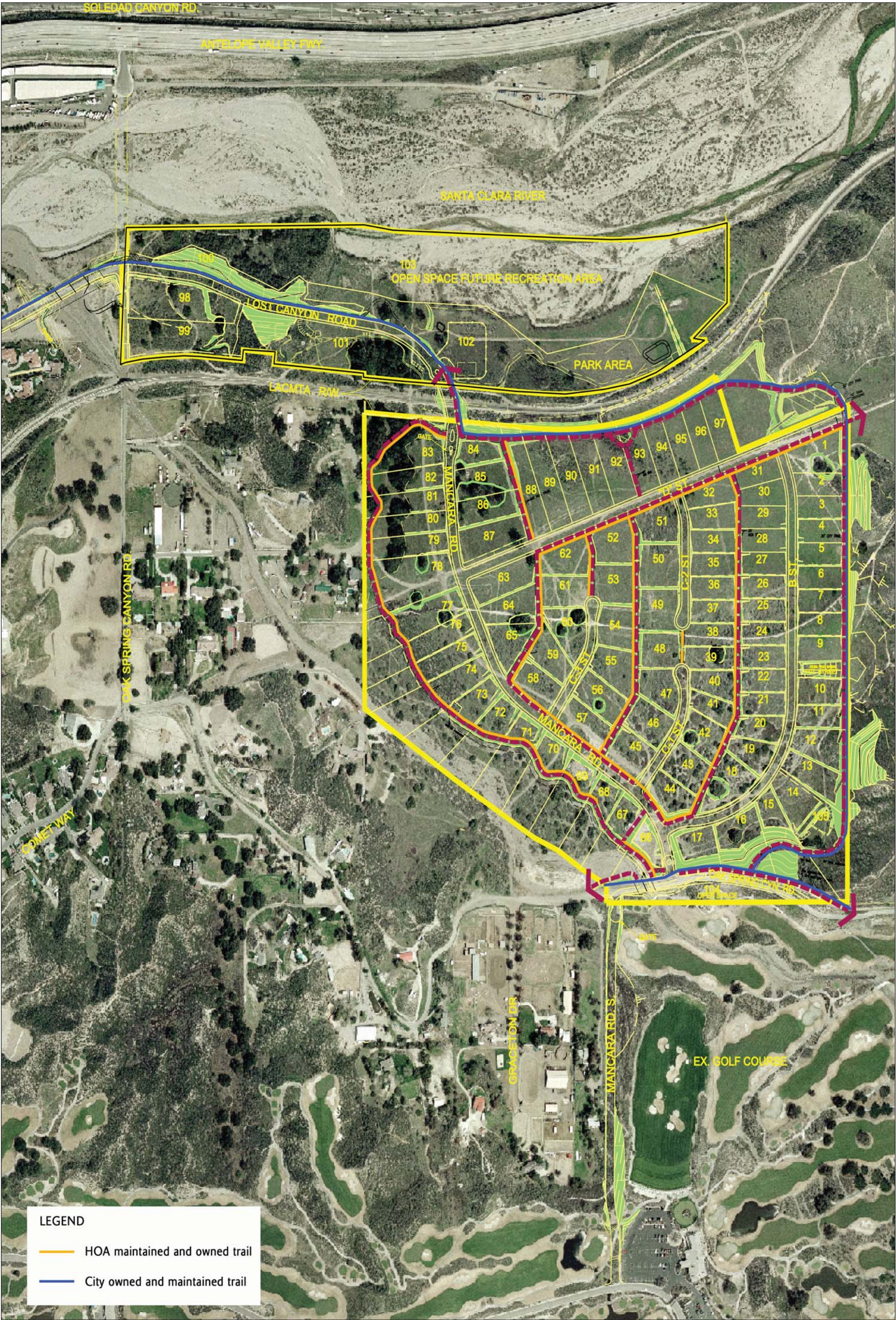
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Lost Canyon Road Improvements

Exhibit 3-6d





Private trails would be a minimum of 12-feet in width, but generally would range between 16- and 25-feet wide. The on-site trails would connect to the Santa Clarita Valley's regional trail system, as well as trails in the Angeles National Forest immediately east of the project site. As described above, an equestrian trailhead would also be provided within the northern portion of the site adjacent to the Santa Clara River.

City Multi-Use Recreation Trail

The City multi-use recreation trail crosses onto the property on Lost Canyon Road, east of the new bridge over Oak Spring Canyon Wash, and continues to immediately south of the Metrolink railroad right-of-way. For this segment, the 6- to 12-foot trail would be located along the northerly side of the ROW (refer to *Exhibits 3-6c* and *3-6d*). Beginning at Mancara Road south of the Metrolink railroad right-of-way, the trail crosses to the north and east side of the residential properties along the project boundary and extend southerly to Oak Spring Canyon Road. The easement would be improved with a 16- to 30-foot multi-use trail. This multi-use trail would provide a linkage to the parks, recreational, and open space components of the project described below in Section 3.6.3, Parks, Recreation, and Open Space. Refer to Exhibit 3-6a through Exhibit 3-6d.

The off-site Lost Canyon Road equestrian trail improvements would extend from Sand Canyon Road to the project site. As previously discussed, the width of the trail would vary from 5- to 10 feet wide or greater placed on the north and south side of Lost Canyon Road.

3.6.4 FLOOD PROTECTION

Oak Spring Canyon Wash traverses the southwestern portion of the project site, flowing in a southeasterly to northwesterly direction. Oak Spring Canyon Wash is tributary to the Santa Clara River to the north. Residential development would occur adjacent to the Wash's northeastern bank.

Embankment protection is proposed along the northeastern bank to prevent bank failure along residential lots proposed along Oak Spring Canyon Wash. The embankment protection would include buried bank stabilization in areas adjacent to and outside of the Oak Spring Canyon Wash floodway. Design options for the embankment protection include: 1) colored concrete; 2) rock riprap; 3) gabions; or 4) soil cement. Maintenance of the buried bank stabilization would be provided by either the City of Santa Clarita or the Los Angeles County Flood Control District. The buried bank stabilization and/or areas adjacent to them would be vegetated with planted backfill to minimize the aesthetic impact.

3.6.5 GRADING PLAN

The proposed project would be graded in one phase, and would require earthwork volume of approximately 624,000 cubic yards, in addition to approximately 660,000 cubic yards of remedial grading. However, all earthwork would be balanced on-site, with no need for import or export of soil. It is anticipated that site grading would take approximately four to six months to complete.



The grading quantities described above would include off-site grading for roadway facilities (i.e., Lost Canyon Road Extension, the Lost Canyon Road Extension bridge over Oak Spring Canyon Wash, and Mancara Road South). Additional off-site grading would be required along the project site's eastern boundary. Specifically, four areas of off-site grading along the eastern boundary would occur, adjacent to Lots 1-4, 6-9, 12-13, and 97. Refer to Exhibit 3-8, Grading Map.

3.6.6 PROJECT PHASING

The proposed project would be implemented in up to nine phases depending on market conditions following project approval. It is anticipated that construction would begin in late 2012 and each phase would be constructed over a period of approximately 12 months. Depending on market conditions and financing, completion of the project is expected by 2021.

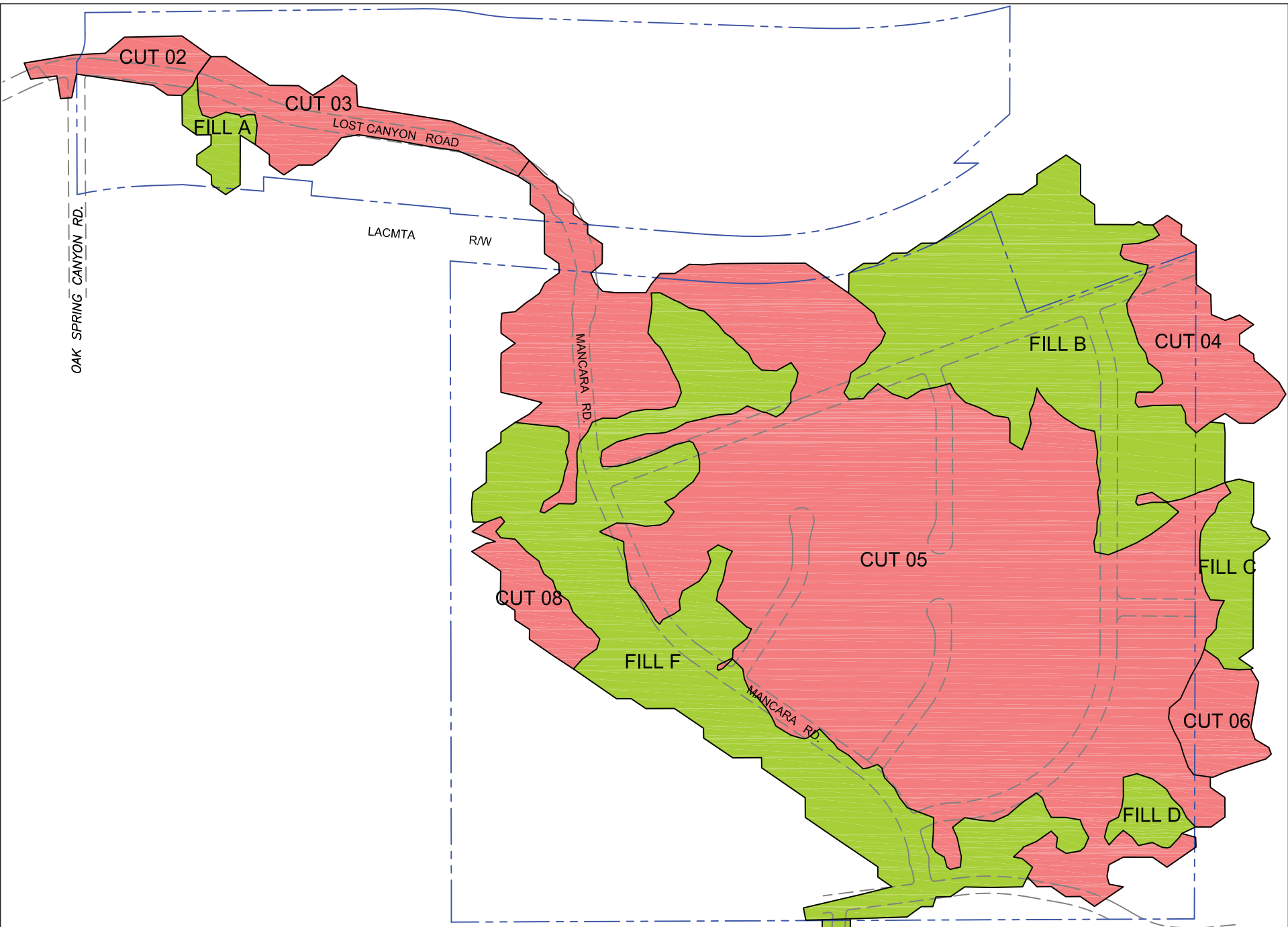
3.7 PROJECT AGREEMENTS, PERMITS, AND APPROVALS

3.7.1 CITY OF SANTA CLARITA

As the public agency with the principal responsibility of approving the project, the City of Santa Clarita would serve as the lead agency for the purposes of *CEQA*. A certified EIR, as described in Section 2.0, Introduction and Purpose, is required pursuant to *CEQA*. The Tentative Tract Map, Conditional Use Permit, and Oak Tree Permit shall be approved by the City of Santa Clarita Planning Commission (unless it is appealed, in which case it would also require the approval of the City Council).

Implementation of the project is contingent upon approval of the following entitlements:

- **Tentative Tract Map No. 063022:** Approval of the Tentative Tract Map is required to subdivide the site into 105 lots, including 99 single-family residential lots, five open space/recreation lots, and one lot for drainage desilting basin.
- **Conditional Use Permit 05-019.** A CUP is required to permit the proposed vehicular gates at project access points for the proposed project and adjacent parcel to the east and for a park or equestrian use located in a residential zone. The CUP procedure provides flexibility in the *UDC* use regulations to account for the widely varying needs of certain uses. Because of the project's unique characteristics, and in order to achieve the special purposes in certain districts, conditional uses require special consideration.
- **Oak Tree Permit 05-039.** Pursuant to the *City of Santa Clarita Oak Tree Preservation Ordinance*, an Oak Tree Permit is required for 96 of the 347 oak trees potentially affected by the project. Of these 96 oak trees, 8 oak trees are planned for permanent removal; none of these are Heritage Trees. The remaining 88 oak trees may be encroached upon during construction of the proposed project.



* WINDOWED QUANTITY REPORT *

DESCRIPTION	CUT	FILL
CUSTOM 6 GROUND - NATURAL GROUND COMPARED TO CUSTOM 6 GROUND		
FILL A	300 CY	5,496 CY
FILL B	1,789 CY	114,054 CY
FILL C	310 CY	14,678 CY
FILL D	37 CY	2,546 CY
FILL E	740 CY	1,474 CY
FILL F	3,531 CY	145,629 CY
CUT 01	9,515 CY	302 CY
CUT 02	16,238 CY	484 CY
CUT 03	67,595 CY	3,692 CY
CUT 04	67,054 CY	1,151 CY
CUT 05	392,013 CY	14,947 CY
CUT 06	72,811 CY	463 CY
CUT 08	41,634 CY	165 CY
	673,567 CY	305,081 CY

EARTHWORK SUMMARY		
CUT	FILL	
674,000 CY	305,000 CY	
- 44,000 (3)	+ 265,000 (1)	
	570,000 CY	
	+ 48,000 (2)	
630,000 CY	618,000 CY	
- 6,000 CY	+ 6,000 (4)	
624,000 CY	624,000 CY	BALANCED

LEGEND:

	TRACT BOUNDARY
	CUT
	CUT AREA 02
	FILL
	FILL AREA D

- (1) R & R and Shrinkage
S'Iy Area ----- 35± Ac X 5'± X 1613 CY/Ac = 286,000 CY
N'Iy Area ----- 40± Ac X 12'± X 1613 CY/Ac = 774,000 CY
1060,000 CY X 0.25 = 265,000 CY Shrinkage
- 400,000 CY Cut
660,000 CY R & R
- (2) Subsidence ----- 75± Ac X 0.4' X 1613 CY/Ac = 48,000 CY
- (3) Reduction for existing Oak Trees to remain in graded area = 44,000± CY
- (4) 630,000 CY - 618,000 CY = 12,000 CY /75 Ac X 1613 CY/Ac = 0.1± Raise of Pad Elev.
to balance at 624,000 CY

NOTE: TOTAL EARTHWORK 624,000 CY + 660,000 CY = 1,284,000 CY

Source: Sikand, November 2011.

NOT TO SCALE



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MANCARA RESIDENTIAL PROJECT
ENVIRONMENTAL IMPACT REPORT

Cut and Fill Map

Exhibit 3-8